CITY OF WEST SACRAMENTO
REGULAR MEETING OF THE WEST SACRAMENTO CITY COUNCIL AND
WEST SACRAMENTO REDEVELOPMENT SUCCESSOR AGENCY & WEST SACRAMENTO FINANCING AUTHORITY
AUGUST 22, 2018 AGENDA

Christopher L. Cabaldon, Mayor

Christopher T. Ledesma, Mayor Pro Tem
Mark F. Johannessen, Council Member

Beverly A. Sandeen, Council Member
Quirina Orozco, Council Member

Aaron Laurel, City Manager
Jeffrey Mitchell, City Attorney

6:00 PM Closed Session – See attached agenda
7:00 PM Pledge of Allegiance

The meeting will be held at City Hall, City Council Chambers, 1110 West Capitol Avenue, West Sacramento

Anyone wishing to address the Council, or any agenda item, should fill out the Request to Speak card and present it to the City Clerk prior to the completion of staff presentation. Items on the Consent Agenda will be considered in one motion and the card should be turned in prior to the first item on Consent.

GENERAL ADMINISTRATION FUNCTION – PART I

1A. **PRESENTATIONS BY THE PUBLIC ON MATTERS NOT ON THE AGENDA WITHIN THE JURISDICTION OF THE COUNCIL.**
The Council is prohibited from discussing issues not on the agenda brought to them at this time. According to State Law (the Brown Act), items must first be noticed on the agenda before any discussion or action.

1B. **COUNCIL COMMUNICATIONS / ASSIGNMENTS**

Age-Friendly Communities Task Force ---------- Sandeen, Johannessen
Align Capital Region ---------- Cabaldon
Bikeshare Policy Steering Committee ---------- Cabaldon
City/County 2x2 ---------- Sandeen, Orozco
City/School 2x2 ---------- Orozco, Cabaldon
Delta Protection Commission ---------- Cabaldon; Alternate - Ledesma
Executive Commission for the Homeless 10-Year Plan ---------- Johannessen
EIFD Public Financing Authority ---------- Ledesma, Cabaldon, Sandeen
Local Agency Formation Commission ---------- Alternate – Sandeen
Port District Commission ---------- Ledesma; Johannessen; Sandeen; Cabaldon; Alternate - Orozco
Regional Water Authority ---------- Orozco
River City Regional Stadium Financing Authority ---------- Cabaldon, Orozco
Riverfront Joint Powers Authority ------ Cabaldon, Ledesma; Alternate - Sandeen, Alternate - Johannessen
Sacramento Area Council of Governments (SACOG) ---------- Cabaldon; Alternate - Ledesma
Sacramento Regional County Sanitation District Board ---------- Orozco; Alternate - Sandeen
Streetcar Policy Steering Committee ---------- Cabaldon; Ledesma; Alternate - Orozco
Water Resources Association ---------- Sandeen
West Sacramento Area Flood Control Agency JPA ---------- Sandeen; Alternate - Ledesma
West Sacramento Housing Development Corporation Liaison ---------- Johannessen
Yolo County Children's Alliance ---------- Orozco
Yolo County Consolidated Redevelopment Successor Agency Oversight Board ---------- Ledesma
Yolo County Housing Authority ---------- Johannessen
Yolo County Transportation District ---------- Ledesma; Alternate - Sandeen
Yolo Habitat Conservancy ---------- Ledesma; Alternate - Orozco
Yolo-Solano Air Quality Management District ---------- Johannessen; Alternate – Vacant
Yolo Subbasin Sustainable Groundwater Agency ---------- Sandeen

1C. **COUNCIL APPOINTMENTS, REAPPOINTMENTS, REMOVALS TO/FROM CITY AND NON-CITY BOARDS AND COMMISSIONS**
Economic Development & Housing Commission; Planning Commission

If you need special assistance to participate in this meeting, please contact the City Clerk's Office, 617-4500. Notification of at least 48 hours prior to the meeting will assist staff in assuring that reasonable arrangements can be made to provide accessibility to the meeting. Assisted listening devices are available at this meeting.

CONSENT AGENDA

2. **ADMINISTRATIVE SERVICES/FINANCE**
CONSIDERATION OF RESOLUTION 18-47 AND APPROPRIATION OF MEASURE V FUNDS FOR THE WEST SACRAMENTO AREA FLOOD CONTROL AGENCY (WSAFCA) FLOOD IMPROVEMENT PROJECTS IN AN AMOUNT NOT TO EXCEED $5.5 MILLION. (RAPER)

Comment: The citizens of West Sacramento approved a supplemental sales tax and advisory measure recommending that the proceeds be used for both the streetcar project and flood improvements, and this report requests a budget appropriation of Measure V funds to be transferred to West Sacramento Area Flood Control Agency (WSAFCA) for flood improvement projects.

3. **ADMINISTRATIVE SERVICES/FINANCE**
CONSIDERATION OF THE TREASURER'S QUARTERLY FINANCIAL REPORT ON THE CITY'S INVESTMENT PORTFOLIO FOR THE PERIOD ENDING JUNE 30, 2018. (RAPER)

Comment: It is responsibility of the City Treasurer to submit a report to the City Council on investment activities and practices, including an annual review of compliance with the City's Investment Policies. This is a quarterly report on the City's investment portfolio as of June 30, 2018.

4. **ADMINISTRATIVE SERVICES/INFORMATION TECHNOLOGY**
CONSIDERATION OF LEASE BETWEEN THE CITY OF WEST SACRAMENTO AND XEROX FINANCIAL SERVICES FOR COPIER PROGRAM WITH ENCOMPASS. (RAPER)

Comment: The City's existing copiers are at the end of their useful life. Staff is recommending to replace the copiers and enter into a lease with Xerox Financial Services and a service contract with Encompass, which will increase City efficiency and result in long-term cost savings.

5. **COMMUNITY DEVELOPMENT/DEVELOPMENT ENGINEERING**
CONSIDERATION OF RESOLUTION 18-55 APPROVING PARCEL MAP 5133, LOCATED IN WESTBRIDGE PLAZA, PHASE II. (HAMILTON)

Comment: This item seeks approval of Parcel Map 5133 which creates three new commercial parcels located within Westbridge Plaza, Phase II. The Parcels are located due north of Lowes.

6. **COMMUNITY DEVELOPMENT/DEVELOPMENT ENGINEERING**
CONSIDERATION OF A CONSULTANT CONTRACT BETWEEN THE CITY OF WEST SACRAMENTO AND PONTICELLO ENTERPRISES CONSULTING ENGINEERS, INC. FOR ON-CALL PLAN CHECKING SERVICES. (HAMILTON)

Comment: This item seeks approval of a consultant contract for plan check services between the City of West Sacramento and Ponticello Enterprises Consulting Engineers, Inc. This is a two-year contract for professional plan check services which will be primarily focused on technical review of civil construction documents.

7. **ECONOMIC DEVELOPMENT & HOUSING**
CONSIDERATION OF LAND TRANSFERS WITH THE PORT OF WEST SACRAMENTO REQUIRED BY THE STONE LOCK ONE DEVELOPMENT AGREEMENT AND REMOVAL OF TITLE EXCEPTIONS FROM THE STONE LOCK PROPERTY. (JACOBSON)

Comment: The City's Stone Lock One Development Agreement (DA) with the Port of West Sacramento became effective by City ordinance in June 2018. This item would approve land transfers with the Port as required by the DA, terminate a 2005 "Agreement Regarding Resolution of Existing Issues" with the City, and quitclaim irrevocable offers of dedication from the Port of West Sacramento on the Stone Lock One property.

8. **POLICE**
CONSIDERATION OF APPROVAL OF A PURCHASE OF A ROBOT FOR THE YOLO COUNTY BOMB SQUAD FROM NORTHROP GRUMMAN CORPORATION AND ADOPTION OF RESOLUTION 18-57 APPROVING AMENDMENTS OF $240,000 TO THE EQUIPMENT REPLACEMENT FUND BUDGET (GRANT FUNDED). (MCDONALD)

Comment: The Police Department seeks approval to expend funds for a fully reimbursable grant to purchase a bomb robot for the Yolo County Bomb Squad.

9. **PUBLIC WORKS/ENGINEERING**
CONSIDERATION OF PROJECT APPROVAL AND ADOPTION OF RESOLUTION 18-51 CERTIFYING THE FINAL INITIAL STUDY/MITIGATED NEGATIVE DECLARATION, MITIGATION MONITORING PLAN AND FINDINGS OF FACT FOR THE SYCAMORE TRAIL PHASE II & III PROJECT. (ANBIAH)

Comment: The purpose of this item is to certify the Initial Study Mitigated Negative Declaration for the Sycamore Trail Phase II & III Project in compliance with the California Environmental Quality Act (CEQA).

10. **PUBLIC WORKS/ENGINEERING**
CONSIDERATION OF PROJECT APPROVAL AND ADOPTION OF RESOLUTION 18-36 ADOPTING THE FINAL INITIAL STUDY/MITIGATED NEGATIVE DECLARATION, AND MITIGATION MONITORING PLAN FOR THE LINDEN ACRES WATER MAIN REPLACEMENT PROJECT. (ANBIAH)

Comment: The purpose of this item is to certify the Initial Study Mitigated Negative Declaration for the Linden Acres Water Main Replacement Project in compliance with the California Environmental Quality Act (CEQA).

11. **PUBLIC WORKS/ENGINEERING**
CONSIDERATION OF RESOLUTION 18-54 APPROVING A CAPITAL IMPROVEMENT PROGRAM BUDGET AMENDMENT TO THE WASHINGTON DISTRICT SUSTAINABLE COMMUNITY INFRASTRUCTURE PROJECT FOR FISCAL YEAR 2018-19. (ANBIAH)

Comment: The purpose of this item is to increase the construction contingency budget to the Washington District Sustainable Community Infrastructure Project, which will increase the paving limits throughout the project.

12. **PUBLIC WORKS/ENGINEERING**
CONSIDERATION OF APPROVAL OF CONTRACT AMENDMENT NO. 1 TO THE CONSTRUCTION CONTRACT TO TEICHERT CONSTRUCTION; AND CONSIDERATION OF RESOLUTION 18-56 APPROVING A CAPITAL IMPROVEMENT PROGRAM BUDGET AMENDMENT TO THE WEST CAPITOL AVENUE PARKING LOT PROJECT BUDGET FOR FISCAL YEAR 2018-19. (ANBIAH)

Comment: The purpose of this item is to approve a contract amendment to Teichert Construction to construct driveway, grind, pave and stripe a parking lot at 1271 West Capitol Avenue.

13. **PUBLIC WORKS/FACILITIES**
CONSIDERATION OF APPROVAL OF CONTRACT FOR PROFESSIONAL SERVICES WITH SIMPSON GUMPERTZ & HEGER INC. TO DESIGN FIRE STATION 45 FACILITY REPAIRS. (ANBIAH)

Comment: Fire Station 45 is in immediate need of specialized engineering services to be applied in a timely manner, so that construction services may be obtained to repair existing leaks and to reduce future risk of damage to the facility. Seeking City Council consideration to award a contract for professional services to Simpson Gumpertz & Heger Inc. for leak detection and construction design document services for the Station 45 Facility Repairs project.

14. **PUBLIC WORKS/TRANSPORTATION**
CONSIDERATION OF UPDATE ON THE PILOT ON-DEMAND RIDESHARE SERVICE. (ANBIAH)

Comment: The Pilot On-Demand Rideshare service is an innovative public transportation model being tested by the City as an early action item of the broader Mobility Action Plan (MAP). The Pilot will operate for one (1) full year using a fleet of dedicated, co-branded Mercedes Benz Metris vans to provide on-demand share rides anywhere in the City for a flat fare. This is a recurring monthly report per City Council direction.

15. **CITY MANAGER**
CONSIDERATION OF RESPONSE TO YOLO COUNTY GRAND JURY REPORT ON YOLO COUNTY CITY PENSION AND RETIREMENT MEDICAL COSTS. (BERLIN)

Comment: On June 13 staff received the Grand Jury report regarding pension and retirement medical costs. According to Penal Code Section 933 and 933.05, the City Council is required to respond in writing and in electronic form to certain findings and recommendations of the Grand Jury report within 90 days.

16. **CITY MANAGER/CITY CLERK**
CONSIDERATION OF APPROVAL OF A LETTER OF PUBLIC CONVENIENCE AND NECESSITY (PCN) FOR THE GROCERY OUTLET INC. (BERLIN)

Comment: Certain areas of West Sacramento are deemed "over concentrated" with alcohol sales licenses and the State Office of Alcoholic Beverage Control will not issue a new license unless the City determines that the issuance of the license would serve a public convenience or necessity. Grocery Outlet is located in Census Tract 102.03, which is over concentrated. Although the area is over-concentrated, staff recommends approving the PCN with conditions.

17. **CITY MANAGER/HUMAN RESOURCES**
CONSIDERATION OF UPDATED BENEFIT SUMMARY FOR CITY MANAGER 2018/2019 AND JOB DESCRIPTION UPDATES FOR ASSOCIATE CIVIL ENGINEER, SENIOR CIVIL ENGINEER, CHIEF TREATMENT PLANT MECHANIC, AND TREATMENT PLANT MECHANIC. (BERLIN)

Comment: The City Manager Benefit Summary is being updated to reflect the current City Manager's salary. The job classification descriptions for Associate Civil Engineer and Senior Civil Engineer are being updated to allow for registration as a Professional Civil engineer outside of California with the ability to obtain California registration within one (1) year of appointment. The job classification descriptions for Treatment Plant Mechanic II and Chief Treatment Plant Mechanic are being updated to include the requirement of a Class A driver license.

18. **CITY MANAGER/CITY CLERK**
CONSIDERATION OF APPROVAL OF THE MINUTES OF THE JULY 18, 2018 REGULAR CITY COUNCIL MEETING. (RANKIN)

REGULAR AGENDA

19. **ECONOMIC DEVELOPMENT & HOUSING**
CONSIDERATION AND APPROVAL OF CONTRACT WITH DOWNTOWN STREETS, INC. AND ADOPTION OF RESOLUTION 18-53 APPROVING APPROPRIATION FROM MEASURE E FUND. (JACOBSON)

Comment: This item will consider approving a contract with Downtown Streets Team, a non-profit organization, to provide a work experience and case management program for individuals experiencing or at risk of experiencing homelessness within the City.

Recommendation: Staff respectfully recommends that the City Council 1) authorize the City Manager to execute a one-year contract with Downtown Streets, Inc. in the amount of $200,000; and 2) adopt Resolution 18-53 approving an appropriation of $200,000 from the Measure E Fund, Fund 110, for the program.

20. **PUBLIC WORKS/ENGINEERING**
PRESENTATION AND DISCUSSION ON THE 2018 PAVEMENT MANAGEMENT SYSTEM UPDATE. (ANBIAH)

Comment: The purpose of this report is to present findings on 2018 Pavement Management System Update and obtain Council direction and policy guidance given the limited available funding relative to the projected costs to maintain City roads.

Recommendation: Staff respectfully recommends that the City Council receive the presentation and provide comments and policy direction to prepare a multi-year pavement rehabilitation program

21. **CITY MANAGER**
CONSIDERATION OF RESOLUTION 18-52 DELEGATING AUTHORITY TO THE CITY MANAGER TO APPROVE DEVELOPMENT IMPACT FEE DEFERRAL AGREEMENTS AND REVIEW OF ADMINISTRATIVE POLICY SPECIFYING CERTAIN AUTHORITIES DELEGATED TO THE CITY MANAGER. (BERLIN)

Comment: The general powers and duties of the City Manager are outlined in Section 2.08 of the Municipal Code. The City Council has periodically added more specificity to the broad powers and duties or delegated other administrative authority.

Recommendation: Staff respectfully recommends that the City Council 1) consider and adopt Resolution 18-52 delegating authority to the City Manager to approve development impact fee deferral agreements; 2) review, approve or modify the Administrative Policy providing more specificity to and consolidating certain authorities that have been granted to the City Manager within the context of those outlined in Section 2.08 of the Municipal Code; and 3) authorize the City Clerk to amend this policy as additional authorities are granted or existing ones are modified by the City Council.

ADMINISTRATIVE FUNCTION – PART II

22.
A. Council Calendar
B. City Manager Report
C. City Attorney Report
D. Staff Direction from City Council Members
E. Future Agenda Item Requests by Council
F. Adjourn

CITY OF WEST SACRAMENTO
REGULAR MEETING OF THE WEST SACRAMENTO CITY COUNCIL AND
WEST SACRAMENTO REDEVELOPMENT SUCCESSOR AGENCY & WEST SACRAMENTO FINANCING AUTHORITY
AUGUST 22, 2018 CLOSED SESSION AGENDA

Christopher L. Cabaldon, Mayor

Christopher T. Ledesma, Mayor Pro Tem
Mark F. Johannessen, Council Member

Beverly A. Sandeen, Council Member
Quirina Orozco, Council Member

Aaron Laurel, City Manager
Jeffrey Mitchell, City Attorney

6:00 PM Call to Order

1. CITY ATTORNEY

Conference with Legal Counsel – Significant Exposure to Litigation - GC §54956.9(b): 1

2. CITY ATTORNEY

Conference with Real Property Negotiator – GC §54956.8
Negotiating Parties: Eric Will (City); Bobby Gill (Outfront Media); Rob LaGrone (Clear Channel Outdoor); Mark Friedman (Fulcrum Property)
Property: 4761 West Capitol Avenue (APN 067-101-026-000); 501 North Harbor Blvd. (APN 014-792-001-000); and 1301 South River Road (APN 058-350-002)
Under Negotiation: Price and Terms of Payment

3. CITY ATTORNEY

Conference with Real Property Negotiator – GC §54956.8
Negotiating Parties: Charline Hamilton (City); Brian Dewey (California Department of Parks & Recreation)
Property: APNs 014-610-004, 014-610-010, 014-610-014, 014-610-020, 014-620-003, 014-620-010, 014-620-012, 014-620-014, 014-760-020, and 014-760-023
Under Negotiation: Price and Terms of Payment

I, Kryss Rankin, City Clerk, declare under penalty of perjury that the foregoing agenda for the August 22, 2018 regular and closed session meetings of the West Sacramento City Council, Redevelopment Successor Agency and Financing Authority was posted August 17, 2018 in the office of the City Clerk, 1110 West Capitol Avenue, West Sacramento, CA and was available for public review.

Kryss Rankin

Kryss Rankin, City Clerk

NOTE: If you challenge the nature of a proposed action in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the City Council at, or prior to, the public hearing.

All public materials related to an item on this agenda submitted to the City Council after distribution of the agenda packet are available for public inspection in the City Clerk's Office at 1110 West Capitol Avenue during normal business hours. Any document provided at the meeting by staff will also be available to the public. Any document provided at the meeting by the public will be available the next business day following the meeting.

City Council meetings are broadcast live on AT&T Channel 99 and Wave Cable Channel 20, and rerun the next day at 12:00 pm and the following Saturday at 6:00 pm. The agenda and agenda reports are also available on the City's website at www.cityofwestsacramento.org.

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 2

SUBJECT:
CONSIDERATION OF RESOLUTION 18-47 AND APPROPRIATION OF MEASURE V FUNDS FOR THE WEST SACRAMENTO AREA FLOOD CONTROL AGENCY (WSAFCA) FLOOD IMPROVEMENT PROJECTS IN AN AMOUNT NOT TO EXCEED $5.5 MILLION

INITIATED OR REQUESTED BY:

[ ] Council [X] Staff

[ ] Other

REPORT COORDINATED OR PREPARED BY:
Paul Blumberg, Public Finance Manager

Roberta Raper, Director of Administrative Services

ATTACHMENT [X] Yes [ ] No [ ] Information [ ] Direction [X] Action

OBJECTIVE
The purpose of this report is to appropriate City funding for construction of the West Sacramento Area Flood Control Agency (WSAFCA) flood improvement projects.

RECOMMENDED ACTION
Staff respectfully recommends that the City Council adopt Resolution 18-47 which authorizes an appropriation and transfer of Measure V funds for the WSAFCA flood improvement projects in an amount not to exceed $5.5 million.

BACKGROUND
WSAFCA's goal is to provide a minimum 200-year level of protection to the City of West Sacramento ("City") by 2025. WSAFCA is advancing a comprehensive program referred to as the West Sacramento Levee Improvement Program ("WSLIP") to achieve its flood risk reduction goals. WSAFCA has developed local funding sources that provide local cost share required for federal and state funding that supports WSLIP implementation. This report addresses the budget appropriation associated with a local source of funds previously earmarked to fund the WSLIP.

WSAFCA initiated the WSLIP in 2006 in response to changes to levee design criteria. Under the WSLIP, WSAFCA has partnered with the Department of Water Resources to construct "Early Implementation Projects" (EIP), such as the I Street, CHP Academy, and Rivers Phase I levee improvement projects. The current major project, the Sacramento Southport Levee Improvement Project (Southport Project), is funded with a combination of local funding and state funding under both the EIP and Urban Flood Risk Reduction Program (UFRR). Construction of the Southport Project commenced in May 2017, and since mid-April the contractor has ramped up earthwork activities to continue with levee embankment placement throughout the entire project area. Slurry wall installation also continues, and the contractor has started levee degrade work at the breaches in the south offset area. The construction season will continue to be very busy as WSAFCA anticipates completing over $35 million in work in calendar year 2018.

With a total project cost of $206 million, the Southport Project is larger than any infrastructure project in the City's history. A significant portion of the state funding for the project has been secured ($148.16 million). Local funding sources consist of both WSAFCA funds (the flood assessment) and City funds (Measure V sales tax).

A key funding source for the project is $10 million earmarked by the City Council from a Measure V revenue bond issue. While authorized in 2015, the bond issuance, then estimated at $35.8 million, has been delayed indefinitely. On April 20, 2016, the City Council approved an appropriation of $4,500,000 for WSAFCA Flood Improvement Projects, and on April 21, 2018, the WSAFCA Board approved the acceptance of those funds. In addition, on February 28, 2017, WSAFCA entered into a Revolving Line of Credit (RLOC) with First Northern Bank of Dixon to ensure that additional funding in the amount of $3,500,000 is available to assist with the project cash flow through June 30, 2021. The use of the remaining $5.5 million in Measure V funds was always included in the WSLIP cash flow model, shown as proceeds from the City's bonding of Measure V funds. The timing of the bonding continues to be on hold due the uncertainty of the timing of construction of the Streetcar Project.

The Finance Division meets on a regular basis with the Community Development Department to analyze project cash flows to ensure adequate funding for all WSLIP activities. The current cash flow model indicates that the

second allocation of Measure V funds is needed by December for the flood projects for cash flow purposes; however, the model also anticipates a $30 million advance to be received in August. If this advance arrives from the state later than expected, part of the $5.5 million in Measure V funds could be needed sooner to ensure appropriate cash available to continue imperative work on the project. At this time, staff is recommending that the City Council adopt Resolution 18-47 approving a budget appropriation in the amount of $5.5 million to ensure that WSAFCA has adequate local cash resources through the end of calendar year 2018. Should the City Council approve staff's recommendation, the WSAFCA Board will be asked to accept the funds at a special meeting on August 23, 2018.

ANALYSIS

On July 15, 2015, the City Council authorized the issuance of lease revenue bonds, secured by future Measure V revenues. At that time, it was assumed the bond issue would occur by the end of 2015, and that the bond issue would be sized to generate a total of $35.8 million in project funds, with project funds being allocated to the Streetcar Project and $10 million to fund the WSLIP. Since that time, uncertainty surrounding federal funding and the timing of the Streetcar Project construction has delayed the issuance of the revenue bonds. Instead, Measure V funds have been allocated on a "pay as you go" basis to both the Streetcar and Flood Projects, with $7.8 million being allocated to the Streetcar Project to date, and $4.5 million to the Flood Projects.

Based on the current WSAFCA cash flow model, it appears that a source of local funding will be needed in the current fiscal year 2018/19 to cover projected construction related expenses for the Southport Project. The WSLIP cash flow model for 2018/19, included as an attachment to this report, provides an indication of projected revenue needs, including the additional appropriation of Measure V funds.

Staff is recommending that the entire appropriation be approved, which would allow the Director of Administrative Services to make full or partial transfers as needed for cash flow purposes and to ensure that there is no interruption of construction activities. Staff does not believe this appropriation will have an impact on the City's ability to also fund commitments to the Streetcar Project. In November 2013, the City Council approved a $3 million commitment to the Streetcar Project associated with the next phase of design and engineering in addition to two additional funding agreements in the amount of $2.4 million and $3.1 million. The adopted City Biennial Budget for 2018-19 anticipates receipt of $3.3 million in Measure V revenue which, combined with an unencumbered FY 2017/18 fund balance of $6.1 million, will be more than sufficient to fund the WSAFCA appropriation. Staff will continue to monitor the need to issue the Measure V Revenue Bonds.

Environmental Considerations
Not applicable

Commission Recommendation
Not applicable

Strategic Plan Integration
The City Council's adopted 2018 Strategic Plan includes as a top priority the implementation of flood control improvements and the action recommended in this report is in furtherance of this goal.

Alternatives
The staff recommendation is to authorize a budget appropriation of Measure V sales tax revenue for the Southport Project from available balances in the Measure V fund in lieu of issuing bond debt. Alternatively, the City Council may choose to authorize something less than $5.5 million at this time, in which case staff would return in the future to request an appropriation for the remaining amount.

Coordination and Review
Preparation of this report was coordinated with the Community Development Department and the City Manager's Office.

Budget/Cost Impact
The budget appropriation of $5.5 million will come from Measure V funds (109-9910-5310) from the fiscal year 2018/19 fund balance. As of June 30, 2018, the unencumbered available balance is $6.1 million. The amount of Measure V revenue projected for fiscal year 2018/19 is $3.3 million. Therefore, approval of this budget appropriation would encumber the FY 2017/18 fund balance and a portion of anticipated Measure V revenues for the current fiscal year (depending on the timing of the funds transfer). The funds will be transferred to O&M budget 871-0000-4825 and CIP Work Order 871-41164-4825.

ATTACHMENTS

1. Resolution 18-47
2. WSAFCA Cash Flow Model

RESOLUTION 18-47

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF WEST SACRAMENTO APPROVING AMENDMENTS TO THE MEASURE V FUND BUDGET FOR FISCAL YEAR 2018/19

WHEREAS, on July 15, 2015, the City Council authorized the issuance of lease revenue bonds, secured by Measure V revenues to generate approximately $35.8 million in project funds, with $10 million to be allocated to the WSAFCA Flood Improvements and the remainder of the bond proceeds to the Streetcar Project; and

WHEREAS, uncertainty about federal funding for the Streetcar Project and the timing of construction has delayed the need for a large amount of bond proceeds for constructing the Streetcar Project, and has delayed the proposed bond issuance; and

WHEREAS, given the delay in issuing the Measure V Revenue Bonds, on April 21, 2016, the City Council previously authorized an appropriation of $4.5 million in Measure V fund balance for the WSAFCA Flood Improvements; and

WHEREAS, as the remaining $5.5 million in Measure V funds earmarked for WSAFCA has been assumed to be available in fiscal year 2018/19 to be funded from Measure V bond proceeds,

WHEREAS, after considering previous commitments to funding Streetcar Project expenses, staff has identified a projected Measure V fund balance for FY 2018/19 which would allow for the planned appropriation of $5.5 for WSAFCA without having to issue debt;

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of West Sacramento as follows:

1. The City Council hereby approves amendments to the O&M and CIP Budgets for the 2018/19 fiscal year as listed below.

Sources:
Amount: $5,500,000 Fund: 109-9910-5310

Uses:
Amount: $5,500,000 Fund: 871-0000-4825/ 871-41164-8825

2. The City Council hereby finds that the facts set forth in the recitals to this Resolution are true and correct and establish the factual basis for the City Council's adoption of this Resolution.

3. This Resolution shall take effect immediately upon its adoption.

PASSED AND ADOPTED by the West Sacramento City Council this 22nd day of August, 2018, by the following vote:

AYES:
NOES:
ABSENT:

Christopher L. Cabaldon, Mayor

ATTEST:

Kryss Rankin, City Clerk

West Sacramento Area Flood Control Agency

Southport Projected Expenses and Cash Flow Model

(July 2017 to June 2022)

Monthly Planning Level Cash Flow Model

		Jul-17	Aug-17	Sep-17	Oct-17	Nov-17	Dec-17	Jan-18	Feb-18	Mar-18	Apr-18	May-18	Jun-18
D-1: Cash Balance (Beginning of Period) [1]		**$7,883,295**	**$35,307,783**	**$33,579,459**	**$31,221,401**	**$26,705,695**	**$21,886,423**	**$19,922,621**	**$19,095,264**	**$22,072,839**	**$21,201,504**	**$13,697,661**	**$16,324,586**
D-2: Local Funding Sources (Revenue)	**To Date**	**Jul-17**	**Aug-17**	**Sep-17**	**Oct-17**	**Nov-17**	**Dec-17**	**Jan-18**	**Feb-18**	**Mar-18**	**Apr-18**	**May-18**	**Jun-18**
Special Tax/Assessment [2]	$51,428,292	$30,490	$4,222	$34	$566,990	($22,744)	($124)	$2,275,958	$5,459	$5,005	$22,578	$2,049,966	($68,264)
2008 Bond Proceeds	$9,187,418	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
2011 Bond Proceeds	$12,000,000	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
2015 Bond Proceeds	$20,000,000	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Remaining Bond Capacity [3]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
State Funding Actuals to Date	$47,695,583												
State Funding EIP-UFRR Payments [4]	$51,582,871	$0	$0	$5,324,718	$0	$0	$0	$2,609,711	$7,692,443	$1,010,079	$0	$1,887,109	$397,404
State Funding EIP-UFRR Real Estate Reimbursement [4]	$20,469,445	$0	$0	$0	$1,508,378	$0	$0	$0	$3,533,761	$0	$0	$5,914,880	$0
257 Account Advances [5]	$26,445,114	$26,200,767	$0	$0	$0	$0	$0	$0	$0	$0	$244,347	$0	$0
257 Account Draws [5]	($21,028,849)	$0	$0	($5,324,718)	$0	$0	$0	($2,609,711)	($7,692,443)	($442,488)	$0	($4,959,489)	($397,404)
RFMP Reimbursements [6]	$1,866,140	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$50,000
Other Local Funding (New Assessment Bond, In-Lieu Fee, etc.)	$156,208	$0	$0	$0	$0	$0	$156,208	$0	$0	$0	$0	$0	$0
Line of Credit	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Measure V [7]	$4,500,000	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Sub Total Local Funding Sources		**$26,231,257**	**$4,222**	**$34**	**$2,075,368**	**($22,744)**	**$156,084**	**$2,275,958**	**$3,539,220**	**$572,596**	**$266,925**	**$4,892,466**	**($18,264)**
D-3: Total Cash Available (Before Cash Out) [D1 + D2]		**$34,114,552**	**$35,312,006**	**$33,579,492**	**$33,296,769**	**$26,682,951**	**$22,042,508**	**$22,198,579**	**$22,634,484**	**$22,645,435**	**$21,468,428**	**$18,590,128**	**$16,306,322**
D-4: JPA Operation Costs (GO&A-O&M)	**To Date**	**Jul-17**	**Aug-17**	**Sep-17**	**Oct-17**	**Nov-17**	**Dec-17**	**Jan-18**	**Feb-18**	**Mar-18**	**Apr-18**	**May-18**	**Jun-18**
General Overhead and Administrative Expenses	$8,271,871	$26,331	$15,174	$16,412	$17,740	$10,781	$13,294	$49,534	$9,653	$27,007	$13,281	$37,486	$61,500
2008 Debt Service	$4,752,765	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
2011 Debt Service [8]	$5,280,011	$0	$0	$0	$0	$0	$0	$303,644	$0	$0	$0	$0	$0
2015 Debt Service [8]	$4,206,642	$0	$0	$0	$0	$0	$0	$1,290,571	$0	$0	$0	$0	$0
Future Debt Service	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Line of Credit - Repayment	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
O&M Transfers (RD 900, RD 537 & COWS) [9]	$6,898,370	$0	$0	$105,776	$0	$0	$0	$0	$352,941	$0	$0	$0	($10,376)
Repair, Replacement, & Rehabilitation [10]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
SMARA Reserve Fund [11]	$1,100,000	$1,100,000	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Subtotal JPA Operation Costs (GO&A-O&M)		**$1,126,331**	**$15,174**	**$122,188**	**$17,740**	**$10,781**	**$13,294**	**$1,643,749**	**$362,594**	**$27,007**	**$13,281**	**$37,486**	**$51,124**
D-5: Capital Improvement Projects Funding Uses	**To Date**	**Jul-17**	**Aug-17**	**Sep-17**	**Oct-17**	**Nov-17**	**Dec-17**	**Jan-18**	**Feb-18**	**Mar-18**	**Apr-18**	**May-18**	**Jun-18**
North Area Projects [12]	$36,603,726	$2,640	$3,358	$2,020	$4,443	$6,518	$2,876	$2,016	$6,121	$8,714	$6,677	$12,855	$0
Southport Project (Advance Federal Project) [13]	$67,896,709	($2,326,473)	$1,688,085	$2,125,177	$5,402,736	$4,729,515	$1,936,620	$1,404,281	$212,288	$1,079,913	$596,165	$2,178,205	$4,615,177
Southport Project Real Estate [13]	$34,694,588	$0	$0	$59,000	$1,050,767	$7,550	$135,900	$0	($34,100)	$231,950	$7,118,695	$0	$0
Other Planning Efforts [14]	$12,490,937	$3,048	$25,091	$23,527	$65,327	$41,909	$28,316	$22,469	$9,258	$78,689	$31,367	$33,618	$0
Federal Cost Share Agreement [15]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
DWSC East Slip Repair [16]	$174,207	$1,222	$839	$26,180	$50,061	$255	$2,880	$30,800	$5,483	$17,658	$4,582	$3,378	$571,429
Yolo Bypass (In-Kind Work to Advance Federal Project) [17]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Subtotal Capital Improvement Projects		**($2,319,563)**	**$1,717,373**	**$2,235,904**	**$6,573,334**	**$4,785,746**	**$2,106,592**	**$1,459,566**	**$199,050**	**$1,416,925**	**$7,757,486**	**$2,228,056**	**$5,186,605**
D-6: Total Cash Paid Out [D4 + D5]		**($1,193,231)**	**$1,732,547**	**$2,358,092**	**$6,591,074**	**$4,796,527**	**$2,119,887**	**$3,103,315**	**$561,645**	**$1,443,931**	**$7,770,767**	**$2,265,542**	**$5,237,729**
D-7: End Cash Position (870/871) [D3 - D6]		**$35,307,783**	**$33,579,459**	**$31,221,401**	**$26,705,695**	**$21,886,423**	**$19,922,621**	**$19,095,264**	**$22,072,839**	**$21,201,504**	**$13,697,661**	**$16,324,586**	**$11,068,593**

Attachment 2

West Sacramento Area Flood Control Agency

Southport Projected Expenses and Cash Flow Model

(July 2017 to June 2022)

Monthly Planning Level Cash Flow Model

	Jul-18	Aug-18	Sep-18	Oct-18	Nov-18	Dec-18	Jan-19	Feb-19	Mar-19	Apr-19	May-19	Jun-19
D-1: Cash Balance (Beginning of Period) [1]	**$11,068,593**	**$765,759**	**$23,474,745**	**$12,719,599**	**$5,707,947**	**$1,559,301**	**$3,821,434**	**$6,144,940**	**$6,799,977**	**$6,948,534**	**$5,353,853**	**$6,425,079**
D-2: Local Funding Sources (Revenue)	**Jul-18**	**Aug-18**	**Sep-18**	**Oct-18**	**Nov-18**	**Dec-18**	**Jan-19**	**Feb-19**	**Mar-19**	**Apr-19**	**May-19**	**Jun-19**
Special Tax/Assessment [2]	$0	$0	$0	$562,801	($27,113)	$0	$2,319,588	$0	$0	$0	$2,087,629	($69,630)
2008 Bond Proceeds	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
2011 Bond Proceeds	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
2015 Bond Proceeds	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Remaining Bond Capacity [3]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
State Funding Actuals to Date												
State Funding EIP-UFRR Payments [4]	$2,183,365	$4,900,414	$6,509,493	$5,934,837	$5,877,244	$7,524,308	$2,372,945	$1,749,718	$946,297	$760,393	$760,393	$760,393
State Funding EIP-UFRR Real Estate Reimbursement [4]	$1,245,599	$1,809,420	$0	$0	$0	$285,792	$0	$0	$0	$0	$0	$0
257 Account Advances [5]	$0	$30,248,000	$0	$0	$0	$0	$0	$0	$2,158,712	$0	$0	$1,987,453
257 Account Draws [5]	($3,428,964)	($5,049,273)	($6,509,493)	($5,934,837)	($5,877,244)	($7,810,100)	($656,948)	$0	($946,297)	($760,393)	($452,022)	($760,393)
RFMP Reimbursements [6]	$50,000	$50,000	$50,000	$50,000	$50,000	$50,000	$50,000	$50,000	$50,000	$50,000	$50,000	$50,000
Other Local Funding (New Assessment Bond, In-Lieu Fee, etc.)	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Line of Credit	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Measure V [7]	$0	$0	$0	$0	$0	$5,500,000	$0	$0	$0	$0	$0	$0
Sub Total Local Funding Sources	**$50,000**	**$31,958,560**	**$50,000**	**$612,801**	**$22,887**	**$5,550,000**	**$4,085,584**	**$1,799,718**	**$2,208,712**	**$50,000**	**$2,446,000**	**$1,967,823**
D-3: Total Cash Available (Before Cash Out) [D1 + D2]	**$11,118,593**	**$32,724,319**	**$23,524,745**	**$13,332,400**	**$5,730,834**	**$7,109,301**	**$7,907,018**	**$7,944,658**	**$9,008,690**	**$6,998,534**	**$7,799,853**	**$8,392,903**
D-4: JPA Operation Costs (GO&A-O&M)	**Jul-18**	**Aug-18**	**Sep-18**	**Oct-18**	**Nov-18**	**Dec-18**	**Jan-19**	**Feb-19**	**Mar-19**	**Apr-19**	**May-19**	**Jun-19**
General Overhead and Administrative Expenses	$166,529	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500
2008 Debt Service	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
2011 Debt Service [8]	$0	$0	$578,644	$0	$0	$0	$0	$0	$300,206	$0	$0	$0
2015 Debt Service [8]	$0	$0	$1,058,969	$0	$0	$0	$0	$0	$615,269	$0	$0	$0
Future Debt Service	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Line of Credit - Repayment	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
O&M Transfers (RD 900, RD 537 & COWS) [9]	$0	$0	$0	$85,546	($4,121)	$0	$352,577	$0	$0	$0	$317,320	($10,584)
Repair, Replacement, & Rehabilitation [10]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
SMARA Reserve Fund [11]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Subtotal JPA Operation Costs (GO&A-O&M)	**$166,529**	**$61,500**	**$1,699,113**	**$147,046**	**$57,379**	**$61,500**	**$414,077**	**$61,500**	**$976,975**	**$61,500**	**$378,820**	**$50,916**
D-5: Capital Improvement Projects Funding Uses	**Jul-18**	**Aug-18**	**Sep-18**	**Oct-18**	**Nov-18**	**Dec-18**	**Jan-19**	**Feb-19**	**Mar-19**	**Apr-19**	**May-19**	**Jun-19**
North Area Projects [12]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Southport Project (Advance Federal Project) [13]	$9,435,244	$8,616,645	$8,534,605	$6,905,978	$3,542,725	$2,654,939	$1,348,001	$1,083,181	$1,083,181	$1,083,181	$995,954	$995,954
Southport Project Real Estate [13]	$179,632	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Other Planning Efforts [14]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Federal Cost Share Agreement [15]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$500,000	$0	$0
DWSC East Slip Repair [16]	$571,429	$571,429	$571,429	$571,429	$571,429	$571,429	$0	$0	$0	$0	$0	$0
Yolo Bypass (In-Kind Work to Advance Federal Project) [17]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Subtotal Capital Improvement Projects	**$10,186,305**	**$9,188,074**	**$9,106,034**	**$7,477,407**	**$4,114,154**	**$3,226,367**	**$1,348,001**	**$1,083,181**	**$1,083,181**	**$1,583,181**	**$995,954**	**$995,954**
D-6: Total Cash Paid Out [D4 + D5]	**$10,352,834**	**$9,249,574**	**$10,805,146**	**$7,624,453**	**$4,171,533**	**$3,287,867**	**$1,762,079**	**$1,144,681**	**$2,060,156**	**$1,644,681**	**$1,374,773**	**$1,046,870**
D-7: End Cash Position (870/871) [D3 - D6]	**$765,759**	**$23,474,745**	**$12,719,599**	**$5,707,947**	**$1,559,301**	**$3,821,434**	**$6,144,940**	**$6,799,977**	**$6,948,534**	**$5,353,853**	**$6,425,079**	**$7,346,033**

West Sacramento Area Flood Control Agency

Southport Projected Expenses and Cash Flow Model

(July 2017 to June 2022)

Monthly Planning Level Cash Flow Model

	Jul-19	Aug-19	Sep-19	Oct-19	Nov-19	Dec-19	Jan-20	Feb-20	Mar-20	Apr-20	May-20	Jun-20
D-1: Cash Balance (Beginning of Period) [1]	**$7,346,033**	**$5,785,794**	**$5,001,843**	**$4,317,474**	**$3,364,329**	**$2,433,208**	**$2,128,104**	**$3,738,732**	**$3,871,273**	**$3,160,817**	**$1,798,321**	**$3,555,635**
D-2: Local Funding Sources (Revenue)	**Jul-19**	**Aug-19**	**Sep-19**	**Oct-19**	**Nov-19**	**Dec-19**	**Jan-20**	**Feb-20**	**Mar-20**	**Apr-20**	**May-20**	**Jun-20**
Special Tax/Assessment [2]	$0	$0	$0	$574,057	($27,655)	$0	$2,365,980	$0	$0	$0	$2,129,382	($71,022)
2008 Bond Proceeds	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
2011 Bond Proceeds	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
2015 Bond Proceeds	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Remaining Bond Capacity [3]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
State Funding Actuals to Date												
State Funding EIP-UFRR Payments [4]	$699,160	$699,160	$662,484	$662,484	$662,484	$640,439	$640,439	$640,439	$313,371	$313,371	$41,960	$41,960
State Funding EIP-UFRR Real Estate Reimbursement [4]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
257 Account Advances [5]	$0	$0	$1,921,316	$0	$0	$668,702	$0	$0	$0	$0	$0	$60,782
257 Account Draws [5]	($699,160)	($527,901)	($662,484)	($662,484)	($596,348)	($640,439)	($28,263)	$0	$0	$0	$0	($41,960)
RFMP Reimbursements [6]	$50,000	$50,000	$50,000	$33,860	$0	$0	$0	$0	$0	$0	$0	$0
Other Local Funding (New Assessment Bond, In-Lieu Fee, etc.)	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Line of Credit	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Measure V [7]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Sub Total Local Funding Sources	**$50,000**	**$221,259**	**$1,971,316**	**$607,917**	**$38,481**	**$668,702**	**$2,978,155**	**$640,439**	**$313,371**	**$313,371**	**$2,171,341**	**($10,241)**
D-3: Total Cash Available (Before Cash Out) [D1 + D2]	**$7,396,033**	**$6,007,053**	**$6,973,159**	**$4,925,392**	**$3,402,810**	**$3,101,910**	**$5,106,259**	**$4,379,171**	**$4,184,644**	**$3,474,188**	**$3,969,662**	**$3,545,394**
D-4: JPA Operation Costs (GO&A-O&M)	**Jul-19**	**Aug-19**	**Sep-19**	**Oct-19**	**Nov-19**	**Dec-19**	**Jan-20**	**Feb-20**	**Mar-20**	**Apr-20**	**May-20**	**Jun-20**
General Overhead and Administrative Expenses	$166,529	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500
2008 Debt Service	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
2011 Debt Service [8]	$0	$0	$585,206	$0	$0	$0	$0	$0	$296,288	$0	$0	$0
2015 Debt Service [8]	$0	$0	$1,065,269	$0	$0	$0	$0	$0	$606,269	$0	$0	$0
Future Debt Service	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Line of Credit - Repayment	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
O&M Transfers (RD 900, RD 537 & COWS) [9]	$0	$0	$0	$87,257	($4,204)	$0	$359,629	$0	$0	$0	$323,666	($10,795)
Repair, Replacement, & Rehabilitation [10]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
SMARA Reserve Fund [11]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Subtotal JPA Operation Costs (GO&A-O&M)	**$166,529**	**$61,500**	**$1,711,975**	**$148,757**	**$57,296**	**$61,500**	**$421,129**	**$61,500**	**$964,056**	**$61,500**	**$385,166**	**$50,705**
D-5: Capital Improvement Projects Funding Uses	**Jul-19**	**Aug-19**	**Sep-19**	**Oct-19**	**Nov-19**	**Dec-19**	**Jan-20**	**Feb-20**	**Mar-20**	**Apr-20**	**May-20**	**Jun-20**
North Area Projects [12]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Southport Project (Advance Federal Project) [13]	$943,710	$943,710	$943,710	$912,306	$912,306	$912,306	$446,398	$446,398	$59,771	$59,771	$28,861	$28,861
Southport Project Real Estate [13]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Other Planning Efforts [14]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Federal Cost Share Agreement [15]	$500,000	$0	$0	$500,000	$0	$0	$500,000	$0	$0	$1,554,596	$0	$0
DWSC East Slip Repair [16]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Yolo Bypass (In-Kind Work to Advance Federal Project) [17]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Subtotal Capital Improvement Projects	**$1,443,710**	**$943,710**	**$943,710**	**$1,412,306**	**$912,306**	**$912,306**	**$946,398**	**$446,398**	**$59,771**	**$1,614,367**	**$28,861**	**$28,861**
D-6: Total Cash Paid Out [D4 + D5]	**$1,610,239**	**$1,005,210**	**$2,655,685**	**$1,561,063**	**$969,602**	**$973,806**	**$1,367,527**	**$507,898**	**$1,023,828**	**$1,675,867**	**$414,027**	**$79,566**
D-7: End Cash Position (870/871) [D3 - D6]	**$5,785,794**	**$5,001,843**	**$4,317,474**	**$3,364,329**	**$2,433,208**	**$2,128,104**	**$3,738,732**	**$3,871,273**	**$3,160,817**	**$1,798,321**	**$3,555,635**	**$3,465,828**

West Sacramento Area Flood Control Agency

Southport Projected Expenses and Cash Flow Model

(July 2017 to June 2022)

Monthly Planning Level Cash Flow Model

	Jul-20	Aug-20	Sep-20	Oct-20	Nov-20	Dec-20	Jan-21	Feb-21	Mar-21	Apr-21	May-21	Jun-21
D-1: Cash Balance (Beginning of Period) [1]	**$3,465,828**	**$4,222,327**	**$4,152,227**	**$2,460,091**	**$1,311,671**	**$1,197,389**	**$1,167,809**	**$1,279,843**	**$899,994**	**$797,371**	**($1,137,073)**	**$554,659**
D-2: Local Funding Sources (Revenue)	**Jul-20**	**Aug-20**	**Sep-20**	**Oct-20**	**Nov-20**	**Dec-20**	**Jan-21**	**Feb-21**	**Mar-21**	**Apr-21**	**May-21**	**Jun-21**
Special Tax/Assessment [2]	$0	$0	$0	$585,538	($28,209)	$0	$2,413,299	$0	$0	$0	$2,171,969	($72,443)
2008 Bond Proceeds	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
2011 Bond Proceeds	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
2015 Bond Proceeds	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Remaining Bond Capacity [3]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
State Funding Actuals to Date												
State Funding EIP-UFRR Payments [4]	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$9,666,669
State Funding EIP-UFRR Real Estate Reimbursement [4]	$2,505,047	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
257 Account Advances [5]	$0	$0	$60,782	$0	$0	$60,782	$0	$0	$60,782	$0	$0	$60,782
257 Account Draws [5]	($18,822)	$0	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)
RFMP Reimbursements [6]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Other Local Funding (New Assessment Bond, In-Lieu Fee, etc.)	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Line of Credit	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Measure V [7]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Sub Total Local Funding Sources	**$2,506,485**	**$20,261**	**$60,782**	**$585,538**	**($28,209)**	**$60,782**	**$2,413,299**	**$0**	**$60,782**	**$0**	**$2,171,969**	**$9,634,747**
D-3: Total Cash Available (Before Cash Out) [D1 + D2]	**$5,972,313**	**$4,242,588**	**$4,213,008**	**$3,045,629**	**$1,283,462**	**$1,258,170**	**$3,581,108**	**$1,279,843**	**$960,776**	**$797,371**	**$1,034,896**	**$10,189,406**
D-4: JPA Operation Costs (GO&A-O&M)	**Jul-20**	**Aug-20**	**Sep-20**	**Oct-20**	**Nov-20**	**Dec-20**	**Jan-21**	**Feb-21**	**Mar-21**	**Apr-21**	**May-21**	**Jun-21**
General Overhead and Administrative Expenses	$166,529	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500
2008 Debt Service	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
2011 Debt Service [8]	$0	$0	$586,288	$0	$0	$0	$0	$0	$289,038	$0	$0	$0
2015 Debt Service [8]	$0	$0	$1,076,269	$0	$0	$0	$0	$0	$594,519	$0	$0	$0
Future Debt Service	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Line of Credit - Repayment	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
O&M Transfers (RD 900, RD 537 & COWS) [9]	$0	$0	$0	$89,002	($4,288)	$0	$366,821	$0	$0	$0	$330,139	($11,011)
Repair, Replacement, & Rehabilitation [10]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
SMARA Reserve Fund [11]	$0	$0	$0	$0	$0	$0	$0	$0	($1,100,000)	$0	$0	$0
Subtotal JPA Operation Costs (GO&A-O&M)	**$166,529**	**$61,500**	**$1,724,056**	**$150,502**	**$57,212**	**$61,500**	**$428,321**	**$61,500**	**($154,944)**	**$61,500**	**$391,639**	**$50,489**
D-5: Capital Improvement Projects Funding Uses	**Jul-20**	**Aug-20**	**Sep-20**	**Oct-20**	**Nov-20**	**Dec-20**	**Jan-21**	**Feb-21**	**Mar-21**	**Apr-21**	**May-21**	**Jun-21**
North Area Projects [12]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Southport Project (Advance Federal Project) [13]	$28,861	$28,861	$28,861	$28,861	$28,861	$28,861	$318,348	$318,348	$318,348	$318,348	$88,598	$88,598
Southport Project Real Estate [13]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Other Planning Efforts [14]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Federal Cost Share Agreement [15]	$1,554,596	$0	$0	$1,554,596	$0	$0	$1,554,596	$0	$0	$1,554,596	$0	$0
DWSC East Slip Repair [16]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Yolo Bypass (In-Kind Work to Advance Federal Project) [17]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Subtotal Capital Improvement Projects	**$1,583,457**	**$28,861**	**$28,861**	**$1,583,457**	**$28,861**	**$28,861**	**$1,872,944**	**$318,348**	**$318,348**	**$1,872,944**	**$88,598**	**$88,598**
D-6: Total Cash Paid Out [D4 + D5]	**$1,749,986**	**$90,361**	**$1,752,917**	**$1,733,959**	**$86,073**	**$90,361**	**$2,301,266**	**$379,848**	**$163,405**	**$1,934,444**	**$480,238**	**$139,087**
D-7: End Cash Position (870/871) [D3 - D6]	**$4,222,327**	**$4,152,227**	**$2,460,091**	**$1,311,671**	**$1,197,389**	**$1,167,809**	**$1,279,843**	**$899,994**	**$797,371**	**($1,137,073)**	**$554,659**	**$10,050,319**

West Sacramento Area Flood Control Agency

Southport Projected Expenses and Cash Flow Model

(July 2017 to June 2022)

Monthly Planning Level Cash Flow Model

	Jul-21	Aug-21	Sep-21	Oct-21	Nov-21	Dec-21	Jan-22	Feb-22	Mar-22	Apr-22	May-22	Jun-22	Total [18]
D-1: Cash Balance (Beginning of Period) [1]	**$10,050,319**	**$8,240,596**	**$8,090,497**	**$6,317,624**	**$5,119,397**	**$4,944,900**	**$4,855,583**	**$5,238,296**	**$5,088,198**	**$4,135,325**	**$2,430,630**	**$4,159,199**	
D-2: Local Funding Sources (Revenue)	**Jul-21**	**Aug-21**	**Sep-21**	**Oct-21**	**Nov-21**	**Dec-21**	**Jan-22**	**Feb-22**	**Mar-22**	**Apr-22**	**May-22**	**Jun-22**	**Total Program**
Special Tax/Assessment [2]	$0	$0	$0	$597,249	($28,773)	$0	$2,461,565	$0	$0	$0	$2,215,409	($73,891)	$93,187,199
2008 Bond Proceeds	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$9,187,418
2011 Bond Proceeds	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$12,000,000
2015 Bond Proceeds	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$20,000,000
Remaining Bond Capacity [3]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
State Funding Actuals to Date													$47,695,583
State Funding EIP-UFRR Payments [4]	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$20,261	$109,301,949
State Funding EIP-UFRR Real Estate Reimbursement [4]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$26,315,302
257 Account Advances [5]	$0	$0	$60,782	$0	$0	$60,782	$0	$0	$60,782	$0	$0	$60,782	$64,766,490
257 Account Draws [5]	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($20,261)	($64,766,490)
RFMP Reimbursements [6]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$2,700,000
Other Local Funding (New Assessment Bond, In-Lieu Fee, etc.)	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$156,208
Line of Credit	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Measure V [7]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$10,000,000
Sub Total Local Funding Sources	**$0**	**$0**	**$60,782**	**$597,249**	**($28,773)**	**$60,782**	**$2,461,565**	**$0**	**$60,782**	**$0**	**$2,215,409**	**($13,110)**	**$330,543,657**
D-3: Total Cash Available (Before Cash Out) [D1 + D2]	**$10,050,319**	**$8,240,596**	**$8,151,279**	**$6,914,873**	**$5,090,625**	**$5,005,681**	**$7,317,148**	**$5,238,296**	**$5,148,979**	**$4,135,325**	**$4,646,039**	**$4,146,089**	
D-4: JPA Operation Costs (GO&A-O&M)	**Jul-21**	**Aug-21**	**Sep-21**	**Oct-21**	**Nov-21**	**Dec-21**	**Jan-22**	**Feb-22**	**Mar-22**	**Apr-22**	**May-22**	**Jun-22**	**Total JPA/GO&A Costs**
General Overhead and Administrative Expenses	$166,529	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$61,500	$15,182,632
2008 Debt Service	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$4,752,765
2011 Debt Service [8]	$0	$0	$594,038	$0	$0	$0	$0	$0	$281,413	$0	$0	$0	$12,293,055
2015 Debt Service [8]	$0	$0	$1,089,519	$0	$0	$0	$0	$0	$582,144	$0	$0	$0	$17,565,767
Future Debt Service	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Line of Credit - Repayment	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
O&M Transfers (RD 900, RD 537 & COWS) [9]	$0	$0	$0	$90,782	($4,373)	$0	$374,158	$0	$0	$0	$336,742	($11,232)	$13,245,724
Repair, Replacement, & Rehabilitation [10]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
SMARA Reserve Fund [11]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Subtotal JPA Operation Costs (GO&A-O&M)	**$166,529**	**$61,500**	**$1,745,056**	**$152,282**	**$57,127**	**$61,500**	**$435,658**	**$61,500**	**$925,056**	**$61,500**	**$398,242**	**$50,268**	**$63,039,943**
D-5: Capital Improvement Projects Funding Uses	**Jul-21**	**Aug-21**	**Sep-21**	**Oct-21**	**Nov-21**	**Dec-21**	**Jan-22**	**Feb-22**	**Mar-22**	**Apr-22**	**May-22**	**Jun-22**	**Total Capital Proj.**
North Area Projects [12]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$36,603,726
Southport Project (Advance Federal Project) [13]	$88,598	$88,598	$88,598	$88,598	$88,598	$88,598	$88,598	$88,598	$88,598	$88,598	$88,598	$88,598	$131,233,818
Southport Project Real Estate [13]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$34,874,220
Other Planning Efforts [14]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$12,490,937
Federal Cost Share Agreement [15]	$1,554,596	$0	$0	$1,554,596	$0	$0	$1,554,596	$0	$0	$1,554,596	$0	$0	$39,310,300
DWSC East Slip Repair [16]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$4,174,207
Yolo Bypass (In-Kind Work to Advance Federal Project) [17]	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0	$0
Subtotal Capital Improvement Projects	**$1,643,194**	**$88,598**	**$88,598**	**$1,643,194**	**$88,598**	**$88,598**	**$1,643,194**	**$88,598**	**$88,598**	**$1,643,194**	**$88,598**	**$88,598**	**$258,687,208**
D-6: Total Cash Paid Out [D4 + D5]	**$1,809,723**	**$150,098**	**$1,833,655**	**$1,795,476**	**$145,725**	**$150,098**	**$2,078,852**	**$150,098**	**$1,013,655**	**$1,704,694**	**$486,840**	**$138,867**	**$321,727,151**
D-7: End Cash Position (870/871) [D3 - D6]	**$8,240,596**	**$8,090,497**	**$6,317,624**	**$5,119,397**	**$4,944,900**	**$4,855,583**	**$5,238,296**	**$5,088,198**	**$4,135,325**	**$2,430,630**	**$4,159,199**	**$4,007,222**	**($16,791,302)**

West Sacramento Area Flood Control Agency

Southport Projected Expenses and Cash Flow Model

(July 2017 to June 2022)

NOTES:

[1] Starting Fund Balance on July 1, 2017 as shown in the June 2018 finance report to the WSAFCA Board.
[2] Projections based on 2016 Assessment Role adjusted for actuals received in January 2017, Direct Billings, and adjustment for uncollected assessments with a 2% annual increase in assessment revenue.
[3] Remaining bond capacity and associated debt service not currently programmed in the model.
[4] State Funding EIP-UFRR Payments include advances, true-ups, and retention releases. Assumes a 88% State cost share for North Area projects and a blended State Cost Share of 78% for the Southport project.
[5] 257 Account is net State advances and account draws from true-ups on construction and real estate.
[6] Regional Flood Management Planning Reimbursements are based on projected expenditures totaling $2,700,000.
[7] Measure V Revenue is assumed as two payments: first payment of $4.5 million in April 2016 and the second payment of $5.5 million in December 2018.
[8] Debt service payments based on the WSAFCA Assessment Revenue Bonds Series 2015 Bond Closing Memo dated March 17, 2015.
[9] O&M Transfers assume 15.2% of annual assessment revenue transferred to RD 900, RD 537, and the City in accordance with the WSAFCA Assessment District Final Engineers Report dated July 16, 2007.
[10] Long term Repair, Replacement, and Rehabilitation costs not inlcuded.
[11] SMARA Reserve Fund assumes debit of $1.1 million in July 2017 and credit of $1.1 million in March 2021.
[12] North Area Projects are complete and WSAFCA is working through project close out with the State of California.
[13] Southport Project assumes a total project cost of $175.0 million and approximately $63.5 million is projected from Jun. 2018 through Jun. 2026. Construction cost assumes low bidder at $49.9 million with $10 million contingency (20%). Offset plantings assumed to begin in Fall of 2018.
[14] Other planning efforts include costs associated with work order numbers 40002, 40005, 40010, 40012, 40013, 41150, 41165, 41180, and 41190.
[15] Total local share of the Federal Cost Share Agreement Cost is approximately $39.3 million.
[16] Deep Water Ship Channel Slip Repair Project is based on projected expenditures through May 2018 and Construction is estimated at $4 million from Jun. 2018 through December 2018.
[17] In-Kind Work to advance the Federal Project not included.
[18] Totals are for the program through June 2026. Assumes completion of the Federal Project per Senate Bill 5 requirements.

NEAR TERM STATE PAYMENT SUMMARY:

June 2018: 257 Account Draw of $397,404 for Construction True-up.
July 2018: Total 257 Account Draw of $3,428,964 - Consruction True-up of $2,183,365, and Real Estate True-up of $1,245,599 (Laudenschlager [$84,567], Lacomb [$111,133], and Kubo [$1,049,899]).
August 2018: DFA North Area Rentention Release ($380,000), DFA Southport Retention Release ($1,280,000), 257 Account Net State Advance of $30,248,000, and a Total 257 Account Draw of $5,049,000 - Construction True-up of $3,240,000 and Real Estate True-up of $1,809,000 (Lemens [$12,160] SRSD [$6,534], McCray [$26,460], McCray AS [$8,100], Luthra et al [$1,959], Avila [$133,992], Barker AS [169,632], Lemens AS [$14,220], WUSD [$127,349], WUSD AS [$411], Paik AS [153,830], R&A Kubo [14,208], Yokoyama [$10,824], Yokoyama AS [$9,626], Culbreth [$3,676], Calfee [$32,256], Calfee AS [$11,232], A&J Rodgers [$76,560], A&J Rodgers AS [$66,200], M. Rodgers [$112,224], M. Rodgers AS [$25,675], Rodgers et al [$81,195] Rodegers et al AS [$64,825], Sun M AS [406,326], RMD [$12,888], A. Rodgers [$102,976], A. Rodgers AS [$64,806], Freeman [$19,520], and Freeman AS [$39,754]).
September 2018: 257 Account Draw of $6,509,000 for Construction True-up.
October 2018: 257 Account Draw of $5,935,000 for Construction True-up.

STATE PAYMENT RISK LEGEND:

High Degree of Certainty-DWR approvals are secured payment is processed.
Medium Degree of Certainty-Information provided to DWR is adequate, or minor additional information is required and awaiting DWR payment processing.
Low Degree of Certainty-No Information Submitted to DWR or information provided but significant payment process/policy agreement risks exist.

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 3

SUBJECT:
CONSIDERATION OF THE TREASURER'S QUARTERLY FINANCIAL REPORT ON THE CITY'S INVESTMENT PORTFOLIO FOR THE PERIOD ENDING JUNE 30, 2018

INITIATED OR REQUESTED BY:

[ ] **Council** [X] **Staff**

[ ] **Other**

REPORT COORDINATED OR PREPARED BY:
Paul Blumberg, Public Finance Manager

Roberta Raper, Director of Administrative Services

ATTACHMENT [X] **Yes** [ ] **No** [ ] **Information** [ ] **Direction** [X] **Action**

OBJECTIVE
The purpose of this report is to provide the City Council with a quarterly report on the City's investment portfolio for the quarter ending June 30, 2018.

RECOMMENDED ACTION
Staff respectfully recommends that the City Council receive and file the Treasurer's Quarterly Financial Report on the City's investment portfolio for the period ending June 30, 2018.

BACKGROUND
All local agencies are required to file regular financial reports with their legislative body on the status of their investment portfolio. One of the roles of the City's Administrative Services Department, Finance Division, is to manage investments of the City to ensure that cash flow needs are met while maximizing the investment portfolio.

Under section 53646(b)(1) of the Government Code, the City Treasurer may prepare and transmit a quarterly financial report to the City Council on the City's investment portfolio. This report includes:

1. The type of investment, issuer, date of maturity, and par and dollar amount invested on all securities, investments and funds held by the City;
2. A current market value as of the date of the report;
3. A stated compliance of the portfolio to the statement of the City's adopted investment policy, or manner in which the portfolio is not in compliance; and
4. A statement denoting the ability of the local agency to meet its pool's expenditure requirements for the next six months, or provide an explanation as to why sufficient money shall or may not be available. Once a year, the statement of investment policy is included in the report.

This quarterly report was prepared by FTN Financial Services, and provides a report format that staff believes will provide enhanced reporting with more analysis, better graphics, and an expanded analysis of the City's Investment Portfolio. FTN Financial Services is a Las Vegas based firm with municipal clients throughout California and the United States.

ANALYSIS
This quarterly financial report summarizes the status of pooled treasury investments as of June 30, 2018. For the quarter, the City-held treasury investments consisted of investments with a book value of $167,211,455. The largest amount of the City's portfolio continues to be invested in 100% liquidity, including money markets, and pooled investment funds. However, because of the diversification strategy that commenced in 2014, the portfolio now also includes certificates of deposit, federal agencies, medium term notes ("corporate bonds"), and municipal bonds. Historically, it had been the City's practice to focus the majority of investment choices in two municipal "pooled" investment funds: 1) the Local Agency Investment Fund (LAIF), and 2) the California Asset Management Program (CAMP). The concentration in 100% liquid investments has not changed significantly from the previous quarter and is 67.45% of the total portfolio. However, staff continues to seek to increase portfolio returns through other higher-yielding investments in compliance with the City's adopted Investment Policy (see attachment).

Discussion of Performance
The following is a summary of performance by investment type.

Investment type	Par Value	Market Value	Book Value	% of Portfolio	Days to Maturity	Yield to Maturity
CDARS	1,018,296	1,018,296	1,018,296	0.61	1	1.430
Money Market Funds	1,482,677	1,482,677	1,482,677	0.89	1	2.060
LAIF	90,727,732	90,727,732	90,727,732	54.26	1	1.900
CAMP	20,567,186	20,567,186	20,567,186	12.30	1	2.050
Corporate Notes	24,250,000	23,822,988	24,202,190	14.47	837	2.368
Federal Agency Non-Callables	6,600,000	6,447,900	6,579,146	3.93	848	1,522
Federal Agency Callables	3,000,000	2,945,110	2,998,600	1.79	1,367	2.090
CD-FDIC	10,671,000	10,478,233	10,671,000	6.38	788	1.912
Municipal Bonds	8,860,000	8,701,631	8,964,629	5.36	994	1.955
	167,176,890	**166,191,751**	**167,211,455**	**100.00%**	280	**1.977**

This quarter, the returns on the City's portfolio were 1.977%, up slightly from the previous quarter ending 3/31/2018 when the Yield to Maturity (YTM) was 1.654%. The YTM for the entire portfolio is 1.977% as of 3/31/2018 with a book value of $167,211,455, versus $153,655,620 the previous quarter. This increase in the portfolio is typical due to receipt of the 2nd property tax distribution from the County in June.

The investment strategy now has $54,433,861, or 32.55%, of the portfolio in assets maturing in longer term maturities of 180+ days. Compared to a year prior, portfolio returns have been increased due to the investment, or reinvestment of portfolio cash in higher yielding investments, including Negotiable CDs (1.912% YTM), Non-Callable Agencies (1.522% YTM), Callable Agencies (2.090% YTM), Medium Term Notes (2.368% YTM), and Municipal Bonds (1.955% YTM).

However, the continued dominance of completely liquid funds in the portfolio at 67.45% of the total is a determining factor in the overall portfolio YTM. During the quarter ending June 30, 2018, the yields for LAIF were 1.900% YTM. Investments in CAMP, while generally on par with LAIF, were at 2.050% YTM.

Miscellaneous investments in completely liquid time deposits, mutual funds, and cash balances with YTM from 0.00% to 0.010% also limit overall return to the portfolio.

It should be noted that the "paper" gain or loss within a given quarter is not an issue since the City intends to hold the securities to maturity. The "Book Value" represents what the City paid for the security whereas "Market Value" is a constantly changing amount based on the current value. Market values fluctuate with changes in interest rates, and price has an inverse relationship to interest rates. Thus, while the City is required to calculate market values on an ongoing basis, at maturity the investment will achieve the stated "Yield to Maturity" based on the Book Value, or what the City paid for the security. Overall, the book value of the portfolio exceeds market value by $1,101,704, an increase from last quarter that is largely due to purchases of new securities, and an increase in interest rates in the last quarter. Increasing interest rates decrease the value of the portfolio, although a loss would not be realized unless certain assets were to be sold prior to the maturity date.

In the current quarter, staff will continue to evaluate liquidity needs and, where possible, continue the investment in higher yielding securities. At the same time, the fixed income market is experiencing significant instability and sharply rising yields. This will likely be reflected in lower market values to be reported in the next quarterly report.

Environmental Considerations
Not applicable

Commission Recommendation
Not applicable

Strategic Plan Integration
The City's mission includes a *City Government that is Financially Sound,* and the Council's goal for achieving a *Financially Sound City Government* is furthered by investing the City treasury in a manner that ensures safety and liquidity first, and where possible a yield on "excess cash". The preparation of regular quarterly reports on the City's investments supports the mission and this goal.

Alternatives
Since this agenda report transmits a quarterly financial report pursuant to Government Code section 53646(b)(1), alternatives have not been presented.

Coordination and Review
Preparation of this report was coordinated with the City Manager's Office.

Budget/Cost Impact
The investment portfolio, as a whole, earned returns at least consistent with the revenue estimates contained in the budget, and the portfolio yield of 1.977% exceeded the LAIF yield of 1.524%, which is the City's benchmark for evaluating returns on the portfolio.

ATTACHMENTS
1. Excerpts from FTN Financial Investment Report (June 30, 2018)
2. City's adopted Investment Policy (IV-C-2)

FTN FINANCIAL
MAIN STREET ADVISORS
A Subsidiary of First Tennessee Bank National Association

Client Management Team

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Quarterly Investment Report

City of West Sacramento

June 2018

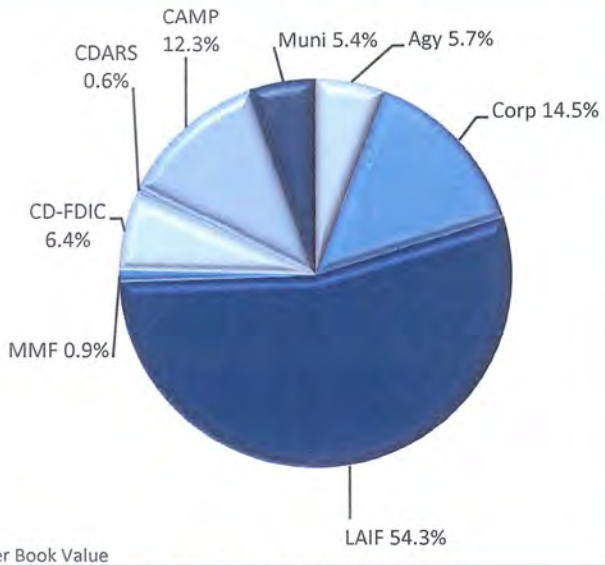
Attachment 1

FTN FINANCIAL
MAIN STREET ADVISORS

Portfolio Summary
6/30/2018

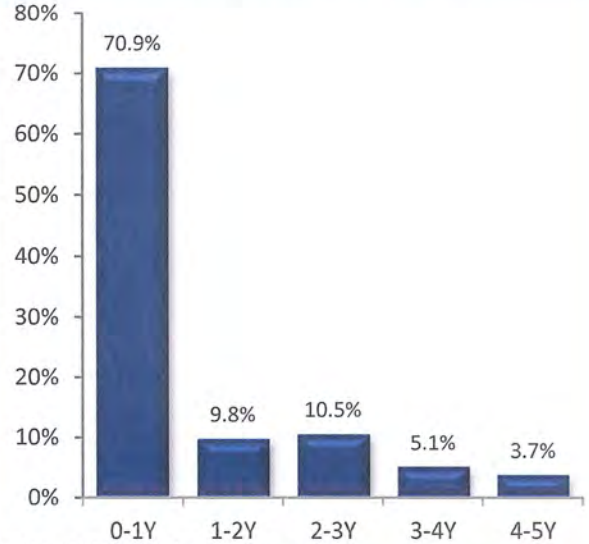
City of West Sacramento

SECTOR ALLOCATION



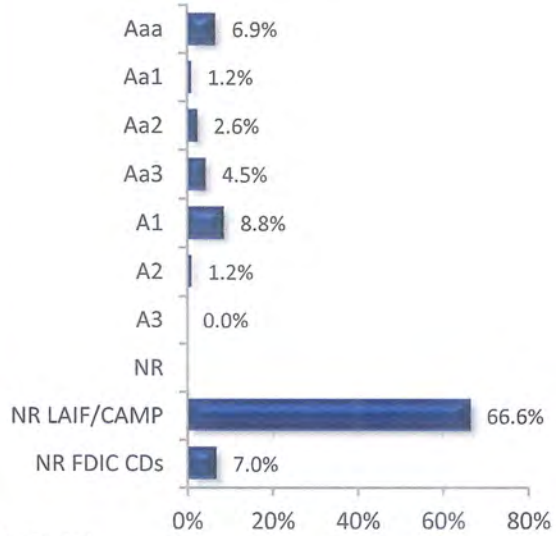
Per Book Value

MATURITY DISTRIBUTION



Per Book Value, Includes Checking

CREDIT QUALITY (MOODY'S)



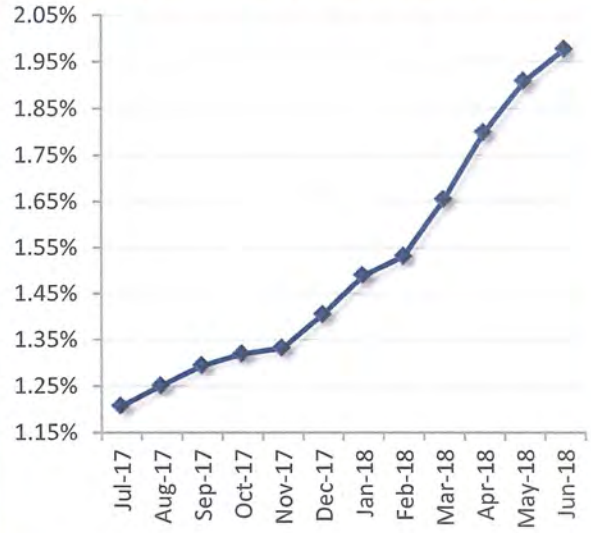
NR: Not Rated

ACCOUNT SUMMARY

	6/30/18	3/31/18
Market Value	$166,191,751	$152,806,822
Book Value*	$167,211,455	$153,655,620
Variance	-$1,019,704	-$848,799
Par Value	$167,176,890	$153,545,873
Net Asset Value	$99.390	$99.448
Book Yield	1.98%	1.65%
Years to Maturity	0.77	0.77
Modified Duration	0.73	0.72

*Book Value is at Amortized Cost

MONTH-END PORTFOLIO BOOK YIELD



Includes Checking

TOP ISSUERS

Issuer	% Portfolio
LAIF	54.3%
CAMP	12.3%
FFCB	2.4%
State of California	2.4%
FNMA	1.2%
Branch Banking & Trust	1.0%
Citibank	1.0%
PNC Bank	0.9%
University of California	0.9%
Berkshire Hathaway	0.9%
Bank of New York	0.9%
US Bank	0.9%
FHLMC	0.9%
Oracle	0.9%
Coca Cola	0.9%

Per Book Value

Item / Sector	Parameters	In Compliance	
Weighted Average Maturity	Weighted Average Maturity (WAM) must not exceed 3 years	Yes	0.77 Yrs
U.S. Treasuries	No limit, maximum maturity 5 years.	Yes:	0.0%
U.S. Federal Agencies	No sector limit, no issuer limit, max maturity 5 years.	Yes:	5.7%
Bankers' Acceptances	Sector limit 40%, issuer limit 5%, maximum maturity 180 days, A-1/P-1 by two.	Yes:	0.0%
Time Deposits	Federally Insured (non-negotiable certificates of deposits) in state or federally chartered banks, savings and loans, or credits unions; amount limited to maximum covered under federal insurance.	Yes:	0.0%
Negotiable Certificates of Deposit	Sector limit 30%, max maturity 5 years, issued by nationally or state-chartered bank, savings association, state or federal credit union, or federally licensed branch of foreign bank.	Yes:	6.4%
Placement Service CDs (CDARS)	Sector limit 30% (including Certificates of Deposits), max 5 years maturity. *	Yes:	0.6%
Commercial Paper	Sector limit 25%, issuer limit 5%, minimum short-term ratings A-1,P-1, or F-1; 25% limit, maximum maturity 270 days, issued by a domestic corporation w/ at least $500 million of assets and long-term rating of A or higher.	Yes:	0.0%
Local Agency Investment Fund	Max allowable per State Treasurer's Office (currently $65 million; City has 3 accounts).	Yes:	90.7 Mil
Joint Powers Authority (CAMP)	No limit, registered or exempt with SEC, advisor has at least 5 years experience, AUM of $500 million, invests in securities authorized by California Code 5360 subdivisions a. to o. *	Yes:	12.3%
Corporate Medium-Term Notes	Sector limit 30%, issuer limit 5%, maximum maturity 5 years, a minimum rating of A or A2, and issued by corporations organized and operating within United States or by depository institutions licensed by the United States or any state operating in the United States.	Yes:	14.5%
Mutual/Money Market Funds	Sector limit 20%, Issuer limit 10%, AAA by at least two rating services or Investment Advisor or has 5yrs experience investing in securities and obligations authorized by California Code 53601 subdivisions (a) to (j), with $500MM AUM.	Yes:	0.9%
Municipal Bonds	30% sector limit, minimum rating of A or A2 by S&P or Moody's, maximum maturity 5 years.	Yes:	5.4%
Collateralized Bank Deposits	No sector limit, collateralized with treasuries, U.S. Agencies, including MBS; or obligations of any state, city, county, authority with a minimum rating of AA by two rating agencies.	Yes:	0.0%

Securities' market values are derived from the Entity's custodian.

*Investment Parameters per California Code 53601

The City certifies the investment portfolio provides sufficient liquidity to meet the City's cash flow obligations for the next six months.

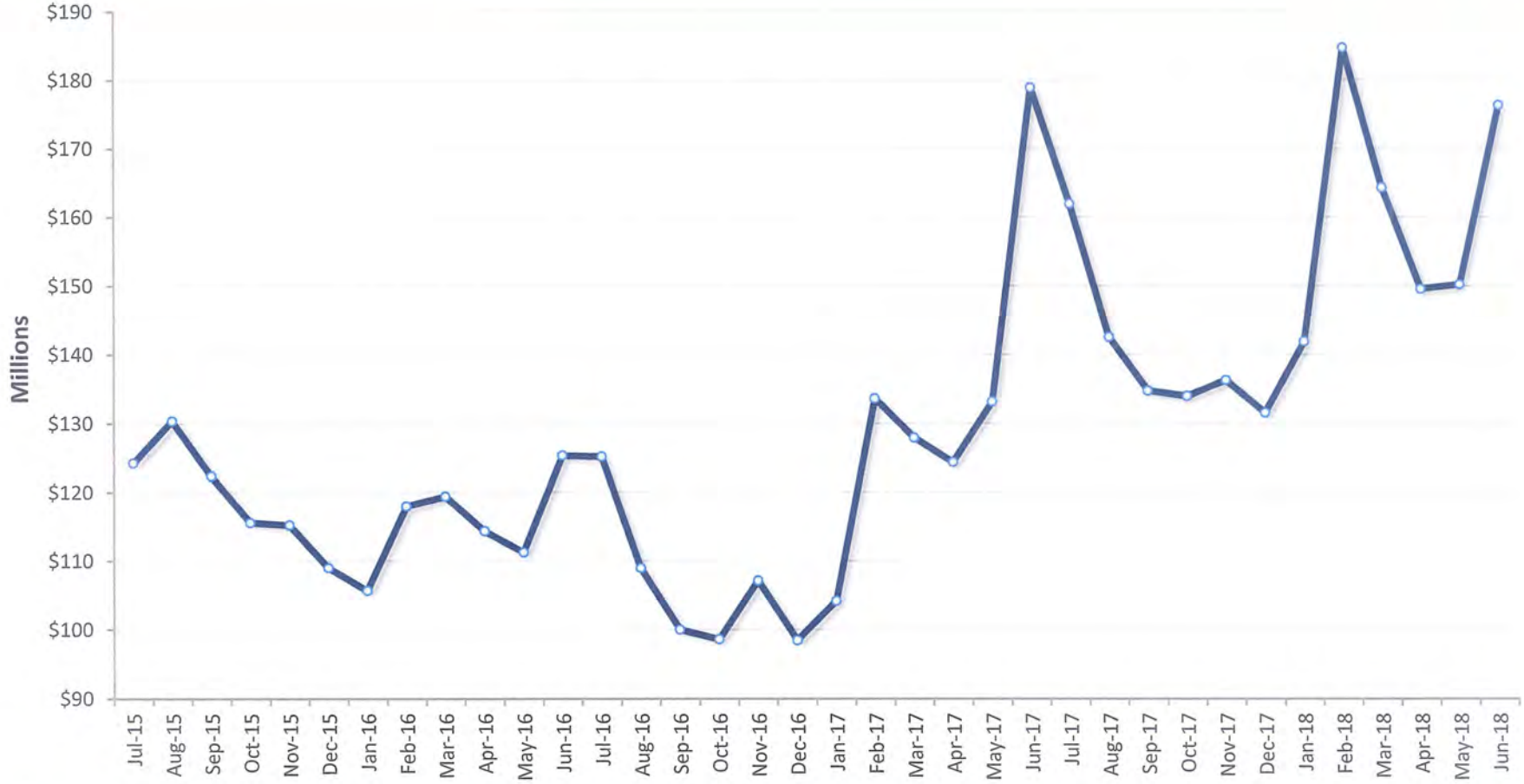
Portfolio Metrics	6/30/2018	3/31/2018	Change
Market Value	$166,191,751	$152,806,822	
Book Value	$167,211,455	$153,655,620	
Par Value	$167,176,890	$153,545,873	
Net Asset Value	$0.994	$0.994	-$0.001
Yield to Maturity	1.98%	1.65%	0.32%
2Yr Treasury Note Yield	2.53%	2.27%	0.26%
LAIF Yield (monthly avg)*	1.85%	1.52%	0.33%
Average Years to Maturity	0.77	0.77	0.00
Effective Duration	0.73	0.72	0.01

*LAIF rate is estimated for current month/quarter end

Sectors (Book Value)	6/30/2018	3/31/2018	Change
Federal Agency	$9,577,746	$9,577,746	$0
LAIF	$90,727,732	$87,418,547	$3,309,185
Corporate	$24,202,190	$16,026,858	$8,175,332
Certificates of Deposit	$10,671,000	$10,432,000	$239,000
CDARS	$1,018,296	$1,014,673	$3,623
Municipal Bonds	$8,964,629	$8,465,144	$499,485
CAMP	$20,567,186	$20,467,683	$99,502
Money Market Funds	$1,482,677	$252,970	$1,229,706
Total	$167,211,455	$153,655,620	$13,555,834

Historical Book Values
6/30/2018

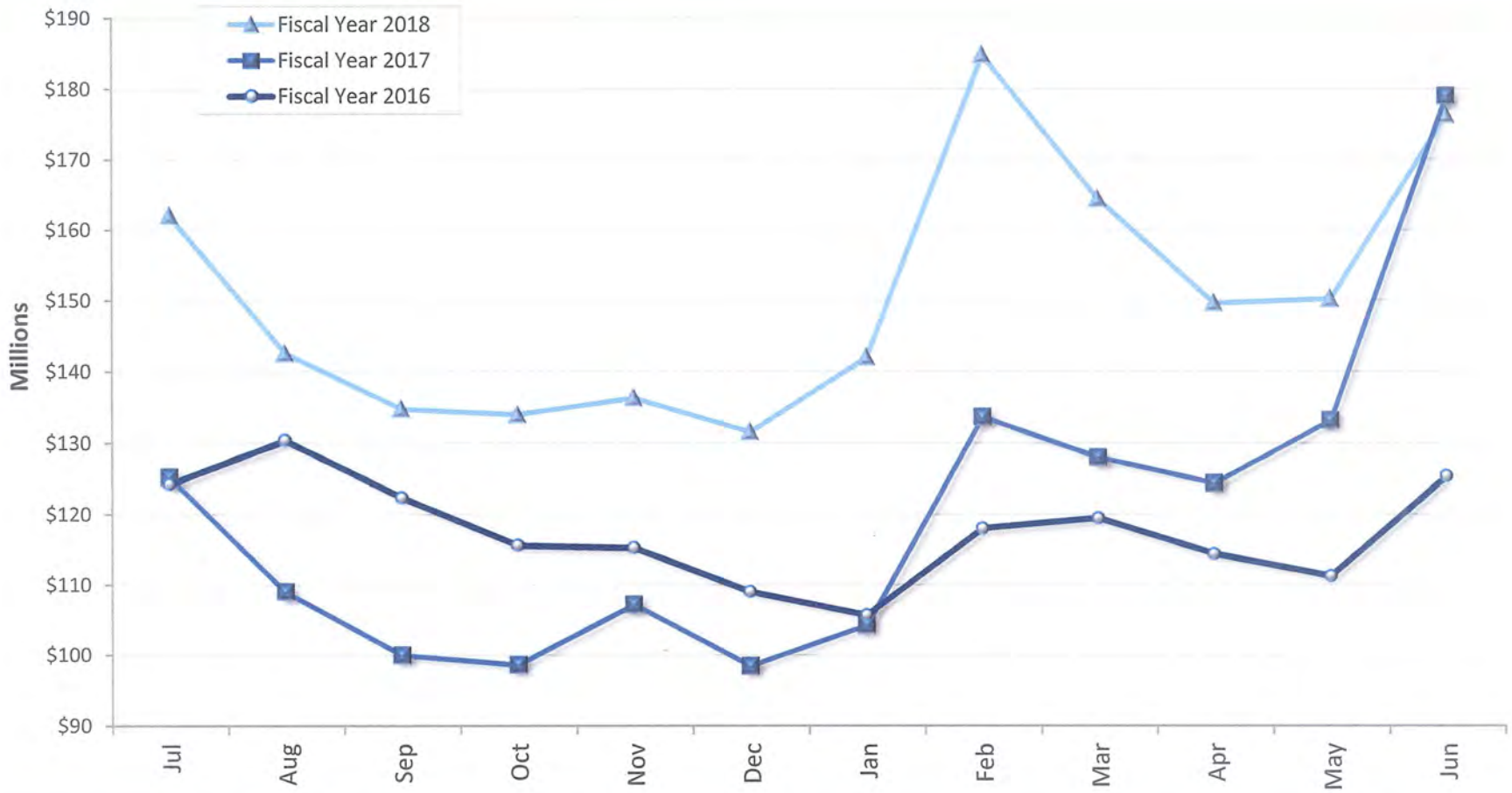
City of West Sacramento



	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun
Fiscal Year 2016	$124.2	$130.4	$122.4	$115.6	$115.2	$109.0	$105.7	$118.0	$119.4	$114.4	$111.3	$125.3
Fiscal Year 2017	$125.2	$109.0	$100.0	$98.6	$107.2	$98.4	$104.2	$133.6	$128.0	$124.3	$133.2	$178.9
Fiscal Year 2018	$162.0	$142.5	$134.8	$134.0	$136.3	$131.5	$141.9	$184.8	$164.3	$149.5	$150.2	$176.4

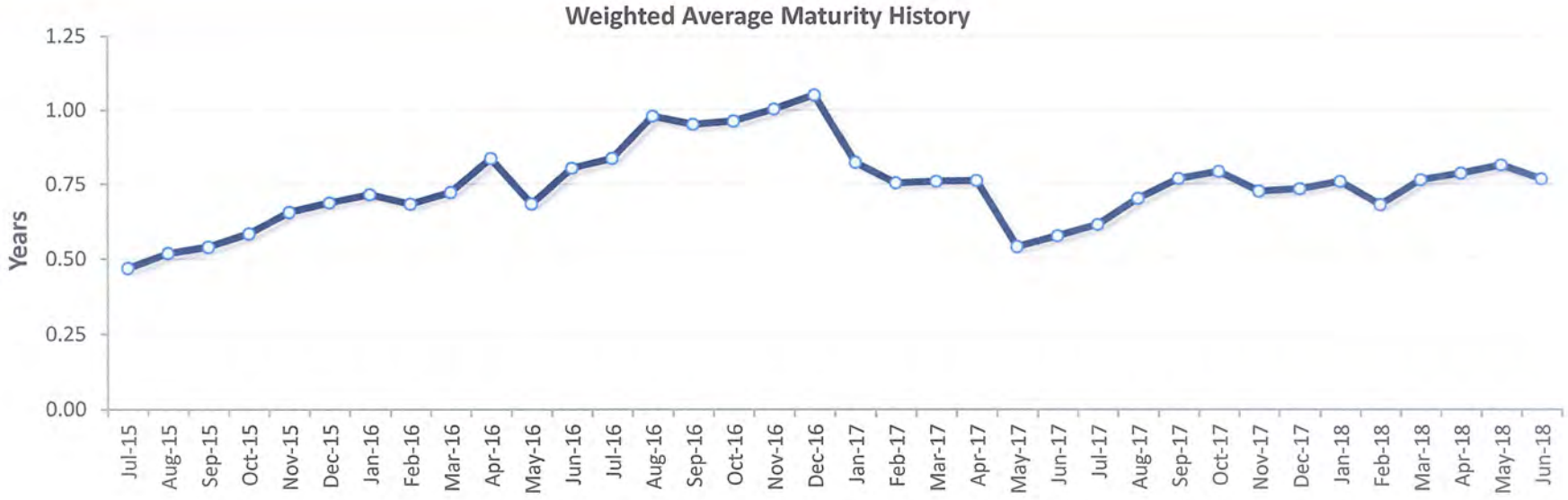
Figures in Millions, Average Daily Balance

Historical Book Values by Fiscal Year
6/30/2018



	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun
Fiscal Year 2016	$124.2	$130.4	$122.4	$115.6	$115.2	$109.0	$105.7	$118.0	$119.4	$114.4	$111.3	$125.3
Fiscal Year 2017	$125.2	$109.0	$100.0	$98.6	$107.2	$98.4	$104.2	$133.6	$128.0	$124.3	$133.2	$178.9
Fiscal Year 2018	$162.0	$142.5	$134.8	$134.0	$136.3	$131.5	$141.9	$184.8	$164.3	$149.5	$150.2	$176.4

Figures in Millions, Average Daily Balance


Weighted Average Maturity History
Years
1.25
1.00
0.75
0.50
0.25
0.00
Jul-15
Aug-15
Sep-15
Oct-15
Nov-15
Dec-15
Jan-16
Feb-16
Mar-16
Apr-16
May-16
Jun-16
Jul-16
Aug-16
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Oct-16
Nov-16
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Jan-17
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May-17
Jun-17
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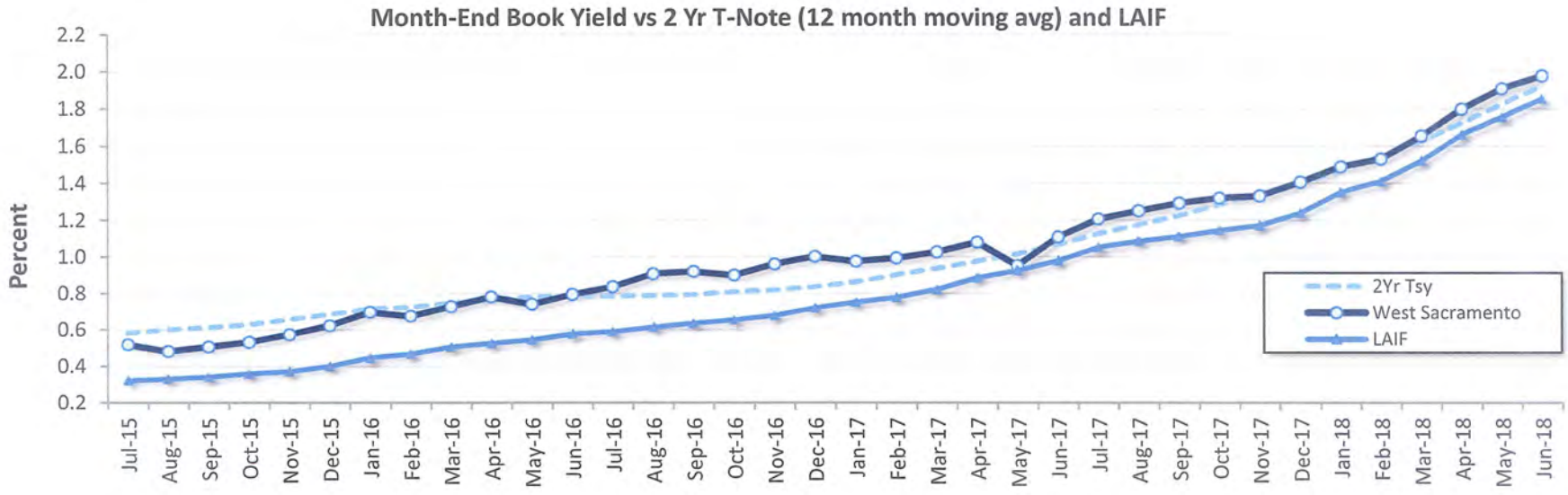
May-18
Jun-18


Month-End Book Yield vs 2 Yr T-Note (12 month moving avg) and LAIF
Percent
2.2
2.0
1.8
1.6
1.4
1.2
1.0
0.8
0.6
0.4
0.2
2Yr Tsy
West Sacramento
LAIF
Jul-15
Aug-15
Sep-15
Oct-15
Nov-15
Dec-15
Jan-16
Feb-16
Mar-16
Apr-16
May-16
Jun-16
Jul-16
Aug-16
Sep-16
Oct-16
Nov-16
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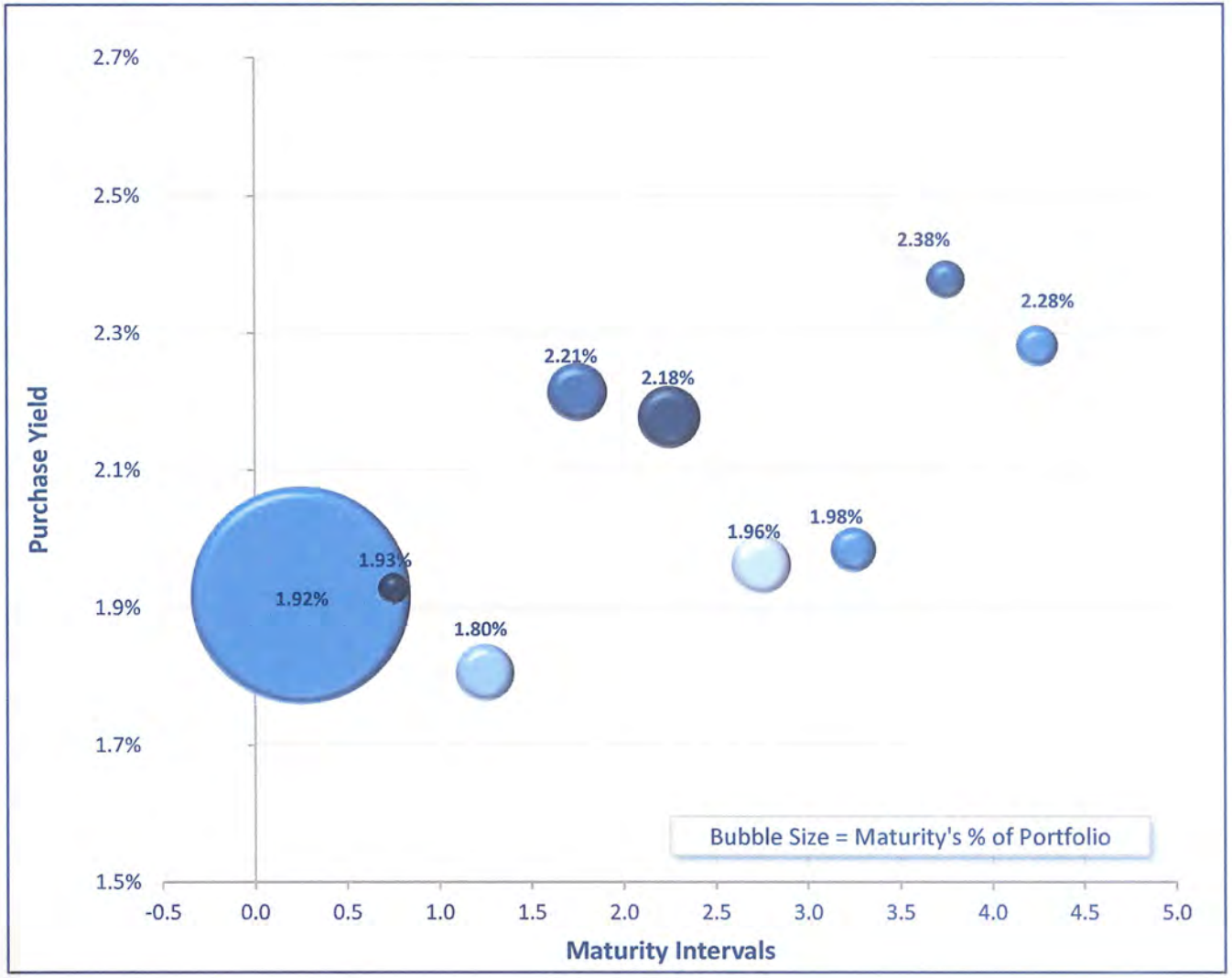
Nov-17
Dec-17
Jan-18
Feb-18
Mar-18
Apr-18
May-18
Jun-18

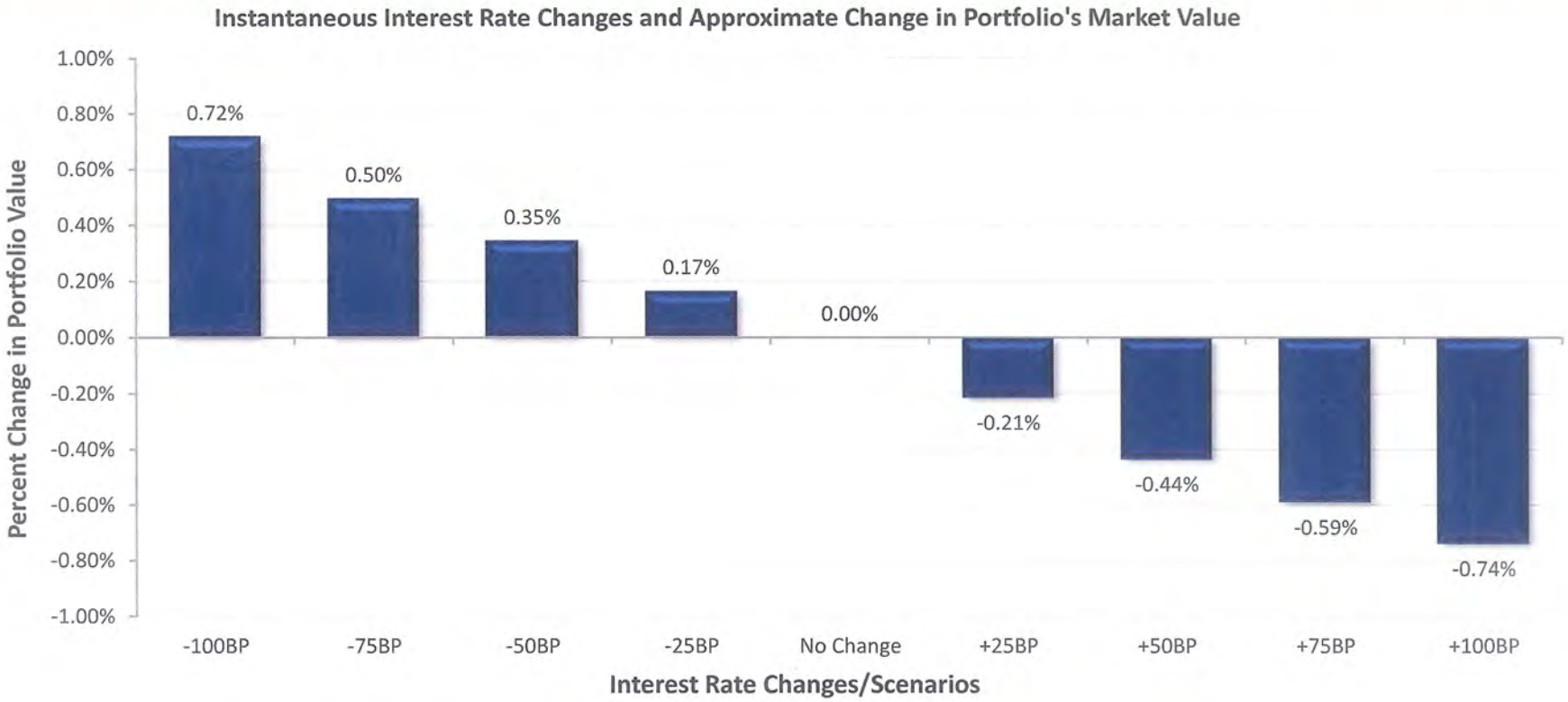
Purchase YTM Per 6-Month Maturity Intervals
6/30/2018

City of West Sacramento

Years	Purchase Yield	% of Portfolio*
0 to .5	1.92%	69.24%
.5 to 1.0	1.93%	1.35%
1.0 to 1.5	1.80%	4.80%
1.5 to 2.0	2.21%	5.11%
2.0 to 2.5	2.18%	5.61%
2.5 to 3.0	1.96%	5.08%
3.0 to 3.5	1.98%	2.96%
3.5 to 4.0	2.38%	2.10%
4.0 to 4.5	2.28%	2.54%
4.5 to 5.0	2.96%	1.19%

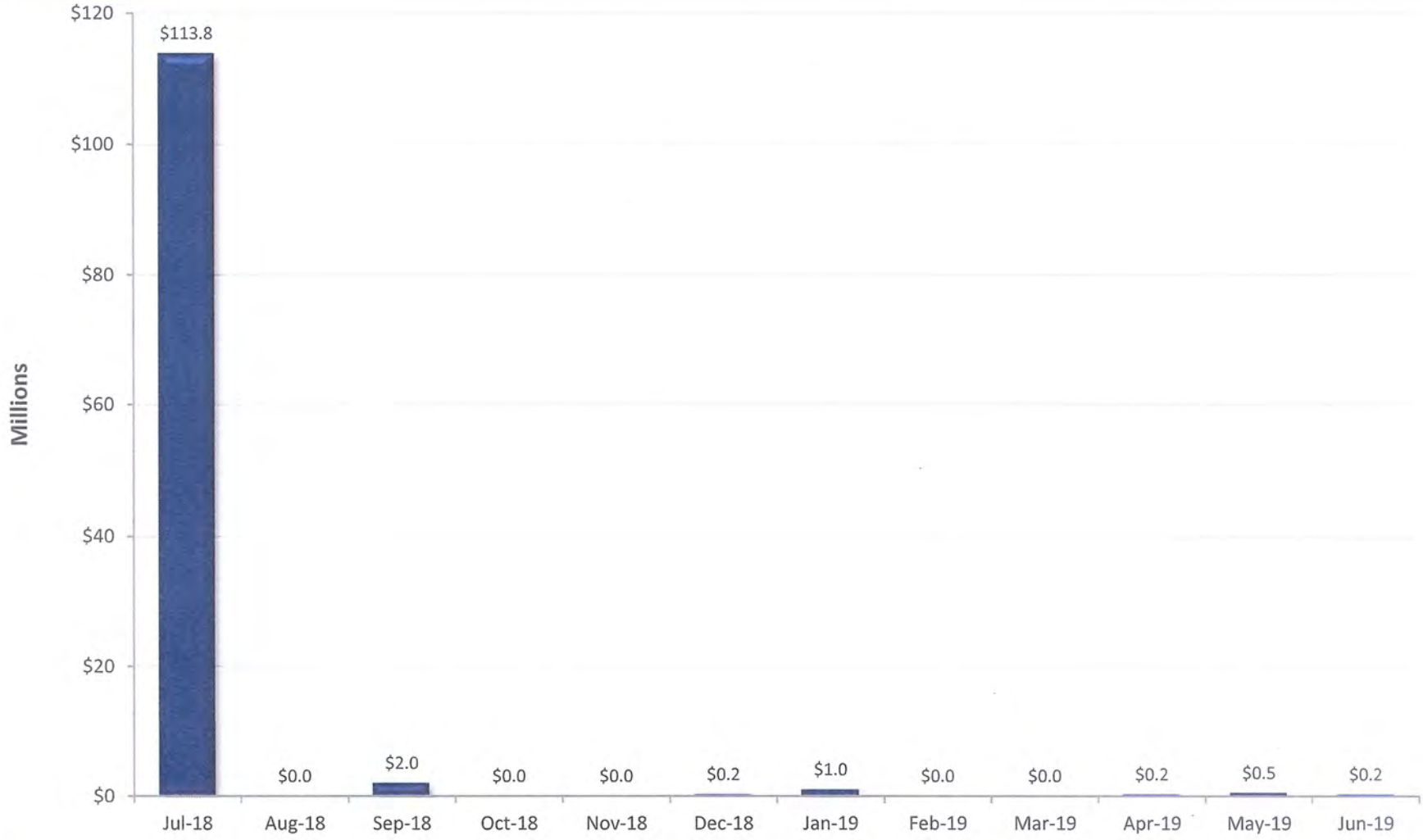
*Based on Book Value





Interest Rate Change	Portfolio Value	Value Change	Percent Change
-100 Basis Points	$167,388,332	$1,196,581	0.72%
-75 Basis Points	$167,017,392	$825,641	0.50%
-50 Basis Points	$166,766,110	$574,359	0.35%
-25 Basis Points	$166,466,965	$275,214	0.17%
No Change	$166,191,751	$0	0.00%
+25 Basis Points	$165,835,104	-$356,647	-0.21%
+50 Basis Points	$165,466,158	-$725,593	-0.44%
+75 Basis Points	$165,207,896	-$983,855	-0.59%
+100 Basis Points	$164,961,932	-$1,229,819	-0.74%

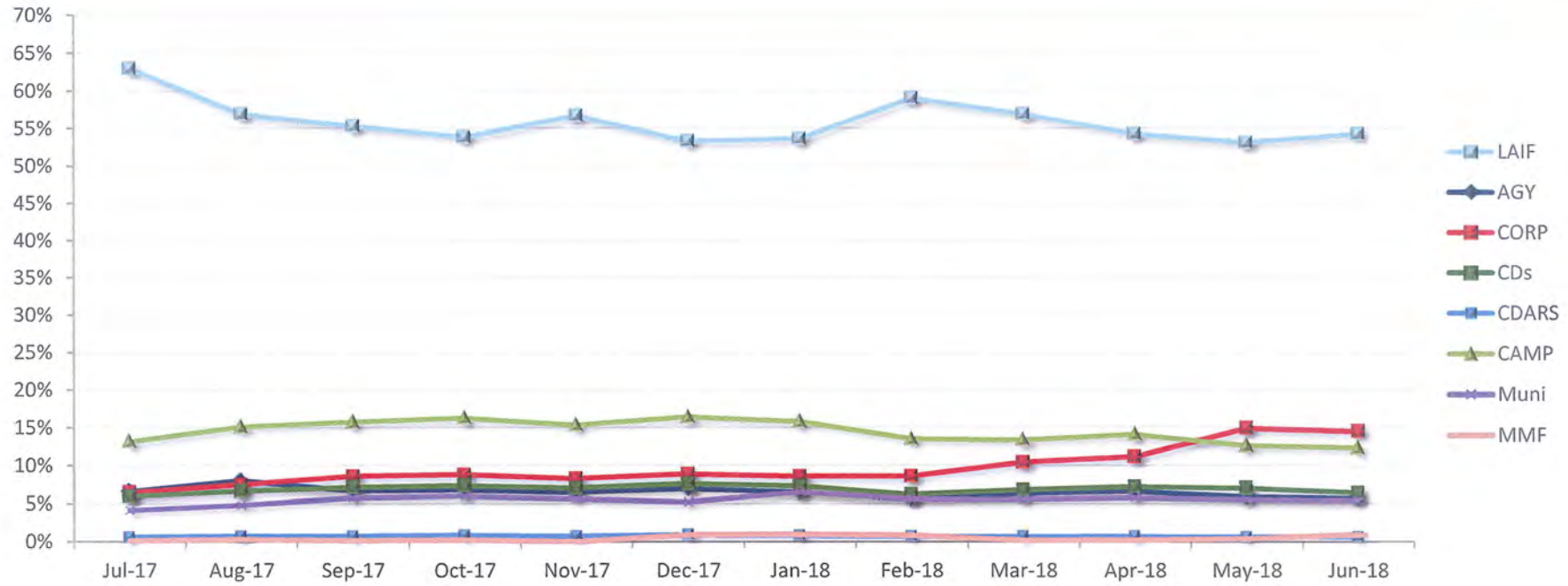
Next Twelve Months Maturities
6/30/2018



	Jul-18	Aug-18	Sep-18	Oct-18	Nov-18	Dec-18	Jan-19	Feb-19	Mar-19	Apr-19	May-19	Jun-19
Maturities	$113.8	$0.0	$2.0	$0.0	$0.0	$0.2	$1.0	$0.0	$0.0	$0.2	$0.5	$0.2

Par Value in Millions, Including Checking

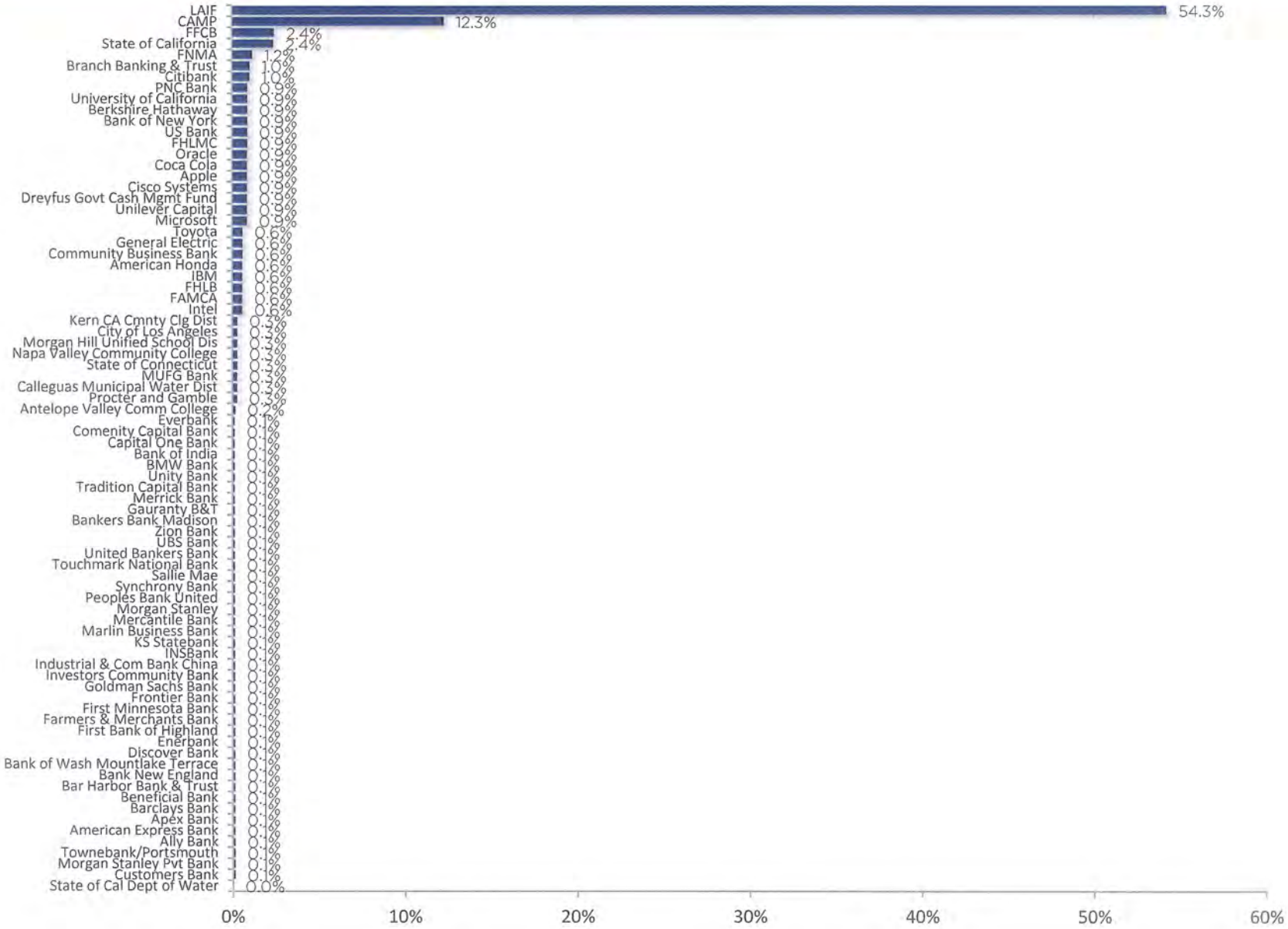
Historical Sector Allocation
6/30/2018



Sector	Jul-17	Aug-17	Sep-17	Oct-17	Nov-17	Dec-17	Jan-18	Feb-18	Mar-18	Apr-18	May-18	Jun-18
Agency	6.6%	8.0%	6.7%	6.8%	6.4%	6.9%	6.5%	5.6%	6.3%	6.6%	5.9%	5.7%
Corporate	6.5%	7.5%	8.6%	8.8%	8.3%	8.9%	8.6%	8.6%	10.4%	11.1%	14.9%	14.5%
Municipal Bonds	4.1%	4.8%	5.8%	6.0%	5.6%	5.2%	6.5%	5.6%	5.5%	5.8%	5.5%	5.4%
CDARS	0.6%	0.7%	0.7%	0.8%	0.7%	0.9%	0.8%	0.7%	0.7%	0.7%	0.6%	0.6%
CDs	6.0%	6.7%	7.1%	7.3%	7.0%	7.6%	7.3%	6.2%	6.8%	7.2%	7.0%	6.4%
MMF	0.1%	0.3%	0.1%	0.2%	0.0%	0.9%	0.9%	0.8%	0.2%	0.2%	0.4%	0.9%
CAMP	13.1%	15.1%	15.7%	16.2%	15.3%	16.4%	15.8%	13.5%	13.3%	14.1%	12.6%	12.3%
LAIF	63.0%	56.9%	55.3%	53.8%	56.7%	53.3%	53.6%	59.0%	56.9%	54.3%	53.1%	54.3%
Total	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.0%	100.1%	100.0%	100.0%	100.0%

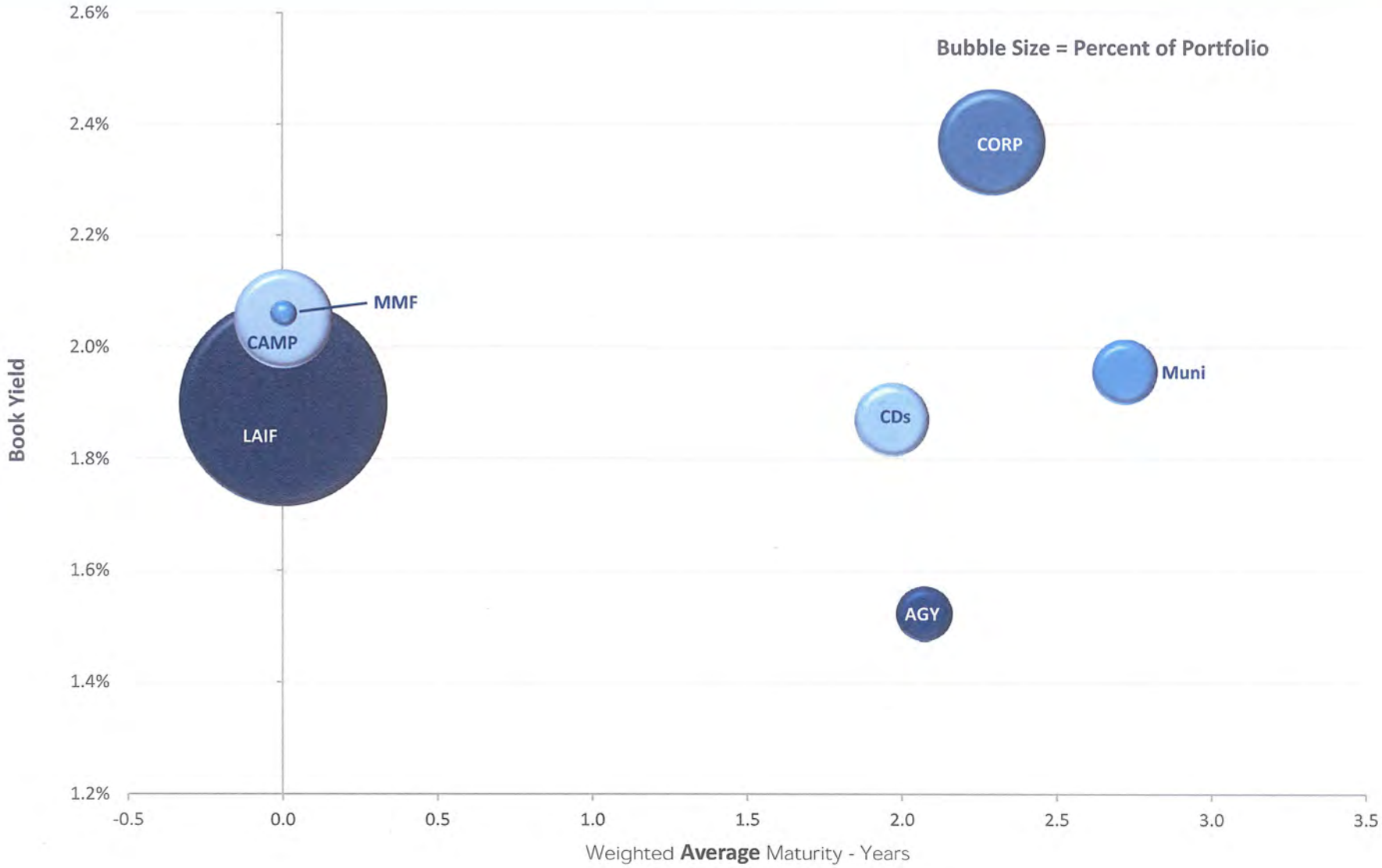
Issuer Allocation
6/30/2018

City of West Sacramento



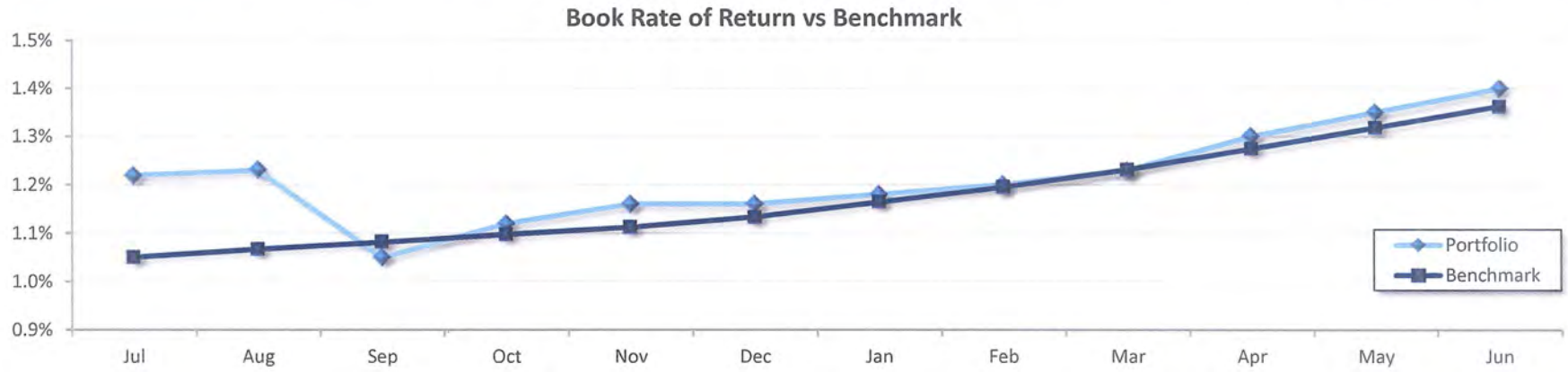
Purchase Yield and Weighted Avg Maturity
6/30/2018

City of West Sacramento



Historical Earnings and Book Rate of Return Performance
6/30/2018

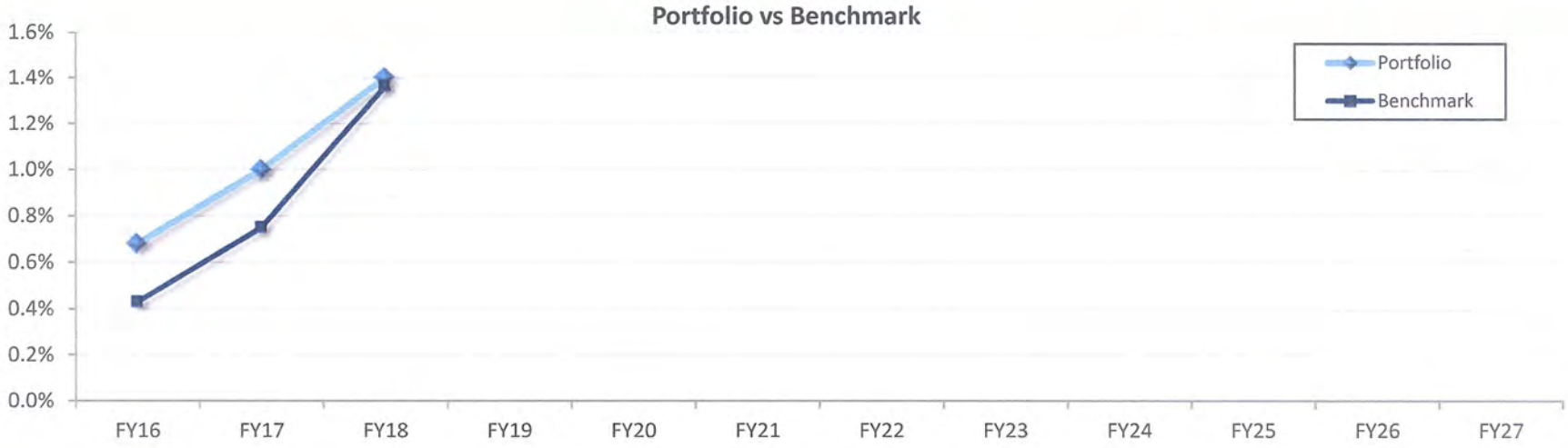
Fiscal YTD ($000s)	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun
FY 2016	$58.0	$113.2	$164.8	$219.7	$275.1	$332.9	$399.1	$470.4	$554.0	$631.1	$710.9	$796.4
FY 2017	$93.6	$180.5	$263.3	$324.7	$410.8	$497.3	$595.2	$701.7	$816.9	$929.9	$1,056.2	$1,204.2
FY 2018	$167.3	$319.4	$389.2	$539.0	$691.1	$817.7	$974.9	$1,158.8	$1,365.2	$1,613.2	$1,846.3	$2,111.4

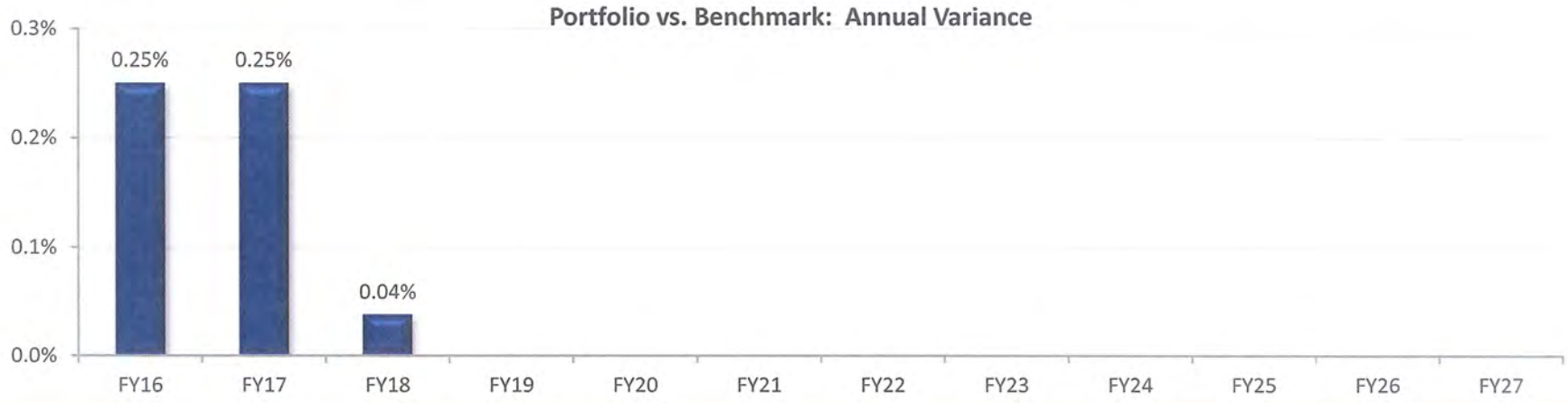


Fiscal YTD	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun
Book Rate of Rtn	1.22%	1.23%	1.05%	1.12%	1.16%	1.16%	1.18%	1.20%	1.23%	1.30%	1.35%	1.40%
Custom Benchmark*	1.05%	1.07%	1.08%	1.10%	1.11%	1.13%	1.16%	1.20%	1.23%	1.27%	1.32%	1.36%
Variance	0.17%	0.16%	-0.03%	0.02%	0.05%	0.03%	0.02%	0.00%	0.00%	0.03%	0.03%	0.04%

*Benchmark: LAIF FYTD Rate--Averages Over the Fiscal Year Period

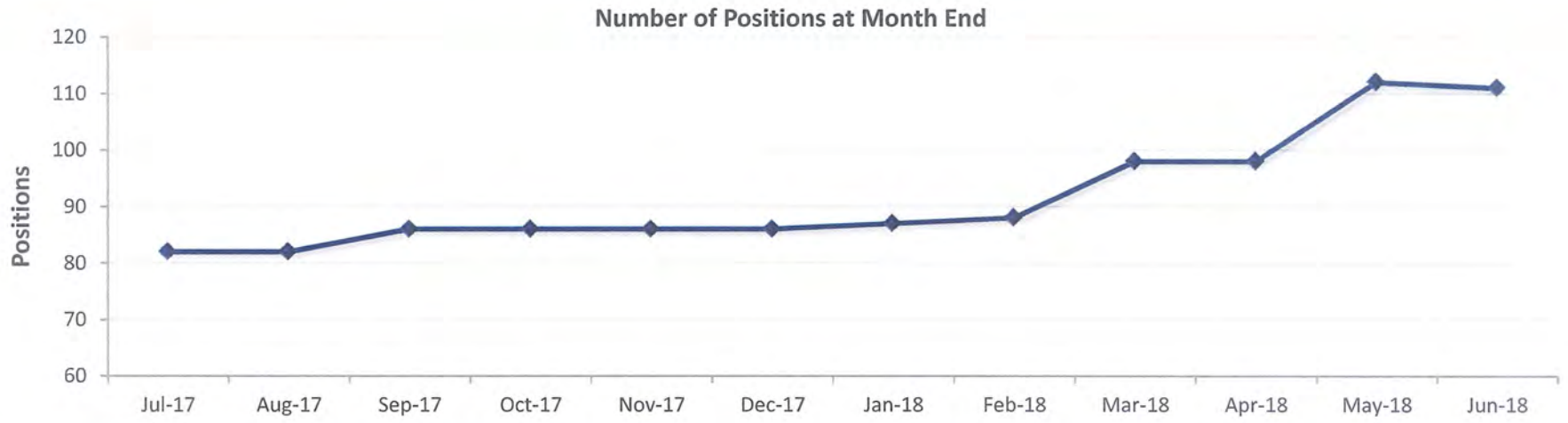
Historical Book Rate of Return Performance
6/30/2018



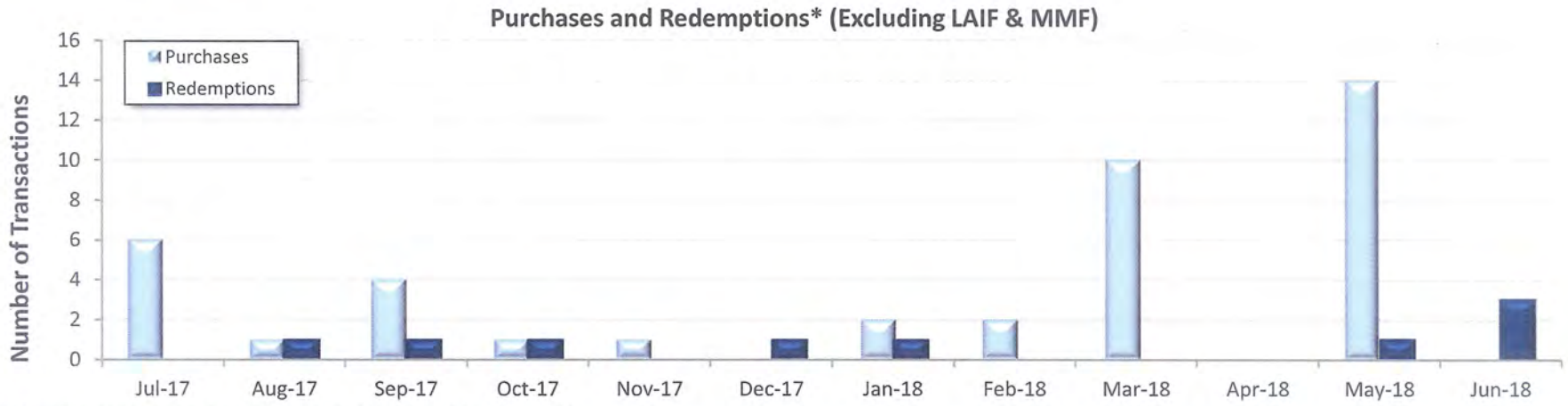


Fiscal YTD	FY16	FY17	FY18	FY19	FY20	FY21	FY22	FY23	FY24	FY25	FY26	FY27
Portfolio	0.68%	1.00%	1.40%									
Benchmark*	0.43%	0.75%	1.36%									
Variance	0.25%	0.25%	0.04%									

*Benchmark: LAIF Monthly Rate FYTD--Averages Over the Fiscal Year Period



	Jul-17	Aug-17	Sep-17	Oct-17	Nov-17	Dec-17	Jan-18	Feb-18	Mar-18	Apr-18	May-18	Jun-18
Positions	82	82	86	86	86	86	87	88	98	98	112	111



*Redemptions include maturities, calls, and sells (excluding MMF, LAIF, CAMP)

	Jul-17	Aug-17	Sep-17	Oct-17	Nov-17	Dec-17	Jan-18	Feb-18	Mar-18	Apr-18	May-18	Jun-18
Purchases	6	1	4	1	1	0	2	2	10	0	14	0
Redemptions	0	1	1	1	0	1	1	0	0	0	1	3
Total Transactions	6	2	5	2	1	1	3	2	10	0	15	3

City of West Sacramento
Portfolio Management
Portfolio Summary
June 30, 2018

Investments	Par Value	Market Value	Book Value	% of Portfolio	Days to Maturity	YTM 365 Equiv.
CDARS	1,018,296.06	1,018,296.06	1,018,296.06	0.61	1	1.430
Money Market Funds	1,482,676.93	1,482,676.93	1,482,676.93	0.89	1	2.060
LAIF	90,727,731.55	90,727,731.55	90,727,731.55	54.26	1	1.900
CAMP	20,567,185.60	20,567,185.60	20,567,185.60	12.30	1	2.050
Corporate Notes	24,250,000.00	23,822,987.50	24,202,189.97	14.47	837	2.368
Federal Agency Non-Callables	6,600,000.00	6,447,900.00	6,579,146.00	3.93	757	1.522
Federal Agency Callables	3,000,000.00	2,945,110.00	2,998,600.00	1.79	1,367	2.090
CD-FDIC	10,671,000.00	10,478,232.87	10,671,000.00	6.38	788	1.912
Municipal Bonds	8,860,000.00	8,701,630.50	8,964,628.55	5.36	994	1.955
Investments	**167,176,890.14**	**166,191,751.01**	**167,211,454.66**	**100.00%**	**280**	**1.977**
Cash and Accrued Interest						
Passbook/Checking (not included in yield calculations)	3,975,177.96	3,975,177.96	3,975,177.96		1	0.010
Accrued Interest at Purchase		32,516.73	32,516.73			
Subtotal		4,007,694.69	4,007,694.69			
Total Cash and Investments	**171,152,068.10**	**170,199,445.70**	**171,219,149.35**		**280**	**1.977**

Total Earnings	June 30 Month Ending	Fiscal Year To Date	Fiscal Year Ending
Current Year	262,621.96	2,111,361.30	2,111,361.30
Average Daily Balance	**176,354,292.16**	**151,029,404.74**	
Effective Rate of Return	**1.81%**	**1.40%**	

Paul Blumberg, Public Finance Manager

Reporting period 06/01/2018-06/30/2018

Run Date: 07/25/2018 - 12:33

Portfolio WSAC
CP
PM (PRF_PM1) 7.3.0
Report Ver. 7.3.6.1

CITY OF WEST SACRAMENTO

ADMINISTRATIVE POLICY

NUMBER: IV-C-2

DATE: September 8, 1992
Revised: February 1, 1999
Revised: October 15, 2014
Revised: November 1, 2017

SUBJECT: INVESTMENT POLICY

AUTHORITY: City Council

PURPOSE

This policy sets forth the procedural regulations for investing surplus funds. The intent of the Investment Policy of the City of West Sacramento ("City") is to define the parameters within which funds are to be managed.

POLICY

In methods, procedures, and practices, the policy formalizes the framework for the City's investment activities that must be exercised to ensure effective and judicious fiscal and investment management of the City's funds. The guidelines are intended to be broad enough to allow the investment officer to function properly within the parameters of responsibility and authority, yet specific enough to safeguard the investment assets. All investment activity will conform to the California Government Code, Sections 53601 through 53659.

PROCEDURE

A. SCOPE

This policy applies to the investment of surplus operating and capital funds and proceeds from certain bond issues. These funds are accounted for in the City's Comprehensive Annual Financial Report (CAFR).

1. *Pooling of Funds*: Except for cash in certain restricted and special funds, the City will consolidate cash balances from all funds to maximize investment earnings. Investment income will be allocated to the various funds based on their respective participation and in accordance with generally accepted accounting principles.

B. GENERAL OBJECTIVES

The primary objectives, in priority order, of investment activities shall be safety, liquidity, and yield.

1. *Safety*: Safety of principal is the foremost objective of the investment program. Investments shall be undertaken in a manner that seeks to ensure the preservation of capital in the overall portfolio. The objective will be to mitigate credit risk and interest rate risk.

   a. Credit Risk: The City will minimize credit risk, the risk of loss due to the failure of the security issuer or backer, by:

      - Limiting investments to the safest types of securities.

- Prequalifying the financial institutions, broker/dealers, intermediaries, and advisors with which the City will do business.

- Diversifying the investment portfolio so that potential losses on individual securities will be minimized.

b. Interest Rate Risk: The City will minimize the risk that the market value of securities in the portfolio will fall due to changes in general interest rates, by:

- Structuring the investment portfolio so that securities mature to meet cash requirements for ongoing operations, thereby avoiding the need to sell securities on the open market prior to maturity.

- Limiting the overall weighted average maturity of the portfolio to 3.0 years.

2. *Liquidity*: The investment portfolio shall remain sufficiently liquid to meet all operating requirements that may be reasonably anticipated. This is accomplished by structuring the portfolio so that securities mature concurrent with cash needs to meet anticipated demands (static liquidity). Furthermore, since all possible cash demands cannot be anticipated, the portfolio should consist largely of securities with active secondary or resale markets (dynamic liquidity). A portion of the portfolio also may be placed in money market mutual funds or local government investment pools which offer same-day liquidity for short-term funds. It is generally not the intention to liquidate a security prior to maturity. The City will hold securities to maturity with the following exceptions:

- A security with declining credit may be sold early to minimize loss of principal.

- A security swap would improve the quality, yield, or target duration in the portfolio.

- Liquidity needs of the portfolio require that the security be sold.

Yield: The investment portfolio shall be designed with the objective of attaining a market rate of return throughout budgetary and economic cycles, taking into account the investment risk constraints and liquidity needs. Return on investment is of secondary importance compared to the safety and liquidity objectives described above. The core of investments are limited to relatively low risk securities in anticipation of earning a fair return relative to the risk being assumed.

C. STANDARDS OF CARE

1. *Prudence*: The standard of prudence to be used by investment officials shall be the "prudent person" standard (California Government Code section 53600.3) and shall be applied in the context of managing an overall portfolio. Investment officers acting in accordance with written procedures and this investment policy and exercising due diligence shall be relieved of personal responsibility for an individual security's credit risk or market price changes, provided deviations from expectations are reported in a timely fashion and the liquidity and the sale of securities are carried out in accordance with the terms of this policy.

   Investments shall be made with judgment and care, under circumstances then prevailing, which persons of prudence, discretion and intelligence exercise in the management of their own affairs, not for speculation, but for investment, considering the probable safety of their capital as well as the probable income to be derived.

2. *Ethics and Conflicts of Interest*: Officers and employees involved in the investment process shall refrain from personal business activity that could conflict with the proper execution and management of the investment program, or that could impair their ability to make impartial decisions. Employees and investment officials shall disclose any material interests in financial institutions with which they conduct business. They shall further disclose any personal financial/ investment positions that could be related to the performance of the investment portfolio. Employees and officers shall refrain from undertaking personal investment transactions with the same individual with whom business is conducted on behalf of the City.

3. *Delegation of Authority*: Authority to manage the investment program is granted to the Director of Finance/City Treasurer, hereinafter referred to as investment officer, and derived from Ordinance 87-12. Responsibility for the operation of the investment program is hereby delegated to the investment officer, who shall act in accordance with established written procedures and internal controls for the operation of the investment program consistent with this investment policy. Procedures should include references to: safekeeping, delivery vs. payment, investment accounting, repurchase agreements, wire transfer agreements, and collateral/depository agreements. No person may engage in an investment transaction except as provided under the terms of this policy and the procedures established by the investment officer. The investment officer shall be responsible for all transactions undertaken and shall establish a system of controls to regulate the activities of subordinate officials.

D. SAFEKEEPING AND CUSTODY

1. *Authorized Financial Dealers and Institutions*: A list will be maintained of financial institutions authorized to provide investment services. In addition, a list also will be maintained of approved security broker/dealers selected by creditworthiness (e.g., a minimum capital requirement of $10,000,000 and at least five years of operation). These may include "primary" dealers or regional dealers that qualify under Securities and Exchange Commission (SEC) Rule 15C3-1 (uniform net capital rule).

   All financial institutions and broker/dealers who desire to become qualified for investment transactions must supply the following as appropriate:

   - Audited financial statements.
   - Proof of National Association of Securities Dealers (NASD) certification.
   - Proof of state registration.
   - Completed broker/dealer questionnaire.
   - Certification of having read and understood and agreeing to comply with the City's investment policy.
   - Evidence of adequate insurance coverage.

   An annual review of the financial condition and registration of qualified financial institutions and broker/dealers will be conducted by the investment officer.

   From time to time, the investment officer may choose to invest in instruments offered by minority and community financial institutions. In such instances, a waiver to the criteria under Paragraph 1 may be granted. All terms and relationships will be fully disclosed prior

to purchase and will be reported to the appropriate entity on a consistent basis and should be consistent with state or local law. These types of investment purchases should be approved by the appropriate legislative or governing body in advance.

2. *Internal Controls*: The investment officer is responsible for establishing and maintaining an internal control structure designed to ensure that the assets of the City are protected from loss, theft or misuse. Specifics for the internal controls shall be documented in an investment procedures manual that shall be reviewed and updated periodically by the Investment Officer. An example of such a procedure would include the requirement that the Director of Finance approve trades transacted by other authorized personnel. The internal control structure shall be designed to provide reasonable assurance that these objectives are met. The concept of reasonable assurance recognizes that (1) the cost of a control should not exceed the benefits likely to be derived and (2) the valuation of costs and benefits requires estimates and judgments by management.

   Accordingly, the investment officer shall establish a process for an annual independent review by an external auditor to assure compliance with policies and procedures. The internal controls shall address the following points:

   - Control of collusion.
   - Separation of transaction authority from accounting and recordkeeping.
   - Custodial safekeeping.
   - Avoidance of physical delivery securities.
   - Clear designation of authority to subordinate staff members.
   - Written confirmation of transactions for investments and wire transfers.
   - Dual authorizations of wire transfers.
   - Staff training.
   - Development of a wire transfer agreement with the lead bank and third-party custodian.

3. *Delivery vs. Payment*: All trades where applicable will be executed by delivery vs. payment (DVP) to ensure that securities are deposited in an eligible financial institution prior to the release of funds. Securities will be held by a third-party custodian as evidenced by safekeeping receipts. The only exception to the foregoing shall be depository accounts and securities purchase made with: Local Agency Investment Fund (LAIF), California Asset Management Program, time certificates of deposit and money market mutual funds, since the purchased securities are not deliverable.

4. *Third-Party Safekeeping*: Securities will be held by an independent third-party safekeeping institution selected by the City. All securities will be evidenced by safekeeping receipts in the City's name. The safekeeping institution shall annually provide a copy of its most recent report on internal controls – Service Organization Control Reports (formerly 70, or SAS 70) prepared in accordance with the Statements of Standards for Attestation Engagements (SSAE) No. 16 (effective June 15, 2011).

E. AUTHORIZED INVESTMENTS

1. *Investment Types*: The City is authorized by California Code Section 53600 and 53635 to invest in the types of securities listed below. If additional types of securities are approved for investment by public funds by state statute, they will not be eligible for investment by the City until this Policy has been amended and the amended version adopted by the City Council. Typical types of authorized securities are:

   - U.S. Treasury and other government obligations that carry the full faith and credit guarantee of the United States for the payment of principal and interest;

   - Federal Agency or U.S. Government Sponsored Enterprises (GSE) obligations, participations or other instruments;

   - Banker's Acceptances, issued by domestic or foreign banks, which are eligibile for purchase by the Federal Reserve System. Purchases of Bankers' Acceptances may not exceed 180 days to maturity, or 40% of the City's surplus funds, not may the City invest more than 5% of its surplus funds in the Bankers' Acceptances of any one commercial bank, and must have a rating of at least A-1 or P-1 by S&P or Moody's;

   - Federally insured time deposits (non-negotiable certificates of deposit) in state or federally chartered banks, savings and loans, or credit unions, provided that the amount per institution is limited to the maximum covered under federal insurance;

   - Negotiable certificates of deposit issued by nationally or state-charted bank, a savings association or federal association (as defined by Section 5102 of the Financial Code), a state or federal credit union, or by a federally- or state-licensed branch of a foreign bank. Purchases of Negotiable Certificates of Deposit shall not exceed 30% of the cost value of the portfolio;

   - Commercial paper with a minimum short-term debt rating of A-1, P-1, or F, and a minimum long-term debt rating of A or A2 by a nationally recognized statistical rating organization, and issued by a domestic corporation having assets in excess of $500,000,000. The purchase of commercial paper may not exceed 25% of the City's surplus funds, and is limited to 5% per issuer. The maximum maturity may not exceed 270 days;

   - Medium-Term Notes ("MTN"), issued by corporations organized and operating within the United States or by depository institutions licensed by the United States or any state operating within the United States. Medium-term notes must have a minimum rating of A, or A2 by a nationally recognized statistical rating organization. The purchase of MTNs may not exceed 30% of the portfolio, and is limited to 5% per issuer;

   - Registered Treasury Notes or Bonds of any of the other 49 states in addition to California, including bonds payable solely out of the revenues from a revenue-producing property owned, controlled, or operated by a state, or by a department, board, agency, or authority of any of these states. These securities shall have a minimum rating of A or A2 by S&P or Moody's. Purchases in this category shall not exceed 30%;

   - Fully collateralized Repurchase Agreements collateralized in compliance with this Policy, governed by SIFMA Master Repurchase Agreement and with a maximum

maturity. Capital project funds may be invested in a single flex repurchase agreement with a maximum stated maturity that shall be matched to the expenditure plan;

- SEC registered money market mutual funds. To be eligible for investment pursuant to this subdivision these companies shall either (1) attain the highest ranking letter or numerical rating provided by not less than two of the three largest nationally recognized rating services or (2) have an investment advisor registered or exempt from registration with the Securities and Exchange Commission with not less than five years' experience investing in money market instruments with assets in excess of $500,000,000. No more than 20% of the City's surplus funds may be invested in money market funds, while no more than 10% may be invested in any one money marked; and

- State of California Local Agency Investment Fund (LAIF). Investment of funds in LAIF is limited to the maximum allowed by the LAIF Board of Directors (currently $65 million) per government entity.

2. *Collateralization*: Acceptable collateral for bank deposits and repurchase agreements shall include only:

   - Obligations of the U.S. Government, its agencies and GSEs, including mortgage backed securities.

   - Obligations of any state, city, county or authority rated at least AA (or equivalent) by two nationally recognized statistical rating organizations.

F. INVESTMENT PARAMETERS

1. *Diversification*: The investments shall be diversified by:

   - limiting investments to avoid over-concentration in securities from a specific issuer or business sector (excluding U.S. Treasury securities),

   - limiting investment in securities that have higher credit risks,

   - investing in securities with varying maturities, and

   - continuously investing a portion of the portfolio in readily available funds such as local government investment pools (LGIPs), money market funds or overnight repurchase agreements to ensure that appropriate liquidity is maintained in order to meet ongoing obligations.

2. *Maximum Maturities*: To the extent possible, the City shall attempt to match its investments with anticipated cash flow requirements. Unless matched to a specific cash flow, the City will not directly invest in securities maturing more than five years from the date of purchase or in accordance with state and local statutes and ordinances. The overall weight average maturity shall not exceed 3.0 years.

   Reserve funds and other funds with longer-term investment horizons may be invested in securities exceeding five years if the maturity of such investments are made to coincide as nearly as practicable with the expected use of funds. The intent to invest in securities with longer maturities shall be disclosed in advance in writing to the legislative body (per Government Code Section 53601).

Because of inherent difficulties in accurately forecasting cash flow requirements, a portion of the portfolio should be continuously invested in readily available funds such as LGIPs, money market funds, or overnight repurchase agreements to ensure that appropriate liquidity is maintained to meet ongoing obligations.

G. INVESTMENT STRATEGIES:

The investment program shall seek to augment returns consistent with the intent of this policy, identified risk limitations and prudent investment principles. These objectives will be achieved by use of the following strategies:

The source of and purpose for surplus funds determines the goal to be accomplished by the strategy. Bond proceeds, for example, typically have three purposes*: reserve funds*, *project funds*, and *debt service funds*. The goal and corresponding investment strategy is different for each, as follows:

- Reserve Funds. The goal is to have these funds available in case of default while minimizing negative arbitrage. The strategy is to call for competitive bids for long-term *investment agreements* with a call provision for default or redemption from AAA-rated limitations.

- Project Funds. The goal is to maintain purchasing power. The strategy, assuming a normal yield curve, is to match *investment agreement* maturities with construction draw schedules. Funds needed within 12 months are currently placed in an AAA-rated mutual fund of U.S. Treasury Securities.

- Debt Service Funds. The goal is to maintain liquidity. Generally, funds are placed in a money market mutual fund related investment category.

Surplus funds in the City pool come from a mixed bag of services for a wide variety of purposes. This blend of funds leads to the general investment goal of earning an appropriate yield consistent with the City's high standards for safety and liquidity.

H. INVESTMENT PERFORMANCE

In a normal financial market, the yield curve shows greater rewards for longer terms.

Since the shortest-term investment strategy (for surplus funds in the City pool) is to play the market against LAIF, the minimum acceptable return ought to be greater than LAIF. In addition, the quarter to date LAIF apportionment rate, the three-month U.S. Treasury Bill and the two-year U.S. Treasury Note shall also be considered benchmarks of the City's portfolio performance.

Accordingly, LAIF and U.S. Treasuries will be the benchmark for evaluating performance.

I. REPORTING

1. *Methods*: The investment officer shall prepare an investment report at least quarterly, including a management summary that provides an analysis of the status of the current investment portfolio and the individual transactions executed over the last quarter. This report shall include the type of investment, issuer, date of maturity, and par and dollar amount invested on all securities, investments and moneys held by the City. The report shall also include a current market value as of the date of the report. It shall state compliance of the portfolio to the statement of investment policy or manner in which the

portfolio id not in compliance. Finally, the report shall include a statement denoting the ability of the local agency to meet its pool's expenditure requirements for the next six months or provide an explanation as to why sufficient money shall, or may, not be available. Once a year, the statement of investment policy shall be included in the report.

2. *Performance Standards*: The investment portfolio will be managed in accordance with the parameters specified within this policy. The portfolio should obtain a market average rate of return during a market/economic environment of stable interest rates. A series of appropriate benchmarks shall be established against which portfolio performance shall be compared on a regular basis.

3. *Marking to Market*: The market value of the portfolio shall be calculated at least quarterly and a statement of the market value of the portfolio shall be issued at least quarterly. This will ensure that review of the investment portfolio, in terms of value and price volatility, has been performed consistent with the GFOA Recommended Practice on "Mark-to-Market Practices for State and Local Government Investment Portfolios and Investment Pools." In defining market value, considerations should be given to the GASB Statement 31 pronouncement.

4. *Annual Reports*: The investment policy shall be reviewed at least annually within 120 days of the end of the fiscal year to ensure its consistency with the overall objectives of preservation of principal, liquidity and return, and its relevance to current law and financial and economic trends. A comprehensive annual report shall be presented in conjunction with the investment policy review. This report shall include comparisons of City's return to the City's benchmark return, shall suggest policies and improvements that might enhance the investment program, and shall include an investment plan for the coming year.

5. *Annual Audit*: Management shall establish an annual process of independent review by the external auditor to assure compliance with internal controls. Such audit will include tests deemed appropriate by the auditor.

J. POLICY CONSIDERATIONS

1. *Exemption:* Any investment currently held as of October 15, 2014 that does not meet the guidelines of this policy shall be exempted from the requirements of this policy. At maturity or liquidation, such monies shall be reinvested only as provided by this policy.

2. *Amendments*: This policy shall be reviewed on an annual basis. Any changes must be approved by the investment officer and City Council, as well as the individuals charged with maintaining internal controls.

K. GLOSSARY OF TERMS

US Treasury Obligation: Direct obligations of the United States Treasury whose payment is guaranteed by the United States.

GSE Obligations: US Government Agencies, Government Sponsored Enterprises (GSEs), Corporations or Instrumentalities of the US Government – Federal Instrumentality Securities include, but are not limited to Federal Nation Mortgage Association, the Federal Home Loan Mortgage Corporation, Federal Home Loan Banks, and the Federal Farm Credit Bureau.

TLGP Debt Obligations: Senior unsecured debt obligations guaranteed by the Federal Deposit Insurance Corporation under the Temporary Liquidity Guarantee Program (TLGP) or

other federal government guaranteed programs; Eligible debt obligations must carry the full faith and credit of the United States Government (if allowable).

Commercial Paper: Commercial Paper that is rated A1/P1 and has long term bonds which have a minimum rating of AA- by Standard and Poor's and Aa3 by Moody's.

Banker's Acceptances: Banker's acceptances, if the banker's acceptances are (i) guaranteed by, and carried on the books of, a qualified financial institution; (ii) eligible for discount by the Federal Reserve System; and (iii) issued by a qualified financial institution whose short-term letter of credit rating is rated in the highest category by one or more nationally recognized statistical rating organizations.

Repurchase Agreements: An agreement with an approved broker/dealer that provides for sell and simultaneous purchase of an allowable collateral security. The difference in the sales and purchase price is the earning rate on the agreement. A master repurchase agreement must be in place with the approved broker dealer.

Corporate Indebtedness: Corporate Indebtedness that has a minimum long-term debt rating of AA- rated by Standard and Poor's and a Aa3 rating by Moody's and must be rated on the settlement date P-1 or Aa or better by Moody's Investors Service or A-1 or AA or better by Standard and Poor's Corporation or equivalent rating by any nationally recognized statistical rating organization.

Local Government Investment Pool: State treasurer's local short-term investment fund up to the statutory limit per state statute.

Certificates of Deposit/Bank Deposit/Savings Accounts: Time deposits open accounts, certificates of deposit and savings accounts in insured institutions as defined in state statute, in credit unions as defined in state statute, or in federal credit unions, if the institution or credit union maintains a head office or branch in the State of California.

Municipal Obligations: Lawfully issued debt obligations of the agencies and instrumentalities of the State of California and its political subdivisions that have a long-term rating of "A" or an equivalent rating or better or are rated on the settlement date in the highest category for short-term municipal debt by a nationally recognized statistical rating organization.

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 4

SUBJECT:

CONSIDERATION OF LEASE BETWEEN THE CITY OF WEST SACRAMENTO AND XEROX FINANCIAL SERVICES FOR COPIER PROGRAM WITH ENCOMPASS

INITIATED OR REQUESTED BY:

[ ] **Council** [X] **Staff**

[ ] **Other**

REPORT COORDINATED OR PREPARED BY:
Robert Miller, Information Technology Manager

Roberta Raper, Administrative Services Director

ATTACHMENT [X] **Yes** [ ] **No** [ ] **Information** [ ] **Direction** [X] **Action**

OBJECTIVE
The objective of this agenda item is to obtain City Council approval to enter a City-wide lease for copier equipment and services from Encompass.

RECOMMENDED ACTION
Staff respectfully recommends that the City Council approve the lease between the City of West Sacramento and Xerox Financial Services for a copier program with Encompass.

BACKGROUND
The City's existing Ricoh copiers are over 8 years old and are at end of their useful life. These copiers are experiencing a high rate of failure and repairs are taking longer to complete due to the lack of availability of parts. This increased downtime, coupled with the high cost per copy, has highlighted the need to replace the existing copiers.

ANALYSIS
The City is currently spending over $75,000 a year on copies alone with our Ricoh contract. Since 2011, copy expenses with Ricoh have risen on average 19% per year. This is primarily due to an increase in cost per copy and higher copier usage. Copier usage has increased as more and more people are using the networked copiers for their printing needs rather than the more expensive network printers. On average, color usage has increased 25.1% annually since 2011. Black and white usage has only increased an average of 1.8% annually since 2011. Moving to Encompass would reduce our copy cost to an estimated $39,423 for the first year, down from the current annual cost of $75,000, a nearly 50% savings.

The annual lease expense is $46,477 and includes the replacement of all 24 existing City copiers and associated software. Based on feedback from City departments, the I.T. Division plans to replace the work room and high use copiers with 55 pages per minute (PPM) devices (upgraded from 40 PPM). The remaining copiers will be upgraded from 40 PPM to 45 PPM to improve efficiency of all machines. In addition to the speed increase, all devices are color ready, can hold up to three different sizes of paper at one time, and have a finishing unit for stapling and hole punch needs. Previously, only select high-use copiers contained some of these features.

The current "FollowMe" printing solution will be replaced with "PaperCut MF". In addition to providing the same secure print anywhere ability as FollowMe, PaperCut MF also introduces printing policies. Printing policies allow print jobs to be automatically analyzed prior to actually being printed, providing intelligent printing recommendations to staff on how they can modify their print job settings to reduce cost on every print job.

Additionally, facsimile processing will be migrated to Xmedius Fax. Xmedius Fax is a cloud-based fax solution that will allow our end users to send and receive faxes from any device (i.e. copier, desktop, mobile, etc.) rather than having to maintain separate fax-only machines. This allows I.T. to replace aging fax hardware and save on communication costs, while providing greater flexibility to staff.

After soliciting prices and proposals from several vendors, I.T. recommends Encompass as the vendor of choice. Encompass received the highest marks for customer service by all agencies contacted, and they were the only vendor who had a constant price per copy for the life of the contract. This recommendation allows the City to utilize one vendor for all of the City's copier and printing needs, as Encompass currently supports and maintains the City's fleet of printers and has done so satisfactorily for the past two years.

Environmental Considerations
Not applicable

Commission Recommendation
Not applicable

Strategic Plan Integration
This item is consistent with the City's mission to provide quality municipal services.

Alternatives
As an alternative to approving the proposed contract with Encompass, the City Council may decide to:

1. Reject the proposed actions or direct staff to alter the proposal;
2. Defer or cancel the proposal.

Coordination and Review
The copier contract was prepared and reviewed by the Information Technology and Finance Divisions of the Administrative Services Department. All other City departments have been involved in the discussion of needs and benefits of replacing the existing copier systems.

Budget/Cost Impact
Information Technology has an approved budget of $93,866 for copies and associated expenses City-wide in the FY 2018/19 budget. Annual copy prices vary dramatically based on usage and City initiatives. As shown in the attached solicitation summary, all new vendor options include both lease costs and copy costs and become less expensive than our current Ricoh contract, which includes copy costs only, by year three of the lease, and the City will experience significant savings from the existing vendor in years four and five.

ATTACHMENTS
1. Copier Contract Comparison
2. Master Lease/Purchase Agreement

Copy Services Solicitation Summary

Fiscal Year	Caltronics	Encompass	Inland	Ricoh
2018/2019	$ 86,437.52	$ 85,900.96	$ 82,575.85	$ 74,983.78
2019/2020	$ 95,222.38	$ 94,640.76	$ 90,785.71	$ 89,452.10
2020/2021	$ 106,181.31	$ 105,553.81	$ 101,024.90	$ 112,566.48
2021/2022	$ 123,626.71	$ 119,185.05	$ 117,413.81	$ 136,221.03
2022/2023	$ 146,411.73	$ 136,216.16	$ 138,785.36	$ 166,786.82
Totals	**$ 557,879.65**	**$ 541,496.73**	**$ 530,585.64**	**$ 580,010.21**

Historical Copy Usage

Color Usage				
Fiscal Year	**Copies**	**Prints**	**Totals**	**% +/-**
2010/2011	53105	116446	169551	
2011/2012	70551	161550	232101	36.9%
2012/2013	88346	204206	292552	26.0%
2013/2014	145355	283143	428498	46.5%
2014/2015	160674	424959	585633	36.7%
2015/2016	177873	362076	539949	-7.8%
2016/2017	177322	481300	658622	22.0%
2017/2018	211540	548134	759674	15.3%
Totals	**1084766**	**2581814**	**3666580**	
			Average	**25.1%**

Black Usage				
Fiscal Year	**Copies**	**Prints**	**Totals**	**% +/-**
2010/2011	628921	153252	782173	
2011/2012	549302	211704	761006	-2.7%
2012/2013	558976	181925	740901	-2.6%
2013/2014	606710	204070	810780	9.4%
2014/2015	584634	96126	680760	-16.0%
2015/2016	619500	122719	742219	9.0%
2016/2017	547644	154396	702040	-5.4%
2017/2018	673512	173415	846927	20.6%
Totals	**4769199**	**1297607**	**5284633**	
			Average	**1.8%**

Caltronics

Lease Expense					
Description	Monthly Lease Amount/Device	Quantity	Total Monthly Lease Amount	Annual Lease Amount	Contract Lease Amount
Konica Minolta Bizhub C558	$ 150.69	11	$ 1,657.59	$ 19,891.08	$ 99,455.40
Konica Minolta Bizhub C458	$ 134.00	9	$ 1,206.00	$ 14,472.00	$ 72,360.00
Konica Minolta Bizhub C3851	$ 45.71	4	$ 182.84	$ 2,194.08	$ 10,970.40
PaperCut MF	$ 529.16	1	$ 529.16	$ 6,349.92	$ 31,749.60
Xmedius Fax	$ 133.62	1	$ 133.62	$ 1,603.44	$ 8,017.20
		Total	**$ 3,709.21**	**$ 44,510.52**	**$ 222,552.60**

Copy Expense								
Description	Color			Black			Total Montly Cost	Annual Cost
	Monthly Pages	Rate/Page	Montly Cost	Monthly Pages	Rate/Page	Montly Cost		
Copies/Prints (Year 1)	54705	$ 0.05250	$ 2,872.01	78722	$ 0.00790	$ 621.90	$ 3,493.92	$ 41,927.00
Copies/Prints (Year 2)	68436	$ 0.05250	$ 3,592.89	80139	$ 0.00790	$ 633.10	$ 4,225.99	$ 50,711.86
Copies/Prints (Year 3)	85614	$ 0.05250	$ 4,494.74	81582	$ 0.00790	$ 644.50	$ 5,139.23	$ 61,670.79
Copies/Prints (Year 4)	107104	$ 0.05513	$ 5,904.11	83051	$ 0.00830	$ 688.91	$ 6,593.02	$ 79,116.19
Copies/Prints (Year 5)	133988	$ 0.05788	$ 7,755.39	84546	$ 0.00871	$ 736.37	$ 8,491.77	$ 101,901.21
							Total	**$ 335,327.05**

Encompass

Lease Expense					
Description	Monthly Lease Amount/Device	Quantity	Total Monthly Lease Amount	Annual Lease Amount	Contract Lease Amount
Xerox C8055	$ 166.55	11	$ 1,832.05	$ 21,984.60	$ 109,923.00
Xerox C8045	$ 150.54	9	$ 1,354.86	$ 16,258.32	$ 81,291.60
Xerox C405	$ 14.23	4	$ 56.92	$ 683.04	$ 3,415.20
PaperCut MF	$ 485.18	1	$ 485.18	$ 5,822.16	$ 29,110.80
Xmedius Fax	$ 144.10	1	$ 144.10	$ 1,729.20	$ 8,646.00
		Total	**$ 3,873.11**	**$ 46,477.32**	**$ 232,386.60**

Copy Expense								
Description	Color			Black			Total Montly Cost	Annual Cost
	Monthly Pages	Rate/Page	Montly Cost	Monthly Pages	Rate/Page	Montly Cost		
Copies/Prints (Year 1)	54705	$ 0.05250	$ 2,872.01	78722	$ 0.00525	$ 413.29	$ 3,285.30	$ 39,423.64
Copies/Prints (Year 2)	68436	$ 0.05250	$ 3,592.89	80139	$ 0.00525	$ 420.73	$ 4,013.62	$ 48,163.44
Copies/Prints (Year 3)	85614	$ 0.05250	$ 4,494.74	81582	$ 0.00525	$ 428.31	$ 4,923.04	$ 59,076.49
Copies/Prints (Year 4)	107104	$ 0.05250	$ 5,622.96	83051	$ 0.00525	$ 436.02	$ 6,058.98	$ 72,707.73
Copies/Prints (Year 5)	133988	$ 0.05250	$ 7,034.37	84546	$ 0.00525	$ 443.87	$ 7,478.24	$ 89,738.84
							Total	**$ 309,110.13**

Inland Business Systems

Lease Expense					
Description	Monthly Lease Amount/Device	Quantity	Total Monthly Lease Amount	Annual Lease Amount	Contract Lease Amount
Xerox C8055	$ 173.65	11	$ 1,910.15	$ 22,921.80	$ 114,609.00
Xerox C8045	$ 129.13	9	$ 1,162.17	$ 13,946.04	$ 69,730.20
Xerox C405	$ 18.05	4	$ 72.20	$ 866.40	$ 4,332.00
PaperCut MF	$ 372.36	1	$ 372.36	$ 4,468.32	$ 22,341.60
Xmedius Fax	$ 166.82	1	$ 166.82	$ 2,001.84	$ 10,009.20
		Total	**$ 3,683.70**	**$ 44,204.40**	**$ 221,022.00**

Copy Expense									
Description	Color			Black			Total Montly Cost	Monthly Discount	Annual Cost
	Monthly Pages	Rate/Page	Montly Cost	Monthly Pages	Rate/Page	Montly Cost			
Copies/Prints (Year 1)	54705	$ 0.04900	$ 2,680.55	78722	$ 0.00800	$ 629.78	$ 3,310.32	$ (112.70)	$ 38,371.45
Copies/Prints (Year 2)	68436	$ 0.04900	$ 3,353.36	80139	$ 0.00800	$ 641.11	$ 3,994.48	$ (112.70)	$ 46,581.31
Copies/Prints (Year 3)	85614	$ 0.04900	$ 4,195.09	81582	$ 0.00800	$ 652.66	$ 4,847.74	$ (112.70)	$ 56,820.50
Copies/Prints (Year 4)	107104	$ 0.05150	$ 5,515.86	83051	$ 0.00840	$ 697.63	$ 6,213.48	$ (112.70)	$ 73,209.41
Copies/Prints (Year 5)	133988	$ 0.05410	$ 7,248.75	84546	$ 0.00882	$ 745.70	$ 7,994.45	$ (112.70)	$ 94,580.96
								Total	**$ 309,563.64**

Ricoh

Copy Expense									
Description	Color			Black			Total Montly Cost	Other Expenses	Annual Cost
	Monthly Pages	Rate/Page	Montly Cost	Monthly Pages	Rate/Page	Montly Cost			
Copies/Prints (Year 1)	54705	$ 0.08650	$ 4,731.82	78722	$ 0.01271	$ 1,000.16	$ 5,731.98	$ 6,200.00	$ 74,983.78
Copies/Prints (Year 2)	68436	$ 0.08650	$ 5,919.51	80139	$ 0.01271	$ 1,018.17	$ 6,937.67	$ 6,200.00	$ 89,452.10
Copies/Prints (Year 3)	85614	$ 0.09082	$ 7,775.55	81582	$ 0.01334	$ 1,088.32	$ 8,863.87	$ 6,200.00	$ 112,566.48
Copies/Prints (Year 4)	107104	$ 0.09082	$ 9,727.19	83051	$ 0.01334	$ 1,107.90	$ 10,835.09	$ 6,200.00	$ 136,221.03
Copies/Prints (Year 5)	133988	$ 0.09104	$ 12,198.00	84546	$ 0.01401	$ 1,184.24	$ 13,382.24	$ 6,200.00	$ 166,786.82
							Total		**$ 580,010.21**

Xerox Financial Services LLC
45 Glover Avenue
Norwalk, CT 06856

Lease Agreement

xerox

Supplier Name-Address:			
Owner: **XEROX FINANCIAL SERVICES LLC** - 45 Glover Avenue, Norwalk, CT 06856			Agreement Number:
CUSTOMER INFORMATION			
Full Legal Name: City of West Sacramento			Phone: (916) 617-4517
Billing Address: 1110 West Capitol Ave			Contact Name: Robert Miller
City: West Sacramento	State: Ca	Zip Code: 95961	Contact Email: rmiller@cityofwestsacramento.org

EQUIPMENT

Quantity	Model and Description	Quantity	Model and Description
11	Xerox C8055/HXF2 (includes bundled Xmedius & Papercut software)	25	PRX-80581 HID card readers
9	Xerox C8045/HXF2 (includes bundled Xmedius & Papercut software)		
4	Xerox C405/DNM (includes bundled Xmedius & Papercut software)		

Equipment Location (if different from Billing Address):

TERM	LEASE PAYMENT - (Monthly frequency unless otherwise noted)	PURCHASE OPTION - ('FMV' unless otherwise noted)
Initial Term: (in months) 60	Lease Payment (plus applicable taxes): $ 3,873.11 Frequency: [X] Monthly [ ] Quarterly [ ] Annually	[X] Fair Market Value Purchase Option, ("FMV") [ ] $1 Purchase Option

CUSTOMER ACCEPTANCE

BY YOUR SIGNATURE BELOW, YOU ACKNOWLEDGE THAT YOU ARE ENTERING INTO A NON-CANCELLABLE AGREEMENT AND THAT YOU HAVE READ AND AGREED TO ALL APPLICABLE TERMS AND CONDITIONS SET FORTH ON PAGES 1 AND 2 HEREOF.

Authorized Signer X:	Date:	**Federal Tax ID # (Required):**
Print Name:	Title:	

OWNER ACCEPTANCE

Accepted By: Xerox Financial Services LLC	Name and Title:	Date:

TERMS & CONDITIONS

1. Definitions. The words "you" and "your" mean the legal entity identified in "Customer Information" above, and "XFS," "we," "us", "Owner" and "our" mean Xerox Financial Services LLC. "Party" means you or XFS, and "Parties" means both you and XFS. "Supplier" means the entity identified as "Supplier" above. "Acceptance Date" means the date (a) Supplier determines Equipment installed by Supplier is operating satisfactorily and is available for your use, or (b) Equipment identified by Supplier as being installable by you is delivered to your premises. "Agreement" means this Lease Agreement, including any attached Equipment schedule. "Commencement Date" will be a date after the Acceptance Date, as set forth in our first invoice, for the purpose of facilitating an orderly transition and to provide a uniform billing cycle. "Discount Rate" means 3% per annum. "Equipment" means the items identified in "Equipment" above and in any attached Equipment schedule, plus any Software, attachments, accessories, replacement parts, substitutions, additions and repairs thereto. "Interim Period" means the period, if any, between the Acceptance Date and the Commencement Date. "Interim Payment" means one thirtieth of the Lease Payment multiplied by the number of days in the Interim Period. "Payment" means the Lease Payment specified above and other charges you, Supplier and XFS agree will be invoiced by XFS on a monthly basis, plus Taxes. "Maintenance Agreement" means a separate agreement between you and Supplier for maintenance and support purposes. "Origination Fee" means a one-time fee of $125 billed on your first invoice which you agree to pay, covering the origination, documentation, processing and certain other initial costs for the Agreement. "Term" means the Interim Period, if any, together with the Initial Term plus any subsequent renewal or extension terms. "UCC" means the Uniform Commercial Code of the State of Connecticut.

2. Agreement, Payments and Late Payments. You agree and represent all Equipment was selected, configured and negotiated by you based upon your own judgment and has been, or is being, supplied by Supplier. At your request, XFS has acquired, or will acquire, the same to lease to you under this Agreement and you agree to lease the same from XFS. The Initial Term, which is indicated above, commences on the Commencement Date. You agree to pay XFS the first Payment plus any applicable Interim Payment no later than 30 days after the Commencement Date; each subsequent Payment shall be payable on the same date of each month thereafter, whether or not XFS invoices you. **If any Payment is not paid in full within 5 days after its due date, you will pay a late charge of the greater of 10% of the amount due or $25, not to exceed the maximum amount permitted by law.** For each dishonored or returned Payment, you will be assessed the applicable returned item fee, which shall not exceed $35. Restrictive covenants on any method of payment will be ineffective.

3. Equipment and Software. To the extent that the Equipment includes intangible property or associated services such as software licenses, such intangible property shall be referred to as "Software." You acknowledge and agree that XFS has no right, title or interest in the Software and you will comply throughout the Term with any license and/or other agreement ("Software License") with the supplier of the Software ("Software Supplier"). You are responsible for determining whether any Software Licenses are required, and entering into them with the Software Supplier(s) no later than the Acceptance Date. You agree the Equipment is for your lawful business use in the United States (including its possessions and territories), will not be used for personal, household or family purposes, and is not being acquired for resale. You will not attach the Equipment as a fixture to real estate or make any permanent alterations to it.

4. Non-Cancellable Agreement. THIS AGREEMENT CANNOT BE CANCELLED OR TERMINATED BY YOU PRIOR TO THE END OF THE INITIAL TERM. YOUR OBLIGATION TO MAKE ALL PAYMENTS IS ABSOLUTE AND UNCONDITIONAL AND NOT SUBJECT TO DELAY, REDUCTION, SET-OFF, DEFENSE, COUNTERCLAIM OR RECOUPMENT FOR ANY REASON WHATSOEVER, IRRESPECTIVE OF THE PERFORMANCE OF THE EQUIPMENT, SUPPLIER, ANY THIRD PARTY OR XFS. Any pursued claim by you against XFS for alleged breach of our obligations hereunder shall be asserted solely in a separate action; provided, however, that your obligations under this Agreement shall continue unabated.

5. End of Agreement Options. If a $1 Purchase Option is applicable, you will be deemed to have exercised your option to purchase the Equipment as of the Acceptance Date. If an FMV purchase option is designated on the first page hereof, if you are not in default and if you provide no greater than 150 days and no less than 60 days' prior written notice to XFS, you may, at the end of the Initial Term or any renewal term, either (a) purchase all, but not less than all, of the Equipment "AS IS, WHERE IS" and "WITH ALL FAULTS" by paying its fair market value, as determined by XFS in its sole but reasonable discretion, ("Determined FMV"), plus Taxes, or (b) de-install and return the Equipment within 30 days of the end of the then applicable term, at your expense, fully insured, to a continental US location XFS specifies. If you have not elected one of the above options, this Agreement shall renew for successive 3-month terms. Either party may terminate the Agreement at the end of any 3-month renewal term on 30 days' prior written notice and by taking one of the actions identified in (a) or (b) in the preceding sentence of this section. Any FMV purchase option shall be exercised with respect to each item of Equipment on the day immediately following the date of expiration of the Term of such item, and by the delivery at such time by you to XFS of payment, in form acceptable to XFS, of the amount of the applicable purchase price thereof. Upon payment of the applicable amount, XFS shall transfer our interest in the Equipment to you on an "AS IS, WHERE IS," "WITH ALL FAULTS" basis, without representation or warranty of any kind or nature whatsoever.

6. Equipment Delivery and Maintenance. Equipment will be delivered to you by Supplier at the location(s) specified herein, and you agree to execute a Delivery & Acceptance Certificate at XFS's request (and confirm same via telephone and/or electronically) confirming that you have received, inspected and accepted the Equipment, and that XFS is authorized to fund the Supplier for the Equipment. If you fail to accept the Equipment, this Agreement shall terminate; however, you assume all responsibility for any Equipment purchase order or other contract issued on your behalf directly with Supplier. Equipment may not be moved to another location without first obtaining XFS's written consent, which shall not be unreasonably withheld. You shall permit XFS or its agent to inspect Equipment and any maintenance records relating thereto during your normal business hours upon reasonable notice. You represent you have entered into a Maintenance Agreement with Supplier to maintain the Equipment in good working order in accordance with the manufacturer's maintenance guidelines and to provide you with Equipment supplies. **You acknowledge that XFS is acting solely as an administrator for Supplier with respect to the billing and collecting of the charges under the Maintenance Agreement (if applicable).** IN NO EVENT WILL XFS BE LIABLE TO YOU FOR ANY BREACH BY SUPPLIER OF ANY OF ITS OBLIGATIONS TO YOU, NOR WILL ANY OF YOUR OBLIGATIONS UNDER THIS AGREEMENT BE MODIFIED, RELEASED OR EXCUSED BY ANY ALLEGED BREACH BY SUPPLIER.

7. Equipment Ownership, Labeling and UCC Filing. If and to the extent a court deems this Agreement to be a security agreement under the UCC, and otherwise for precautionary purposes only, you grant XFS a first priority security interest in your interest in the Equipment and all proceeds thereof in order to secure your performance under this Agreement. Unless a $1 Purchase Option is applicable, XFS is and shall remain the sole owner of the Equipment, except the Software. XFS may label the Equipment to identify our ownership interest in it. You authorize XFS to file by any permissible means a UCC financing statement to show, and to do all other acts to protect, our interest in the Equipment. You agree to pay any filing fees and administrative costs for the filing of such financing statements. You agree to keep the Equipment free from any liens or encumbrances and to promptly notify XFS if there is any change in your organization such that a refiling or amendment to XFS's UCC financing statement against you becomes necessary.

8. Equipment Return. If the Equipment is returned to XFS, it shall be in the same condition as when delivered to you, except for ordinary wear and tear and, if not in such condition, you will be liable for all expenses XFS incurs to return the Equipment to such "ordinary wear and tear" condition. **IT IS SOLELY YOUR RESPONSIBILITY TO SECURE ANY SENSITIVE DATA AND PERMANENTLY DELETE SUCH DATA FROM THE INTERNAL MEDIA STORAGE PRIOR TO RETURNING THE EQUIPMENT TO XFS. YOU SHALL HOLD XFS HARMLESS FROM YOUR FAILURE TO SECURE AND PERMANENTLY DELETE ALL SUCH CUSTOMER DATA AS OUTLINED IN THIS SECTION.**

9. Assignment. YOU MAY NOT ASSIGN, SELL, PLEDGE, TRANSFER, SUBLEASE OR PART WITH POSSESSION OF THE EQUIPMENT, THIS AGREEMENT OR ANY OF YOUR RIGHTS OR OBLIGATIONS UNDER THIS AGREEMENT (COLLECTIVELY "ASSIGNMENT") WITHOUT XFS'S PRIOR WRITTEN CONSENT, WHICH SHALL NOT BE UNREASONABLY WITHHELD, BUT SUBJECT TO THE SOLE EXERCISE OF XFS'S REASONABLE CREDIT DISCRETION AND EXECUTION OF ANY NECESSARY ASSIGNMENT DOCUMENTATION. If XFS agrees to an Assignment, you agree to pay the applicable assignment fee and reimburse XFS for any costs we incur in connection with that Assignment. XFS may sell, assign or transfer all or any part of the Equipment, this Agreement and/or any of our rights (but none of our obligations except for invoicing and tax administration) under this Agreement. XFS's assignee will have the same rights that we have to the extent assigned, and YOU AGREE NOT TO ASSERT AGAINST SUCH ASSIGNEE ANY CLAIMS, DEFENSES, COUNTERCLAIMS, RECOUPMENTS, OR SET-OFFS THAT YOU MAY HAVE AGAINST XFS, and you agree to remit Payments to such Assignee if so designated. XFS agrees and acknowledges that any Assignment by us will not materially change your obligations under this Agreement.

10. Taxes. You will be responsible for, indemnify and hold XFS harmless from, all applicable taxes, fees or charges (including sales, use, personal property and transfer taxes (other than net income taxes), plus interest and penalties) assessed by any governmental entity on the Equipment, this Agreement, or the amounts payable hereunder (collectively, "Taxes"), which will be included in XFS's invoices to you unless you timely provide continuing proof of your tax exempt status. For jurisdictions where certain taxes are calculated and paid at the time of agreement initiation, you authorize XFS to finance and adjust your Lease Payment to include such Taxes over the Initial Term. Unless and until XFS notifies you in writing to the contrary, the following shall apply to personal property taxes and returns. If an FMV purchase option is applicable, XFS will file all personal property tax returns covering the Equipment, pay the personal property taxes levied or assessed thereon, and collect from your account all personal property taxes on the Equipment. If a $1 purchase option is applicable you will file all personal property tax returns covering the Equipment, pay the personal property taxes levied or assessed thereon, and provide us proof thereof upon our request. XFS MAKES NO WARRANTY, EXPRESS OR IMPLIED, REGARDING THE TAX OR ACCOUNTING TREATMENT OF THIS AGREEMENT.

11. Equipment Warranty Information and Disclaimers. XFS HAS NO INVOLVEMENT IN THE SALE, MANUFACTURE, CONFIGURATION, DELIVERY, INSTALLATION, USE OR MAINTENANCE OF THE EQUIPMENT. XFS DISCLAIMS, AND YOU WAIVE SOLELY AGAINST XFS, ALL EQUIPMENT WARRANTIES, WHETHER EXPRESS OR IMPLIED, INCLUDING, BUT NOT LIMITED TO, THE IMPLIED WARRANTIES OF MERCHANTABILITY, NON-INFRINGEMENT AND FITNESS FOR PARTICULAR PURPOSE, AND XFS MAKES NO REPRESENTATIONS OF ANY KIND OR TYPE, INCLUDING, BUT NOT LIMITED TO, THE EQUIPMENT'S SUITABILITY, FUNCTIONALITY, DURABILITY, OR CONDITION. Since you have selected the Equipment and Supplier, you acknowledge that you are aware of the name of the manufacturer of each item of Equipment, Supplier's contact information, and agree that you will contact manufacturer and/or Supplier for a description of any warranty rights you may have under the Equipment supply contract, sales order, or otherwise. Provided you are not in default hereunder, XFS hereby assigns to you any warranty rights we may have against Supplier or manufacturer with respect to the Equipment. If the Equipment is returned to XFS, such rights are deemed reassigned by you to XFS. **IF THE EQUIPMENT IS NOT PROPERLY INSTALLED, DOES NOT OPERATE AS WARRANTED, BECOMES OBSOLETE, OR IS UNSATISFACTORY FOR ANY REASON WHATSOEVER, YOU SHALL MAKE ALL RELATED CLAIMS SOLELY AGAINST MANUFACTURER OR SUPPLIER AND NOT AGAINST XFS, AND YOU SHALL NEVERTHELESS CONTINUE TO PAY ALL PAYMENTS AND OTHER SUMS PAYABLE UNDER THIS AGREEMENT.**

12. Liability and Indemnification. XFS IS NOT RESPONSIBLE FOR ANY LOSSES, DAMAGES, EXPENSES OR INJURIES OF ANY KIND OR TYPE, INCLUDING, BUT NOT LIMITED TO, ANY SPECIAL, INDIRECT, INCIDENTAL, CONSEQUENTIAL OR PUNITIVE DAMAGES (COLLECTIVELY, "CLAIMS"), TO YOU OR ANY THIRD PARTY CAUSED BY THE EQUIPMENT OR ITS USE, EXCEPT THOSE CLAIMS ARISING DIRECTLY AND PROXIMATELY FROM XFS'S GROSS NEGLIGENCE OR WILLFUL MISCONDUCT. In addition, except for Claims arising directly and proximately from XFS's gross negligence or willful misconduct, you assume the risk of liability for, and hereby agree to indemnify and hold safe and harmless, and covenant to defend, XFS, its employees, officers and agents from and against: (a) any and all Claims (including legal expenses of every kind and nature) arising out of the manufacture, purchase, shipment and delivery of the Equipment to you, acceptance or rejection, ownership, leasing, possession, operation, use, return or other disposition of the Equipment, including, without limitation, any liabilities that may arise from patent or latent defects in the Equipment (whether or not discoverable by you), any claims based on absolute/strict tort liability or warranty and any claims based on patent, trademark or copyright infringement; and (b) any and all loss or damage of or to the Equipment.

13. Default and Remedies. You will be in default under this Agreement if XFS does not receive any Payment within 10 days after its due date, or you breach any other obligation under this Agreement or any other agreement with XFS. If you default, and such default continues for 10 days after XFS provides notice to you, XFS may, in addition to other remedies (including disabling or repossessing the Equipment and/or requesting Supplier to cease performing under the Maintenance Agreement), require you to promptly return the Equipment as provided in Sections 5 and 6 hereof, and require immediate payment, as liquidated damages for loss of bargain and not as a penalty, of the sum of: (a) all amounts then past due, plus interest from the due date until paid at the rate of 1.5% per month; (b) the Payments remaining in the Term (including the fixed maintenance component thereof, if permitted under the Maintenance Agreement), discounted at the Discount Rate to the date of default, and (c) Taxes. In addition, if you do not return the Equipment as required above, you agree to pay XFS the Determined FMV, discounted at the Discount Rate to the date of default. You agree to pay all reasonable costs, including attorneys' fees and disbursements, incurred by XFS to enforce this Agreement.

14. Risk of Loss and Insurance. You assume and agree to bear the entire risk of loss, theft, destruction or other impairment of the Equipment upon delivery. You, at your own expense, (i) shall keep Equipment insured against loss or damage at a minimum of full replacement value thereof, and (ii) shall carry public liability insurance against bodily injury, including death, and against property damage in the amount of at least $2 million (collectively, "Required Insurance"). All such Equipment loss/damage insurance shall be with lender's loss payable to "XFS, its successors and/or assigns, as their interests may appear," and shall be with companies reasonably acceptable to XFS. In addition, XFS shall be similarly named as an additional insured on all public liability insurance policies. The Required Insurance shall provide for 30 days' prior notice to XFS of cancellation. YOU MUST PROVIDE XFS OR OUR DESIGNEES WITH SATISFACTORY WRITTEN EVIDENCE OF REQUIRED INSURANCE WITHIN 30 DAYS OF THE ACCEPTANCE DATE AND ANY SUBSEQUENT WRITTEN REQUEST BY XFS OR OUR DESIGNEES. **IF YOU DO NOT DO SO, THEN IN LIEU OF OTHER REMEDIES FOR DEFAULT, XFS IN OUR DISCRETION AND AT OUR SOLE OPTION MAY (BUT IS NOT REQUIRED TO) OBTAIN INSURANCE FROM AN INSURER OF XFS'S CHOOSING, WHICH MAY BE AN XFS AFFILIATE, IN SUCH FORMS AND AMOUNTS AS XFS DEEMS REASONABLE TO PROTECT XFS'S INTERESTS (COLLECTIVELY "EQUIPMENT INSURANCE"). EQUIPMENT INSURANCE WILL COVER THE EQUIPMENT AND XFS; IT WILL NOT NAME YOU AS AN INSURED AND MAY NOT COVER ALL OF YOUR INTEREST IN THE EQUIPMENT AND WILL BE SUBJECT TO CANCELLATION AT ANY TIME. YOU AGREE TO PAY XFS PERIODIC CHARGES FOR EQUIPMENT INSURANCE (COLLECTIVELY "INSURANCE CHARGES") THAT INCLUDE: AN INSURANCE PREMIUM THAT MAY BE HIGHER THAN IF YOU MAINTAINED THE REQUIRED INSURANCE SEPARATELY; A FINANCE CHARGE OF UP TO 1.5% PER MONTH ON ANY ADVANCES MADE BY XFS OR OUR AGENTS; AND COMMISSIONS, BILLING AND PROCESSING FEES; ANY OR ALL OF WHICH MAY GENERATE A PROFIT TO XFS OR OUR AGENTS. XFS MAY ADD INSURANCE CHARGES TO EACH PAYMENT. XFS shall discontinue billing or debiting Insurance Charges for Equipment Insurance upon receipt and review of satisfactory evidence of Required Insurance.** You must promptly notify XFS of any loss or damage to Equipment which makes any item of Equipment unfit for continued or repairable use. You hereby irrevocably appoint XFS as your attorney-in-fact to execute and endorse all checks or drafts in your name to collect under any such Required Insurance. Insurance proceeds from Required Insurance or Equipment Insurance received shall be applied, at XFS's option, to (x) restore the Equipment so that it is in the same condition as when delivered to you (normal wear and tear excepted), or (y) if the Equipment is not restorable, to replace it with like-kind condition Equipment from the same manufacturer, or (z) pay to XFS the greater of (i) the total unpaid Payments for the entire Term hereof (discounted to present value at the Discount Rate) plus, if an FMV purchase option is designated on the first page hereof, XFS's residual interest in such Equipment (herein agreed to be 20% of the Equipment's original cost to XFS) plus any other amounts due to XFS hereunder, or (ii) the Determined FMV immediately prior to the loss or damage. **NO LOSS OR DAMAGE TO EQUIPMENT, OR XFS'S RECEIPT OF INSURANCE PROCEEDS, SHALL RELIEVE YOU OF ANY OF YOUR REMAINING OBLIGATIONS UNDER THIS AGREEMENT.** Notwithstanding procurement of Equipment Insurance or Required Insurance, you remain primarily liable for performance under this Section in the event the applicable insurance carrier fails or refuses to pay any claim. **YOU AGREE (I) AT XFS'S SOLE ELECTION TO ARBITRATE ANY DISPUTE WITH XFS, OUR AGENTS OR ASSIGNS REGARDING THE EQUIPMENT INSURANCE AND/OR INSURANCE CHARGES UNDER THE RULES OF THE AMERICAN ARBITRATION ASSOCIATION IN FAIRFIELD COUNTY, CT, (II) THAT IF XFS MAKES THE FOREGOING ELECTION ARBITRATION (NOT A COURT) SHALL BE THE EXCLUSIVE REMEDY FOR SUCH DISPUTES; AND (III) THAT CLASS ARBITRATION IS NOT PERMITTED. This arbitration option does not apply to any other provision of this Agreement.**

15. Finance Lease and Customer Waivers. The parties agree this Agreement is a "finance lease" under UCC Article 2A. You waive, solely against XFS, its successors and assigns, all rights and remedies conferred on a lessee under Article 2A (Sections 508-522) of the UCC, (C.G.S.A. §§42a-2A-724–737), and any rights you now or later may have which require XFS to sell, rent or otherwise use Equipment to reduce our damages including our realization of the remaining value of the Equipment, or which may otherwise limit or modify any of our rights or remedies.

16. Authorization of Signer and Credit Review. You represent that you may lawfully enter into, and perform, this Agreement, that the individual signing this Agreement on your behalf has all necessary authority to do so, and that all financial information you provide accurately represents your financial condition. You agree to furnish financial information that XFS may request now, including your tax identification number, and you authorize XFS to obtain credit reports on you in the future should you default or fail to make prompt payments under this Agreement.

17. Original and Sole Controlling Document. No Modifications Unless in Writing. This Agreement constitutes the entire agreement between the Parties as to the subjects addressed herein, and representations or statements not included herein are not part of this Agreement and are not binding on the Parties. You agree that an executed copy of this Agreement that is signed by your authorized representative and by XFS's authorized representative (an original manual signature or such signature reproduced by means of a reliable electronic form, such as electronic transmission of a facsimile or electronic signature) shall be marked "original" by XFS and shall constitute the only original document for all purposes. All other copies shall be duplicates. To the extent this Agreement constitutes chattel paper (as defined in the UCC), no security interest in this Agreement may be created except by the possession or transfer of the copy marked "original" by XFS. IF A PURCHASE ORDER OR OTHER DOCUMENT IS ISSUED BY YOU, NONE OF ITS TERMS AND CONDITIONS SHALL BE BINDING ON XFS, AS THE TERMS AND CONDITIONS OF THIS AGREEMENT EXCLUSIVELY GOVERN THE TRANSACTION DOCUMENTED HEREIN. **SUPPLIER AND ITS REPRESENTATIVES ARE NOT OUR AGENTS AND ARE NOT AUTHORIZED TO MODIFY OR NEGOTIATE THE TERMS OF THIS AGREEMENT. THIS AGREEMENT MAY NOT BE AMENDED OR SUPPLEMENTED EXCEPT IN A WRITTEN AGREEMENT SIGNED BY AUTHORIZED REPRESENTATIVES OF THE PARTIES AND NO PROVISIONS CAN BE WAIVED EXCEPT IN A WRITING SIGNED BY XFS.** XFS's failure to object to terms contained in any communication from you will not be a waiver or modification of the terms of this Agreement. You authorize XFS to insert or correct missing information on this Agreement, including but not limited to your proper legal name, agreement/numbers, serial numbers and other Equipment information, so long as there is no material impact to your financial obligations.

18. Governing Law, Jurisdiction, Venue and JURY TRIAL WAIVER. THIS AGREEMENT IS GOVERNED BY, AND SHALL BE CONSTRUED IN ACCORDANCE WITH, THE LAWS OF THE STATE OF CONNECTICUT (WITHOUT REGARD TO CONFLICT OF LAW PRINCIPLES THAT WOULD OTHERWISE REQUIRE APPLICATION OF LAWS OF ANOTHER JURISDICTION). THE JURISDICTION AND VENUE OF ANY ACTION TO ENFORCE THIS AGREEMENT, OR OTHERWISE RELATING TO THIS AGREEMENT, SHALL BE IN A FEDERAL OR STATE COURT IN FAIRFIELD COUNTY, CONNECTICUT OR, EXCLUSIVELY AT XFS'S OPTION, IN ANY OTHER FEDERAL OR STATE COURT WHERE THE EQUIPMENT IS LOCATED OR WHERE XFS'S OR YOUR PRINCIPAL PLACES OF BUSINESS ARE LOCATED, AND YOU HEREBY WAIVE ANY RIGHT TO TRANSFER VENUE. **THE PARTIES HEREBY WAIVE ANY RIGHT TO TRIAL BY JURY IN ANY ACTION RELATED TO OR ARISING OUT OF THIS AGREEMENT.**

19. Miscellaneous. Your obligations under the "Taxes" and "Liability" Sections commence upon execution, and survive the expiration or earlier termination, of this Agreement. Notices under this Agreement must be in writing. Notices to you will be sent to the "Billing Address" provided on the first page hereof, and notices to XFS shall be sent to our address provided on the first page hereof. Notices will be deemed given 5 days after mailing by first class mail or 2 days after sending by nationally recognized overnight courier. Invoices are not considered notices and are not governed by the notice terms hereof. You authorize XFS to communicate with you by any electronic means (including cellular phone, email, automatic dialing and recorded messages) using any phone number (including cellular) or electronic address you provide to us. If a court finds any term of this Agreement unenforceable, the remaining terms will remain in effect. The failure by either Party to exercise any right or remedy will not constitute a waiver of such right or remedy. If more than one party has signed this Agreement as Customer, each such party agrees that its liability is joint and several. The following four sentences control over every other part of this Agreement. Both Parties will comply with applicable laws. XFS will not charge or collect any amounts in excess of those allowed by applicable law. Any part of this Agreement that would, but for the last four sentences of this Section, be read under any circumstances to allow for a charge higher than that allowed under any applicable legal limit, is modified by this Section to limit the amounts chargeable under this Agreement to the maximum amount allowed under the legal limit. If, in any circumstances, any amount in excess of that allowed by law is charged or received, any such charge will be deemed limited by the amount legally allowed and any amount received by XFS in excess of that legally allowed will be applied by us to the payment of amounts legally owed under this Agreement or refunded to you.

Cost Per Print Agreement

THIS IS A LEGALLY BINDING NON-CANCELABLE CONTRACT

Agreement No:

encompass
www.encompass-mps.com

Customer – *Use EXACT registered name if a corp., LLC or LP*	Customer's Contact Person:			Contact Person's E-mail:	
City of West Sacramento	Robert Miller			rmiller@cityofwestsacramento.org	
Street	City	State	**County**	**Zip Code**	**Customer's Telephone**
1110 West Capitol Ave	West Sacramento	CA	Yolo	95691	(916)617-4517

In this Cost Per Print Agreement, (the **"Agreement"**), the word **"Customer"** means the Customer named above. **"Company"** means **Encompass**, an authorized Xerox® Managed Print Services channel partner. **"Xerox"** means Xerox Corporation.

1. SERVICES. COMPANY OR XEROX WILL PROVIDE THE SERVICES ("SERVICES") AS DESCRIBED ON ONE OR MORE ATTACHED PRINT SERVICE SCHEDULES(S) ("SCHEDULES") FOR THE DEVICES ON SUCH SCHEDULES ("MANAGED DEVICES"). The Tools (as defined below) will be used to monitor the networked Managed Devices. The Tools provide automatic service and supplies alerts to the Help Desk (e.g. low toner) and are used for automated meter collection, new device discovery, and device data collection for reporting. The Tools periodically scan approved ranges of Customer's network for devices which may be eligible to be added as Managed Devices. Devices identified through this process may be added as Managed Devices and billed at the rates identified in the Schedules. Customer agrees to pay to Company the "Minimum Monthly Charges" set forth on Schedules, any excess print charges, and all other amounts stated herein.

2. TERM AND TERMINATION. The term of this Agreement will begin on the date Company accepts and signs the Agreement and will continue for as long as devices are covered by a Schedule to this Agreement ("Term"). Each Schedule has its own term ("Schedule Term") for the Managed Devices covered by such Schedule. Company may terminate any Managed Device covered by this Agreement at the end of a Schedule Term if Xerox gives Company notice that such device is no longer supported. Each Managed Device covered by a Schedule to this Agreement will automatically renew for additional one-year terms unless either party provides the other party with written notice of non-renewal at least 45 days, but not more than 90 days, before the end of the existing Schedule Term. Upon Customer's request, Company will provide Customer with revised pricing for the renewal term within 90 days before the end of the existing Schedule Term for the Managed Devices that will be supported at the time of renewal and will use reasonable efforts to notify Customer of the devices that are no longer supported. At the end of the first year and once each successive 12 month period, the cost per print rates will increase by 5%.

3. BREAK FIX SERVICES. Xerox & Encompass will keep the Managed Devices in good working order ("Break Fix Services").

a) Break Fix Services may be initiated by contacting Company (Encompass) by phone, email or through the web portal.

b) Encompass will respond within **4-hours to emergency service calls**, next business day response time to non-emergency service calls during regular business hours.

c) On-site Break Fix Services are provided Monday through Friday (excluding New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day and Christmas Day) from 8:00AM to 4:00PM local time ("Service Hours"). Break-Fix Services exclude repairs due to: (i) misuse, neglect, abuse or operation of a Managed Device outside the original equipment manufacturers ("OEM") specifications; (ii) failure of the Customer's PC to comply with the OEM's published specifications; (iii) act of God or other force majeure event; (iv) relocation, alterations, or use of options, accessories, service or supplies not provided by Xerox; (v) failure to perform any Customer Responsibilities identified in the section titled "Customer Responsibilities"; or (vi) acts or omissions of Customer or any party not affiliated with Company or Xerox.

d) If Xerox determines that a Managed Device is beyond repair, or is classified by the OEM as service discontinued, or parts or Supplies (defined below) are no longer commercially available, Customer may (i): replace the device at its own expense with a device that is then supported by Xerox, or (ii): notify Company to delete the device from the Agreement.

e) Replacement parts may be new or used and all removed, replaced parts become Company's property.

f) If a maintenance kit or drum is required for a Managed Device, Xerox will provide Customer with the drum or maintenance kit component(s). Drums and maintenance kits are included in pricing. Maintenance kits and drums if such items are designated as CRUs, are user installable and do not necessarily require technician services.

4. SUPPLIES. Xerox will furnish toner or ink ("Supplies"), as applicable, for Managed Devices.

a) Encompass will manage inventory of supplies

b) Supplies are Xerox's property until used by Customer, and Customer will use them only with the Managed Devices. Selling, transferring, bartering or otherwise conveying Supplies to anyone is strictly prohibited and is subject to civil and criminal penalties.

c) Upon request, Customer will provide an inventory of Supplies in its possession. Each Managed Device may have a maximum of 1 set of consumable supplies on hand as backup. Upon expiration or termination of the Agreement Customer will, at Xerox's option and expense, return any unused Supplies to Xerox, permit access to its facilities to permit collection, or dispose of them as directed in writing by Xerox.

d) To prevent excessive shipment of Supplies, when Supplies are ordered for Managed Devices, Xerox will: (i) check the current consumables level, if available, to validate a low consumables condition; and (ii) check the metered impression volume since the last Supplies shipment to determine if impression volume exceeds the expected yield. If these conditions are not satisfied, Supplies will be billed to Customer.

d) Xerox may not provide Supplies if a current meter read is not received for a Managed Device. Pricing set forth on the Schedules includes standard ground shipping. If Customer requests expedited shipping, the cost of second day, overnight, or other non-standard shipping will be billed to Customer.

5. CUSTOMER RESPONSIBILITIES. Customer will:

a) Notify Company if Customer wishes to relocate a Managed Device, and Company will advise Customer if Services are available at the new location.

b) Assure that networked Managed Devices are Simple Network Management Protocol ("SNMP") enabled and can route SNMP over the network. If meter reads are not provided, Company (i) may estimate the number of prints used and invoice Customer accordingly.

c) Provide a dedicated PC (or server) that is connected to Customer's network at all times ("Customer PC"); and allow Company to install, use, access, update and maintain the Tools on the Customer PC. The Tools cannot be installed on a PC where other SNMP-based applications or other Xerox Tools are installed, because they may interfere with the Tools.

d) Ensure that proper virus protection is installed, maintained, and enabled on any servers, desktop workstations, laptop computers and other hardware attached to the Customer's network and output environment, as well as on any server or computer hosting the Tools or any data on the Customer's network. Neither Company nor Xerox is responsible for the disruption of Services or loss of functionality of the Tools caused by any of the foregoing. If the Tools become inoperable due to Customer implemented changes to its network, Company will work with Customer to remotely re-install Tools.

e) Assist in implementation of the Tools by providing relevant network information such as the IP address ranges or subnets on which Managed Devices reside.

f) Provide reasonable access to Customer's facilities and personnel as required for the performance of the Services.

g) Ensure that Managed Devices are installed and operating within the OEM's specifications and are readily accessible to the Xerox authorized service representative.

h) Request Break Fix Services from the Help Desk for Managed Devices that are not compatible with the Tools.

Neither Company nor Xerox will be liable for delays or services failures, including but not limited to implementation delays if Customer does not perform or facilitate completion of its designated responsibilities.

6. *PRINT CHARGES; LATE FEES AND TAXES*. Each month, Customer agrees to pay Company, by the due date as set forth on the invoice, all print charges and all other amounts due under this Agreement (including all applicable taxes). If Company does not receive payment in full on or before its due date, Customer shall pay (i) a fee equal to the greater of 1.5% of the amount that is late or $25, plus (ii) interest on the part of the payment that is late in the amount of 1.5% per month from the due date to the date paid. Customer is responsible for all taxes (including, without limitation, sales, use and personal property taxes, and excluding only taxes based on Company's income), levies, assessments, license and registration fees and other governmental charges relating to this Agreement (collectively, **"Taxes"**). Company may periodically bill Customer for, and Customer agrees to promptly pay any Taxes. Customer authorizes Company to pay any Taxes when and as they may become due, and Customer agrees to reimburse Company promptly upon demand for the full amount (less any estimated amounts previously paid by Customer).

7. XEROX CLIENT TOOLS & XEROX TOOLS. Company will use certain Xerox software tools that are installed on Customer's network ("Xerox Client Tools"), and/or certain other proprietary Xerox software ("Xerox Tools"), to perform Company's obligations under this Agreement. Xerox Client Tools and Xerox Tools (collectively, "Tools") are Xerox trade secrets. Xerox Client Tools and any related documentation are licensed under a separate clickwrap or shrinkwrap license agreement that Customer must accept at the time of installation. Xerox Tools are operated and used only by Company and Xerox, and Customer has no right to use, access or operate the Xerox Tools. Customer shall not decompile or reverse engineer the Tools. The Tools will be removed by Company at the expiration or termination of this Agreement. Xerox Client Tools facilitate performance of the Services through automatic collection and transmission of data to a secure off-site location. Examples of automatically transmitted data include product registration, meter read, supply level, equipment configuration and settings, software version, and problem/fault code data. All such data will be transmitted in a secure manner. The automatic data transmission capability does not allow Company or Xerox to read, view or download the content of any of Customer's documents residing on or passing through the Managed Devices or Customer's information management systems. If a meter reading is not generated by Xerox Client Tools or, upon request, Customer fails to provide a meter reading, Company may estimate the reading and bill Customer accordingly.

9. INTELLECTUAL PROPERTY INDEMNITY. Xerox will defend, and pay any settlement agreed to by Xerox or any final judgment for, any claim that the Tools infringe a third party's U.S. intellectual property rights. Customer must promptly notify Company and Xerox of any alleged infringement and permit Xerox to direct the defense. Neither Company nor Xerox is responsible for any non-litigation expenses or settlements unless Xerox and Company pre-approve them in writing. To avoid infringement, Xerox may modify or substitute an equivalent tool, or obtain any necessary licenses. Xerox is not liable for any infringement based upon a modification of the Tools to Customer's specifications or the Tools being used by Customer in a manner not permitted by this Agreement.

10. NO WARRANTIES; LIMITATION OF LIABILITY. The Services will be performed in a skillful and workmanlike manner. NEITHER XEROX NOR COMPANY MAKE ANY OTHER WARRANTIES AND BOTH XEROX AND COMPANY DISCLAIM ALL IMPLIED WARRANTIES OF NON-INFRINGEMENT AND FITNESS FOR A PARTICULAR PURPOSE. Any liability for any cause whatsoever shall be limited to amounts paid by Customer under this Agreement. This limitation shall apply regardless of the form of action, whether contract or tort, including without limitation negligence actions, provided,

however, that this limitation shall not apply to damages resulting from personal injury caused by Company's negligence. In no event shall Xerox or Company be liable to Customer for any special, indirect, incidental, consequential, exemplary or punitive damages in any way arising out of or relating to this Agreement. Notwithstanding anything to the contrary in this Agreement, Xerox shall not be liable to Customer or be responsible to provide any Break-Fix Services or Supplies in the event that Company is in breach of its agreement(s) with Xerox.

11. LOSS AND DAMAGE. Customer bears the risk of loss and damage to the Managed Devices and Customer shall continue to perform its obligations even if it becomes damaged or suffers a loss.

12. ASSIGNMENT. CUSTOMER SHALL NOT SELL, ASSIGN, OR OTHERWISE TRANSFER (collectively, "TRANSFER") THIS AGREEMENT, IN WHOLE OR IN PART, WITHOUT THE PRIOR WRITTEN CONSENT OF COMPANY. Any attempted assignment or delegation without the prior written consent of Company shall be void. Although Xerox is not a party to this Agreement, Xerox is a third party beneficiary of this Agreement. Customer acknowledges that Company may, without notice to Customer, Transfer this Agreement to Xerox or a third party reseller of Xerox products.

13. DEFAULT. Customer will be in default hereunder if Customer fails to pay any amount within 60 days of the due date as set forth in an invoice issued under this Agreement or if Customer fails to perform any other obligation under this Agreement. If Customer defaults, Company may do any or all of the following, at Company's option: (a) terminate this Agreement, (b) require Customer to pay to Company, on demand, an amount equal to the sum of (i) all amounts then due and past due, (ii) all remaining Minimum Monthly Charges for the Term and (iii) all other amounts that may thereafter become due hereunder to the extent that Company will be obligated to collect and pay such amounts to a third party, and/or (c) exercise any other remedy available to Company under law. Customer also agrees to reimburse Company on demand for all reasonable expenses of enforcement (including, without limitation, reasonable attorneys' fees and other legal costs).

14. *NOTICES*. Notices must be in writing and will be deemed given five (5) days after mailing, or two (2) days after sending by nationally recognized overnight courier, to the other party's business address, or to such other address designated by either party to the other by written notice given pursuant to this sentence.

15. *APPLICABLE LAW; VENUE; JURISDICTION*. Any action related to this Agreement shall be governed by the laws of California without regard to choice of law principles, and any litigation hereunder shall take place in the state or federal courts located in Sacramento, CA. Each term hereof shall be interpreted to the maximum extent possible so as to be enforceable under applicable law.

16. *MISCELLANEOUS*. This Agreement may be executed in counterparts, all of which together shall constitute the same document. Customer agrees that a facsimile or other copy containing the signatures of both parties shall be as enforceable as the original executed Agreement. The failure of either party to enforce at any time any provision of this Agreement shall not be construed to be a waiver of such provision or the right thereafter to enforce each and every provision hereof. No waiver by either party, either express or implied, or any breach of these terms or conditions shall be construed as a waiver of any other term or condition. The provisions of this Agreement that by their nature continue in effect shall survive the termination or expiration of this Agreement. The Agreement represents the final and only agreement between Customer and Company and may not be contradicted by evidence of prior, contemporaneous or subsequent oral agreements. The Agreement can be changed only by a written agreement between the parties. ***Customer hereby represents to Company that this Agreement is legally binding and enforceable against Customer in accordance with its terms.***

Accepted by **Encompass**	**Customer:**	**City of West Sacramento**	
By: _____ / _____ (Date)	By:	(Date) /	
	Print Name:		Title:

Pricing Schedule for Standard Laser Devices
Black and White Prints/Color Prints

THIS IS A LEGALLY BINDING NON-CANCELABLE CONTRACT

Customer – *Use EXACT registered name if a corp., LLC or LP:* **City of West Sacramento**

THIS PRINT SERVICES SCHEDULE ("Schedule") amends the Cost Per Print Agreement ("Agreement") between the Customer named above ("you" or "your") and Encompass ("Company"). **"Consumables" provided under this Schedule include consumables, maintenance kits, waste trays, and imaging units, as applicable.**

Equipment Description:		**Black and White Prints (Duplex Prints = 2 Prints)**		**Color Prints* (Duplex Prints = 2 Prints)****	
Brand, Model	**Base Monthly Charge:**	**Number of Copies/Prints Included in Base Monthly Charge:**	**Cost For Each Additional Print:**	**Number of Copies/Prints Included in Base Monthly Charge:**	**Cost For Each Additional Print:**
Xerox C8045/HXF2	**$0**	**0**	**$0.00525**	**0**	**$0.0525**
Xerox C8055/HXF2	**$0**	**0**	**$0.00525**	**0**	**$0.0525**
Xerox C405/DNM	**$0**	**0**	**$0.015**	**0**	**.10**

* Color Prints – If any color print is on a page, it counts as a color page. Note that grey scale images printed with the composite black setting on (the default setting on most devices) count as color pages because color consumables are used.

** Duplex Color Prints – Note that duplex prints with any color print counts as two (2) pages even if color print is only on one side.

PRICING:
Up to 20% coverage for monochrome only pages
Up to 40% total coverage for color pages

Equipment Location (if different than Customer's address shown on page 1 of the Agreement):

Term: 60 months	**Commencement Date (start of Term):** Delivery Date of Equipment

Capitalized terms that are not defined in this Schedule shall have the meaning assigned to them in the Agreement. Except as set forth above, the Agreement shall continue in full force and effect. In the event of a conflict between the terms of the Agreement and this Schedule, this Schedule shall control.

Accepted by **Encompass**	**Customer:** **City of West Sacramento**
By: _____ / _____ (Date)	By: **X** _____ / (Date)
	Print Name: _____ Title:

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 5

SUBJECT:

CONSIDERATION OF RESOLUTION 18-55 APPROVING PARCEL MAP 5133, LOCATED IN WESTBRIDGE PLAZA, PHASE II

INITIATED OR REQUESTED BY:

[ ] Council [X] Staff

[ ] Other

REPORT COORDINATED OR PREPARED BY:
Mark Collier, Principal Engineer

Charline Hamilton, Director
Community Development Department

ATTACHMENT [X] Yes [ ] No **[ ] Information [ ] Direction [X] Action**

OBJECTIVE
The purpose of this report is to obtain City Council approval of Parcel Map 5133 (see Attachment 1) which creates three parcels located in Westbridge Plaza, Phase II, due north of Lowes.

RECOMMENDED ACTION
Staff respectfully recommends that the City Council adopt Resolution 18-55 (see Attachment 2) approving Parcel Map 5133.

BACKGROUND
Tentative Parcel Map 5133 was conditionally approved by the Planning Commission on March 15, 2018. This Parcel Map creates three commercial parcels of 2.1 acres, 1.5 acres, and 1.1 acres. The property is owned by Port Phase II Pad Partners, LP.

ANALYSIS
All Conditions of Approval of the tentative map have been satisfied. Required improvements have been completed to the satisfaction of the City Engineer.

Environmental Considerations
The Planning Commission found that the tentative map was consistent with the General Plan Environmental Impact Report. No further consideration is required for this map.

Commission Recommendation
Planning Commission conditionally approved the tentative map on March 15, 2018.

Strategic Plan Integrating
Approval of Parcel Map 5133 further promotes the City's Principles to Guide the Future, specifically by providing a "Strong Diverse Local Economy".

Alternatives
In accordance with Section 66458 of the Government Code, if the map conforms to all requirements of the Subdivision Map Act and local subdivision ordinance applicable at the time of tentative map approval or conditional approval and any rulings made there under, the map shall be approved at the meeting at which the map is received or at the next meeting.

Coordination and Review
Parcel Map 5133 has been approved by the Community Development Department and the City Surveyor.

Budget/Cost Impact
The map proponent pays for costs associated with the review and approval of this map per the approved fee.

ATTACHMENT
1) Final Map 5133
2) Resolution 18-55
3) Vicinity Map

ATTACHMENT 1

PARCEL MAP OF QUICK QUACK CAR WASH

PARCEL MAP NO. 5133

PARCEL 2, AS SHOWN ON FINAL MAP OF
"WEST SACRAMENTO SELF STORAGE",
PARCEL MAP NO. 5028 FILED IN
BOOK 2013 OF MAPS AT PAGE 78,
YOLO COUNTY RECORDS
CITY OF WEST SACRAMENTO
COUNTY OF YOLO, CALIFORNIA
JULY 2018 SHEET 1 OF 2

BURRELL CONSULTING GROUP, INC.
1001 Enterprise Way, Suite 100
Roseville, CA 95678 (916) 783-8898

OWNERS STATEMENT

THE UNDERSIGNED, BEING THE ONLY PARTY HAVING RECORD TITLE INTEREST IN ALL THE LAND DELINEATED WITHIN THE EXTERIOR BOUNDARY OF THIS PARCEL MAP HEREBY CONSENTS TO THE PREPARATION AND FILING OF THIS PARCEL MAP NO. 5133 ENTITLED "QUICK QUACK CAR WASH", AND DO HEREBY DEDICATE TO THE CITY OF WEST SACRAMENTO THE FOLLOWING:

A. RIGHTS OF WAY AND EASEMENTS FOR THE COLLECTION AND CONVEYANCE OF SANITARY SEWAGE AND STORM WATER DRAINAGE INCLUDING THE INSTALLATION, AND MAINTENANCE OF SEWER AND DRAINAGE PIPES AND STRUCTURES TOGETHER WITH ANY APPURTENANCES PERTAINING THERETO, ON, OVER, UNDER AND ACROSS THOSE STRIPS OF LAND DESIGNATED AS "PUBLIC SANITARY SEWER & STORM DRAIN EASEMENT"

B. RIGHTS OF WAY AND EASEMENTS FOR PEDESTRIAN ACCESS INCLUDING THE INSTALLATION AND MAINTENANCE OF A PEDESTRIAN WALKWAY, TOGETHER WITH ANY AND ALL APPURTENANCES PERTAINING THERETO ON, OVER, AND ACROSS THAT STRIP OF LAND DESIGNATED AS "PUBLIC PEDESTRIAN EASEMENT"

THE SAME UNDERSIGNED DOES ALSO HEREBY STATE THAT THE PRIVATE RECIPROCAL INGRESS/EGRESS EASEMENTS ACROSS PARCELS 1 THROUGH 3 AS SHOWN HEREON ARE INTENDED TO BE USED FOR THE PURPOSES OF INGRESS AND EGRESS TOGETHER WITH ANY AND ALL APPURTENANCES PERTAINING THERETO, FOR THE BENEFIT OF THE PRESENT AND FUTURE OWNERS OF SAID PARCELS 1 THROUGH 3 AND SHALL BE APPROPRIATELY CONVEYED UPON THE SALE OF SAID PARCELS 1 THROUGH 3.

PORT PHASE II PAD PARTNERS L.P., A CALIFORNIA LIMITED PARTNERSHIP,

BY: _____

NAME: _____

ITS: _____

NOTARY'S ACKNOWLEDGEMENT:

A NOTARY PUBLIC OR OTHER OFFICER COMPLETING THIS CERTIFICATE VERIFIES ONLY THE IDENTITY OF THE INDIVIDUAL WHO SIGNED THE DOCUMENT TO WHICH THIS CERTIFICATE IS ATTACHED, AND NOT THE TRUTHFULNESS, ACCURACY, OR VALIDITY OF THE DOCUMENT.

STATE OF CALIFORNIA)
) SS
COUNTY OF _____)

ON _____ BEFORE ME, _____,

A NOTARY PUBLIC, PERSONALLY APPEARED _____

_____.

WHO PROVED TO ME ON THE BASIS OF SATISFACTORY EVIDENCE TO BE THE PERSON(S) WHOSE NAME(S) IS/ARE SUBSCRIBED TO THE WITHIN INSTRUMENT AND ACKNOWLEDGED TO ME THAT HE/SHE/THEY EXECUTED THE SAME IN HIS/HER/THEIR AUTHORIZED CAPACITY(IES), AND THAT BY HIS/HER/THEIR SIGNATURE(S) ON THE INSTRUMENT THE PERSON(S), OR THE ENTITY UPON BEHALF OF WHICH THE PERSON(S) ACTED, EXECUTED THE INSTRUMENT.

I CERTIFY UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE STATE OF CALIFORNIA THAT THE FOREGOING PARAGRAPH IS TRUE AND CORRECT.

WITNESS MY HAND

SIGNATURE: _____

PRINT NAME: _____

PRINCIPAL PLACE OF BUSINESS: COUNTY OF _____

MY COMMISSION NUMBER IS: _____ AND EXPIRES ON: _____

SURVEYOR'S STATEMENT

THIS MAP WAS PREPARED BY ME OR UNDER MY DIRECTION AND IS BASED UPON A FIELD SURVEY IN CONFORMANCE WITH THE REQUIREMENTS OF THE SUBDIVISION MAP ACT AND LOCAL ORDINANCE AT THE REQUEST OF QUICK QUACK CAR WASH ON MAY 9, 2017. I HEREBY STATE THAT THAT THIS MAP SUBSTANTIALLY CONFORMS TO THE APPROVED TENTATIVE MAP, THAT ALL MONUMENTS SHOWN HEREON WILL BE OF THE CHARACTER AND WILL OCCUPY THE POSITIONS INDICATED BY JUNE OF 2019 AND THAT THE MONUMENTS WILL BE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED.

_____ _____
JIM C. KOO, P.L.S. 7829 DATE
LICENSE EXPIRES 12/31/18

PROFESSIONAL LAND SURVEYOR
JIM C. KOO
NO. 7829
STATE OF CALIFORNIA

STATEMENT OF CITY ENGINEER

I HEREBY CERTIFY THAT I HAVE EXAMINED THIS PLAT OF PARCEL MAP NO. 5133 AND FIND IT TO BE SUBSTANTIALLY THE SAME AS THE TENTATIVE MAP APPROVED BY THE PLANNING COMMISSION OF THE CITY OF WEST SACRAMENTO, AND THAT ALL PROVISIONS OF THE SUBDIVISION MAP ACT AND ALL LOCAL ORDINANCES APPLICABLE AT THE TIME OF APPROVAL HAVE BEEN COMPLIED WITH.

_____ _____
DATE MARK COLLIER R.C.E. 54031
CITY ENGINEER
CITY OF WEST SACRAMENTO

CITY SURVEYOR'S STATEMENT

I HEREBY STATE THAT I HAVE EXAMINED THIS PLAT OF PARCEL MAP NO. 5133 AND THAT I AM SATISFIED SAID PARCEL MAP IS TECHNICALLY CORRECT.

_____ _____
DATE BRIAN L. SOUSA, L.S. 7917
CITY SURVEYOR
CITY OF WEST SACRAMENTO

PROFESSIONAL LAND SURVEYOR
BRIAN L. SOUSA
NO. 7917
STATE OF CALIFORNIA

STATEMENT OF CITY CLERK

I HEREBY STATE THAT THE CITY COUNCIL OF THE CITY OF WEST SACRAMENTO HAS APPROVED THIS PLAT OF PARCEL MAP NO. 5133 AND HEREBY ACCEPTS, SUBJECT TO IMPROVEMENT, ON BEHALF OF THE PUBLIC, ALL OF THE LAND AND EASEMENTS OFFERED FOR DEDICATION FOR PUBLIC USE IN CONFORMITY WITH THE TERMS OF THE OFFER OF DEDICATION AS SET FORTH ON THIS PARCEL MAP.

_____ _____
DATE KRYSS RANKIN, CITY CLERK
CITY OF WEST SACRAMENTO

COUNTY RECORDER'S STATEMENT

FILED THIS _____ DAY OF _____, 2018 AT _____ _.M., IN BOOK _____ OF MAPS, AT PAGE _____, AT THE REQUEST OF BURRELL CONSULTING GROUP, INC.

COUNTY RECORDER, COUNTY OF YOLO.

BY: _____
COUNTY RECORDER OF YOLO COUNTY

BY: _____
DEPUTY

COUNTY TAX COLLECTOR'S STATEMENT

I, THE UNDERSIGNED, TAX COLLECTOR AND REDEMPTION OFFICER OF YOLO COUNTY, DO HEREBY CERTIFY THAT ACCORDING TO THE RECORDS OF THIS OFFICE THERE ARE NO LIENS AGAINST THIS SUBDIVISION OR ANY PART THEREOF FOR UNPAID STATE, COUNTY, MUNICIPAL OR LOCAL TAXES OR SPECIAL ASSESSMENTS COLLECTED AS TAXES, EXCEPT TAXES OR SPECIAL ASSESSMENTS NOT YET PAYABLE.

FURTHERMORE, PURSUANT TO THE YOLO COUNTY BOARD OF SUPERVISORS RESOLUTION NO. 89-168, I DO HEREBY CERTIFY ON BEHALF OF THE CLERK OF THE BOARD OF SUPERVISORS THAT CHAPTER 4, ARTICLE 8, OF THE SUBDIVISION MAP ACT HAS BEEN COMPLIED WITH REGARDING DEPOSITS.

IN WITNESS WHEREOF, THE UNDERSIGNED HAS CAUSED HIS NAME TO BE SUBSCRIBED ON THIS _____ DAY OF, 20____.

_____ _____
HOWARD H. NEWENS DATED
COUNTY TAX COLLECTOR
YOLO COUNTY

PARCEL MAP OF QUICK QUACK CAR WASH

PARCEL MAP NO. 5133

PARCEL 2, AS SHOWN ON FINAL MAP OF "WEST SACRAMENTO SELF STORAGE", PARCEL MAP NO. 5028 FILED IN BOOK 2013 OF MAPS AT PAGE 78, YOLO COUNTY RECORDS
CITY OF WEST SACRAMENTO
COUNTY OF YOLO, CALIFORNIA
JULY 2018 SHEET 2 OF 2

BURRELL CONSULTING GROUP, INC.
1001 Enterprise Way, Suite 100
Roseville, CA 95678 (916) 783-8898

LEGEND

SUBDIVISION BOUNDARY
NEW PARCEL LINE
EASEMENT LINE
ADJOINER'S PARCEL LINE
SET 3/4" IRON PIPE WITH PLUG, L.S. 7829
SET NAIL AND TAG, L.S. 7829
FOUND MONUMENT AS NOTED PER 2013 MAPS 178
FOUND STANDARD CITY MONUMENT BRASS DISK STAMPED R.C.E. 31570 PER [1]
CITY OF WEST SACRAMENTO CONTROL NETWORK MONUMENT AS NOTED PER 13 M&S 61
DIMENSION POINT
OFFICIAL RECORDS . . . O.R.
RECORD DATA . . . [ ]
SEARCHED FOUND NOTHING . . . SFN

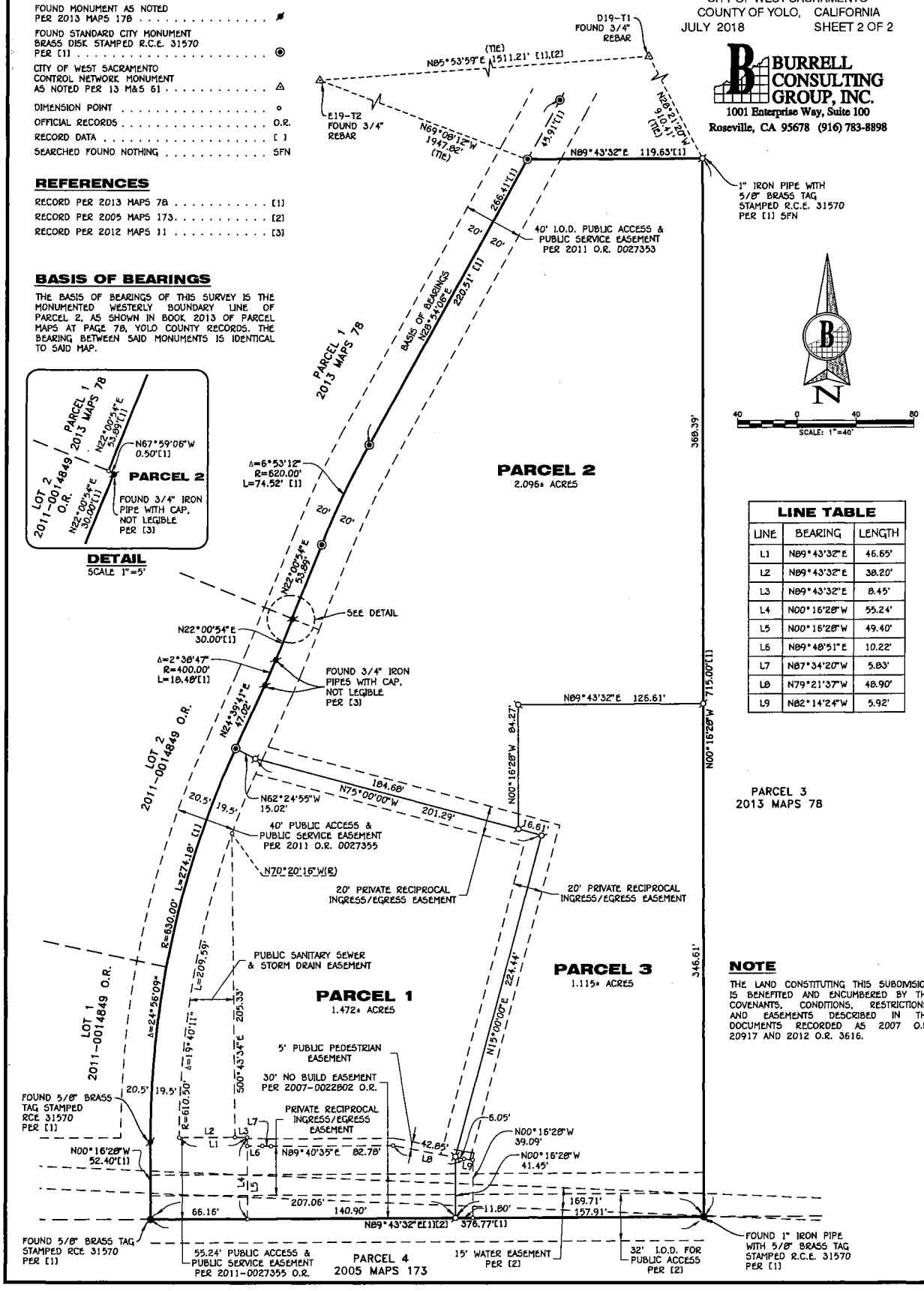
REFERENCES

RECORD PER 2013 MAPS 78 . . . [1]
RECORD PER 2005 MAPS 173 . . . [2]
RECORD PER 2012 MAPS 11 . . . [3]

BASIS OF BEARINGS

THE BASIS OF BEARINGS OF THIS SURVEY IS THE MONUMENTED WESTERLY BOUNDARY LINE OF PARCEL 2, AS SHOWN IN BOOK 2013 OF PARCEL MAPS AT PAGE 78, YOLO COUNTY RECORDS. THE BEARING BETWEEN SAID MONUMENTS IS IDENTICAL TO SAID MAP.

DETAIL
SCALE 1"=5'



LINE TABLE		
LINE	BEARING	LENGTH
L1	N89°43'32"E	46.65'
L2	N89°43'32"E	38.20'
L3	N89°43'32"E	8.45'
L4	N00°16'28"W	55.24'
L5	N00°16'28"W	49.40'
L6	N89°48'51"E	10.22'
L7	N87°34'20"W	5.83'
L8	N79°21'37"W	48.90'
L9	N82°14'24"W	5.92'

NOTE

THE LAND CONSTITUTING THIS SUBDIVISION IS BENEFITTED AND ENCUMBERED BY THE COVENANTS, CONDITIONS, RESTRICTIONS, AND EASEMENTS DESCRIBED IN THE DOCUMENTS RECORDED AS 2007 O.R. 20917 AND 2012 O.R. 3616.

ATTACHMENT 2

RESOLUTION 18-55

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF WEST SACRAMENTO APPROVING THE PARCEL MAP 5133

WHEREAS, said Parcel Map for Subdivision 5133 has been prepared and presented to the City Council of the City of West Sacramento for approval; and

WHEREAS, the subject parcel map is consistent with the approved tentative map and modifications, thereto; and

WHEREAS, all conditions of approval of the Tentative Map have been satisfied and said Parcel Map conforms to the requirements of the Subdivision Map Act and local ordinances;

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of West Sacramento hereby approves Parcel Map 5133.

PASSED AND ADOPTED by the City Council of the City of West Sacramento this 22nd day of August, 2018 by the following vote:

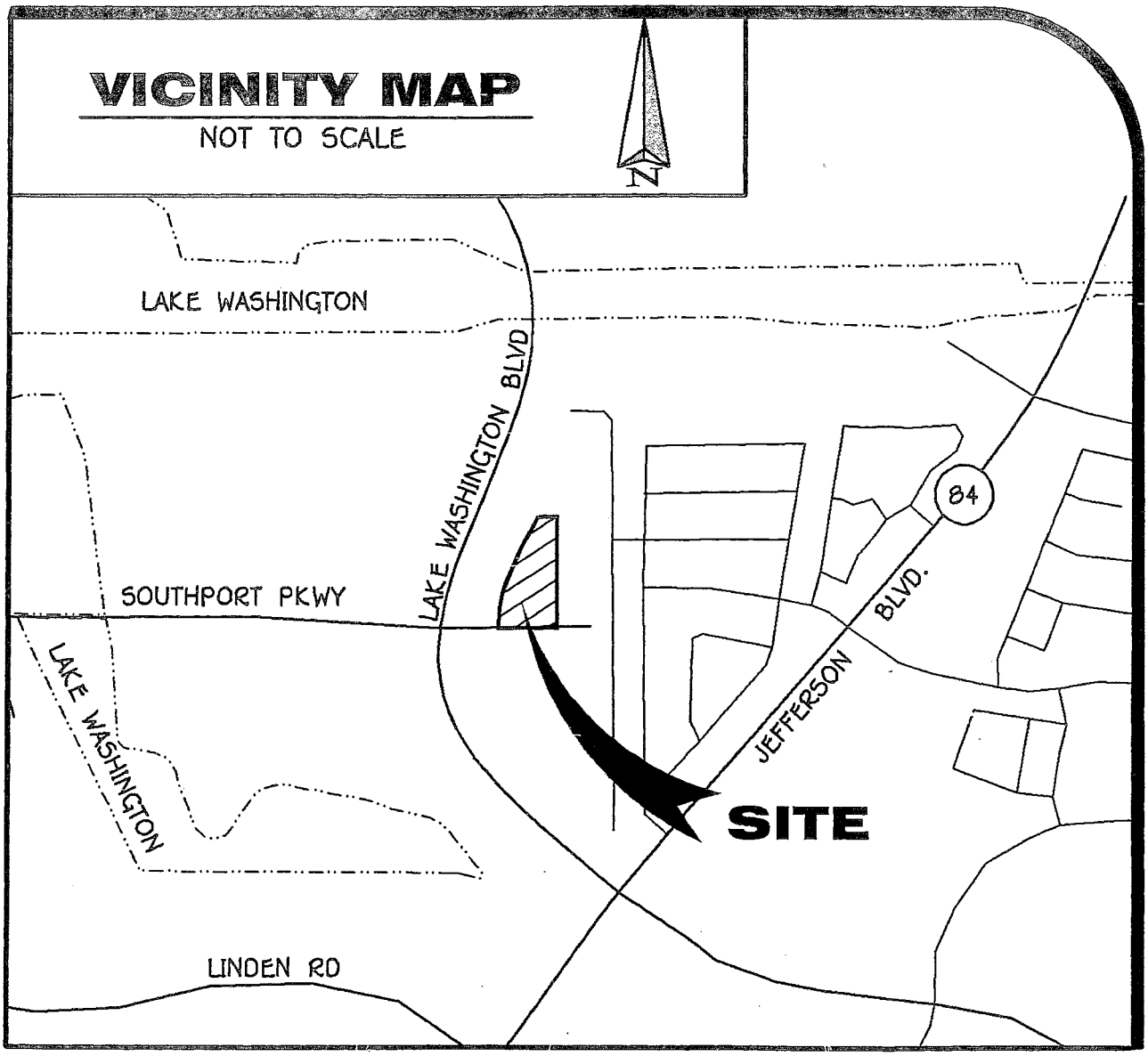
AYES:
NOES:
ABSENT:

_____
Christopher L. Cabaldon, Mayor

ATTEST:

_____
Kryss Rankin, City Clerk

ATTACHMENT 3



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NOES:
ABSENT:

_____
Christopher L. Cabaldon, Mayor

ATTEST:

_____
Kryss Rankin, City Clerk

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 6

SUBJECT:

CONSIDERATION OF A CONSULTANT CONTRACT BETWEEN THE CITY OF WEST SACRAMENTO AND PONTICELLO ENTERPRISES CONSULTING ENGINEERS, INC. FOR ON-CALL PLAN CHECKING SERVICES

INITIATED OR REQUESTED BY:

[ ] Council [X] Staff

[ ] Other

REPORT COORDINATED OR PREPARED BY:
Mark Collier, Principal Engineer

Charline Hamilton, Director
Community Development Department

ATTACHMENT [X] Yes [ ] No **[ ] Information [ ] Direction [X] Action**

OBJECTIVE
This item seeks City Council approval of a contract for services between the City of West Sacramento and Ponticello Enterprises Consulting Engineers, Inc. for on-call plan checking services.

RECOMMENDED ACTION
Staff respectfully recommends that the City Council approve the proposed consultant contract between the City of West Sacramento and Ponticello Enterprises Consulting Engineers, Inc. for on-call plan checking services and authorize the Mayor to execute the contract on behalf of the City Council.

BACKGROUND
In September of 2017, the Community Development Department distributed a Request for Proposals (RFP) for on-call plan checking services in order to supplement Development Engineering staff during periods of excessive work load. Ponticello Enterprises and 4-Leaf, Inc. were the only two firms to respond.

Because the need for support was pressing at the time, the City Manager approved contracts with both firms, for an amount not to exceed $25,000 each. Both firms have been used to varying degrees since then. In July of 2018, the City Manager approved an Amendment to the contract with Ponticello Enterprises, providing for an additional $25,000.

ANALYSIS
Ponticello Enterprises has performed exceptionally well in the execution of their services offered under the existing, limited contract. Community Development anticipates an ongoing need for supplemental assistance in plan checking services, in order to provide for timely processing of development applications. The proposed Contract is for a two-year period with no monetary cap on the value of the contract. Services will be requested as needed, with all costs paid by the development applicant.

Environmental Considerations
The subject action is for approval of a Contract to perform administrative functions and is not subject to environmental review.

Commission Recommendation
Approval of this contract does not fall under the review of any City Commissions.

Strategic Plan Integration
Approval of the consultant contract for plan check services promotes a number of tactics under the City's strategic planning strategies, specifically: providing Financially Sound City Government.

Alternatives
Staff recommends that the City Council approve the proposed consultant contract between the City of West Sacramento and Ponticello Enterprises Consulting Engineers, Inc. Alternatively, the City Council may instruct staff to recirculate a new RFP or consider other methods of fulfilling the need for timely development application reviews.

Coordination and Review
The selection panel was comprised of members of the Community Development Department.

Budget/Cost Impact
This is an on-call, time, and material-based contract. Costs associated with the contract will be funded by pass through income generated by developer fees. The City's current policy authorizes the payment of fees for plan checking services.

ATTACHMENTS
1) RFP dated September, 2017
2) Contract Documents

ATTACHMENT 1

REQUEST FOR PROPOSAL
FOR
CIVIL ENGINEERING PLAN CHECKING SERVICES
FOR THE CITY OF WEST SACRAMENTO

September, 2017

The City of West Sacramento Community Development Department invites you to submit a proposal for consultant services to provide plan check services of civil engineering improvement plans.

BACKGROUND

Due to an increase in development activity within the City of West Sacramento (City) and the desire to provide quality plan review in a timely and efficient manner, the City desires to supplement its existing staff with professionals who have the knowledge and expertise to review civil engineering improvement plans for compliance with City Design Standards and good engineering practice. In consideration of a fluctuating work load, the City has elected to outsource for this position. Assistance will generally be requested during periods of high work load.

This request for proposal will lead to a contract to provide plan check services as needed for the Community Development Department of the City of West Sacramento. The term of the Contract will be two years, renewable annually thereafter at the discretion of the City.

SCOPE OF SERVICES

The following is a general description of the required scope of services:

1. Review civil improvement plans, both public and private, for accuracy and compliance with City Design Standards and good engineering practice. Prior experience with government agencies is desirable, but not required.

2. Review related submittal material such as supporting calculations and technical reports.

3. Be available to meet with professional consultants, city staff, and others involved in the plan review process.

4. Prepare technical response letter to City staff regarding plan and related submittal review

PROPOSAL REQUIREMENTS

1. Personnel Statements

   Identify specific individuals who will be responsible for the work, their roles and qualifications, and resumes.

2. Staff Availability

   Number of hours available per week for proposed staff member(s). Any time/day restrictions

3. Hourly Cost

   Provide hourly rates for the work to be performed. Complete cost proposals will be requested of the top candidate's.

4. References

   A minimum of three references, including telephone numbers, of clients for whom the candidate has performed related work in the past five years. These client references must be for similar types of work, and these clients must be familiar with the personnel who are anticipated to work on this project.

5. Conflict of Interest Statement

   Statement of any recent, current or anticipated contractual obligations that relate to similar work within the City of West Sacramento, or Sacramento which may have a potential to conflict with the Consultant's work on the project.

6. Number of Copies

   Submit three (3) copies of the proposal

SELECTION PROCESS

After receipt of all proposals, the Community Development Department will conduct an evaluation of the written proposals. An oral presentation of competitive proposals may be conducted, but may not be required. The final selection will be made by a panel consisting of members of the Community Development Department. Selection criteria includes:

1. Qualifications of the Consultant's key personnel assigned to the project

2. Cost

Award of the Contract may be subject to approval of the City Council.

SUBMITTAL DEADLINE

Three (3) copies of your proposal must be submitted by 5:00 pm on Friday, September 22, 2017 to:

Mark Collier, Principal Engineer
City of West Sacramento
Community Development Department
1110 West Capitol Avenue
West Sacramento, Ca. 95691

ATTACHMENT 2

CONTRACT FOR SERVICES

THIS CONTRACT is made on _____, by and between the CITY OF WEST SACRAMENTO ("City"), and Ponticello Enterprises Consulting Engineers, Inc. ("Consultant").

WITNESSETH:

WHEREAS, the City proposes to have Consultant provide general plan check services within the City of West Sacramento as needed;

WHEREAS, the Consultant has presented a proposal for such services to the City, dated September 22, 2017, (attached hereto and incorporated herein as **Exhibit "A"**) and is duly licensed, qualified and experienced to perform those services;

NOW, THEREFORE, the parties hereto mutually agree as follows:

1. SCOPE OF SERVICES:

A. Consultant shall do all work, attend all meetings, produce all reports and carry out all activities necessary to completion of the services described in Exhibit "A". This Contract and its exhibits shall be known as the "Contract Documents." Terms set forth in any Contract Document shall be deemed to be incorporated in all Contract Documents as if set forth in full therein. In the event of conflict between terms contained in these Contract Documents, the more specific term shall control. If any portion of the Contract Documents shall be in conflict with any other portion, provisions contained in the Contract shall govern over conflicting provisions contained in the exhibits to the Contract.

B. Consultant enters into this Contract as an independent contractor and not as an employee of the City. The Consultant shall have no power or authority by this Contract to bind the City in any respect. Nothing in this Contract shall be construed to be inconsistent with this relationship or status. All employees, agents, contractors or subcontractors hired or retained by the Consultant are employees, agents, contractors or subcontractors of the Consultant and not of the City. The City shall not be obligated in any way to pay any wage claims or other claims made against Consultant by any such employees, agents, contractors or subcontractors, or any other person resulting from performance of this Contract.

C. The Consultant agrees it has satisfied itself by its own investigation and research regarding the conditions affecting the work to be done and labor and materials needed, and that its decision to execute this Contract is based on such independent investigation and research.

2. TERM OF CONTRACT:

A. This Contract is for on-call services. The services of Consultant are to commence upon execution of this Contract by the City, and shall and shall extend for a period of two (2) years.

B. Consultant's failure to complete work in accordance with the Task Orders may result in delayed compensation as described in Section 3.

C. The City Manager or his or her designee may, by written instrument signed by the Parties, extend the duration of this Contract for a period of one year in the manner provided in Section 5.

3. COMPENSATION:

A. The Consultant shall be paid at the completion of services for actual fees, costs and expenses per the Cost Proposal and Fee Schedule For Services (attached hereto and incorporated herein as Exhibit "B").

B. Said amount shall be paid upon submittal of a monthly billing showing completion of the tasks that month. Consultant shall furnish City with invoices for all expenses as well as for all materials authorized by this Contract. The invoices shall be submitted with the monthly billings. If Consultant's performance is not in conformity with the Scope of Services, payments may be delayed or denied, unless the Consultant's failure to perform in conformity with the Scope of Services is a documented result of the City's failure to conform with the Scope of Services.

C. If the work is halted at the request of the City, compensation shall be based upon the proportion that the work performed bears to the total work required by this Contract, subject to Section 4.

4. TERMINATION:

A. This Contract may be terminated by either party, provided that the other party is given not less than **30** calendar days' written notice (delivered by certified mail, return receipt requested) of intent to terminate.

B. The City may temporarily suspend this Contract, at no additional cost to City, provided that the Consultant is given written notice (delivered by certified mail, return receipt requested) of temporary suspension. If City gives such notice of temporary suspension, Consultant shall immediately suspend its activities under this Contract.

C. Notwithstanding any provisions of this Contract, Consultant shall not be relieved of liability to the City for damages sustained by the City by virtue of any breach of this Contract by Consultant, and the City may withhold any payments due to Consultant until such time as the exact amount of damages, if any, due the City from Consultant is determined.

D. In the event of termination, the Consultant shall be compensated as provided for in this Contract, except as provided in Section 4C. Upon termination, the City shall be entitled to all work, including but not limited to, appraisals, inventories, studies, analyses, drawings and data estimates performed to that date in accordance with Section 7 hereof.

5. AMENDMENTS, CHANGES OR MODIFICATIONS:

Amendments, changes or modifications in the terms of this Contract may be made at any time by mutual written agreement between the parties hereto and shall be signed by the persons authorized to bind the parties hereto.

6. EXTENSIONS OF TIME:

City, or Consultant may, for good cause, request extensions of time to perform the services required hereunder. Such extensions shall be authorized in advance by the City in writing and shall be incorporated in written amendments to this Contract or the attached Work Program in the manner provided in Section 3.

7. PROPERTY OF CITY:

A. It is mutually agreed that all materials prepared by the Consultant under this Contract shall become the property of the City, and the Consultant shall have no property right therein whatsoever. Immediately upon termination, the City shall be entitled to, and the Consultant shall deliver to the City, all data, drawings, specifications, reports, estimates, summaries and other such materials as may have been prepared or accumulated to date by the Consultant in performing this Contract which is not Consultant's privileged information, as defined by law, or Consultant's personnel information, along with all other property belonging exclusively to the City which is in the Consultant's possession.

B. Additionally, it is agreed that the parties intend this to be a contract for services and each considers the products and results of the services to be rendered by Consultant hereunder (the "Work") to be a work made for hire. Consultant acknowledges and agrees that the Work (and all rights therein, including, without limitation, copyright) belongs to and shall be the sole and exclusive property of the City

8. COMPLIANCE WITH ALL LAWS:

A. Consultant shall comply with all applicable laws, ordinances, and codes of federal, State and local governments, and shall commit no trespass on any public or private property in performing any of the work authorized by this Contract. It shall be City's responsibility to obtain all rights of way and easements to enable Consultant to perform its services hereunder. Consultant shall assist City in providing the same.

B. Consultant warrants to the City that it is licensed by all applicable governmental bodies to perform this Contract and will remain so licensed throughout the progress of the Work, and that it has, and will have, throughout the progress of the Work, the necessary experience, skill and financial resources to enable it to perform this Contract.

9. WARRANTIES AND RESPONSIBILITIES - CONSULTANT:

A. Consultant agrees and represents that it is qualified to properly provide the services set forth in **Exhibit "A"** in a manner which is consistent with the generally accepted standards of Consultant's profession.

B. Consultant agrees and represents that the work performed under this Contract shall be in accordance with applicable federal, State and local law in accordance with Section 17A hereof.

C. Consultant shall designate a project manager who at all times shall represent the Consultant before the City on all matters relating to this Contract. The project manager shall

continue in such capacity unless and until he or she is removed at the request of the City, is no longer employed by Consultant, or is replaced with the written approval of the City, which approval shall not be unreasonably withheld.

D. Consultant shall provide corrective services without charge to the City for services which fail to meet the above professional and legal standards and which are reported to Consultant in writing within sixty (60) days of discovery. Should Consultant fail or refuse to perform promptly its obligations, the City may render or undertake performance thereof and the Consultant shall be liable for any expenses thereby incurred.

10. SUBCONTRACTING:

None of the services covered by this Contract shall be subcontracted without the prior written consent of the City, which will not be unreasonably withheld. Consultant shall be as fully responsible to the City for the negligent acts and omissions of its contractors and subcontractors, and of persons either directly or indirectly employed by them, as it is for the negligent acts and omissions of persons directly employed by Consultant.

11. ASSIGNABILITY:

Consultant shall not assign or transfer any interest in this Contract whether by assignment or novation, without the prior written consent of the City which will not be unreasonably withheld. However, claims for money due or to become due Consultant from the City under this Contract may be assigned to a financial institution, or to a trustee in bankruptcy, without such approval. Notice of any assignment or transfer whether voluntary or involuntary shall be furnished promptly to the City.

12. INTEREST IN CONTRACT:

Consultant covenants that neither it, nor any of its employees, agents, contractors, subcontractors has any interest, nor shall they acquire any interest, direct or indirect, in the subject of the Contract, nor any other interest which would conflict in any manner or degree with the performance of its services hereunder. Consultant shall make all disclosures required by the City's conflict of interest code in accordance with the category designated by the City, unless the City Manager determines in writing that Consultant's duties are more limited in scope than is warranted by the category designated by the City code and that a narrower disclosure category should apply. Consultant also agrees to make disclosure in compliance with the City conflict of interest code if, at any time after the execution of this Contract, City determines and notifies Consultant in writing that Consultant's duties under this Contract warrant greater disclosure by Consultant than was originally contemplated. Consultant shall make disclosures in the time, place and manner set forth in the conflict of interest code and as directed by the City.

13. MATERIALS CONFIDENTIAL:

All of the materials prepared or assembled by Consultant pursuant to performance of this Contract are confidential and Consultant agrees that they shall not be made available to any individual or organization without the prior written approval of the City, except by court order.

14. LIABILITY OF CONSULTANT-NEGLIGENCE:

Consultant shall be responsible for performing the work under this Contract in a manner which is consistent with the generally-accepted standards of the Consultant's profession and shall be liable for its own negligence and the negligent acts of its employees, agents, contractors and subcontractors. The City shall have no right of control over the manner in which the work is to be done but only as to its outcome, and shall not be charged with the responsibility of preventing risk to Consultant or its employees, agents, contractors or subcontractors.

15. INDEMNITY AND LITIGATION COSTS:

Consultant shall indemnify, defend, and hold harmless the City, its officers, officials, agents, and employees against all claims, damages, demands, liability, costs, losses and expenses, including without limitation court costs and reasonable attorneys' fees, arising from Consultant's negligent acts or negligent failure to act, errors, omissions or willful misconduct incident to the performance of this Contract except such loss or damage caused by the active negligence, sole negligence, or willful misconduct of the City. The provisions of this paragraph shall survive termination or suspension of this Contract.

16. CONSULTANT TO PROVIDE INSURANCE:

A. Consultant shall not commence any work before obtaining, and shall maintain in force at all times during the duration and performance of this Contract, the policies of insurance specified in this Section. Such insurance must have the approval of the City as to limit, form, and amount, and shall be placed with insurers with a current A.M. Best's rating of no less than A VII (an NR rating is acceptable for Worker's Compensation insurance written with the State Compensation Insurance Fund of California).

B. Prior to execution of this Contract and prior to commencement of any work, the Consultant shall furnish the City with certificates of insurance and copies of endorsements providing evidence of coverage for all policies required by the Contract. The Consultant and its contractors and subcontractors shall, at their expense, maintain in effect at all times during the performance of work under the Contract not less than the following coverage and limits of insurance, which shall be maintained with insurers and under forms of policy satisfactory to the City. The maintenance by Consultant and its contractors and subcontractors of the following coverage and limits of insurance is a material element of this Contract. The failure of Consultant or of any of its contractors or subcontractors to maintain or renew coverage or to provide evidence of renewal may be treated by the City as a material breach of this Contract. Approval of the insurance by the City shall not relieve or decrease any liability of Consultant.

1. Worker's Compensation and Employer's Liability Insurance

a. Worker's Compensation - Insurance to protect the Consultant, its contractors and subcontractors from all claims under Worker's Compensation and Employer's Liability Acts, including Longshoremen's and Harbor Worker's Act ("Acts"), if applicable. Such coverage shall be maintained, in type and amount, in strict compliance with all applicable state and Federal statutes and regulations. The Consultant shall execute a certificate in compliance with Labor Code Section 1861, on the form provided in the Contract Documents.

b. Consultant shall provide a Waiver of Subrogation endorsement in favor of the City, its officers, officials, employees, agents and volunteers for losses arising from work performed by the Consultant.

2. Commercial General Liability Insurance

a. The insurance shall be provided on form CG0001, or it's equivalent, and shall include coverage for claims for bodily injury or property damage arising out of premises/operations, products/completed operations, contractual liability, and subconsultant's work and personal and advertising injury resulting from actions, failures to act, or operations of the insured, or by its employees or agents, or by anyone directly or indirectly employed by the insured. The amount of insurance coverage shall not be less than **$1,000,000.00** per occurrence.

b. The commercial general liability insurance shall also include the following:

i. Endorsement equivalent to CG 2010 1185 naming the City, its officers, officials, employees, agents, and volunteers as additional insureds. The endorsement shall contain no special limitations on the scope of protection afforded to the City, its officers, officials, employees or volunteers.

ii. Endorsement stating insurance provided to the City shall be primary as respects the City, its officers, officials, employees and any insurance or self insurance maintained by the City, its officers, officials, employees or volunteers shall be in excess of the Consultant's insurance and shall not contribute with it, to the payment or satisfaction of any defense expenses, loss, or judgment.

iii. Provision or endorsement stating that the Consultant's insurance shall apply separately to each insured against whom claim is made or suit is brought, except with respect to the limits of the insurer's liability.

3. Commercial Automobile Insurance

a. The insurance shall include, but shall not be limited to, coverage for claims for bodily injury or property damage for owned, non-owned, and hired automobiles resulting from actions, failures to act, or operations of the insured, or by its employees or agents, or by anyone directly or indirectly employed by the insured. The amount of insurance coverage shall not be less than **$1,000,000.00** per accident.

b. The commercial automobile insurance shall include the same endorsements required for the commercial general liability policy (see Section 16.B.2.b).

4. Professional Liability. The Consultant and its contractors and subcontractors shall secure and maintain in full force, during the term of this Contract and for five years thereafter, professional liability insurance policies appropriate to the

respective professions and the work to be performed as specified in this Contract. The limits of such professional liability insurance coverage shall not be less than **$1,000,000** per claim.

C. In addition to any other remedy the City may have, if Consultant fails to maintain the insurance coverage as required in this Section, the City may obtain such insurance coverage as is not being maintained, in form and amount substantially the same as is required herein, and the City may deduct the cost of such insurance from any amounts due or which may become due Consultant under this Contract.

D. No policy required by this Contract shall be suspended, cancelled, terminated by either party, or reduced in coverage or in limits unless Consultant has provided thirty (30) days prior written notice by certified mail, return receipt requested, to the City.

E. Any deductibles or self-insured retentions in excess of $10,000 must be declared to, and approved by, the City.

F. The requirement as to types, limits, and the City's approval of insurance coverage to be maintained by Consultant are not intended to, and shall not in any manner, limit or qualify the liabilities and obligations assumed by Consultant under the Contract.

17. MISCELLANEOUS PROVISIONS:

A. Consultant shall keep itself fully informed of, shall observe and comply with, and shall cause any and all persons, firms or corporations employed by it or under its control to observe and comply with, applicable federal, state, county and municipal laws, ordinances, regulations, orders and decrees which in any manner affect those engaged or employed on the work described by this Contract or the materials used or which in any way affect the conduct of the work.

B. Consultant shall not engage in unlawful employment discrimination. Such unlawful employment discrimination includes, but is not limited to, employment discrimination based upon a person's race, religious creed, color, national origin, ancestry, physical handicap, medical condition, marital status, gender, citizenship, or sexual orientation.

C. Consultant shall maintain and make available for inspection by the City and its auditors accurate records of all of its costs, disbursements and receipts with respect to any work under this Contract. Such inspections may be made during regular office hours at any time until six (6) months after the final payments under this Contract are made to the Consultant.

D. This Contract constitutes the entire agreement between the parties relative to the services specified herein and no modification hereof shall be effective unless and until such modification is evidenced by a writing signed by both parties to this Contract. There are no understandings, agreements, conditions, representations, warranties or promises, with respect to this Contract, except those contained in or referred to in the writing.

E. All notices that are required to be given by one party to the other under this Contract shall be in writing and shall be deemed to have been given if delivered personally or enclosed in a properly addressed envelope and deposited in a United States Post Office for delivery by registered or certified mail addressed to the parties at the following addresses:

City: City of West Sacramento
c/o Mark Collier
1110 West Capitol Ave
West Sacramento, ca 95691

Consultant: Ponticello Enterprises Consulting Engineers, Inc.
c/o Alan Mitchell, Principal
446 Third Street
Woodland, Ca. 95695

F. This Contract shall be interpreted and governed by the laws of the State of California.

G. Any action arising out of this Contract shall be brought and maintained in Yolo County California, regardless of where else venue may lie.

H. In any action brought by either party to enforce the terms of this Contract, each party shall be bear responsibility for its attorney's fees and all costs regardless of whether one party is determined to be the prevailing party.

[Signatures on following page]

CITY OF WEST SACRAMENTO

By: _____
Christopher Cabaldon, Mayor

ATTEST:

By: _____
Kryss Rankin, City Clerk

APPROVED AS TO FORM:

By: _____
Jeffrey Mitchell, City Attorney

CONSULTANT

By: _____
Title:

EXHIBIT A
CONSULTANT PROPOSAL

Proposal

Civil Engineering Plan Check Services

For

City of West Sacramento
Community Development Department

CITY OF WEST SACRAMENTO

October 17, 2017

PONTICELLO ENTERPRISES CONSULTING ENGINEERS, INC.
1216 Fortna Avenue, Woodland CA 95776 (530) 668-5883

Proposal

City of West Sacramento – Plan Checking Services

1. Personnel Statements

Ponticello Enterprises is presenting Rudi Golnik, PE, Senior Civil Engineer for service to the City of West Sacramento.

Rudi's consummate experience with plan and technical review comes with over 33 years of practice as a civil engineer within the State of California serving California public agencies as a staff member and consultant. Rudi has performed and managed the development review and plan check processes for multiple public agencies. Rudi's resume is attached for your information.

Alan Mitchell and Nick Ponticello, both Principal Engineers with the firm, will provide assistance to Rudi, if needed, at no additional charge to the City.

2. Staff Availability

Staff is available up to an average of 20 hours per week with some fluctuation to accommodate work load requirements. There are no restrictions to the time of day for on-site service. Rudi has an established home-office, which can be used to perform the services in a timely and cost-effective manner. He is available, as-needed, to meet with staff, design engineers, and developers, in City Hall. This approach is working well with other local agencies.

3. Hourly Cost

The rate for Rudi is $140/hour, through the 2-year term.
Travel time will be billed at $100/hour.
Vehicle Mileage is invoiced at $0.58/mile

4. References

Three references for Rudi's work with Ponticello Enterprises are as follows:

- Yolo County – Beth Gabor – (530) 666-8042
- City of Lincoln – Lindy Childers – (916) 434-2461
- City of Winters – Carol Scianna -- (530) 794-6715

5. Conflict of Interest Statement

Ponticello Enterprises does not currently have, nor anticipates having other contractual relationships with the City of West Sacramento, Sacramento, or land developers within these jurisdictions, which could potentially result in a conflict of interest with this service proposal.

Pertinent Experience

Ponticello Enterprises (2016-Present) – Senior Civil Engineer - Provides staff augmentation and technical design review services to multiple public agencies (city engineer, public works, and community development departments).

Willdan Engineering (2005-2009, 2014-2016) - Engineering Supervisor - Provide staff augmentation service to multiple public agencies throughout northern California and Nevada for city engineer service, public works management, community development engineering, capital project management, construction management and other as necessary.

City of Hollister, Ca. (2010-2014) - City Engineer/Engineering Manager- Management of full service engineering department and public works programs including; development review, capital project management, traffic engineering, construction management, storm water program management, water and wastewater enterprise program management, assessment districts and encroachment permit center.

City of Davis, Ca. (2008) - Assistant Director of Public Works - Management of Public Works Operations including 95 employees within Transportation/Streets, water distribution, wastewater collection and treatment, electrical system maintenance.

City of Grass Valley, Ca. (1991-2005) - Director of Public Works / City Engineer- Managed full service Department including engineering, storm water management, water treatment and distribution, wastewater collection and treatment, administrative service, fleet, facilities, transportation, parks and recreation.

City of Woodland, Ca. (1989-1991) - Senior Civil Engineer - Managed Engineering Division operations for Public Works Department including development review service, capital project management, construction inspection and management, design and traffic engineering.

City of Ventura, Ca. (1984-1989) - Associate Civil Engineer - Capital project design and project management for the Public Works Department including supervision of technical and professional staff.

Kern County, Ca. (1981-1984) - Civil Engineer 2 - Capital projects design and construction inspection for Public Works Department.

EXHIBIT "B"
COST PROPOSAL AND FEE SCHEDULE FOR SERVICES

PONTICELLO ENTERPRISES
CONSULTING ENGINEERS, INC

Mark Collier, Principal Engineer
City of West Sacramento, Community Development Department
1110 West Capitol Avenue
West Sacramento, CA 95691

August 10, 2018

RE: ***CITY OF WEST SACRAMENTO – PLAN CHECKING SERVICES***

Dear Mark,

As requested; I am pleased to submit this proposal to extend the relationship between Ponticello Enterprises and the City of West Sacramento, for plan check services.

Ponticello Enterprises is presenting Rudi Golnik, PE, Senior Civil Engineer to continue providing this service to the City. Rudi's consummate experience with plan and technical review comes with over 34 years of practice as a civil engineer within the State of California serving California public agencies as a staff member and consultant. Rudi has performed plan check with you and your staff for almost a year, and has developed a keen understanding of the City's standards and development process.

Alan Mitchell and Nick Ponticello, both Principal Engineers with the firm, will provide assistance to Rudi, if needed, at no additional charge to the City.

1. **Staff Availability**

   Rudi is available up to an average of 15 hours per week with some fluctuation to accommodate work load requirements. There are no restrictions to the time of day for on-site service. Rudi has an established home-office, which can be used to perform the services in a timely and cost-effective manner. He is available, as-needed, to meet with staff, design engineers, and developers, in City Hall. This approach is working well with the current workload.

2. **Hourly Cost**

   The rate for Rudi is $155/hour, through the term of this contract.
   Vehicle Mileage is invoiced at $0.58/mile

3. **Conflict of Interest Statement**

   Ponticello Enterprises does not currently have, nor anticipates having other contractual relationships with the City of West Sacramento, Sacramento, or land developers within these jurisdictions, which could potentially result in a conflict of interest with this service proposal.

Thank you for the opportunity to serve you and the City.

Sincerely,

Alan Mitchell. P.E.
President

446 Third Street, Woodland, CA 95695 (530) 668-5883

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 7

SUBJECT:
CONSIDERATION OF LAND TRANSFERS WITH THE PORT OF WEST SACRAMENTO REQUIRED BY THE STONE LOCK ONE DEVELOPMENT AGREEMENT AND REMOVAL OF TITLE EXCEPTIONS FROM THE STONE LOCK PROPERTY

INITIATED OR REQUESTED BY:

[ ] Council [X] Staff

[ ] Other

REPORT COORDINATED OR PREPARED BY:
Rick Toft, Port Manager

Aaron Laurel, City Manager

ATTACHMENT [X] Yes [ ] No [ ] Information [ ] Direction [X] Action

OBJECTIVE
The objective of this report is to present sufficient information for the Council to consider approval of land transfers with the Port of West Sacramento required by the Stone Lock One Development Agreement and removal of title exceptions from the Stone Lock property to facilitate the sale and development of the Port's Stone Lock One property.

RECOMMENDED ACTION
Staff respectfully recommends that the City Council:

1. Find that the land transfers between the City and the Port required by the Stone Lock One Development Agreement are exempt from environmental review pursuant to CEQA Guidelines Section 15061(b)(3);
2. Authorize the City Manager to execute land transfer documents as shall be approved by the City Attorney to effectuate transfer of land parcels with the Port of West Sacramento as required by the Stone Lock One Development Agreement (see legal descriptions in Attachment 1);
3. Authorize the City Manager to execute the Termination Agreement with the Port of West Sacramento related to the 1996 Seaway Property Development Agreement (Attachment 2); and
4. Authorize the City Manager to execute the Quitclaim Deeds related to the modification of the City Arlington Park, the 2011 Irrevocable Offers of Dedication, and the 2011 Declaration of Restrictive Covenants on the Stone Lock One property in the form attached to this report with such changes as shall be approved by the City Attorney (Attachment 3).

BACKGROUND
In 2015, the Port of West Sacramento acquired the 210-acre Stone Lock property from the West Sacramento Redevelopment Successor Agency and entered into a development agreement with the City of West Sacramento. The portion of the Stone Lock property west of Jefferson Boulevard (Stone Lock One) was determined to be developable as a standalone project and a logical first phase of the larger Stone Lock project. The Stone Lock One Development Agreement (DA) was adopted by the City via Ordinance 18-8 on May 23, 2018. The DA, which allows for the first phase of the Stone Lock development project to proceed independently, specifies land transfers between the Port and the City to create public trail and park space adjacent to a 16.5-acre development site. These land transfers are the basis for recommended actions #1 and #2, and a companion item will be presented to the Port Commission for its consideration.

In June 2018, the Port issued a Request for Proposals for the purchase and development of the Stone Lock One development site with proposals due by August 6. Should the Port Commission approve a sale of the Stone Lock One site, several agreements which were recorded against the property years ago will need to be terminated or quitclaimed, including a 2005 agreement between the Port and the City which specifies certain fees owed to the City by the Port and to the Port by the City, and a 2011 "Declaration of Restrictive Covenants" related to dissolution of the Redevelopment Agency. Additionally, the Port granted the City a series of 2011 Irrevocable Offers of Dedication (IOD) on the Stone Lock One property (modified in January 2018 for the barge canal trail project) for rights to construct improvements for flood protection, access, recreation and utilities.

ANALYSIS
Land Dedications for Public Trail and Park Improvements

The Stone Lock One DA specifies several land dedications between the Port and the City; those dedications which are required to establish the boundaries of the Stone Lock One development site, the Barge Canal Trail, and Arlington Park are outlined below:

Port to City

- Barge Canal Trail Parcel (20.33 acres)
- In-Line Booster Pump Station Parcel*
- City Arlington Park Modification Parcel*

  *adjacent parcels which have been merged in to one 2.215-acre parcel

City to Port

- Port Arlington Park Modification Parcel (.998 acres)

The DA requires the City to initiate construction of the barge canal trail improvements within one year of the effective date of the DA and Arlington Park within one year of the sale of the Stone Lock One development site. The land dedications from the Port to the City are necessary for construction of these improvement to proceed, and the land dedication from the City to the Port is necessary for the City to secure land sale proceeds per the Stone Lock One DA.

Termination Agreement

Staff has confirmed that the obligations of both the Port and City in the 2005 "Agreement Regarding Resolution of Existing Issues" have been satisfied. This agreement was recorded against Port-owned property and its termination is necessary to provide clean title to the Stone Lock One property. A companion item to recommended action #3 will be presented to the Port Commission for its consideration.

Quitclaim Deeds

The IOD areas are only generally depicted in the three 2011 dedication agreements. The property rights being granted with the recommendation action #2 replace and/or improve the property rights the City would have other otherwise receive with the IODs. Additionally, the quitclaims relinquish any unnecessary areas (expect for the Barge Canal Access Parking Lot) and removes any cloud on title associated with these IODs, preparing the site for transfer to a developer. Although the Barge Canal Access Parking Lot IOD (Instrument No. 2011-0016040) does cover the existing gravel parking lot, it is recommended that this IOD be relinquished at this time and replaced with an interim license agreement between the Port and the City as the future Locks Drive, associated with future development project, would eliminate the much of the existing parking lot.

The City is the only remaining beneficiary of the 2011 Declaration of Restrictive Covenants recorded against the Stone Lock property by the former Redevelopment Agency. The covenants in the Declaration are no longer relevant to the City's interest and are a cloud on title which must be quitclaimed prior to sale of the Stone Lock One property.

Environmental Considerations

The land transfers between the Port and the City satisfy conditions in the Stone Lock One DA, approval of which was found to be exempt from environmental review pursuant to the California Environmental Quality Act Guidelines Section 15061(b)(3). These land transfers are related to public infrastructure improvements which have either already been analyzed in the Barge Canal Trail Project environmental review or which will be identified in the City's Parks Master Plan Update and will be subject to CEQA analysis in association with that plan. The ultimate Stone Lock One development project is not proposed at this time and will require its own discretionary review and CEQA compliance.

Commission Recommendation

The Port Commission is scheduled to consider the land transfers and the Termination Agreement on August 22.

Strategic Plan Integration

The recommended actions in this report will facilitate the Stone Lock One project which supports the City's Strategic Plan goal of "Preferred Place to Live in Region" by facilitating high quality housing and park development.

Alternatives

The Council could decline to approve any of the recommended actions in this report.

Coordination and Review

The land transfer legal descriptions were coordinated with the Port. The actual grant deed documents transferring the properties described in this report will be prepared by the Port and City Attorney. The Termination Agreement and Quitclaim Deeds were prepared by the City Attorney.

Budget/Cost Impact

Approval of the recommended actions will facilitate a sale of the Stone Lock One development site. The net proceeds due to the City from a Stone Lock One property sale are unknown at this time but are anticipated to be sufficient to receive full repayment of the $2,100,000 Stone Lock loan to the Port and provide funding for the Barge Canal Trail and Arlington Park projects.

ATTACHMENTS

1. Fee Transfer to City from the Port (Legals/Plats)
2. Termination Agreement
3. Quitclaim deeds

Attachment 1

Fee Title Transfers to the City from the Port (Plats and Legals)

EXHIBIT 'A'
LEGAL DESCRIPTION
BARGE CANAL & LOCK CENTER
BUILDING SETBACK

AREA 1:

All that portion of Parcel 7 and Parcel 8 as said parcels are described in that certain Grant Deed recorded July 12, 2004 as Document Number 2004-0030805, Official Records of the County of Yolo, situate within the City of West Sacramento, County of Yolo, State of California, Township 8 North, Range 4 East, M.D.M., being more particularly described as follows:

BEGINNING at a point being the northeast corner of the lands described in that certain Grant Deed recorded May 20, 2011 as Document Number 2011-0014130, official records of the County of Yolo, from which, a 3/4 inch iron pipe marking the westerly terminus of the centerline of Aster Avenue, as shown on that map entitled "Arlington Oaks - Unit One", filed for record in Book 4 of Maps, at Page 49, Yolo County Records, bears South 13°38'43" West, a distance of 549.36 feet and a 3/4" iron pipe marking the easterly terminus of said centerline bears South 54°08'18" East, a distance of 894.52 feet; thence from said **POINT OF BEGINNING**, along the north line of last said Grant Deed and the prolongation thereof, and through said Parcel 8, respectively, South 89°17'49" West, a distance of 614.40 feet to the westerly line of said Parcel 8; thence along said westerly line the following three (3) courses and distances:

1) North 08°04'00" East, a distance of 99.71 feet;
2) South 87°10'44" West, a distance of 54.90 feet;
3) North 00°28'34" West, a distance of 18.11 feet, more or less, to the ordinary high water mark of the south bank of the Sacramento Deep Water Ship Channel;

Thence leaving said westerly line, through said Parcel 8 and into said Parcel 7, along said ordinary high water mark the following twelve (12) courses and distances:

1) North 88°12'52" East, a distance of 485.38 feet;
2) North 88°51'16" East, a distance of 353.79 feet;
3) North 82°09'48" East, a distance of 151.27 feet;
4) South 89°01'45" East, a distance of 86.97 feet;
5) South 73°34'56" East, a distance of 58.39 feet;
6) South 78°01'27" East, a distance of 163.32 feet;
7) North 89°54'46" East, a distance of 466.77 feet;
8) South 84°18'21" East, a distance of 141.06 feet;
9) North 67°32'31" East, a distance of 72.83 feet;
10) North 76°50'37" East, a distance of 119.75 feet;
11) North 85°02'04" East, a distance of 346.43 feet;
12) North 86°03'02" East, a distance of 328.51 feet to the westerly line of the lands described as Parcel 1 in that certain Grant Deed recorded July 25, 2003 as Document Number 2003-0044071, Official Records of the County of Yolo;

Thence leaving said ordinary high water mark, along said westerly line, from a radial line which bears South 66°48'26" East, along a non-tangent curve concave to the northwest, having a radius

of 3448.08 feet, southwesterly 112.43 feet along said curve through a central angle of 01°52'06"; thence leaving said westerly line and through said Parcel 7 the following twenty (20) courses and distances:

1) South 78°44'35" West, a distance of 66.91 feet;
2) South 82°40'58" West, a distance of 100.42 feet;
3) South 88°14'03" West, a distance of 99.62 feet;
4) South 87°45'17" West, a distance of 100.01 feet;
5) South 87°23'49" West, a distance of 99.37 feet;
6) South 85°13'41" West, a distance of 99.92 feet;
7) South 77°41'42" West, a distance of 95.98 feet;
8) South 69°02'33" West, a distance of 110.36 feet;
9) South 89°21'20" West, a distance of 100.28 feet;
10) North 84°48'35" West, a distance of 100.89 feet;
11) North 89°48'49" West, a distance of 102.97 feet;
12) South 88°52'17" West, a distance of 100.15 feet;
13) North 89°58'15" West, a distance of 100.07 feet;
14) North 89°49'49" West, a distance of 104.04 feet;
15) North 85°03'14" West, a distance of 100.14 feet;
16) North 76°39'38" West, a distance of 98.70 feet;
17) North 84°23'17" West, a distance of 99.97 feet;
18) North 87°04'02" West, a distance of 99.89 feet;
19) South 82°07'34" West, a distance of 98.32 feet;
20) South 83°07'33" West, a distance of 194.65 feet to the **POINT OF BEGINNING.**

Containing 6.89 acres, more or less.

AREA 2:

All those portions of Parcel 1, Parcel 3 and Parcel 4, as said parcels are described in that certain Grant Deed recorded July 12, 2004 as Document Number 2004-0030805, Official Records of the County of Yolo, situate within the City of West Sacramento, County of Yolo, State of California, Township 8 North, Range 4 East, M.D.M., being more particularly described as follows:

BEGINNING at a point on the most westerly line of said Parcel 3, said westerly line also being a common line with the easterly line of the lands described in that certain Grant Deed recorded December 17, 2012, as Document Number 2012-0042140, Official Records of the County of Yolo, from which, a 3/4 inch iron pipe marking the westerly terminus of the centerline of Aster Avenue, as shown on that map entitled "Arlington Oaks - Unit One", filed for record in Book 4 of Maps, at Page 49, Yolo County Records, bears South 75°08'37" West, a distance of 2497.69 feet and a 3/4" iron pipe marking the easterly terminus of said centerline bears South 67°59'10" West, a distance of 1682.30 feet; thence from said **POINT OF BEGINNING**, along said common line, North 22°46'41" East, a distance of 34.21 feet to the southwest corner of the Acquisition Parcel described in that certain Grant Deed recorded October 30, 2015 as Document Number 2015-0030374, Official Records of the County of Yolo; thence leaving said common line, along the south line of said Acquisition Parcel, North 89°25'26" East, a distance of 981.73 feet to the most southerly southeast corner of said Acquisition Parcel; thence along the easterly line of said

Acquisition Parcel, North 00°34'34" West, a distance of 38.11 feet; thence leaving said easterly line, through said Parcel 3, the following five (5) courses and distances:

1) South 87°32'16" East, a distance of 567.46 feet;
2) South 89°46'05" East, a distance of 697.64 feet;
3) South 75°39'39" East, a distance of 167.84 feet to the most northerly corner of "Unit A" as described in that certain Easement Deed recorded June 4, 2015, as Document Number 2015-0015327, Official Records of the County of Yolo;
4) along the northwesterly line of said Easement Deed, South 18°03'57" West, a distance of 170.13 feet;
5) continuing along said northwesterly line, leaving said Parcel 3 and into said Parcel 1, South 81°46'33" West, a distance of 179.63 feet;

Thence through said Parcel 1, the following ten (10) courses and distances:

1) continuing along said northwesterly line, South 86°25'53" West, a distance of 134.43 feet;
2) continuing along said northwesterly line, South 02°49'59" East, a distance of 105.69 feet;
3) leaving said northwesterly line, South 80°21'00" West, a distance of 156.17 feet;
4) South 84°02'24" West, a distance of 202.90 feet;
5) North 87°23'17" West, a distance of 198.68 feet;
6) South 84°33'22" West, a distance of 332.03 feet;
7) South 88°03'49" West, a distance of 183.91 feet;
8) South 88°22'20" West, a distance of 139.97 feet;
9) North 03°37'02" West, a distance of 377.75 feet;
10) Leaving said Parcel 1, through said Parcel 4 and through said Parcel 3, respectively, South 89°33'06" West, a distance of 829.94 feet to the **POINT OF BEGINNING**.

Containing 13.44 acres, more or less.

See Ex*hibit 'B', Plat to accompany description, attached hereto and made a part hereof.*

The basis of bearings for this description is based upon the California Coordinate System, CCS 83 (2007), Zone 2. All distances cited herein are grid values, which are the basis for the areas shown hereon. To obtain ground values, multiply the distances cited herein by 1.00004540.

END OF DESCRIPTION

PROFESSIONAL LAND SURVEYOR
MICHAEL E. LONG
EXP. 09-30-18
No. 6815
STATE OF CALIFORNIA

Michael E. Long, PLS 6815
July 9, 2018

7/9/18

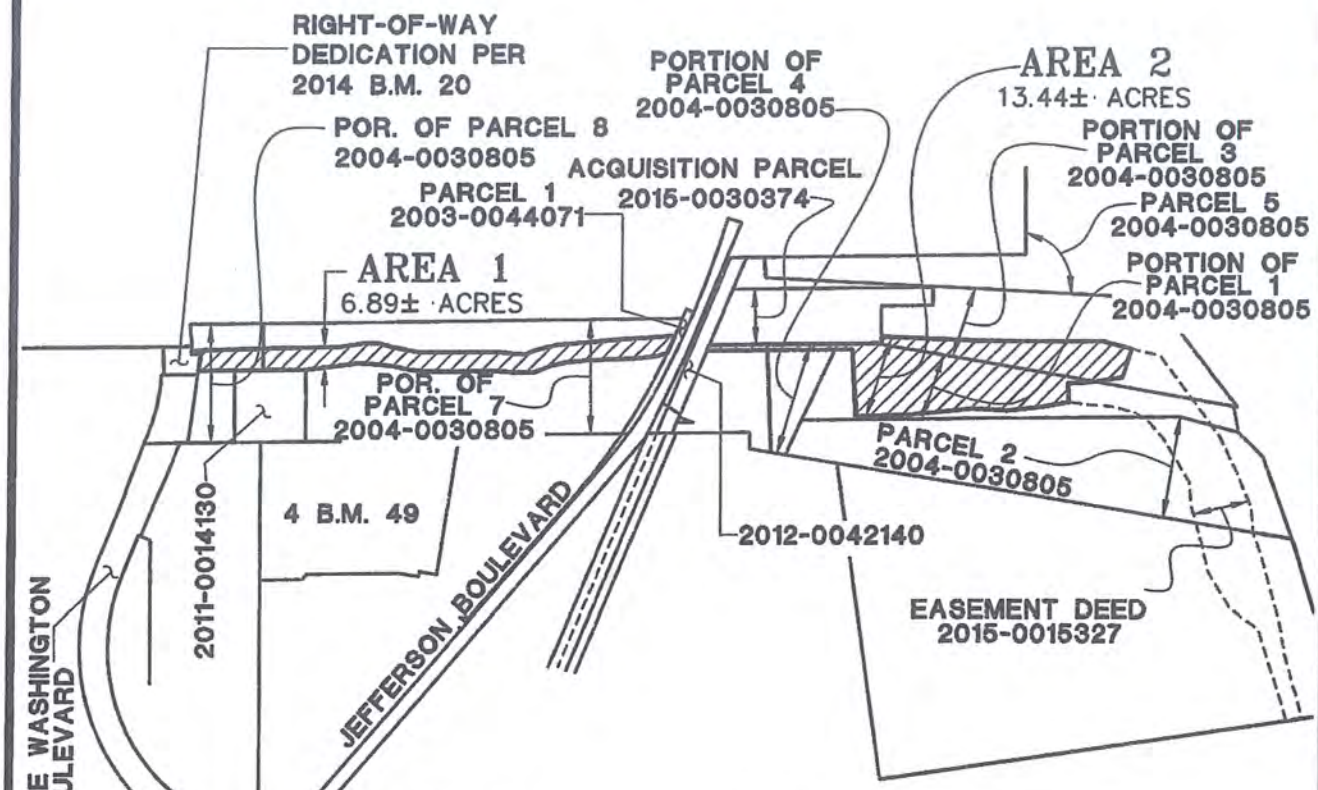
PREPARED BY WOOD-RODGERS, INC.
SACRAMENTO, CALIFORNIA

EXHIBIT 'B'

PLAT TO ACCOMPANY
DESCRIPTION

BARGE CANAL & LOCK CENTER
BUILDING SETBACK

CITY OF WEST SACRAMENTO
COUNTY OF YOLO STATE OF CALIFORNIA



PROFESSIONAL LAND SURVEYOR
MICHAEL E. LONG
EXP:09-30-18
No. 6815
STATE OF CALIFORNIA

7/9/18

LEGEND:

B.M.	BOOK OF MAPS
℄	CENTERLINE
P.O.B.	POINT OF BEGINNING
POR.	PORTION
(R)	RADIAL BEARING
●	FOUND 3/4" IRON PIPE PER 4 B.M. 49
▨	BUILDING SETBACK

0 500 1000 2000

SCALE:1"= 1000'

WOOD RODGERS
BUILDING RELATIONSHIPS ONE PROJECT AT A TIME
3301 C ST, BLDG. 100-B TEL 916.341.7760
SACRAMENTO, CA 95816 FAX 916.341.7767

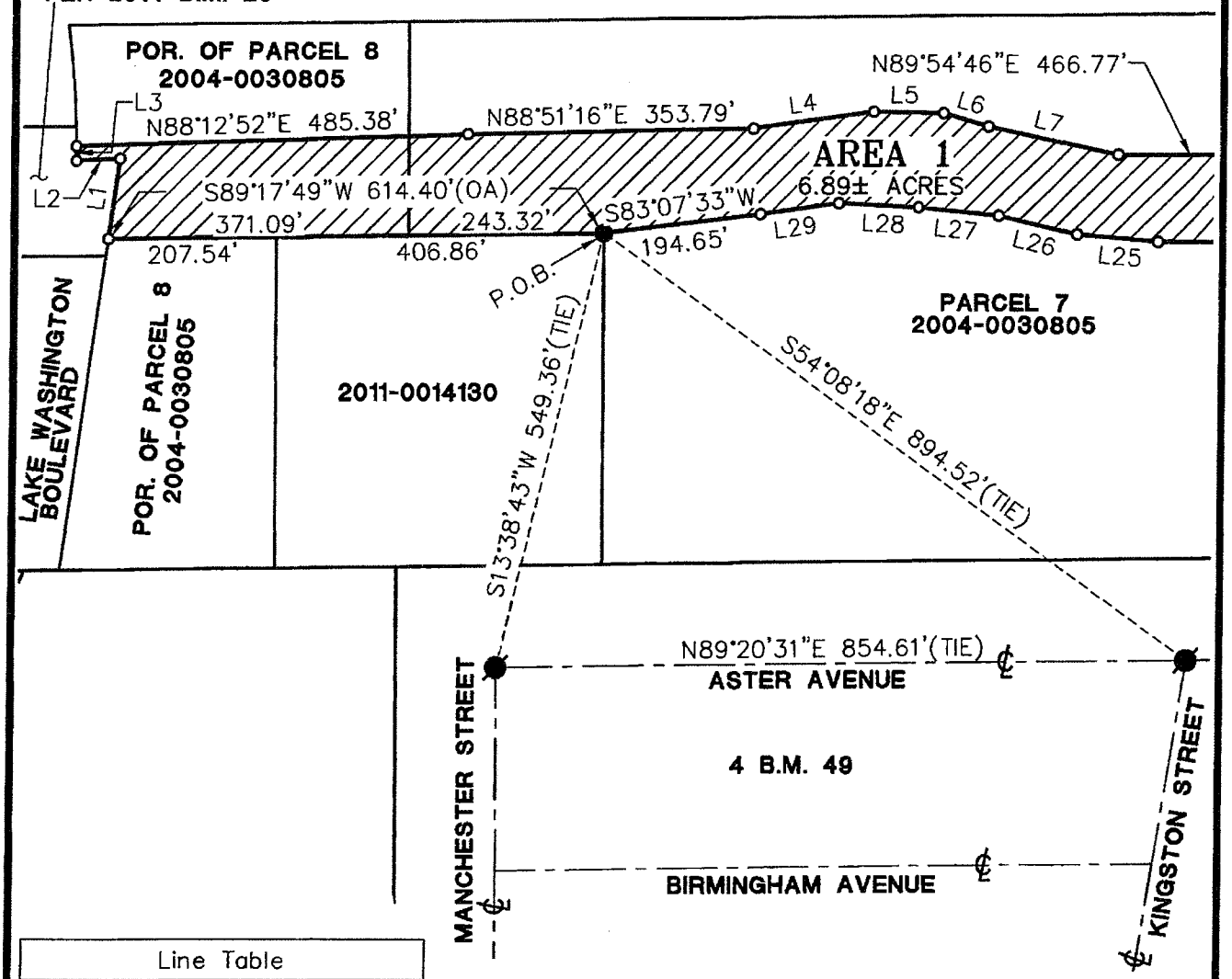
JULY 9, 2018 8621.004 SHEET 1 OF 5

EXHIBIT 'B'

PLAT TO ACCOMPANY
DESCRIPTION

BARGE CANAL & LOCK CENTER
BUILDING SETBACK

CITY OF WEST SACRAMENTO
COUNTY OF YOLO STATE OF CALIFORNIA



Line Table		
No.	Bearing	cLength
L1	N8°04'00"E	99.71'
L2	S87°10'44"W	54.90'
L3	N0°28'34"W	18.11'
L4	N82°09'48"E	151.27'
L5	S89°01'45"E	86.97'
L6	S73°34'56"E	58.39'
L7	S78°01'27"E	163.32'
L25	N85°03'14"W	100.14'
L26	N76°39'38"W	98.70'
L27	N84°23'17"W	99.97'
L28	N87°04'02"W	99.89'
L29	S82°07'34"W	98.32'

0 100 200 400

SCALE:1"= 200'

WOOD RODGERS
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SACRAMENTO, CA 95816 FAX 916.341.7767

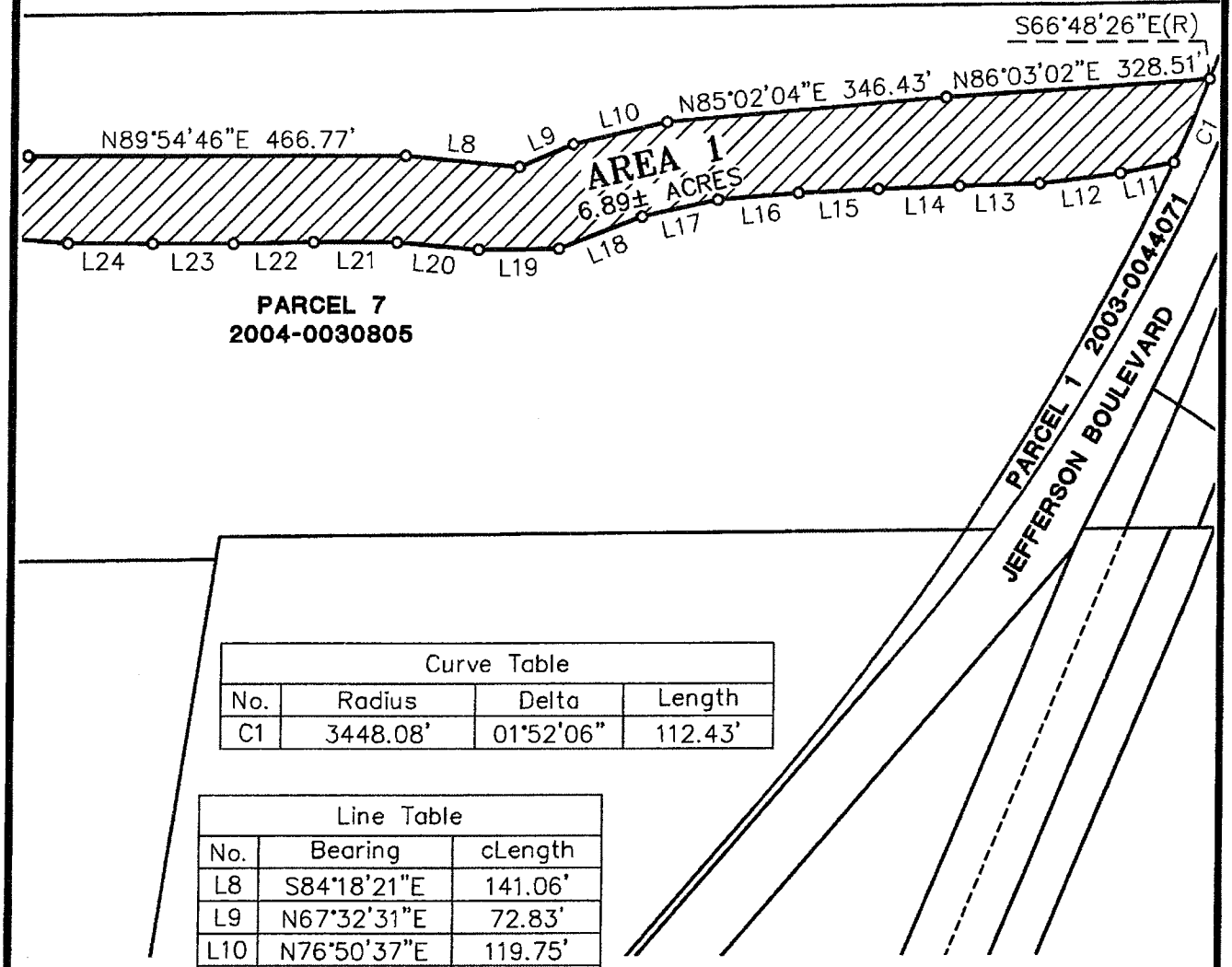
JULY 9, 2018 8621.004 SHEET 2 OF 5

EXHIBIT 'B'

PLAT TO ACCOMPANY
DESCRIPTION

BARGE CANAL & LOCK CENTER
BUILDING SETBACK

CITY OF WEST SACRAMENTO
COUNTY OF YOLO STATE OF CALIFORNIA



Curve Table			
No.	Radius	Delta	Length
C1	3448.08'	01°52'06"	112.43'

Line Table		
No.	Bearing	cLength
L8	S84°18'21"E	141.06'
L9	N67°32'31"E	72.83'
L10	N76°50'37"E	119.75'
L11	S78°44'35"W	66.91'
L12	S82°40'58"W	100.42'
L13	S88°14'03"W	99.62'
L14	S87°45'17"W	100.01'
L15	S87°23'49"W	99.37'
L16	S85°13'41"W	99.92'
L17	S77°41'42"W	95.98'
L18	S69°02'33"W	110.36'
L19	S89°21'20"W	100.28'
L20	N84°48'35"W	100.89'
L21	N89°48'49"W	102.97'
L22	S88°52'17"W	100.15'
L23	N89°58'15"W	100.07'
L24	N89°49'49"W	104.04'

0 100 200 400

SCALE: 1" = 200'

WOOD RODGERS
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SACRAMENTO, CA 95816 FAX 916.341.7767

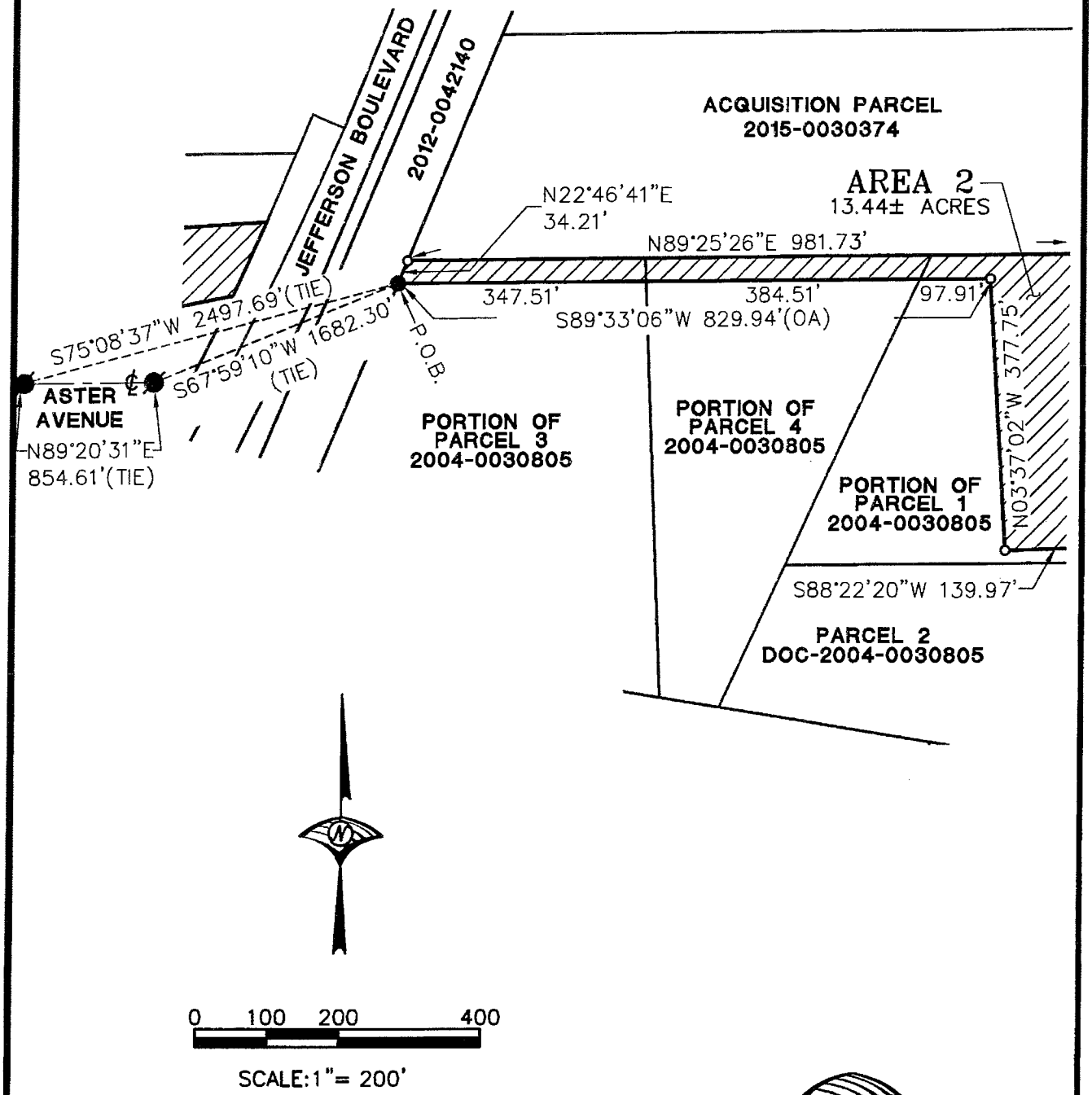
JULY 9, 2018 8621.004 SHEET 3 OF 5

EXHIBIT 'B'

PLAT TO ACCOMPANY
DESCRIPTION

BARGE CANAL & LOCK CENTER
BUILDING SETBACK

CITY OF WEST SACRAMENTO
COUNTY OF YOLO STATE OF CALIFORNIA



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SACRAMENTO, CA 95816 FAX 916.341.7767

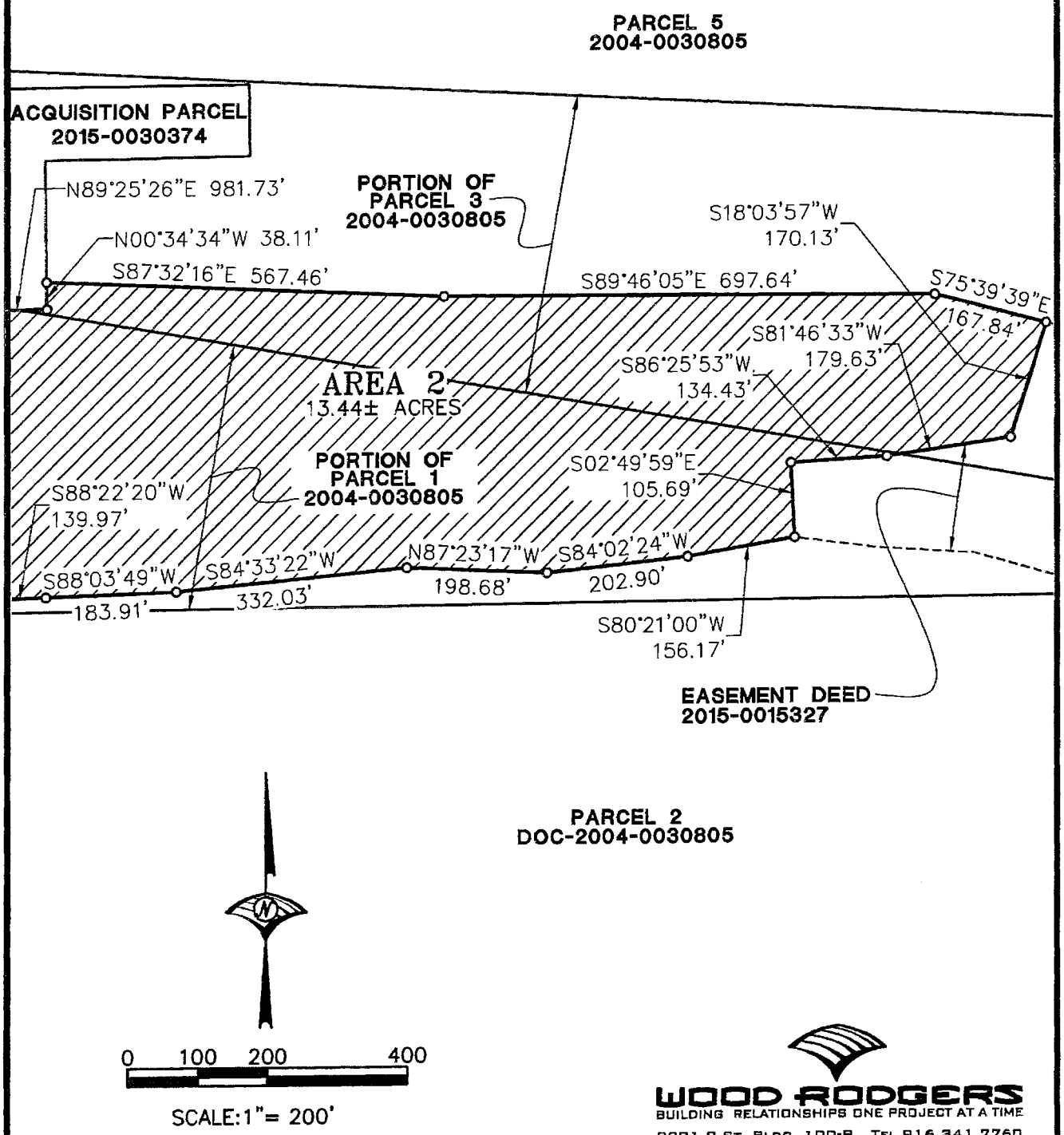
JULY 9, 2018 8621.004 SHEET 4 OF 5

EXHIBIT 'B'

PLAT TO ACCOMPANY
DESCRIPTION

BARGE CANAL & LOCK CENTER
BUILDING SETBACK

CITY OF WEST SACRAMENTO
COUNTY OF YOLO STATE OF CALIFORNIA



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3301 C ST, BLDG. 100-B TEL 916.341.7760
SACRAMENTO, CA 95816 FAX 916.341.7767

JULY 9, 2018 8621.004 SHEET 5 OF 5

EXHIBIT 'A'
LEGAL DESCRIPTION
CITY ARLINGTON PARK BOUNDARY MODIFICATION PARCEL

All that portion of Parcel 8 as said parcel is described in that certain document recorded July 12, 2004 as Document Number 2004-0030805, Official Records of the County of Yolo, situate within the City of West Sacramento, County of Yolo, State of California, Township 8 North, Range 4 East, M.D.M., being more particularly described as follows:

BEGINNING at a point on the south line of said Parcel 8, also being the southwest corner of the lands described in that certain Grant Deed recorded May 20, 2011 as Document Number 2011-0014130, Official Records of the County of Yolo, from which, a 3/4 inch iron pipe marking the westerly terminus of the centerline of Aster Avenue, as shown on that map entitled "Arlington Oaks - Unit One", filed for record in Book 4 of Maps, at Page 49, Yolo County Records, bears South 66°04'19" East, a distance of 301.27 feet and a 3/4" iron pipe marking the easterly terminus of said centerline bears South 84°19'13" East, a distance of 1135.50 feet; thence from said **POINT OF BEGINNING**, along said south line, South 89°22'03" West, a distance of 267.09 feet to the southwest corner of said Parcel 8; thence along the westerly line of said Parcel 8, North 08°04'00" East, a distance of 411.14 feet to a point of intersection with the westerly prolongation of the north line of said Grant Deed; thence leaving said westerly line, along said prolongation, North 89°17'49" East, a distance of 207.54 feet to the northwest corner of said Grant Deed; thence along the west line of last said Grant Deed, South 00°15'39" East, a distance of 406.68 feet to the **POINT OF BEGINNING.**

Containing 2.215 acres, more or less.

See Ex*hibit 'B', Plat to accompany description, attached hereto and made a part hereof.*

The basis of bearings for this description is based upon the California Coordinate System, CCS 83 (2007), Zone 2. All distances cited herein are grid values, which are the basis for the areas shown hereon. To obtain ground values, multiply the distances cited herein by 1.00004540.

END OF DESCRIPTION

Dennis L. Barber, PLS 8067
August 1, 2018

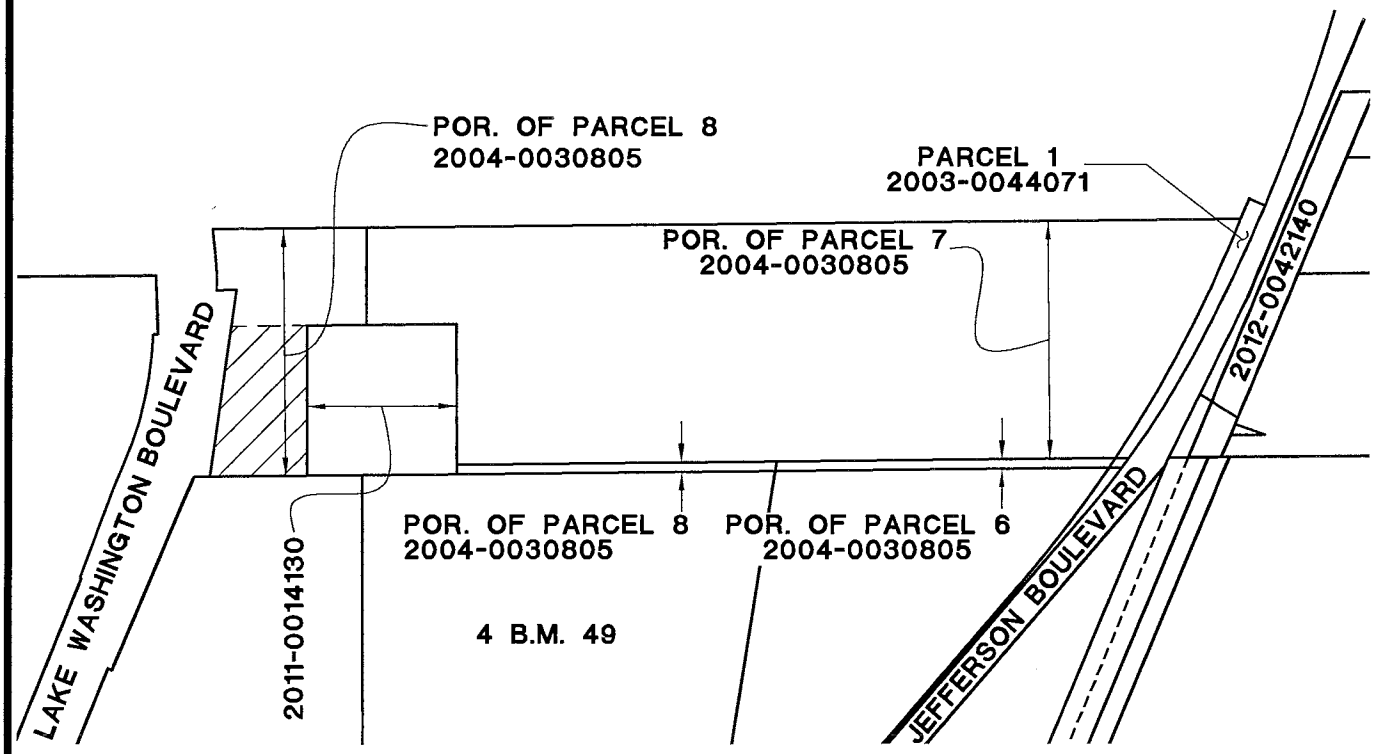
PREPARED BY WOOD-RODGERS, INC.
SACRAMENTO, CALIFORNIA

EXHIBIT 'B'

PLAT TO ACCOMPANY
DESCRIPTION

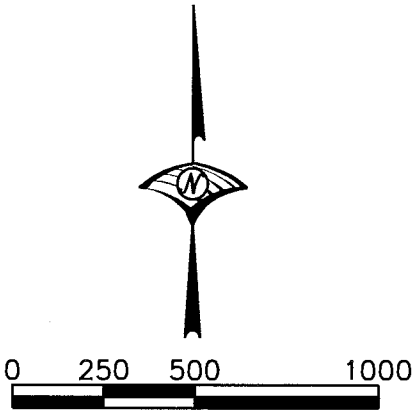
CITY ARLINGTON PARK BOUNDARY MODIFICATION PARCEL

CITY OF WEST SACRAMENTO
COUNTY OF YOLO STATE OF CALIFORNIA



LEGEND:

B.M.	BOOK OF MAPS
℄	CENTERLINE
P.O.B.	POINT OF BEGINNING
POR.	PORTION
(R)	RADIAL BEARING
●	FOUND 3/4" IRON PIPE PER 4 B.M. 49
▨	EASEMENT AREA



SCALE:1"= 500'

WOOD RODGERS
BUILDING RELATIONSHIPS ONE PROJECT AT A TIME
3301 C ST, BLDG. 100-B TEL 916.341.7760
SACRAMENTO, CA 95816 FAX 916.341.7767

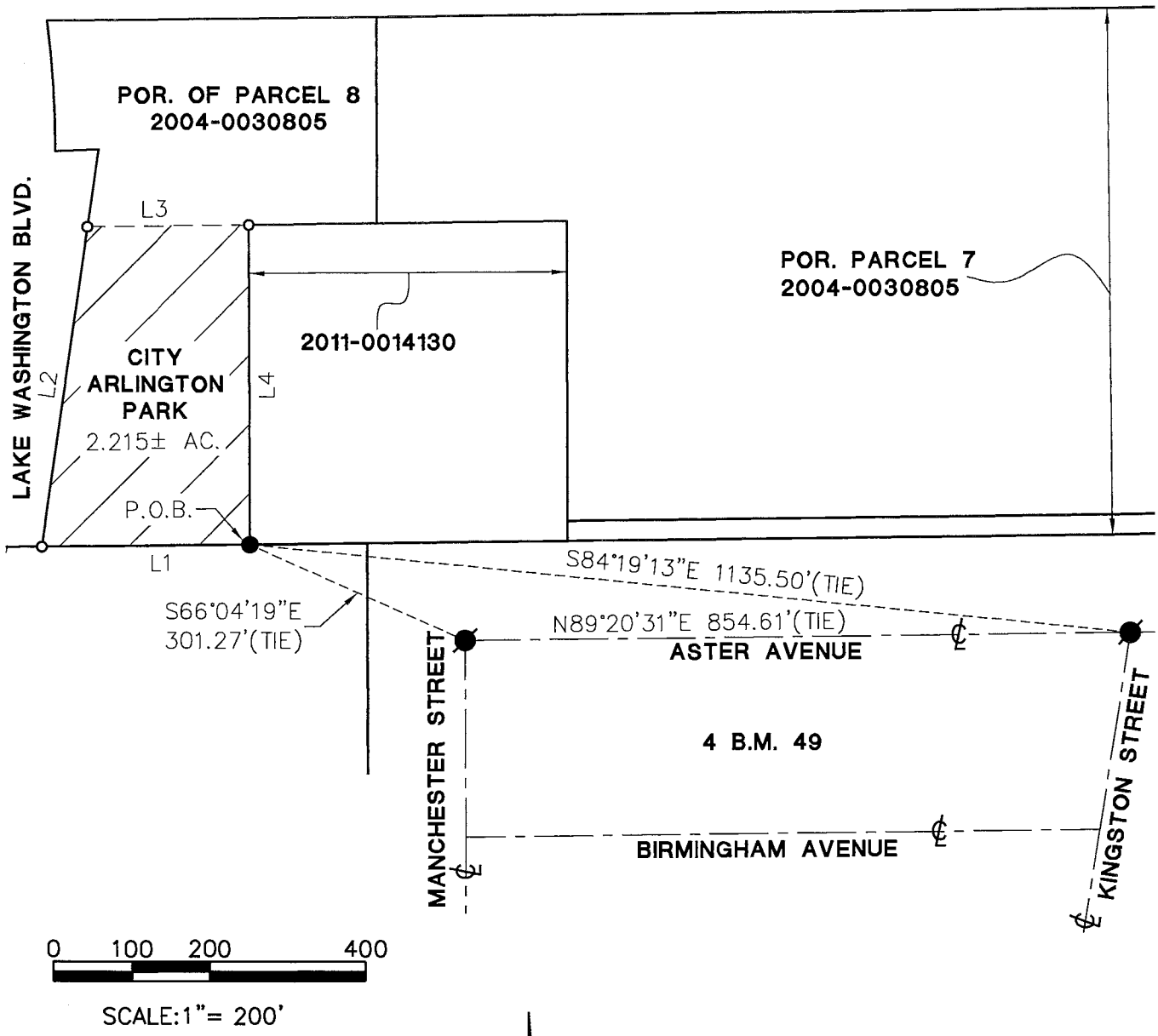
AUGUST 1, 2018 8621.004 SHEET 1 OF 2

EXHIBIT 'B'

PLAT TO ACCOMPANY
DESCRIPTION

CITY ARLINGTON PARK BOUNDARY MODIFICATION PARCEL

CITY OF WEST SACRAMENTO
COUNTY OF YOLO STATE OF CALIFORNIA



Line Table – This Sheet Only		
No.	Bearing	Length
L1	S89°22'03"W	267.09'
L2	N8°04'00"E	411.14'
L3	N89°17'49"E	207.54'
L4	S0°15'39"E	406.68'

WOOD RODGERS
BUILDING RELATIONSHIPS ONE PROJECT AT A TIME
3301 C ST, BLDG. 100-B TEL 916.341.7760
SACRAMENTO, CA 95816 FAX 916.341.7767

AUGUST 1, 2018 8621.004 SHEET 2 OF 2

NO FEE DOCUMENT
Government Code § 6103 & § 27383

RECORDING REQUESTED BY AND WHEN RECORDED MAIL TO:

Sacramento-Yolo Port District
1110 West Capitol Avenue
West Sacramento, CA 95691

(THE ABOVE SPACE FOR RECORDER'S USE ONLY)

TERMINATION OF AGREEMENT

THIS TRANSACTION IS EXEMPT FROM CALIFORNIA DOCUMENTARY TRANSFER TAX PURSUANT TO SECTION 11922 OF THE CALIFORNIA REVENUE AND TAXATION CODE.

WHEREAS, the City of West Sacramento ("City") and the Sacramento-Yolo Port District ("Port") entered into an Agreement Regarding Resolution of Existing Issues, which was executed by the City and the Port on May 25,2005, and recorded in the Official Records of Yolo County, California, on June 21, 2005, as Instrument No. 2005-0029329-00 (the "Agreement") against certain real property owned by the Port; and

WHEREAS, the Port and the City desire to terminate the Agreement and remove the Agreement from title of the real property.

NOW, THEREFORE, FOR A VALUABLE CONSIDERATION, receipt of which is hereby acknowledged,

The parties have terminated the Agreement effective as of August __, 2018, and such, the Agreement is null and void, and of no further force and effect, and neither the City nor the Port shall have further rights against the other under the Agreement.

CITY:

City of West Sacramento,
a California municipal corporation

By: _____
Aaron Laurel, City Manager

PORT:

Sacramento-Yolo Port District,
a California river port district

By: _____
Aaron Laurel, Port CEO

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

ACKNOWLEDGEMENT

State of California)
County of _____)

On _____, before me, _____, Notary Public, personally appeared _____, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument, and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

_____

Notary Public (Notary Seal)

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

ACKNOWLEDGEMENT

State of California)
County of _____)

On _____, before me, _____, Notary Public, personally appeared _____, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument, and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

_____

Notary Public (Notary Seal)

Attachment 3

Quitclaims Deeds

NO FEE DOCUMENT
Government Code § 6103 & § 27383

RECORDING REQUESTED BY AND WHEN RECORDED MAIL TO:

Sacramento-Yolo Port District
1110 West Capitol Avenue
West Sacramento, CA 95691

(THE ABOVE SPACE FOR RECORDER'S USE ONLY)

QUITCLAIM DEED

THIS TRANSACTION IS EXEMPT FROM CALIFORNIA DOCUMENTARY TRANSFER TAX PURSUANT TO SECTION 11922 OF THE CALIFORNIA REVENUE AND TAXATION CODE.

FOR A VALUABLE CONSIDERATION, receipt of which is hereby acknowledged,

City of West Sacramento, as Grantor,

hereby remises, releases and forever quitclaims to:

Sacramento-Yolo Port District, as Grantee,

all of its rights, title, and interest in the Declaration and in the following described real property in the County of Yolo, State of California, more particularly described as follows:

[SEE EXHIBIT A, LEGAL DESCRIPTION AND EXHIBIT B, PLAT, ATTACHED HERETO AND MADE PART HEREOF]

DATED: August __, 2018.

GRANTOR:

City of West Sacramento,
a California municipal corporation

By: _____
Aaron Laurel, City Manager

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

ACKNOWLEDGEMENT

State of California)
County of _____)

On _____, before me, _____, Notary Public, personally appeared _____, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument, and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

_____

Notary Public (Notary Seal)

EXHIBIT 'A'
LEGAL DESCRIPTION
PORT ARLINGTON PARK BOUNDARY MODIFICATION PARCEL

All that portion of the lands described in that certain Grant Deed recorded May 20, 2011 as Document Number 2011-0014130, Official Records of the County of Yolo, situate within the City of West Sacramento, County of Yolo, State of California, Township 8 North, Range 4 East, M.D.M., being more particularly described as follows:

BEGINNING at a point being the southeast corner of said Grant Deed, from which, a 3/4 inch iron pipe marking the westerly terminus of the centerline of Aster Avenue, as shown on that map entitled "Arlington Oaks - Unit One", filed for record in Book 4 of Maps, at Page 49, Yolo County Records, bears South 45°59'11" West, a distance of 182.78 feet and a 3/4" iron pipe marking the easterly terminus of said centerline bears South 80°47'41" East, a distance of 732.53 feet; thence from said **POINT OF BEGINNING**, along the south line of said Grant Deed, South 89°22'03" West, a distance of 106.58 feet to a point of intersection with southerly prolongation of the most easterly line of that certain sewer easement recorded as Document Number 2004-0032332, Official Records of the County of Yolo; thence leaving said south line, along said southerly prolongation and said most easterly line, respectively, North 00°19'23" West, a distance of 407.04 feet to the north line of said Grant Deed; thence along said north line, North 89°17'49" East, a distance of 107.02 feet to the northeast corner of said Grant Deed; thence along the east line of said Grant deed, South 00°15'39" East, a distance of 407.18 feet to the **POINT OF BEGINNING**.

Containing 0.998 acres, more or less.

See Ex*hibit 'B', Plat to accompany description, attached hereto and made a part hereof.*

The basis of bearings for this description is based upon the California Coordinate System, CCS 83 (2007), Zone 2. All distances cited herein are grid values, which are the basis for the areas shown hereon. To obtain ground values, multiply the distances cited herein by 1.00004540.

END OF DESCRIPTION

Dennis L. Barber, PLS 8067
August 1, 2018

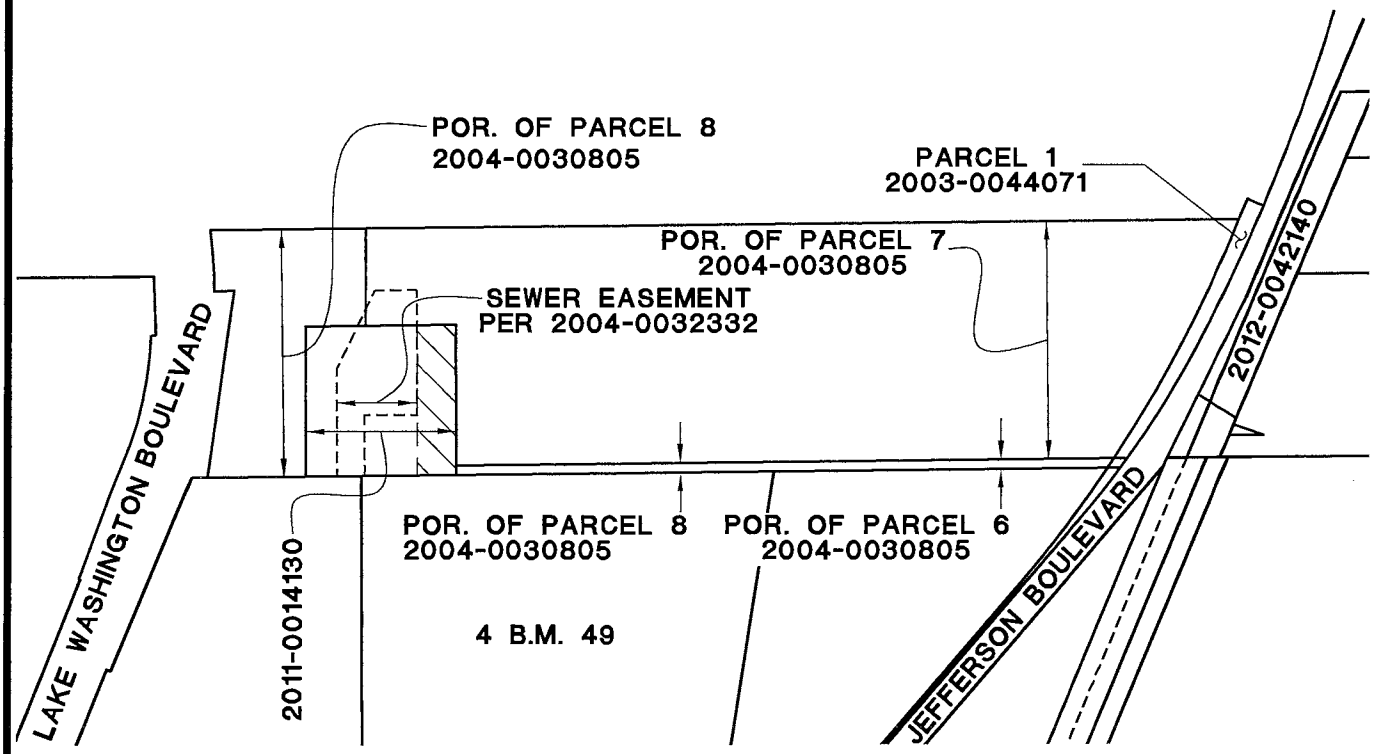
PREPARED BY WOOD-RODGERS, INC.
SACRAMENTO, CALIFORNIA

EXHIBIT 'B'

PLAT TO ACCOMPANY
DESCRIPTION

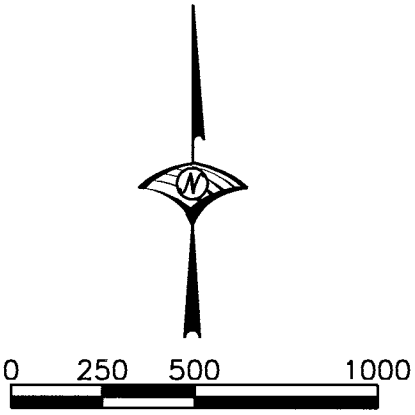
PORT ARLINGTON PARK BOUNDARY MODIFICATION PARCEL

CITY OF WEST SACRAMENTO
COUNTY OF YOLO STATE OF CALIFORNIA



LEGEND:

B.M.	BOOK OF MAPS
℄	CENTERLINE
P.O.B.	POINT OF BEGINNING
POR.	PORTION
(R)	RADIAL BEARING
●	FOUND 3/4" IRON PIPE PER 4 B.M. 49
▨	EASEMENT AREA



SCALE: 1"= 500'

WOOD RODGERS
BUILDING RELATIONSHIPS ONE PROJECT AT A TIME
3301 C ST, BLDG. 100-B TEL 916.341.7760
SACRAMENTO, CA 95816 FAX 916.341.7767

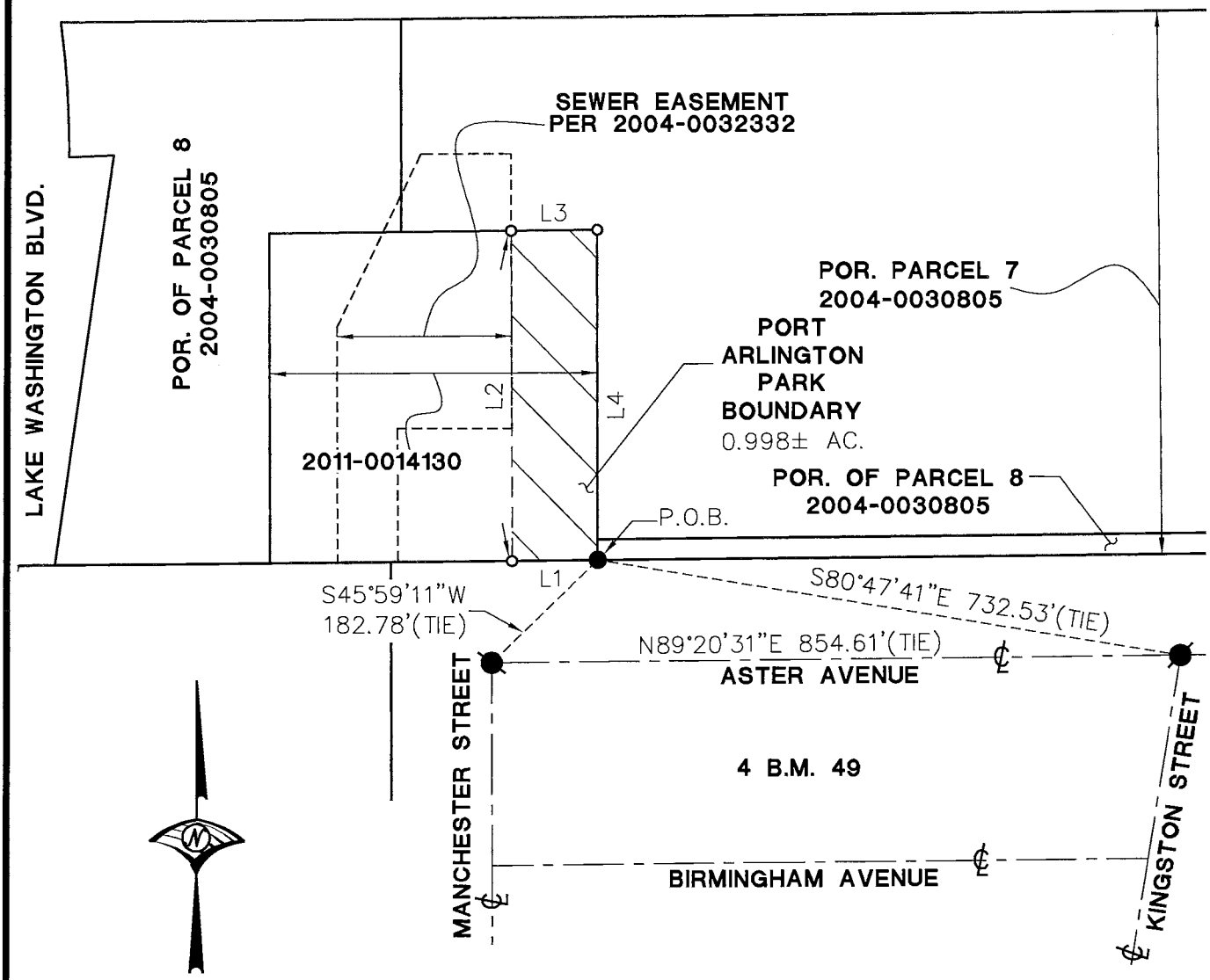
AUGUST 1, 2018 8621.004 SHEET 1 OF 2

EXHIBIT 'B'

PLAT TO ACCOMPANY DESCRIPTION

PORT ARLINGTON PARK BOUNDARY MODIFICATION PARCEL

CITY OF WEST SACRAMENTO
COUNTY OF YOLO STATE OF CALIFORNIA



0 100 200 400

SCALE: 1" = 200'

Line Table – This Sheet Only		
No.	Bearing	Length
L1	S89°22'03"W	106.58'
L2	N0°19'23"W	407.04'
L3	N89°17'49"E	107.02'
L4	S0°15'39"E	407.18'

WOOD RODGERS
BUILDING RELATIONSHIPS ONE PROJECT AT A TIME
3301 C ST, BLDG. 100-B TEL 916.341.7760
SACRAMENTO, CA 95816 FAX 916.341.7767

AUGUST 1, 2018 8621.004 SHEET 2 OF 2

NO FEE DOCUMENT
Government Code § 6103 & § 27383

RECORDING REQUESTED BY AND WHEN RECORDED MAIL TO:

Sacramento-Yolo Port District
1110 West Capitol Avenue
West Sacramento, CA 95691

(THE ABOVE SPACE FOR RECORDER'S USE ONLY)

QUITCLAIM DEED

THIS TRANSACTION IS EXEMPT FROM CALIFORNIA DOCUMENTARY TRANSFER TAX PURSUANT TO SECTION 11922 OF THE CALIFORNIA REVENUE AND TAXATION CODE.

WHEREAS, the former Redevelopment Agency of the City of West Sacramento ("Agency") granted the City of West Sacramento ("City") an Irrevocable Offer of Dedication, dated June 7, 2011, recorded in the Official Records of Yolo County, California, on June 13, 2011, as Instrument No. 2011-0016040-00 (the "IOD"), against certain real property then-owned by the Agency, which is now owned by the Sacramento-Yolo Port District ("Port"); and

WHEREAS, the IOD is for the dedication of a barge canal public access and recreation easement over certain property depicted in the IOD. Because the easement was not for the entire property over which the IOD was recorded, a legal description for the dedicated property was to be created prior to acceptance of the dedicated property. The legal description has now been created; and

WHEREAS, the Port and the City have agreed that the Port will grant the property to the City in fee by grant deed, rather than by easement, which grant deed will be recorded concurrently herewith. The City therefore desires to release and terminate any remaining rights and powers contained in the IOD.

NOW, THEREFORE, FOR A VALUABLE CONSIDERATION, receipt of which is hereby acknowledged,

City of West Sacramento, as Grantor,

hereby remises, releases and forever quitclaims to:

Sacramento-Yolo Port District, as Grantee,

all of its rights, title, and interest in the Declaration and in the following described real property in the County of Yolo, State of California, legally described as:

[**See Exhibit A**]

(the "Property") for the purpose of releasing and terminating any and all rights and powers it has with regard to said Property described in the IOD.

DATED: August __, 2018.

GRANTOR:

City of West Sacramento,
a California municipal corporation

By: _____
Aaron Laurel, City Manager

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

ACKNOWLEDGEMENT

State of California)
County of _____)

On _____, before me, _____, Notary Public, personally appeared _____, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument, and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

_____

Notary Public (Notary Seal)

Exhibit A

(Legal Description)

EXHIBIT "A"
LEGAL DESCRIPTION

THE LAND DESCRIBED HEREIN IS SITUATED IN THE STATE OF CALIFORNIA, COUNTY OF YOLO, CITY OF WEST SACRAMENTO, AND IS DESCRIBED AS FOLLOWS:

BEING A PORTION OF THAT DEED RECORDED IN BOOK 301, PAGE 320 DESCRIBED AS FOLLOWS:

BEGINNING AT A GRANITE MONUMENT MARKING THE SOUTHWEST CORNEROF SWAMP LAND SURVEY NO. 261, YOLO COUNTY SURVEYS, AND RUNNING THENCE FROM SAID POINT OF BEGINNING SOUTH 89 DEGREES, 49' 10" WEST 124.91 FEET; THENCE NORTH 00 DEGREES, 03' 00" EAST 638.70 FEET TO THE CENTERLINE OF THE SACRAMENTO-YOLO PORT DISTRICT BARGE CANAL AS DEFINED IN BOOK 545, PAGE 427 OFFICIAL RECORDS; THENCE ALONG SAID CENTERLINE, NORTH 89 DEGREES, 38' 08" EAST 2464.33 FEET TO THE WESTERLY RIGHT OF WAY LINE OF THE SACRAMENTO NORTHERN R.R.; THENCE ALONG SAID WESTERLY RIGHT OF WAY LINE, SOUTH 23 DEGREES, 04' 40" WEST 703.80 FEET; THENCE LEAVING SAID WESTERLY RIGHT OF WAY LINE, SOUTH 89 DEGREES, 49' 10" WEST 2064.78 FEET TO THE POINT OF BEGINNING.

EXCEPTING THEREFROM THAT PORTION CONVEYED TO THE CITY OF WEST SACRAMENTO IN THE GRANT DEED RECORDED ON JULY 25, 2003, AS INSTRUMENT NO. 2003-0044071, OFFICIAL RECORDS.

ASSESSOR'S PARCEL NUMBER: 067-180-036

PRE.LEGAL

NO FEE DOCUMENT
Government Code § 6103 & § 27383

RECORDING REQUESTED BY AND WHEN RECORDED MAIL TO:

Sacramento-Yolo Port District
1110 West Capitol Avenue
West Sacramento, CA 95691

(THE ABOVE SPACE FOR RECORDER'S USE ONLY)

QUITCLAIM DEED

THIS TRANSACTION IS EXEMPT FROM CALIFORNIA DOCUMENTARY TRANSFER TAX PURSUANT TO SECTION 11922 OF THE CALIFORNIA REVENUE AND TAXATION CODE.

WHEREAS, the former Redevelopment Agency of the City of West Sacramento ("Agency") granted the City of West Sacramento ("City") an Irrevocable Offer of Dedication, dated June 7, 2011, recorded in the Official Records of Yolo County, California, on June 13, 2011, as Instrument No. 2011-0016042-00 (the "IOD"), against certain real property then-owned by the Agency, which is now owned by the Sacramento-Yolo Port District ("Port"); and

WHEREAS, the IOD is for the dedication of a public utility easement for a booster pump station over certain property depicted in the IOD. Because the easement was not for the entire property over which the IOD was recorded, a legal description for the dedicated property was to be created prior to acceptance of the dedicated property. The legal description has now been created; and

WHEREAS, the Port and the City have agreed that the Port will grant the property to the City in fee by grant deed, rather than by easement, which grant deed will be recorded concurrently herewith. The City therefore desires to release and terminate any remaining rights and powers contained in the IOD.

NOW, THEREFORE, FOR A VALUABLE CONSIDERATION, receipt of which is hereby acknowledged,

City of West Sacramento, as Grantor,

hereby remises, releases and forever quitclaims to:

Sacramento-Yolo Port District, as Grantee,

all of its rights, title, and interest in the Declaration and in the following described real property in the County of Yolo, State of California, legally described as:

[**See Exhibit A**]

(the "Property") for the purpose of releasing and terminating any and all rights and powers it has with regard to said Property described in the IOD.

DATED: August __, 2018.

GRANTOR:

City of West Sacramento,
a California municipal corporation

By: _____
Aaron Laurel, City Manager

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

ACKNOWLEDGEMENT

State of California)
County of _____)

On _____, before me, _____, Notary Public, personally appeared _____, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument, and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

_____

Notary Public (Notary Seal)

Exhibit A

(Legal Description)

EXHIBIT "A"
LEGAL DESCRIPTION

BEING A PORTION OF THOSE DEEDS RECORDED IN BOOK 301, PAGE 185 AND 189 AS DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT AT THE CENTER COMMON TO SWAMP LAND SURVEYS NOS. 261, 275 AND 797, YOLO COUNTY SURVEYS, FROM WHICH SAID POINT OF BEGINNING AN OLD GRANITE MONUMENT MARKING THE SOUTHWEST CORNER OF SWAMP LAND SURVEY NO. 261, YOLO COUNTY SURVEYS BEARS SOUTH 89 DEGREES, 49' 10" WEST 996.38 FEET; THENCE FROM SAID POINT OF BEGINNING SOUTH 09 DEGREES, 13' 00" WEST 38.09 FEET; THENCE SOUTH 89 DEGREES, 38' 11" WEST 1546.68 FEET TO THE EASTERLY RIGHT OF WAY LINE OF LAKE WASHINGTON BOULEVARD; THENCE ALONG SAID EASTERLY RIGHT OF WAY LINE NORTH 08 DEGREES, 21' 39" EAST 511.04 FEET; THENCE SOUTH 87 DEGREES, 28' 23" WEST 54.90 FEET; THENCE ALONG A CURVE TO THE LEFT HAVING A RADIUS OF 1314.50 FEET, A DELTA OF 07 DEGREES, 17' 26" AN ARC LENGTH OF 167.26 FEET, SAID CURVE BEING SUBTENDED BY A CHORD BEARING NORTH 03 DEGREES, 25' 57" WEST TO A POINT ON THE CENTERLINE OF THE SACRAMENTO-YOLO PORT DISTRICT BARGE CANAL AS DEFINED IN BOOK 545, PAGE 427 OFFICIAL RECORDS; THENCE ALONG SAID CENTERLINE, NORTH 89 DEGREES, 38' 08" EAST 421.67 FEET; THENCE LEAVING SAID CENTERLINE, SOUTH 00 DEGREES, 03' 00" WEST 638.70 FEET; THENCE NORTH 89 DEGREES, 49' 10" EAST 1120.59 FEET TO THE POINT OF BEGINNING.

ASSESSOR'S PARCEL NUMBER: 067-180-007,-008 AND 024

NO FEE DOCUMENT
Government Code § 6103 & § 27383

RECORDING REQUESTED BY AND WHEN RECORDED MAIL TO:

Sacramento-Yolo Port District
1110 West Capitol Avenue
West Sacramento, CA 95691

(THE ABOVE SPACE FOR RECORDER'S USE ONLY)

QUITCLAIM DEED

THIS TRANSACTION IS EXEMPT FROM CALIFORNIA DOCUMENTARY TRANSFER TAX PURSUANT TO SECTION 11922 OF THE CALIFORNIA REVENUE AND TAXATION CODE.

WHEREAS, the former Redevelopment Agency of the City of West Sacramento ("Agency") granted the City of West Sacramento ("City") an Irrevocable Offer of Dedication, dated June 14, 2011, recorded in the Official Records of Yolo County, California, on June 14, 2011, as Instrument No. 2011-0016292-00 (the "IOD"), against certain real property then-owned by the Agency, which is now owned by the Sacramento-Yolo Port District ("Port"); and

WHEREAS, the IOD is for the dedication of a flood protection and recreation easement for a future flood control project over certain property depicted in the IOD. Because the easement was not for the entire property over which the IOD was recorded, a legal description for the dedicated property was to be created prior to acceptance of the dedicated property. The legal description has now been created; and

WHEREAS, the Port and the City have agreed that the Port will grant the property to the City in fee by grant deed, rather than by easement, which grant deed will be recorded concurrently herewith. The City therefore desires to release and terminate any remaining rights and powers contained in the IOD.

NOW, THEREFORE, FOR A VALUABLE CONSIDERATION, receipt of which is hereby acknowledged,

City of West Sacramento, as Grantor,

hereby remises, releases and forever quitclaims to:

Sacramento-Yolo Port District, as Grantee,

all of its rights, title, and interest in the Declaration and in the following described real property in the County of Yolo, State of California, legally described as:

[**See Exhibit A**]

(the "Property") for the purpose of releasing and terminating any and all rights and powers it has with regard to said Property described in the IOD.

DATED: August __, 2018.

GRANTOR:

City of West Sacramento,
a California municipal corporation

By: _____
Aaron Laurel, City Manager

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

ACKNOWLEDGEMENT

State of California)
County of _____)

On _____, before me, _____, Notary Public, personally appeared _____, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument, and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

_____

Notary Public (Notary Seal)

Exhibit A

(Legal Description)

EXHIBIT "A"
LEGAL DESCRIPTION

THE LAND DESCRIBED HEREIN IS SITUATED IN THE STATE OF CALIFORNIA, COUNTY OF YOLO, CITY OF WEST SACRAMENTO, AND IS DESCRIBED AS FOLLOWS:

PARCEL ONE:

BEING A PORTION OF THAT DEED RECORDED IN BOOK 301, PAGE 320 DESCRIBED AS FOLLOWS:

BEGINNING AT A GRANITE MONUMENT MARKING THE SOUTHWEST CORNEROF SWAMP LAND SURVEY NO. 261, YOLO COUNTY SURVEYS, AND RUNNING THENCE FROM SAID POINT OF BEGINNING SOUTH 89 DEGREES, 49' 10" WEST 124.91 FEET; THENCE NORTH 00 DEGREES, 03' 00" EAST 638.70 FEET TO THE CENTERLINE OF THE SACRAMENTO-YOLO PORT DISTRICT BARGE CANAL AS DEFINED IN BOOK 545, PAGE 427 OFFICIAL RECORDS; THENCE ALONG SAID CENTERLINE, NORTH 89 DEGREES, 38' 08" EAST 2464.33 FEET TO THE WESTERLY RIGHT OF WAY LINE OF THE SACRAMENTO NORTHERN R.R.; THENCE ALONG SAID WESTERLY RIGHT OF WAY LINE, SOUTH 23 DEGREES, 04' 40" WEST 703.80 FEET; THENCE LEAVING SAID WESTERLY RIGHT OF WAY LINE, SOUTH 89 DEGREES, 49' 10" WEST 2064.78 FEET TO THE POINT OF BEGINNING.

EXCEPTING THEREFROM THAT PORTION CONVEYED TO THE CITY OF WEST SACRAMENTO IN THE GRANT DEED RECORDED ON JULY 25, 2003, AS INSTRUMENT NO. 2003-0044071, OFFICIAL RECORDS.

ASSESSOR'S PARCEL NUMBER: 067-180-036

PARCEL TWO:

BEING A PORTION OF THOSE DEEDS RECORDED IN BOOK 301, PAGE 185 AND 189,

EXHIBIT "A"
LEGAL DESCRIPTION continued

DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT AT THE CENTER COMMON TO SWAMP LAND SURVEYS NOS. 261, 275 AND 797, YOLO COUNTY SURVEYS, FROM WHICH SAID POINT OF BEGINNING AN OLD GRANITE MONUMENT MARKING THE SOUTHWEST CORNER OF SWAMP LAND SURVEY NO. 261, YOLO COUNTY SURVEYS BEARS SOUTH 89 DEGREES, 49' 10" WEST 996.38 FEET; THENCE FROM SAID POINT OF BEGINNING SOUTH 09 DEGREES, 13' 00" WEST 38.09 FEET; THENCE SOUTH 89 DEGREES, 38' 11" WEST 1546.68 FEET TO THE EASTERLY RIGHT OF WAY LINE OF LAKE WASHINGTON BOULEVARD; THENCE ALONG SAID EASTERLY RIGHT OF WAY LINE NORTH 08 DEGREES, 21' 39" EAST 511.04 FEET; THENCE SOUTH 87 DEGREES, 28' 23" WEST 54.90 FEET; THENCE ALONG A CURVE TO THE LEFT HAVING A RADIUS OF 1314.50 FEET, A DELTA OF 07 DEGREES, 17' 26" AN ARC LENGTH OF 167.26 FEET, SAID CURVE BEING SUBTENDED BY A CHORD BEARING NORTH 03 DEGREES, 25' 57" WEST TO A POINT ON THE CENTERLINE OF THE SACRAMENTO-YOLO PORT DISTRICT BARGE CANAL AS DEFINED IN BOOK 545, PAGE 427 OFFICIAL RECORDS; THENCE ALONG SAID CENTERLINE, NORTH 89 DEGREES, 38' 08" EAST 421.67 FEET; THENCE LEAVING SAID CENTERLINE, SOUTH 00 DEGREES, 03' 00" WEST 638.70 FEET; THENCE NORTH 89 DEGREES, 49' 10" EAST 1120.59 FEET TO THE POINT OF BEGINNING.

ASSESSOR'S PARCEL NUMBER: 067-180-007, 008 AND 024

EXHIBIT "A"
LEGAL DESCRIPTION

THE LAND DESCRIBED HEREIN IS SITUATED IN THE STATE OF CALIFORNIA, COUNTY OF YOLO, CITY OF WEST SACRAMENTO, AND IS DESCRIBED AS FOLLOWS:

PARCEL ONE:

PORTION OF THE SOUTHWEST ONE QURTER OF SECTION 3 AND THE NORTHWEST ONE QUARTER OF SECTION 10, T. 8 N., R. 4 E., M.D.B. & M., AND BEING BOUNDED AND MORE PARTICULARLY DESCRIBED AS FOLLOWS, TO WIT:

BEGINNING AT A POINT ON THE WESTERLY BANK OF THE SACRAMENTO RIVER, FROM WHICH SAID POINT OF BEGINNING AN IRON PIPE MARKED "R. E. 53", MARKING THE SOUTHEAST CORNER OF SWAMP LAND SURVEY NO. 571, YOLO COUNTY SURVEYS, BEARS S. 11 DEGREES 08' 50" E. 92.71 FEET AND S. 11 DEGREES 53' 50" E. 501.60 FEET AND RUNNING THENCE FROM SAID POINT OF BEGINNING N. 75 DEGREES 21' 50" W. 346.74 FEET; THENCE S. 89 DEGREES 38' 10" W. 2160.52 FEET TO A POINT IN A NORTHERLY AND SOUTHERLY FENCE LINE; THENCE FOLLOWING SAID NORTHERLY AND SOUTHERLY FENCE LINE N. 25 DEGREES 12' 23" E. 514.11 FEET TO A FENCE CORNER; THENCE FOLLOWING AN EASTERLY AND WESTERLY FENCE LINE AND THE PROLONGATION EASTERLY THEREOF, S. 80 DEGREES 05' 27" E. 2282.24 FEET TO A POINT ON THE WESTERLY BANK OF THE SACRAMENTO RIVER; THENCE FOLLOWING THE SAID WESTERLY BANK OF THE SACRAMENTO RIVER DOWNSTREAM, S. 11 DEGREES 08' 50" E. 149.12 FEET TO THE POINT OF BEGINNING.

ASSESSOR'S PARCEL NUMBER: PORTION OF 067-180-003

PARCEL TWO:

A PORTION OF PROJECTED SECTIONS 3 AND 10, TOWNSHIP 8 NORTH, RANGE 4 EAST, M.D.B. & M., DESCRIBED AS FOLLOWS:

BEGINNING AT AN IRON PIPE MONUMENT SET IN A FENCE CORNER AND STAMPED RE 1168, FROM WHICH AN OLD GRANITE MONUMENT MARKING THE SOUTHWEST CORNER OF SWAMP LAND SURVEY NO. 261 BEARS NORTH 80 DEGREES 23' 15" WEST 208.33 FEET; THENCE NORTH 00 DEGREES 56' 50" WEST 92.45 FEET AND THENCE SOUTH 89 DEGREES 49' 10" WEST 2653.87 FEET; THENCE FROM SAID POINT OF BEGINNING NORTH 25 DEGREES 12' 23" EAST 217.44 FEET ALONG A FENCE LINE; THENCE NORTH 89 DEGREES 38' 10" EAST 2160.52 FEET; THENCE SOUTH 75 DEGREES 21' 50" EAST 346.74 FEET TO A POINT ON THE MEAN HIGH WATER LINE ON THE RIGHT BANK OF THE SACRAMENT RIVER; THENCE DOWNSTREAM ALONG SAID MEAN HIGH WATER LINE, FOLLOWING THE MENADERINGS THEREOF, TWO COURSES AND DISTANCES AS FOLLOWS: SOUTH 45 DEGREES 29' 20" EAST 198.35 FEET AND SOUTH 19 DEGREES 48' EAST 505.00 FEET; THENCE LEAVING SAID MEAN HIGH WATER LINE NORTH 80 DEGREES 23' 15" WEST 193.94 FEET TO AN IRON PIPE MONUMENT MARKED RE 53 AND SET IN A FENCE LINE; THENCE CONTINUING NORTH 80 DEGREES 23' 15" WEST 2748.49 FEET ALONG A FENCE LINE TO THE POINT OF BEGINNING.

ASSESSOR'S PARCEL NUMBER: A PORTION OF 067-180-002 AND 003

EXHIBIT "A"
LEGAL DESCRIPTION continued

PARCEL THREE:

A PORTION OF THE SOUTHWEST ONE QUARTER OF SECTION 3, THE SOUTHEAST ONE QUARTER OF SECTION 4, AND THE NORTHWEST ONE QUARTER OF SECTION 10, T. 8N., R. 4 E., M.D.B. & M., AND BEING BOUNDED AND MORE PARTICULARLY DESCRIBED AS FOLLOWS, TO WIT:

BEGINNING AT AN OIL IRON PIPE MARKING THE CORNER COMMON TO THE HEREIN DESCRIBED PROPERTY AND THE PROPERTY NOW OR FORMERLY OWNED BY A.F. TURNER, FROM WHICH SAID POINT OF BEGINNING AN OLD GRANITE MONUMENT MARKING THE SOUTHWEST CORNER OF SWAMP LAND SURVEY NO. 261, YOLO COUNTY SURVEYS, BEARS S. 72 DEGREES 10' 15" W. 2879.60 FEET, AND RUNNING THENCE FROM SAID POINT OF BEGINNING ALONG THE BOUNDARY LINE COMMON TO THE HEREIN DESCRIBED PROPERTY AND THE SAID PROPERTY OF A.F. TURNER, S. 86 DEGREES 51' 04" E. 2225.00 FEET TO A POINT ON THE WESTERLY BANK OF THE SACRAMENTO RIVER; THENCE FOLLOWING THE SAID WESTERLY BANK OF THE SACRAMENTO RIVER DOWNSTREAM, S. 38 DEGREES 02' 10" E. 778.34 FEET; THENCE FOLLOWING A FENCE LINE AND THE PROLONGATION EASTERLY AND WESTERLY THEREOF, N. 80 DEGREES 05' 27" W. 2733.02 FEET; THENCE S. 02 DEGREES 08' 50" E. 721.99 FEET; THENCE NO. 80 DEGREES 23' 15" W. 123.80 FEET; THENCE E. 0 DEGREES 56' 50" W. 92.45 FEET; THENCE S. 89 DEGREES 49' 10" W. 444.87 FEET TO A POINT IN THE EASTERLY RIGHT OF WAY LINE OF THE SACRAMENTO NORTHERN RAILROAD; THENCE FOLLOWING THE SAID EASTERLY RIGHT OF WAY LINE OF THE SACRAMENTO NORTHERN RAILROAD, N. 23 DEGREES 04' 40" E. 1074.89 FEET; THENCE N. 89 DEGREES 38' 10" E. 105.89 FEET; THENCE ALONG THE BOUNDARY LINE COMMON TO THE HEREIN DESCRIBED PROPERTY AND THE AFORESAID PROPERTY OF A. F. TURNER, S. 2 DEGREES 36' 15" E. 114.95 FEET TO THE POINT OF BEGINNING.

ASSESSOR'S PARCEL NUMBER: A PORTION OF 067-180-003 AND 004

PARCEL FOUR:

A PORTION OF THE SOUTHWEST ONE-QUARTER OF SECTION 3, AND THE NORTHWEST ONE-QUARTER OF SECTION 10, TOWNSHIP 8 NORTH, RANGE 4 EAST, M.D.B. & M. , AND BEING BOUNDED AND MORE PARTICULARLY DESCRIBED AS FOLLOWS, TO WIT:

BEGINNING AT A POINT AT THE CORNER COMMON TO THE HEREIN DESCRIBED PROPERTY AND THE PROPERTY NOW OR FORMERLY OWNED BY LAUREN E. VAN TASSEL, ET AL, FROM WHICH SAID POINT OF BEGINNING AN IRON PIPE MARKED "R. E. 53", MARKING THE SOUTHEAST CORNER OF SWAMP LAND SURVEY NO. 571, YOLO COUNTY SURVYES, BEARS SOUTH 80 DEGREES 23' 15" EAST, 2748.49 FEET, AND RUNNING THENCE FROM SAID POINT OF BEGINNING NORTH 80 DEGREES 23' 15" WEST, 84.53 FEET; THENCE NORTH 02 DEGREES 08' 05" WEST, 721.99 FEET; THENCE SOUTH 80 DEGREES 05' 27" EAST, 428.33 FEET TO A POINT AT A FENCE CORNER; THENCE FOLLOWING A NORTHERLY AND SOUTHERLY FENCE LINE SOUTH 25 DEGREES 12' 23" WEST, 731.55 FEET TO THE POINT OF BEGINNING.

ALL AS MODIFIED BY THE BOUNDARY LINE AGREEMENT EXECUTED BY SACRAMENTO-YOLO PORT DISTRICT AND BETWEEN GEORGE P. PARIS, RECORDED MARCH 13, 1950, IN BOOK 315, PAGE 344, OFFICIAL RECORDS.

EXCEPTING THEREFROM THAT PORTION CONVEYED TO THE CITY OF WEST SACRAMENTO BY

EXHIBIT "A"
LEGAL DESCRIPTION continued

DOCUMENT RECORDED NOVEMBER 17, 2003, AS INSTRUMENT NO. 2003-0069853, OFFICIAL RECORDS.

ASSESSOR'S PARCEL NUMBER: A PORTION OF 067-180-002 AND 003

PARCEL FIVE:

BEING A PORTION OF THE SOUTHWEST ONE QUARTER OF SECTION 3, TOWNSHIP 8 NORTH, RANGE 4 EAST, M.D.B. & M., AND BEING BOUNDED AND MORE PARTICULARLY DESCRIBED AS FOLLOWS, TO-WIT:

BEGINNING AT AN OLD IRON PIPE MARKING THE CORNER COMMON TO THE HEREIN DESCRIBED PROPERTY AND THE PROPERTY NOW OR FORMERLY OWNED BY CHARLES PARELLA, FROM WHICH SAID POINT OF BEGINNING AN OLD GRANITE MONUMENT MARKING THE SOUTHWEST CORNER OF SWAMP LAND SURVEY NO. 261, BEARS SOUTH 72 DEGREES, 10' 15" WEST, 2879.60 FEET; THENCE FROM SAID POINT OF BEGINNING NORTH 02 DEGREES, 36' 15" WEST 114.95 FEET ALONG THE BOUNDARY LINE OF THE HEREIN DESCRIBED PROPERTY AND THE PROPERTY NOW OR FORMERLY OWNED BY CHARLES PARELLA; THENCE NORTH 89 DEGREES, 38' 10" EAST 1,478.61 FEET; THENCE NORTH 00 DEGREES, 21' 50" WEST 500.00 FEET; THENCE NORTH 89 DEGREES, 38' 10" EAST 222.00 FEET TO THE MEAN HIGH WATER LINE ON THE RIGHT BANK OF THE SACRAMENTO RIVER; THENCE DOWNSTREAM ALONG SAID MEAN HIGH WATER LINE, FOLLOWING THE MEANDERINGS THEREOF, FOUR COURSES AND DISTANCES AS FOLLOWS:

SOUTH 28 DEGREES, 24' EAST 193.00 FEET, SOUTH 32 DEGREES, 14' EAST 305.00 FEET, SOUTH 38 DEGREES, 30' EAST 295.00 FEET AND SOUTH 45 DEGREES, 40' 45" EAST 127.68 FEET; THENCE, LEAVING SAID MEAN HIGH WATER LINE, NORTH 86 DEGREES, 511' 04" WEST, 2,225.00 FEET TO THE POINT OF BEGINNING.

EXCEPTING FROM PARCEL FIVE, THE FOLLOWING 4 PARCELS:

PARCEL 1:

BEGINNING AT AN IRON PIPE MONUMENT MARKING THE NORTHWESTERLY CORNER OF THE WEST SACRAMENTO SANITARY DISTRICT TREATMENT PLANT SITE AS OCCUPIED, SAID POINT OF BEGINNING BEING SITUATE NORTH 89 DEGREES 33'05" EAST, 1305.45 FEET; AND THENCE NORTH 0 DEGREE 21'50" WEST 164.20 FEET FROM THE POINT OF INTERSECTION OF THE CENTERLINE OF STATE STREET PRODUCED EASTERLY TO THE POINT OF INTERSECTION WITH THE EASTERLY LINE OF THE RIGHT OF WAY OF THE SACRAMENTO NORTHERN RAILWAY COMPANY, AS SAID STATE STREET AND RIGHT OF WAY APPEAR OF RECORD IN THE OFFICE OF THE RECORDER OF YOLO COUNTY, CALIFORNIA, ON THE "MAP OF WEST SACRAMENTO CITY- UNIT ONE" FILED IN MAP BOOK 3 PAGES 8 TO 14 INCLUSIVE, YOLO COUNTY RECORDS; AND EXTENDING THENCE FROM SAID POINT OF BEGINNING SOUTH 0 DEGREE 21'50" EAST ALONG THE WESTERLY LINE OF SAID WEST SACRAMENTO SANITARY DISTRICT TREATMENT PLANT SITE AS FENCED AND OCCUPIED A DISTANCE OF 520.00 FEET TO AN IRON PIPE MONUMENT MARKING THE SOUTHWESTERLY CORNER OF SAID TREATMENT PLANT SITE AS NOW OCCUPIED; THENCE SOUTH 89 DEGREES 38'10" WEST ALONG THE NORTHERLY BOUNDARY OF THE SACRAMENTO YOLO PORT DISTRICT PROPERTY A DISTANCE OF 167.54 FEET; THENCE NORTH 0 DEGREES 21'50" WEST ALONG A LINE PARALLEL WITH THE FIRST MENTIONED COURSE OF THE DESCRIPTION A DISTANCE OF

EXHIBIT "A"
LEGAL DESCRIPTION continued

520.00 FEET; THENCE NORTH 89 DEGREES 38'10" EAST 167.54 FEET TO THE POINT OF BEGINNING.

PARCEL 2:

THAT PORTION OF YOLO COUNTY SWAMP AND OVERFLOW LAND SURVEY #571 DESCRIBED AS FOLLOWS:

BEGINNING AT AN IRON PIPE MONUMENT MARKED R.E. 1168 AND BEING SITUATE NORTH 89 DEGREES 33'05" EAST 1467.10 FEET; THENCE NORTH 0 DEGREE 21'50" WEST, 144.20 FEET FROM THE POINT OF INTERSECTION OF THE CENTER LINE OF STATE STREET PRODUCED EASTERLY TO THE POINT OF INTERSECTION WITH THE EASTERLY LINE OF THE RIGHT OF WAY OF THE SACRAMENTO NORTHERN RAILWAY COMPANY AS SAID STATE STREET AND RIGHT OF WAY APPEAR OF RECORD IN THE OFFICE OF THE RECORDER OF YOLO COUNTY, CALIFORNIA, ON THE "MAP OF WEST SACRAMENTO CITY UNIT ONE", FILED IN MAP BOOK 3, PAGES 8 TO 14, INCLUSIVE YOLO COUNTY RECORDS, AND EXTENDING THENCE NORTH 89 DEGREES 38'10" EAST ALONG THE NORTHERLY BOUNDARY LINE OF THE TRACT REQUIRED BY THE SACRAMENTO-YOLO PORT DISTRICT; A DISTANCE OF 153.00 FEET TO AN IRON PIPE MONUMENT MARKED R.E. 1168 AND SET IN THE WESTERLY HIGH BANK LINE OF THE SACRAMENTO RIVER; THENCE NORTH 27 DEGREES 10' WEST UP AND ALONG THE SAID WEST BANK OF THE SAID SACRAMENTO RIVER A DISTANCE OF 22.41 FEET; THENCE SOUTH 89 DEGREES 38'10" WEST A DISTANCE OF 304.55 FEET; THENCE SOUTH 0 DEGREE 21'50" EAST 520.00 FEET TO A POINT IN THE NORTHERLY LINE OF THE TRACT REQUIRED BY THE SACRAMENTO-YOLO PORT DISTRICT; THENCE NORTH 89 DEGREES 38'10" EAST ALONG THE SAID BOUNDARY LINE A DISTANCE OF 161.65 FEET TO AN IRON PIPE MONUMENT MARKED R.E. 1168; THENCE UP AND ALONG THE BOUNDARY LINE OF THE SAID TRACT REQUIRED BY THE SAID PORT DISTRICT NORTH 0 DEGREES 21'50" WEST 500.00 FEET TO THE POINT OF BEGINNING.

PARCEL 3:

BEGINNING AT A POINT ON THE NORTHERLY BOUNDARY LINE OF THAT CERTAIN PARCEL OF LAND CONVEYED IN EASEMENT TO THE UNITED STATES OF AMERICA, RECORDED JULY 2, 1958, IN BOOK 545, OFFICIAL RECORDS, PAGE 432, YOLO COUNTY RECORDS; SAID POINT ALSO BEING ON THE SOUTHERLY BOUNDARY OF THE PROPERTY OF THE WEST SACRAMENTO SANITARY DISTRICT, AS SAID PROPERTY NOW EXISTS; SAID POINT OF BEGINNING BEARING SOUTH 89 DEGREES 20'11" WEST 126.13 FEET FROM THE SOUTHEASTERLY PROPERTY CORNER OF THE SAID SANITARY DISTRICT; SAID CORNER ALSO BEING COMMON TO THE AFOREMENTIONED PARCEL CONVEYED TO THE UNITED STATES OF AMERICA; COORDINATES OF SAID POINT OF BEGINNING BEING NORTH 326,534.85 EAST, 2,136,837.15; THENCE FROM SAID POINT OF BEGINNING SOUTH 89 DEGREES 20'11" WEST 201.17 FEET; THENCE SOUTH 0 DEGREE 39'49" WEST 201.17 FEET; THENCE NORTH 89 DEGREES 20'11" EAST, 22.00 FEET; THENCE ALONG A CURVE OF 550 FEET RADIUS AND CURVING TO THE LEFT AN ARC DISTANCE OF 182.50 FEET; SAID ARC BEING SUBTENDED BY A CHORD BEARING NORTH 79 DEGREE 49'49" EAST 181.67 FEET TO THE POINT OF BEGINNING.

PARCEL 4:

BEGINNING AT A POINT ON THE BOUNDARY LINE OF THAT CERTAIN PARCEL OF LAND

EXHIBIT "A"
LEGAL DESCRIPTION continued

CONVEYED IN EASEMENT TO THE UNITED STATES OF AMERICA BY THE SACRAMENTO-YOLO PORT DISTRICT, RECORDED JULY 2, 1958, IN BOOK 545 OF OFFICIAL RECORDS PAGE 432, YOLO COUNTY RECORDS; SAID POINT ALSO BEING ON THE EASTERLY BOUNDARY OF THE PROPERTY OF THE WEST SACRAMENTO SANITARY DISTRICT, AS SAID PROPERTY NOW EXISTS; SAID POINT OF BEGINNING BEARS NORTH 0 DEGREE 39'49" WEST 62.52 FEET FROM THE SOUTHEASTERLY PROPERTY CORNER OF THE SAID SANITARY DISTRICT; SAID CORNER ALSO BEING COMMON TO THE AFOREMENTIONED PARCEL CONVEYED TO THE UNITED STATES OF AMERICA; COORDINATES OF SAID POINT OF BEGINNING BEING NORTH 326,598.83; EAST 2,136,962.55; THENCE FROM SAID POINT OF BEGINNING NORTH 0 DEGREE 39'49" WEST 437.48 FEET; THENCE SOUTH 33 DEGREE 10'08" EAST 310.62 FEET; THENCE ALONG A CURVE OF 550 FEET RADIUS AND CURVING TO THE RIGHT AN ARC DISTANCE OF 244.22 FEET; SAID ARC BEING SUBTENDED BY A CHORD BEARING SOUTH 42 DEGREES 53'51" WEST, 242.00 FEET TO THE POINT OF BEGINNING.

ASSESSOR'S PARCEL NUMBER: A PORTION OF 067-180-001 AND 004

NO FEE DOCUMENT
Government Code § 6103 & § 27383

RECORDING REQUESTED BY AND WHEN RECORDED MAIL TO:

Sacramento-Yolo Port District
1110 West Capitol Avenue
West Sacramento, CA 95691

(THE ABOVE SPACE FOR RECORDER'S USE ONLY)

QUITCLAIM DEED

THIS TRANSACTION IS EXEMPT FROM CALIFORNIA DOCUMENTARY TRANSFER TAX PURSUANT TO SECTION 11922 OF THE CALIFORNIA REVENUE AND TAXATION CODE.

WHEREAS, the former Redevelopment Agency of the City of West Sacramento ("Agency") created and recorded a Declaration of Restrictive Covenants, which was executed by the Agency on March 7, 2011, and recorded in the Official Records of Yolo County, California, on March 7, 2011, as Instrument No. 2011-0006724-00 (the "Declaration") against certain real property owned by the Agency;

WHEREAS, the Declaration states that the covenants are for the benefit of all parcels of land owned by the Agency and the City that are located within the boundaries of the Agency and the City, and that the City is a third-party beneficiary of the Declaration;

WHEREAS, the Agency has been dissolved and the only remaining beneficiary of the Declaration is the City;

WHEREAS, the Sacramento-Yolo Port District ("Port") now owns the real property; and

WHEREAS, the Port and the City desire to remove the Declaration from the real property.

NOW, THEREFORE, FOR A VALUABLE CONSIDERATION, receipt of which is hereby acknowledged,

City of West Sacramento, as Grantor,

hereby remises, releases and forever quitclaims to:

Sacramento-Yolo Port District, as Grantee,

all of its rights, title, and interest in the Declaration and in the following described real property in the County of Yolo, State of California, legally described as:

[See Exhibit A]

DATED: August __, 2018.

GRANTOR:

City of West Sacramento,
a California municipal corporation

By: _____
Aaron Laurel, City Manager

A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

ACKNOWLEDGEMENT

State of California)
County of _____)

On _____, before me, _____, Notary Public, personally appeared _____, who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument, and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

_____

Notary Public (Notary Seal)

Exhibit A

(Legal Description of Stone Lock One Property)

The land described herein is situated in the State of California, County of Yolo, City of West Sacramento, described as follows:

Parcel One:

Beginning at a point at the center common to Swamp Land Surveys nos. 261, 275 and 797, Yolo County Surveys, from which said point of beginning an old granite monument marking the Southwest corner of Swamp Land Survey No. 261, Yolo County Surveys bears South 89° 49' 10" West 996.38 feet, and running thence from said point of beginning along the boundary common to said Swamp Land Survey nos. 261 and 275, Yolo County Surveys, North 89° 49'10" East 1068.39 feet to an intersection with the Westerly right of way line of the Sacramento Northern R.R.; thence following the said Westerly right of way line of the Sacramento Northern R.R. South 23° 04'40" West 26.47 feet; thence leaving the said Westerly right of way line of the Sacramento Northern R.R. and running South 89° 38'10" West 1062.52 feet to a point in the boundary line common to Swamp Land Surveys nos. 275 and 797, Yolo County Surveys; thence along said boundary line common to Swamp Land Survey nos. 275 and 797, Yolo County Surveys, North 9° 13' East 28.09 feet to the point of beginning.

Parcel Two:

Being a portion of that deed recorded in Book 301, Page 320 described as follows:

Beginning at a granite monument marking the Southwest corner of Swamp Land Survey No. 261, Yolo County Surveys, and running thence from said point of beginning South 89° 49'10" West 124.91 feet; thence North 00°03'00" East 638.70 feet to the centerline of the Sacramento-Yolo Port District barge canal as defined in Book 545, Page 427 Official Records; thence along said centerline, North 89° 38'08" East 2464.33 feet to the Westerly right of way line of the Sacramento Northern R.R.; thence along said Westerly right of way line, South 23° 04'40" West 703.80 feet; thence leaving said Westerly right of way line, South 89° 49'10" West 2064.78 feet to the point of beginning.

Excepting therefrom that portion conveyed to the City of West Sacramento in the grant deed recorded on July 25, 2003, as Instrument No. 2003-0044071, Official Records.

Further excepting therefrom that portion conveyed to the City of West Sacramento in the grant deed recorded May 20, 2011, Instrument No. 2011-0014130, Official Records.

Parcel Three:

Being a portion of those deeds recorded in Book 301, Page 185 and 189, described as follows:

Beginning at a point at the center common to Swamp Land Surveys nos. 261, 275 and 797, Yolo County Surveys, from which said point of beginning an old granite monument marking the Southwest corner of Swamp Land Survey No. 261, Yolo County Surveys bears South 89°49'10" West 996.38 feet; thence from said point of beginning South 09°13'00" West 38.09 feet; thence South 89° 38'11" West 1546.68 feet to the Easterly right of way line of Lake Washington Boulevard; thence along said Easterly right of way line North 08° 21'39" East 511.04 feet;

thence South 87° 28'23" West 54.90 feet; thence along a curve to the left having a radius of 1314.50 feet, a delta of 07° 17'26" an arc length of 167.26 feet, said curve being subtended by a chord bearing North 03° 25'57" West to a point on the centerline of the Sacramento-Yolo Port District barge canal as defined in Book 545, Page 427 Official Records; thence along said centerline, North 89° 38'08" East 421.67 feet; thence leaving said centerline, South 00° 03'00" West 638.70 feet; thence North 89° 49'10" East 1120.59 feet to the point of beginning.

Excepting therefrom that portion thereof granted to the City of West Sacramento in the grant deed recorded May 20, 2011, Instrument No. 2011-0014130, Official Records.

APN: 067-180-056-000, 046-010-008-000

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 8

SUBJECT:

CONSIDERATION OF ADOPTION OF RESOLUTION 18-57 APPROVING AMMENDMENTS OF $240,000 TO THE EQUIPMENT REPLACEMENT FUND BUDGET (GRANT FUNDED)

INITIATED OR REQUESTED BY:

[ ] Council [X] Staff

[ ] Other

REPORT COORDINATED OR PREPARED BY:

Dawna Jones, Senior Analyst

Thomas McDonald, Chief of Police

ATTACHMENT [X] Yes [ ] No [ ] Information [ ] Direction [X] Action

OBJECTIVE

The objective of this report is to obtain approval to expend funds for a fully reimbursable grant to purchase a bomb robot for the Yolo County Bomb Squad.

RECOMMENDED ACTION

Staff respectfully recommends that Council:

1. Approve the purchase from with Northrop Grumman Corporation in the amount of $240,000.
2. Adopt Resolution 18-57 approving amendments of $240,000 to the Equipment Replacement Fund Budget

BACKGROUND

The City of West Sacramento Police Department is part of the Yolo County Bomb Squad, which is in need of a replacement bomb robot. These robots handle explosive materials in lieu of an actual person, increasing the safety of law enforcement personnel and the public.

The Department, on behalf of the Bomb Squad, applied for and received $240,000 in grant funding from Yocha Dehe Wintun Nation in partnership with Doyuti T'uhkama. This grant agreement requires that the technology be purchased by the grantee and the grant will then be awarded as full reimbursement. As the Bomb Squad is an organization within county law enforcement, governed by an MOU, the City is needed to act as the fiduciary agency for the purchase.

ANALYSIS

Currently, the Bomb Squad has an antiquated robot model manufactured 28 years ago and robot technology has progressed considerably since then. The features and benefits of the new robot include:

- A much faster speed of deployment
- Better quality cameras, especially for nighttime operations
- LED camera lights that are brighter and use much less robot battery energy
- A larger variety of specialized tools for various tasks
- Finer dexterity with the robot manipulator gripper
- Lighter weight
- A smaller footprint for getting into tighter spaces.

Once the robot is purchased, it will be manufactured and received within the next six months.

Environmental Considerations
Not applicable.

Commission Recommendation
Not applicable.

Strategic Plan Integration
This item supports the City's vision of "People Feeling Safe and Secure" as this technology is utilized in explosive device emergencies.

Alternatives
The City Council may elect not to approve the purchase.

Coordination and Review
This report was coordinated by Police Department staff.

Budget/Cost Impact
This purchase will be reimbursed in full (up to $240K) by grant funding received from Yocha Dehe Wintun Nation. The total cost of the purchase, not to exceed $240,000, will be paid out of the Equipment Replacement Fund, Fund 207-9113-5575. The Equipment Replacement Fund will be replenished with the grant funds once the reimbursement is received.

SOURCES:

$240,000	*207-4810*	*Reimbursement - Grant Funds from Yocha Dehe*

USES:

$240,000	*207-9113-5775*	*Equipment Replacement Fund*

ATTACHMENT(S)
1.Resolution 18-57
2.Bomb Robot Grant Award Letter

RESOLUTION 18-57

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF WEST SACRAMENTO APPROVING AMENDMENTS OF $240,000 TO THE EQUIPMENT REPLACEMENT FUND (GRANT FUNDED) BUDGET FOR THE PURCHASE OF A BOMB SQUAD ROBOT FOR FISCAL YEAR 2018-19

WHEREAS, the City Council adopted the biennial Operations and Maintenance (O&M) budget for Fiscal Years 2017-18 and 2018-19 on June 14, 2017; and

WHEREAS, the City Council adopted mid-term amendments to the Operations and Maintenance (O&M) budget for Fiscal Year 2018/19 on May 23, 2018 via Resolution 18-34; and

WHEREAS, based upon information received subsequent to the adoption of the budget, the City Manager has prepared and proposed an amendment to budgeted revenues and expenses for the 2018/19 fiscal year; and

WHEREAS, the City of West Sacramento Police Department is part of the Yolo County Bomb Squad, which is in need of a replacement bomb robot; and

WHEREAS, The Police Department, on behalf of the Bond Squad, applied for and received $240,000 in grant funding from Yocha Dehe Wintun Nation in partnership with Doyuti T'uhkama; and

WHEREAS, The grant agreement requires that the technology be purchased by the grantee and the grant will then be awarded as full reimbursement with no match required; and

WHEREAS, The robot will be purchased out of the Equipment Replacement Fund, and the reimbursement, when received, will be deposited into that fund; and

WHEREAS, the City Council has considered all information related to this matter, as presented at the public meetings of the City Council identified herein, including any supporting reports by City staff, and any other information provided during public meetings.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of West Sacramento as follows:

1. The City Council hereby approves amendments to the O&M Budget for the 2018-19 fiscal year as provided below:

SOURCES

$240,000	207-4810
$240,000	Total Sources

USES

$240,000	207-9113-5775
$240,000	Total Uses

2. The City Council hereby finds that the facts set forth in the recitals to this Resolution are true and correct and establish the factual basis for the City Council's adoption of this Resolution.

3. This Resolution shall take effect immediately upon its adoption.

PASSED AND ADOPTED by the West Sacramento City Council this 22nd day of September 2018, by the following vote:

AYES:
NOES:
ABSENT:

Christopher L. Cabaldon, Mayor

ATTEST:

Kryss Rankin, City Clerk

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 9

SUBJECT:

CONSIDERATION OF PROJECT APPROVAL AND ADOPTION OF RESOLUTION 18-51 ADOPTING THE FINAL INITIAL STUDY/MITIGATED NEGATIVE DECLARATION AND MITIGATION MONITORING AND REPORTING PROGRAM FOR THE SYCAMORE TRAIL PHASE II & III PROJECT

INITIATED OR REQUESTED BY:

[ ] **Council** [X] **Staff**

[ ] **Other**

REPORT COORDINATED OR PREPARED BY:
Edgar Medina, Senior Civil Engineer

Denix Anbiah, Director of Public Works

ATTACHMENT [X] Yes [ ] No [ ] Information [ ] Direction [X] Action

OBJECTIVE

The objective of this report is to obtain Project Approval from the City Council for the Sycamore Trail Phase II & III Project and for the City Council to adopt Resolution No. 18-51 certifying the environmental documents in compliance with CEQA.

RECOMMENDED ACTION

Staff respectfully recommends that the City Council:

1. Adopt Resolution 18-51 adopting the Sycamore Trail Phase II & III Project final Initial Study and Mitigated Negative Declaration (IS/MND), that the IS/MND (Attachment 2) based on findings that it has been prepared in accordance with the California Environmental Quality Act (CEQA); that the Council has considered the Mitigated Negative Declaration and all comments received during the comment period; and, that there is no substantial evidence in the record that the Project will have a significant impact on the environment; and identifying the City Clerk and the Public Works Department as the location and custodian of the documents or other material that constitute the record of proceedings upon which this decision is based; and
2. Provide Project Approval for the Sycamore Trail Phase II & III Project as described in the environmental documents.

BACKGROUND

In 2013, the City of West Sacramento adopted a Bicycle, Pedestrian, and Trails Master Plan with the vision of providing one of the nation's top bicycle and pedestrian friendly communities. To fulfill this vision, the City is improving infrastructure for bicycling and walking, while capitalizing on several of West Sacramento's positive characteristics.

Included in this Master Plan was the Sycamore Trail project that will extend south from Sacramento Avenue/Yolo Street to Barge Canal and ultimately to Lake Washington Boulevard/Jefferson Boulevard intersection: a safe non-vehicular pedestrian and bicycle trail that will provide connections between central City neighborhoods linking people with schools, parks, businesses and commercial centers.

City of West Sacramento (City) staff applied for and received $1.15 million in funding from Sacramento Area Council of Governments (SACOG) for design and received a $5.18 million in funding from the California Urban Greening Grant Program through the Natural Resources Agency for the design and construction of Sycamore Trail Phase II & III Extension Project.

The Project Team has been meeting and remains in close coordination with stakeholder agencies including Caltrans, Sacramento Regional County Sanitation District (Regional SAN), Reclamation District 900, PG&E, Police, Fire, as well as the Washington Unified School District (WUSD) and Westmore Oaks school staff.

The Project lies within approximately 39 acres located between Evergreen Avenue and Stone Boulevard, along the Sacramento Regional County Sanitation District LNWI sewer easement. Land uses surrounding the Project site consists of public open space and public/quasi-public space, residential, light industrial, and business park designations. Adjacent land uses include residences, the School, the Regional County Sanitation District's Lower Northwest Interceptor (LNWI) sewer easement, a community garden, and commercial businesses.

The Project Team has been actively coordinating with Washington Unified School District (WUSD) staff, Westmore Oaks Principal and Caltrans staff to establish the trail alignment and touchdown areas. As a result of those meetings, the proposed project alignment has been accepted by WUSD and Caltrans which is covered by the environmental document.

On July 12, the WUSD School Board approved Amendment No. 2 to the Joint Use Agreement between the City and WUSD. On July 18, 2018, the City Council approved the same Amendment No. 2 to the Joint Use agreement that provides the framework and requirements to allow the use of school district property for the Sycamore Trail project.

ANALYSIS

The Project purpose is to increase the number of people in the City who bicycle or walk to work, to school, for errands, or for recreation. The City proposes to extend the Sycamore Trail from Evergreen Avenue north of US 50 to the intersection of Park and Stone Boulevards and will generally follow the Sacramento Regional County Sanitation District Northwest Interceptor Alignment. A new pedestrian and bicycle overcrossing would be included over US 50, connecting Joey Lopes Park to the north with the School to the south.

The Project Team has developed preliminary project plans that will be submitted to all the project Stakeholders and Agencies for review and comment. The preliminary Project Plans consists of Typical Cross Sections, Layouts, Profiles, Construction Details, Utility Layout and Bridge Plans along Phase II & III of the trail. All improvements will be in accordance with current City, Caltrans, AASHTO and ADA standards.

- Phase II entails construction of a trail between Joey Lopes Park and Westmore Oaks school site (approximately along the Lower Northwest Interceptor sewer pipeline easement) and includes a pedestrian overcrossing bridge at US 50/Business 80.

- Phase III entails construction of a trail between the overcrossing landing at the Westmore Oaks site and the Park Boulevard/Stone Boulevard intersection. Phase III does not incorporate a trail segment through the Westmore Oaks Site, but rather directs pedestrians to Clarendon Avenue to reconnect to the trail at the southern end of the school site. At that point, pedestrians may continue along the trail system from the southern portion of the Westmore Oaks Site to Park Boulevard/Stone Boulevard Intersection.

At this time, the Aesthetic Component has not been developed, as that component will be incorporated upon completion of public workshops with the Parks Commission, Arts, Culture & Historic Preservation Commission and ultimately, the City Council.

The current Tentative Project Schedule is as follows:

- Project Kickoff 09/2017
- Bridge Type Selection 06/2018
- Environmental Certification 08/2018
- Plans and Specifications 12/2018
- Utility Coordination/Certification 03/2019
- Right-of-Way Certification 05/2019
- Construction Start 10/2019
- Construction End 06/2020

Environmental Considerations

The City has examined potential environmental impacts associated with the project. Pursuant to both CEQA and the National Environmental Policy Act (NEPA), all required environmental research and analyses have been performed including environmental surveys, resource agency coordination and public outreach have been conducted. The NEPA document is expected to be a Categorical Exclusion (CE) with technical studies and is expected to be signed by Caltrans in September/October of 2018.

The project team prepared a final Initial Study/Mitigated Negative Declaration for the project. After public meetings, required AB52 consultations with Native American Tribes (Yoche Dehe Wintun Nation, United Auburn Indian Community of the Auburn and Cortina Indian Rancheria of Wintun Indians) and upon Technical

Memorandums concurrence from Caltrans, the Project Team released the IS/MND for a 30-day public comment period beginning on June 27, 2018 and ending on July 27, 2018.

During the public comment period, the project received comments from the Central Valley Regional Water Quality Control Board and the Delta Protection Commission (State of California – Natural Resources Agency). The City's responses to comments are included in the final IS/MND (See Attachment 2).

The final IS/MND has been prepared for the Project to assess the potential impacts on the environment and the significance of those impacts. Based on the results of the final IS/MND, the Project would not have any significant impacts on the environment once mitigation measures are implemented. This conclusion is supported by the following findings:

- The Project would not impact agriculture and forest resources.
- The Project would have a less-than-significant impact on air quality, geology, soils, and seismicity; greenhouse gas emissions; hydrology and water quality; land use and land use planning; mineral resources; population and housing; recreation; and utilities and services.
- The Project would have a less-than-significant impact, once mitigation measures are implemented, on aesthetics, biological resources, cultural resources, hazards and hazardous materials, noise, public services, transportation and traffic, and tribal cultural resources.
- No substantial evidence exists that the Project would have a significant negative or adverse impact on the environment.

In addition to the standard construction measures required by Caltrans Standard Specifications and other applicable laws, regulations, and policies, all mitigation measures identified in the final IS/MND will be implemented as part of the Project to avoid or minimize potential environmental impacts during construction. Implementation of these mitigation measures would reduce the potentially-significant environmental impacts of the Project to a less-than-significant level.

Commission Recommendation

The Project Team is developing Aesthetic Concepts that will be presented as a workshop, in September 2018, to the Parks Commission and the Arts, Culture & Historic Preservation Commission. Upon receiving input from the commissions, the Project Team will conduct a workshop with the City Council for their review and input.

Additionally, the Project Team will be taking the project to the Transportation, Mobility and Infrastructure Commission to solicit their input.

Strategic Plan Integration

The Sycamore Trail Phase II & III Extension Project and recommended action supports several adopted planning strategies implementing the City Council's Strategic Plan goal of Vibrant Neighborhoods and Comfortable Life Style and Living Strategies.

Alternatives

1) Staff respectfully recommends that the City Council:
   a) Adopt Resolution 18-51 adopting the Sycamore Trail Phase II & III Project final Initial Study and Mitigated Negative Declaration (IS/MND), that the IS/MND (Attachment 2) based on findings that it has been prepared in accordance with the California Environmental Quality Act (CEQA); that the Council has considered the Mitigated Negative Declaration and all comments received during the comment period; and, that there is no substantial evidence in the record that the Project will have a significant impact on the environment; and identifying the City Clerk and the Public Works Department as the location and custodian of the documents or other material that constitute the record of proceedings upon which this decision is based; and
   b) Provide Project Approval for the Sycamore Trail Phase II & III Project as described in the environmental documents.

2) The City Council may choose not to adopt Resolution 18-51 and direct that the item be brought back before Council for consideration at a future meeting, pending modifications or additional information as directed. This alternative is not recommended as this is a grant funded project with a tight delivery schedule.

Coordination and Review

This project is being coordinated with the Community Development Department, Public Works Department and the Finance Division of the Administrative Services Department. Resolution 18-51 has been reviewed by the City Attorney.

Budget/Cost Impact

There are no new cost impacts for these actions. The Project (CIP No 14021) is appropriated in the current Capital Improvement Program Budget through Measure G ($1,100,000.00, Fund 406) and Congestion Mitigation Air Quality Grant ($1,151,000.00) and Urban Greening Grant ($5,178,684.00).

The Public Works Department recently applied for a $1.25 million grant within the Active Transportation Program to fund Phase III of the project. Staff anticipates that the project selection/recommendation for this grant program cycle will be made in November 2018.

ATTACHMENT(S)

1. Resolution 18-51
2. Initial Study/Mitigated Negative Declaration/Mitigation Monitoring and Reporting Program

RESOLUTION 18-51

**RESOLUTION OF THE CITY COUNCIL
OF THE CITY OF WEST SACRAMENTO
ADOPTING THE FINAL INITIAL STUDY/MITIGATED NEGATIVE DECLARATION AND MITIGATION MONITORING AND REPORTING PROGRAM IN COMPLIANCE WITH CEQA FOR THE SYCAMORE TRAIL PHASE II & III PROJECT**

WHEREAS, on November 16, 2016, the City Council adopted the General Plan 2035 Update; and

WHEREAS, on July 18, 2018, the City Council adopted the 2018 Bicycle, Pedestrian, and Trails Master Plan; and

WHEREAS, the 2018 Bicycle, Pedestrian, and Trails Master Plan identifies Sycamore Trail Phase II & III as a signature project for City of West Sacramento ("City"); and

WHEREAS, the City proposes to construct a Class I Pedestrian Facility that creates access across US Highway 50 and connects the north and south areas of the City; and

WHEREAS, construction of the improvements will increase the City's bike and trail system; and

WHEREAS, the project is in furtherance of the goals and recommendations contained within the 2018 Bicycle, Pedestrian, and Trails Master Plan; and

WHEREAS, the City conducted the required tribal consultations pursuant to AB52 as part of the environmental review process for the Sycamore Trail Phase II & III Project.

WHEREAS, the City circulated a draft Initial Study and Mitigated Negative Declaration (IS/MND) addressing the potential impacts of the Sycamore Trail Phase II & III Project for 30 days starting in June 2018; and

WHEREAS, the City has prepared an Initial Study and Mitigated Negative Declaration in accordance with the California Environmental Quality Act, CEQA Guidelines, and all other applicable laws and regulations; and

WHEREAS, the City has considered all public comments received on the Mitigated Negative Declaration and that adequate mitigation will be applied to the project; and

WHEREAS, the City Council finds that the public health, safety and general welfare warrant the adoption of the Mitigation Monitoring Plan for the Sycamore Trail Phase II & III Project.

WHEREAS, the City Council has reviewed the Sycamore Trail Phase II & III Project final Initial Study and Mitigated Negative Declaration and finds that the public health, safety and general welfare warrant the adoption of the final IS/MND.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of West Sacramento finds that:

1. The Sycamore Trail Phase II & III Project Initial Study and Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program (MMRP) has been completed in compliance with the California Environmental Quality Act; and

2. The Sycamore Trail Phase II & III project final IS/MND and MMRP has been reviewed and considered by the City Council prior to any action by the City Council on the Project; and

3. The Sycamore Trail Phase II & III project final IS/MND identified all potentially significant impacts to the environment which can and will be avoided or mitigated to less than significant levels through adoption and implementation of the mitigation measures proposed as part of the Project and through implementation of the MMRP; and

4. The Sycamore Trail Phase II & III project final IS/MND and MMRP reflect the City of West Sacramento's independent judgement and analysis; and

5. There is no substantial evidence in the record of the Project, as mitigated, will have a significant effect on the environment; and

6. The administrative record is located in the Office of the City Clerk and Public Works Department located at 1110 West Capitol Avenue, West Sacramento, California, 95691; and,

7. The City Clerk and the Public Works Department are collectively designated as the location and custodian of the documents and other material constituting the record of proceedings upon which this decision is based; and

8. The City Council of the City of West Sacramento has considered and hereby adopts the Findings and Statements of the final IS/MND; and

9. The City Council hereby adopts the Sycamore Trail Phase II & III project final IS/MND and MMRP.

PASSED AND ADOPTED by the City Council of the City of West Sacramento this 22nd day of August 2018, by the following vote:

AYES:
NOES:
ABSENT:

_____
Christopher L. Cabaldon, Mayor

ATTEST:

_____
Kryss Rankin, City Clerk

SYCAMORE TRAIL PHASE II & III EXTENSION PROJECT

Final Initial Study / Mitigated Negative Declaration

August 2018

SYCAMORE TRAIL PHASE II & III EXTENSION PROJECT

FINAL Initial Study / Mitigated Negative Declaration

Yolo County, California

Submitted To:

City of West Sacramento
Department of Public Works
1110 West Capitol Ave.
West Sacramento, CA 95691

Submitted by:

Drake Haglan and Associates, Inc.
11060 White Rock Road, Suite 200
Rancho Cordova, CA 95670
916.363.4210

August 2018

EXECUTIVE SUMMARY

The City of West Sacramento (City) proposes to extend the Sycamore Trail from Evergreen Avenue north of U.S. Route 50 (US 50), connecting Joey Lopes Park to the north with Westmore Oaks Elementary School (School) to the south (Project). The Project is located in southeastern Yolo County, west of the City of Sacramento, and intersecting US 50. Land use surrounding the Project site consists of public open space and public/quasi-public space, residential, light industrial, and business park designations. Adjacent land uses include residences, the School, the Regional County Sanitation District's Lower Northwest Interceptor (LNWI) sewer easement, a community garden, and commercial businesses.

The Project purpose is to increase the number of people in the City who bicycle or walk to work, to school, for errands, or for recreation. The City proposes to extend the Sycamore Trail from Evergreen Avenue north of US 50 to the intersection of Park and Stone Boulevards near the Deep Water Shipping Channel. The Project would generally follow the Sacramento Regional County Sanitation District Northwest Interceptor Alignment (LNWI sewer easement). A new pedestrian and bicycle overcrossing would be included over US 50, connecting Joey Lopes Park to the north with the School to the south. Details of the trail alignment through the school district property have not been finalized; however, all options currently being considered are located within the Project study area. All improvements would be in accordance with current City standards, AASHTO standards and ADA standards.

The Draft Initial Study/Mitigated Negative Declaration (IS/MND) was submitted to the State Clearinghouse on June 27, 2018, for a 30-day public review period that ended on July 28, 2018. During the public review period, the Draft IS/MND was available for review at the City of West Sacramento Public Works Department (1110 West Capitol Avenue, West Sacramento) during business hours.

This Final IS/MND has been prepared for the Project to assess the potential impacts on the environment and the significance of those impacts. Based on the results of the Final IS/MND, the Project would not have any significant impacts on the environment once mitigation measures are implemented. This conclusion is supported by the following findings:

- The Project would not impact agriculture and forest resources.
- The Project would have a less-than-significant impact on air quality, geology, soils, and seismicity; greenhouse gas emissions; hydrology and water quality; land use and land use planning; mineral resources; population and housing; recreation; and utilities and services.
- The Project would have a less-than-significant impact, once mitigation measures are implemented, on aesthetics, biological resources, cultural resources, hazards and hazardous materials, noise, public services, transportation and traffic, and tribal cultural resources.
- No substantial evidence exists that the Project would have a significant negative or adverse impact on the environment.

In addition to standard construction measures required by Caltrans Standard Specifications and other applicable laws, regulations, and policies, the following mitigation measures will be implemented as part of the Project to avoid or minimize potential environmental impacts. Implementation of these mitigation measures would reduce the potentially-significant environmental impacts of the Project to a less-than-significant level.

Table 1. Mitigation Measures

Potential Impact	Mitigation Measures	Timing	Responsible Party	Level of Significance After Mitigation
Aesthetics				
Project implementation has the potential to degrade the existing visual character or quality of the site and its surroundings.	**MM AES-1:** Implement the following into the Project to mitigate potential visual impacts: • Incorporate designs, such as architectural form liners, concrete staining on the exterior girders and bridge railing, and aesthetic treatment on overcrossing fencing to maintain aesthetic consistency with the surrounding residential and industrial community. • Place buffer vegetation plantings directly adjacent to the trail segment fencing on the trail side to minimize the potential visual prominence and imbalance created by fencing. • Choose a fencing design option for the southern trail segment that is aesthetically consistent with the surrounding residential community and minimizes the potential for pedestrians and cyclists to feel too enclosed, claustrophobic, or generally unsafe.	Prior to construction activities during final design	City of West Sacramento	Less than significant
Project implementation has the potential to create a new source of substantial light or glare which would adversely affect daytime or nighttime views in the area.	**MM AES-2:** Chose and develop pedestrian lighting in coordination with community involvement that lessens the potential effects of sunrise, sunset, daytime, and nighttime glare on US 50 roadway users.	Prior to construction activities during final design	City of West Sacramento	Less than significant
Biological Resources				
Project implementation would have the potential to have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special-status species in local or regional	**MM BIO-1:** *Conduct Preconstruction Tree Surveys.* To ensure there are no effects on nesting birds, a qualified biologist will conduct preconstruction tree surveys of the trees to be removed, and within 250 feet of the Project construction area. Survey work will be done no more than two days prior to initiation of construction to minimize potential that nests are initiated after the survey and prior to the start of construction. If any occupied nests are detected the tree will be flagged, a minimum buffer of 100 feet between the nest and construction zone will be established, unless determined otherwise by CDFW, and that area will be avoided until the qualified biologist has determined the nest is no	Prior to construction activities	City of West Sacramento	Less than significant

POTENTIAL IMPACT	MITIGATION MEASURES	TIMING	RESPONSIBLE PARTY	LEVEL OF SIGNIFICANCE AFTER MITIGATION
plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service.	longer active/occupied. Once the biologist has determined that young have fledged and the nest is no longer active, construction can resume in that area. If no active nests are identified during the preconstruction survey, no further mitigation is necessary. If construction activities (i.e. vegetation and tree removal) are scheduled to begin during the nonbreeding season (September to January), preconstruction surveys would not be necessary.			
Project implementation could conflict with a tree preservation policy or ordinance.	**MM BIO-2:** *Replace and Protect Trees.* Implement the following measures prior to and during trail and overcrossing construction to mitigate potential impacts to the West Sacramento Tree Preservation Ordinance. • Prior to removal of any trees, an International Society of Arboriculture-certified arborist shall conduct a tree survey in areas that may be impacted by construction activities. This survey shall document tree resources that may be adversely impacted by Project implementation. The survey will follow standard professional practices. • Existing trees will be retained to extent feasible. A Tree Protection Zone (TPZ) shall be established around any tree or group of trees to be retained. The TPZ will be delineated by an International Society of Arboriculture-certified arborist. The TPZ shall be defined by the radius of the dripline of the tree(s) plus one foot. The TPZ of any protected trees shall be demarcated using fencing that will remain in place for the duration of construction activities. • Construction-related activities shall be limited within the TPZ to those activities that can be done by hand. No heavy equipment or machinery shall be operated within the TPZ; if this is not possible, a six inch layer of bark mulch shall be placed where the equipment shall be under the dripline in order to protect the root system from too much compaction. Grading shall be prohibited within the TPZ. No construction materials, equipment, or heavy machinery shall be stored within the TPZ. • Prior to removal, all trees will be posted for public notice and a permit will be obtained (pursuant to City Code 8.24.040 and 8.24.060). The City will mitigate tree removal by replacing removed trees at a 1:1 ratio. All replacement trees will be oak or other native tree species for the area and would be planted with automatic irrigation to improve tree survivability.	Prior to and during construction activities	City of West Sacramento	Less than significant
Cultural Resources				

POTENTIAL IMPACT	MITIGATION MEASURES	TIMING	RESPONSIBLE PARTY	LEVEL OF SIGNIFICANCE AFTER MITIGATION
Project implementation has the potential to discover unanticipated cultural resources or human remains during ground-disturbing activities.	**MM CUL-1:** *Follow Protocol for the Unanticipated Discovery of Cultural Resources or Human Remains.* If buried cultural materials are encountered during construction, stop work in that area until a qualified archaeologist can evaluate the find's nature and significance. In the event that human remains or associated funerary objects are encountered during construction, cease all work within the vicinity of the discovery. In accordance with CEQA and the California Health and Human Safety Code (14 CCR § 15064; 7 HSC § 7050.5), the County coroner will be contacted immediately. If the human remains are determined to be Native American, the coroner will notify the Native American Heritage Commission, who will notify and appoint a Most Likely Descendent (MLD). The MLD will work with a qualified archaeologist to decide the proper treatment of the human remains and any associated funerary objects.	During construction activities.	City of West Sacramento	Less than significant
Hazards and Hazardous Materials				
Construction activities involve reasonably foreseeable upset and accident conditions that may subject the public and environment to the release of hazardous materials.	**Mitigation Measure HAZ-1:** *Develop a Health and Safety Plan (HASP).* Develop a HASP for the Project. The HASP shall describe appropriate procedures to follow in the event that any contaminated soil or groundwater is encountered during construction activities. Any unknown substances shall be tested, handled and disposed of in accordance with appropriate federal, state and local regulations.	Prior to construction activities	City of West Sacramento	Less than significant
Construction activities involve reasonably foreseeable upset and accident conditions that may subject the public and environment to the release of hazardous materials.	**MM HAZ-2:** *Follow Protocol for Handling Lead Containing Materials.* A California-licensed abatement contractor will conduct a survey for lead containing materials prior to demolition (including concrete elements) and contractor will submit a National Emission Standard for Hazardous Air Pollutants (NESHAP) notification. Per the Asbestos NESHAP regulation, all "demolition activity" requires written notification even if there is no asbestos present. This notification shall be typewritten and postmarked or delivered no later than ten days prior to the beginning of the asbestos demolition or removal activity. If lead-containing materials are found, the following will be required: • Building materials associated with paint on structures, and paint on utilities should be abated by a California-licensed abatement contractor and disposed of as a hazardous waste in compliance with 2015 Caltrans Standard Specification	Prior to and during construction activities	City of West Sacramento	Less than significant

POTENTIAL IMPACT	MITIGATION MEASURES	TIMING	RESPONSIBLE PARTY	LEVEL OF SIGNIFICANCE AFTER MITIGATION
	14-11.12, 2015 Caltrans Standard Specification 14-11.13, and other federal and state regulations for hazardous waste. • A Lead Compliance Plan should be prepared by the contractor for the disposal of lead-based paint. The grindings (which consist of the roadway material and the yellow and white color traffic stripes) shall be removed and disposed of in accordance with the appropriate 2015 Caltrans Standard Specifications. • A California- licensed lead contractor should be required to perform all work that will disturb any lead-based paint as a result of planned or unplanned renovations in the Project area, including the presence of yellow traffic striping and pavement markings that may contain lead-based paint. All such material must be removed and disposed of as a hazardous material in compliance with 2015 Caltrans Standard Specification 14-11.12 and 14-11.13.			
Noise				
Project implementation has the potential to result in a temporary elevation of noise levels.	**MM NO-1**: Implement the following control measures to minimize noise and vibration disturbances at sensitive receptors during construction: • Use newer equipment with improved muffling and ensure that all equipment items have the manufacturers' recommended noise abatement measures, such as mufflers, engine enclosures, and engine vibration isolators intact and operational. Newer equipment will generally be quieter in operation than older equipment. All construction equipment shall be inspected at periodic intervals to ensure proper maintenance and presence of noise control devices (e.g., mufflers and shrouding, etc.). • Utilize construction methods or equipment that provides the lowest level of noise and ground vibration impact. • Turn off idling equipment.	Prior to and during construction activities	City of West Sacramento	Less than significant
Public Services				
Project implementation has the potential to result in inadequate emergency access.	**MM PUB-1:** *Develop a Construction Period Emergency Access Plan.* Prior to the start of construction, the contractor shall coordinate with the City of West Sacramento Police and Fire departments and local public and private ambulance and paramedic providers in the area to prepare a Construction Period Emergency Access Plan. The Emergency Access Plan	Prior to construction activities.	City of West Sacramento	Less than significant

POTENTIAL IMPACT	MITIGATION MEASURES	TIMING	RESPONSIBLE PARTY	LEVEL OF SIGNIFICANCE AFTER MITIGATION
	shall identify Project phases and construction scheduling and shall identify appropriate alternative emergency access routes.			
Transportation and Traffic				
Project implementation has the potential to result in impacts to traffic circulation.	**MM TRAF-1:** *Develop a Standard Traffic Management Plan.* The construction contractor for the Project shall implement a standard traffic management plan to minimize traffic disruption and ensure adequate access is maintained to surrounding properties.	Prior to construction activities	City of West Sacramento	Less than significant
Tribal Cultural Resources				
Project implementation has the potential to result in impacts to tribal cultural resources.	**MM TCR-1:** *Conduct a Tribal Cultural Resource Construction Worker Awareness Training.* A consultant and construction worker tribal cultural resources awareness brochure and training program for all personnel involved in Project implementation will be developed in coordination with interested Native American tribes. Distribute the brochure and conduct the training in coordination with qualified cultural resources specialists and Native American representatives and monitors from culturally affiliated Native American tribes before any stages of Project implementation and construction activities begin on the Project site. The program will include relevant information regarding sensitive tribal cultural resources, including applicable regulations, protocols for avoidance, and consequences of violating State laws and regulations. The worker cultural resources awareness program will also describe the procedure to follow for resources that have the potential to be located on the Project site and will outline what to do and whom to contact if any potential archaeological resources or artifacts are encountered. The program will also underscore the requirement for confidentiality and culturally-appropriate treatment of any finds of significance to Native Americans and behaviors, consistent with Native American tribal values.	Prior to construction activities	City of West Sacramento	Less than significant
	MM TCR-2: *Employ a Native American Monitor*. A minimum of seven days prior to beginning earthwork or other soil disturbance activities, the City shall contact concerned tribes of the construction schedule for ground disturbance activities. A tribal representative shall be invited to inspect the Project site, including any soil piles, trenches, or other disturbed areas, within the first five days of ground breaking activity. During this inspection, a site meeting of construction personnel shall also be held to afford the tribal representative the opportunity to provide tribal cultural resources awareness information.	Prior to and during construction activities	City of West Sacramento	Less than significant

Potential Impact	Mitigation Measures	Timing	Responsible Party	Level of Significance After Mitigation
	If any tribal cultural resources, such as structural features, unusual amounts of bone or shell, artifacts, human remains, or architectural remains are encountered during this initial inspection or during any subsequent construction activities, work shall be suspended within 100 feet of the find. The City shall coordinate any necessary investigation of the site with a tribal representative and a qualified archaeologist approved by the City; and as part of the site investigation and resource assessment, the archeologist shall consult with the tribe(s) and provide proper management recommendations should potential impacts to the resources be found by the City to be significant. A written report detailing the site assessment, coordination activities, and management recommendations shall be provided to the City by the qualified archaeologist. The contractor shall implement the use of a Native American monitor whenever work occurs within 100 feet of the find.			

ACRONYMS AND ABBREVIATIONS

The following is a list of abbreviations used within this document. Each term is defined in full once within the document before the abbreviation is used.

AASHTO: American Association of State Highway and Transportation Officials

AB 32: Assembly Bill 32

AB 52: Assembly Bill 52

ACM: (presumed) asbestos-containing material

APN: Accessor Parcel Number

ARB: California Air Resources Board

BMP: best management practices

CAAQS: California Ambient Air Quality Standards

Caltrans: California Department of Transportation

CDC: California Department of Conservation

CDFG: California Department of Fish and Game

CDFW: California Department of Fish and Wildlife

CESA: California Endangered Species Act

CEQA: California Environmental Quality Act

CFGC: California Fish and Game Code

Channel: Sacramento River Deep Water Ship Channel

CHRIS: California Historical Resources Information System

City: City of West Sacramento

CNDDB: California Natural Diversity Database

CNEL: community-equivalent noise level

CNPS: California Native Plants Society

CMP: corrugated metal pipes

CO: carbon monoxide

CRHR: California Register of Historic Places

dB: decibel

dBA: A-weighted decibel

FEMA: Federal Emergency Management Agency

FHWA: Federal Highway Administration

FTA: Federal Transit Administration

General Plan: City of West Sacramento General Plan 2035 Policy Document

H_2S: hydrogen sulfide

HASP: Health and Safety Plan

HBP: Highway Bridge Program

IPaC: Information for Planning and Consultation

ISA: Initial Site Assessment

IS/MND: Initial Study/Mitigation Negative Declaration

LBP: lead-based paint

MBTA: Migratory Bird Treaty Act

MLD: most likely descendant

MMRP: Mitigation, Monitoring, and Reporting Program

MRZ-1: mineral resources zone (type) 1

MRZ-1: mineral resources zone (type) 3

MSL: mean sea level

Municipal Code: West Sacramento Municipal Code

NAAQS: National Ambient Air Quality Standards

NAHC: Native American Heritage Commission

NESHAP: national emissions standard for hazardous pollutants

NHPA: National Historic Preservation Act

NO_2: nitrogen dioxide

NPDES: National Pollution Discharge Elimination System

NRHP: National Register of Historic Places

O_2: ozone

DWR: California Department of Water Resources

OSHA: Occupational Safety and Health Administration

Parks Master Plan: 2003 City of West Sacramento Parks Master Plan

Pb: lead

$PM_{2.5}$: particulate matter less than 2.5 microns in diameter

REC: recognized environmental conditions

ROG: reactive organic gas

RWQCB: Regional Water Quality Control Board

SACOG: Sacramento Area Council of Governments

SMARA: California Surface Mining and Reclamation Act

SO_2: sulfur dioxide

REC: recognized environmental conditions

ROG: reactive organic gas

RWQCB: Regional Water Quality Control Board

SACOG: Sacramento Area Council of Governments

SMARA: California Surface Mining and Reclamation Act

SWPPP: Stormwater Pollution Prevention Plan

Trails Master Plan: 2013 City of West Sacramento Pedestrian, Bicycle, and Trails Master Plan

TPZ: Tree Protection Zone

UCMP: University of California Museum of Paleontology

US 50: U.S. Route 50

USDA: U.S. Department of Agriculture

U.S. EPA: U.S. Environmental Protection Agency

USGS: U.S. Geological Survey

VdB: root mean square vibration velocity level in decibels

YSAQMD: Yolo Solano Air Quality Management District

OHE: overhead electrical lines

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APPENDICES

Appendix A: The Roadway Construction Emissions Modeling of the Project
APPENDIX B- Comments Received and Response to Public Comments

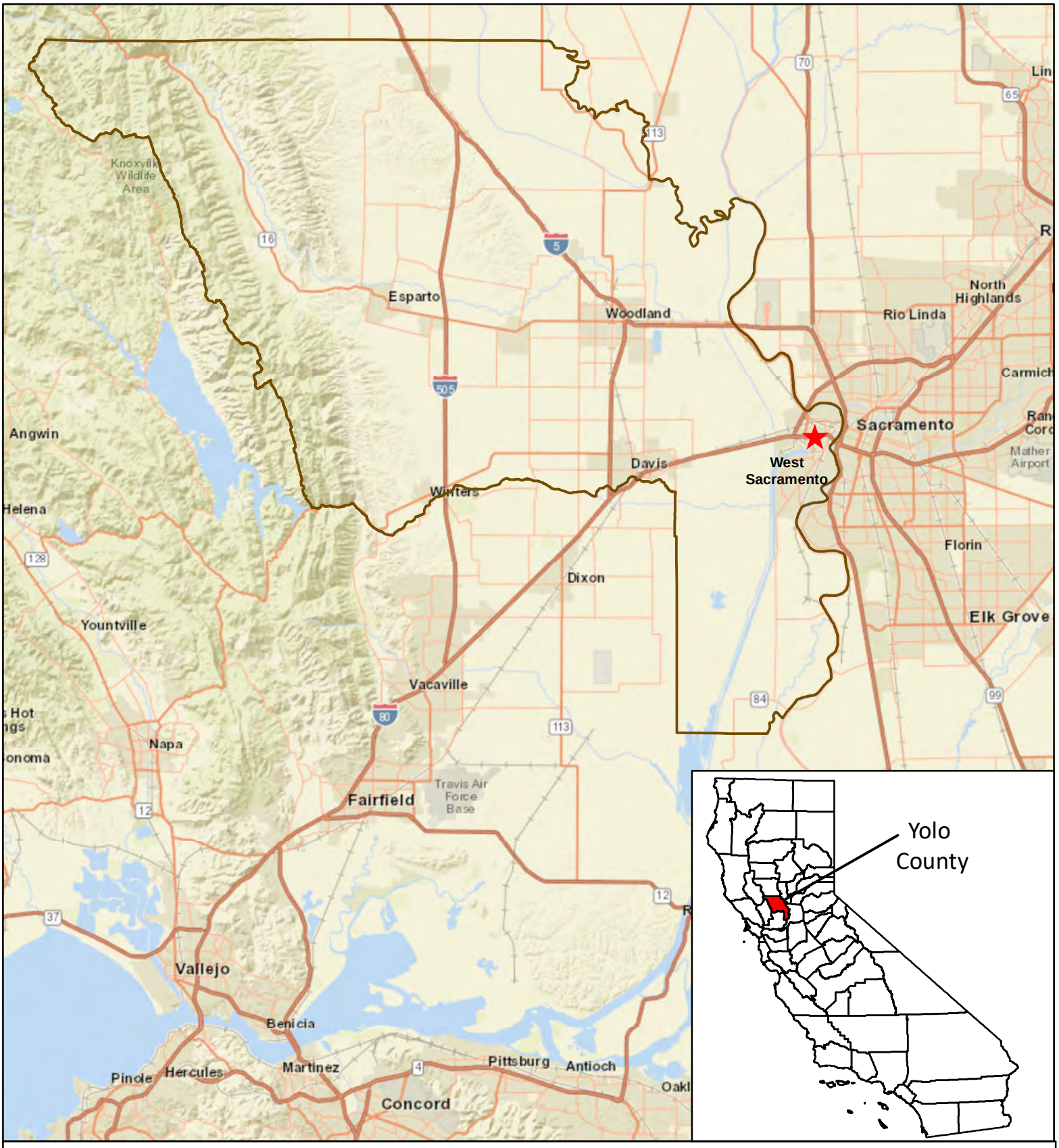
Initial Study

1.	**Project Title:**	Sycamore Trail Phase II & III Extension Project
2.	**Lead Agency Name and Address:**	West Sacramento Department of Public Works 1100 West Capitol Ave. West Sacramento, CA 95691
3.	**Contact Person and Phone Number:**	Edgar Medina Senior Civil Engineer (916) 617-4546
4.	**Project Location:**	City of West Sacramento, Sycamore Avenue and the Lower Northwest Interceptor (LWNI) sewer easement from Evergreen Avenue to Park Boulevard at Stone Boulevard
5.	**Project Sponsor's Name and Address:**	Edgar Medina West Sacramento Department of Public Works 1100 West Capitol Ave, 1st Floor West Sacramento, CA 95691
6.	**General Plan Designation(s):**	Public open space (OS), light residential (LR), high residential (HR), light industrial (LI)
7.	**Zoning Designation(s):**	Public/quasi-public (PQP), residential (R1-A and R3), industrial (M-1), business park (BP)

1 INTRODUCTION

The City of West Sacramento (City) proposes to extend the Sycamore Trail from Evergreen Avenue north of US. Route 50 (US 50), connecting Joey Lopes Park to the north with Westmore Oaks Elementary School (School) to the south. This plan will be referred to as the Project throughout the remainder of this document and the location on which the Project would take place will be referred to as the Project site or the Project area throughout the remainder of this document. The term Project vicinity refers to the area within a two-mile radius of the Project site. The Project is located in southeastern Yolo County, west of the City of Sacramento, and intersecting US 50 **(Figure 1** and **Figure 2).**

The pedestrian overcrossing and all aspects of the trail would meet current applicable City, Caltrans, American Association of State Highway and Transportation Officials (AASHTO), and Americans with Disabilities Act (ADA) standards.



Project Location

County Boundary

0 5 10 20 Miles

N

drake haglan AND ASSOCIATES

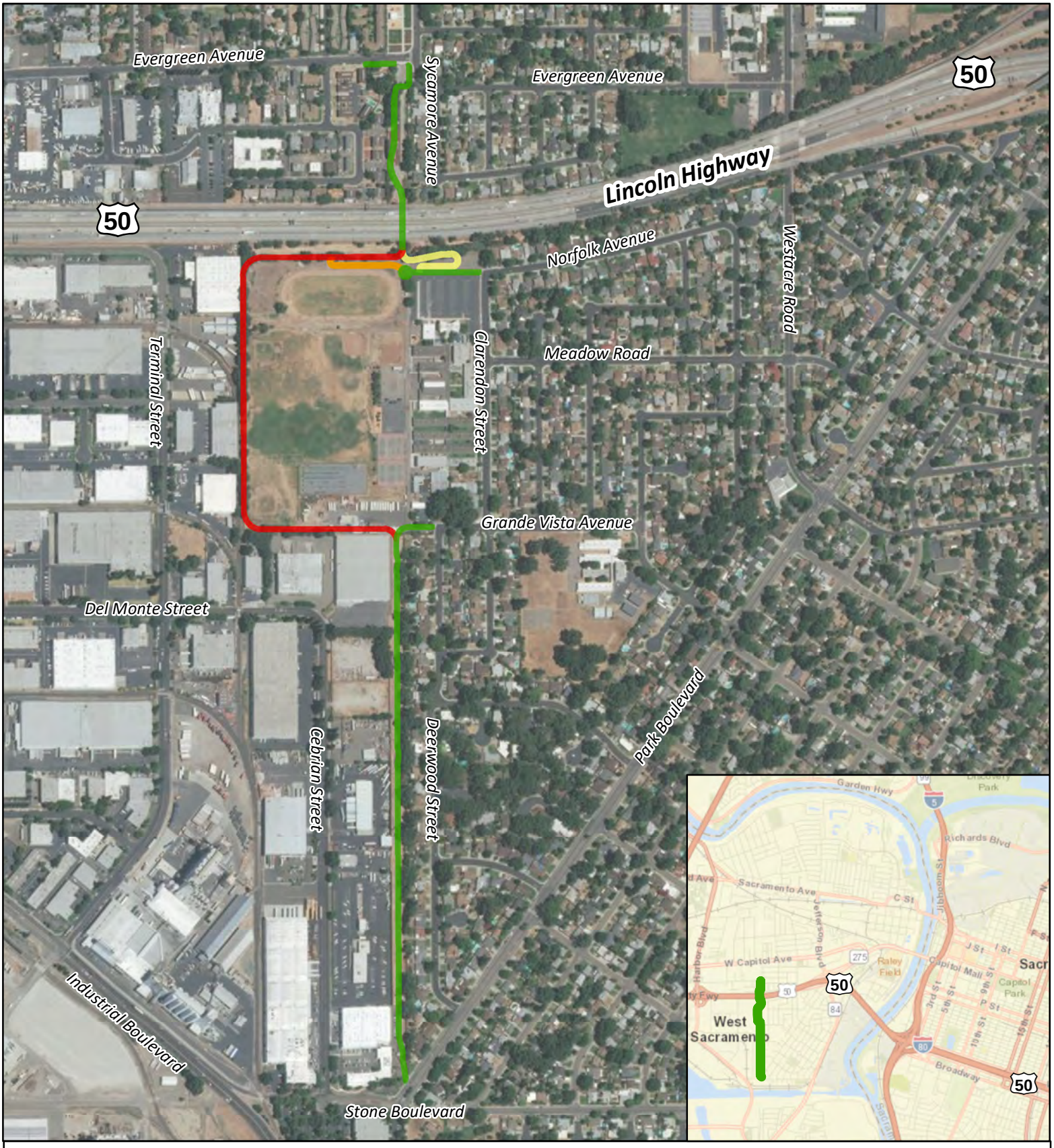
Source: ESRI Online Basemap, Aerial Imagery andm Open Street Map, Yolo County Coordinate System NAD 83 State Plane California II FIPS 0402 Feet

Notes: This map was created for informational and display purposes only

Sycamore Trail Phase II & III Extension Project
West Sacramento, CA

Regional Map

Figure 1



Common between Options

Western Ramp Option

Option 1

Eastern Ramp Option

0 380 760 1,520 Feet

N

drake haglan AND ASSOCIATES

Source: ESRI Online Basemap, Aerial Imagery and Open Street Map, Yolo County Coordinate System NAD 83 State Plane California II FIPS 0402 Feet

Notes: This map was created for informational and display purposes only

Sycamore Trail Phase II & III Extension Project West Sacramento, CA

Project Location Map

Figure 2

This Final IS/MND identifies the potential environmental impacts of the Project to determine whether the Project may have a significant effect on the environment and identifies mitigation measures, where applicable, to reduce or avoid significant effects. This Final Initial Study/Mitigated Negative Declaration has been prepared pursuant to the California Environmental Quality Act (CEQA) and the CEQA Guidelines (14 California Code of Regulations 1500 et seq.), which require that all state and local government agencies consider the environmental consequences of projects over which they have discretionary authority before acting on those projects. The City is a public agency with discretionary authority over the Project and is the Lead Agency under CEQA.

2 PROJECT DESCRIPTION

The City proposes to extend the Sycamore Trail from Evergreen Avenue north of US 50 to the intersection of Park and Stone Boulevards near the Deep Water Shipping Channel. The Project would generally follow the Sacramento Regional County Sanitation District LNWI sewer easement. A new pedestrian and bicycle overcrossing would be included over US 50, connecting Joey Lopes Park to the north with the School to the south. Details of the trail alignment through the School district property have not been finalized; however, all options currently being considered are located within the Project study area. All improvements would be in accordance with current City, AASHTO, and ADA standards.

2.1 Project Purpose and Need

The Project purpose is to increase the number of people in the City who bicycle or walk to work, to school, for errands, or for recreation. The Project is the next step to create and maintain a safe and convenient system of bicycle and pedestrian improvements in the City. When complete, the Project would ease some of the demand on Westacre Road under US 50, which currently serves as the primary access route for School students who live north of US 50. The Project would provide direct access to key destinations, be a critical component of an overall low-stress bicycling network, support development, and address the needs of some of the City's oldest neighborhoods.

The Project is needed because the Westacre Road undercrossing is congested and does not provide adequate safe pedestrian and bicycle access across US 50. Currently, pedestrian and bicycle access along Westacre Road is affected by limited shoulders, numerous driveways, intersections, pavement transitions, utilities, walls, fences, landscape features, and drainage structures. The Project would connect neighborhoods in northern West Sacramento and would provide safe access across US 50 for alternative forms of transportation.

The Project objectives are defined as:

- Improve public safety and interconnectivity within the City;
- Extend the Class I bike trail created in Phase I;
- Improve public safety and enhance pedestrian access across US 50 in the Project area;
- Minimize potential right-of-way take and work with property owners to avoid encroachment on private property; and
- Improve access to School for students north of US 50.

2.2 Project Description

The City plans to construct a trail and a pedestrian crossing over US 50 that would extend south from the newly-developed pedestrian and bicycle trail at Joey Lopes Park to the School site (Phase II) and continue south to the intersection of Park Boulevard and Stone Boulevard (Phase III). The Project site is located in the central region of West Sacramento within the County of Yolo, California. The Project site consists of approximately 39 acres and is located between Evergreen Avenue and Stone Boulevard, along the Sacramento Regional County Sanitation District LNWI sewer easement.

The pedestrian overcrossing design standards and criteria include a minimum vertical clearance of18feet 6 inches over US 50 (Caltrans requirement). The width of the pedestrian-bicycle overcrossing would be either 16 feet (a 14-foot-wide path with two one-foot-wide curbs) or 22 feet (a 20-foot-wide path with two one-foot-wide curbs). The typical trail section includes a 10 to 16 foot path with two two-to-four-foot-wide shoulders. The smallest trail section would have a width of 16 feet (a 12-foot-wide path and two 2-foot-wide shoulders).

2.2.1 Existing Conditions

The Project would generally follow the existing LNWI alignment. To the north of US 50 within the Project footprint there is an existing parking lot, community garden, mitigation basins established as a result of the Joey Lopes Park project, and open space along the LNWI sewer easement. Immediately south of US 50 is the School site. The trail may deviate from the LNWI alignment as it passes through School property. Within this portion of the Project footprint, there is the School parking lot, sports fields, play areas, and solar field and the Washington Unified School District (WUSD) Maintenance, Operations & Transportation Facility (WUSD Facility). Phase III would run from the School along the LNWI alignment south toward the intersection of Stone Boulevard and Park Boulevard.

In the Project vicinity, access across US 50 via designated bicycle facilities is restricted to the undercrossing along Westacre Road, which consists of two travel lanes, a Class III bikeway, and two 6 foot sidewalks. Currently, users along Westacre Road are affected by limited shoulders, numerous driveways, intersections, pavement transitions, utilities, walls, fences, landscape features, and drainage structures. Westacre Road does not provide adequate safe pedestrian and bicycle access under US 50. Crossing US 50 within the Project vicinity is also possible by the Harbor Boulevard overcrossing and the Jefferson Boulevard undercrossing. Neither the Harbor Boulevard overcrossing nor the Jefferson Boulevard undercrossing have bicycle lanes or other designated bicycle facilities, and both facilities have narrow elevated sidewalks.

2.2.2 Proposed Conditions

The Sycamore Trail Project has been divided into multiple phases. Phase I included construction of trail along the LNWI easement between West Capitol Avenue and Rice Avenue as well as the construction of Joey Lopes Park and trail improvements adjacent to the School. Construction of the portion of the Phase I Trail north of Joey Lopes Park (from Rice Avenue to West Capitol Avenue) is scheduled to be complete by late 2019.

Phase II would extend the trail south from Joey Lopes Park, construct a pedestrian bridge overcrossing of US 50, and make a connection to the School. This portion of the trail would result in the relocation of the existing community garden and a portion of the northern mitigation basin.

Phase III would complete trail improvements adjacent to the School, eventually leading to the intersection of Park Boulevard and Stone Boulevard. Pedestrian scale lighting is proposed along the entirety of the new trail to make the route safe and visible for pedestrian and bicycle traffic. The proposed trail and bridge would adhere to ADA specifications and would be designed as a Class I Bikeway.

Wayfinding signs would be installed along Clarendon Street to assist users in navigating the trail system. Wayfinding refers to information systems that guide people through a physical environment and enhance their understanding and experience of the space.

2.2.3 Utility Relocation

There are several public service utilities in the immediate vicinity of the pedestrian crossing and trail extensions:

- Overhead electrical (OHE) and telephone/communication (OHTC) distribution lines are located on wooden poles placed on either side of US 50. Several lines run perpendicular to the LNWI sewer easement at Evergreen Avenue, US 50, Grande Vista Avenue, and Stone Boulevard. Two lines run along the LNWI sewer easement north of US 50 and south of the School.
- Surface utilities are observed along the LNWI sewer easement north of US 50 and south of Westmore Oaks Elementary School. These utilities include electrical facilities, irrigation facilities, sewer vents, and buried sewer accesses. Solar energy facilities and irrigation facilities are also observed on the proposed options to the School property.
- Underground utilities observed along the proposed options include the LNWI sewer easement, storm drainage utilities, and water utilities.

2.2.4 Right-of-Way

The Project site is located within existing City-owned right-of-way, along the LNWI sewer easement, state right-of-way, and within the WUSD Facility. The existing land uses consist of US 50, the School, the WUSD Facility, and residential and industrial properties. The LNWI sewer easement is approximately 100 feet wide north of US 50 and between 40 and 75 feet wide south of the School.

Approximately 22 acres of the School is proposed to be converted to a City-maintained park as part of a separate project. WUSD has provided the City with a Letter of Intent (LOI) to Support the Development of a Recreation Reuse Plan for Portions of the Westmore Oaks Elementary and Washington Middle College High School Properties and Amendment to the Partner in Use of Facilities Agreement. This LOI states that WUSD intends to support and approve the conversion of the 22 acres of the School that may become a City maintained sports facility, pending a recreation reuse plan to be prepared for the facility and/or grant funding awards. The School would most likely maintain ownership of the land for potential future use.

The Project would cross the WUSD Facility driveway to establish connection between Phase II of the Project (the bicycle-pedestrian overcrossing and trail adjacent to or through the School) and Phase III of the Project (the trail segment from the School to the intersection of Park and Stone Boulevards).

Portions or the entirety of a number of LNWI sewer easement parcels (APN 067-260-89, APN 058-220-66, APN 067-170-18, APN 058-220-68, APN 067-170-18, APN 058-231-22, APN 067-170-16, APN 067-170-14, APN 058-240-81, APN 067-170-11, and APN 058-144-34) and School parcels (APN 067-180-14 and APN 067-180-15) would be permanently used for the bicycle and pedestrian trail. Permanent right-of-way acquisition would be required from industrial parcel APN 067-150-01. Permanent right-of-way may be required from part of residential parcel APN 058-144-086 to more safely align the southern trail segment with Park Boulevard. Additional temporary easements may be necessary for the staging of construction equipment.

2.3 Construction Activities

Construction would consist of the following activities in this general order:

2.3.1 Installing Construction Area and Detour Signs

No less than 30 days in advance of construction operations, necessary freeway segment closure signs, signs warning motorists of future construction, and temporary signal systems would be installed along US 50 and at the US 50 exit and entrance ramps designated in the Project detour map **(Figure 3).** Signs would display the exact date and time window in which construction on US 50 would take place and would remain in place throughout the duration of Phase II construction. The number and placement of temporary signal systems and lane closure signs for Phase III construction will be determine during the PS&E phase. All temporary traffic control equipment and procedures would comply with the 2015 Caltrans Standard Specification 12 and the 2015 Caltrans Revised Specifications for Section 12.

2.3.2 Relocating Utilities

Existing utilities which conflict with proposed improvements and equipment would be relocated. Underground relocation may be considered depending on whether it may conflict with the Project.

2.3.3 Clearing, Grubbing, and Tree Removal

Minor ground disturbances and vegetation removal would occur along the LNWI sewer easement and on the School sports fields. The maximum depth of excavation is expected to be up to 10 feet for construction of the pedestrian bridge structure and 3 feet for the trail extensions.

2.3.4 New Bridge Foundations

The new abutment seat and associated foundations would involve excavations of up to 10 feet. It is anticipated that foundation elements would consist of cast-in drilled-hole piling supporting single column bents. Temporary shoulder impacts and potential lane closures would be necessary to install the support in the center divider of US 50.

Road Closure Signs

Roadway Closure

Detour Route

5,300 2,650 0 5,300 Feet

N

drake haglan AND ASSOCIATES

Source: ESRI Online Basemap, Aerial Imagery and Open Street Map, Yolo County Coordinate System NAD 83 State Plane California II FIPS 0402 Feet

Notes: This map was created for informational and display purposes only.

Sycamore Trail Phase II & III Extension Project
West Sacramento, CA

Project Detour Map

Figure 3a

Road Closure Signs

Roadway Closure

Detour Route

1,100 550 0 1,100 Feet

N

drake haglan AND ASSOCIATES

Source: ESRI Online Basemap,
Aerial Imagery and Open Street Map,
Yolo County Coordinate System
NAD 83 State Plane California II
FIPS 0402 Feet

Notes: This map was created for informational and display purposes only.

Sycamore Trail Phase II & III
Extension Project
West Sacramento, CA

Project Detour Map

Figure 3b

Road Closure Signs

Roadway Closure

Detour Route

1,200 600 0 1,200 Feet

N

drake haglan AND ASSOCIATES

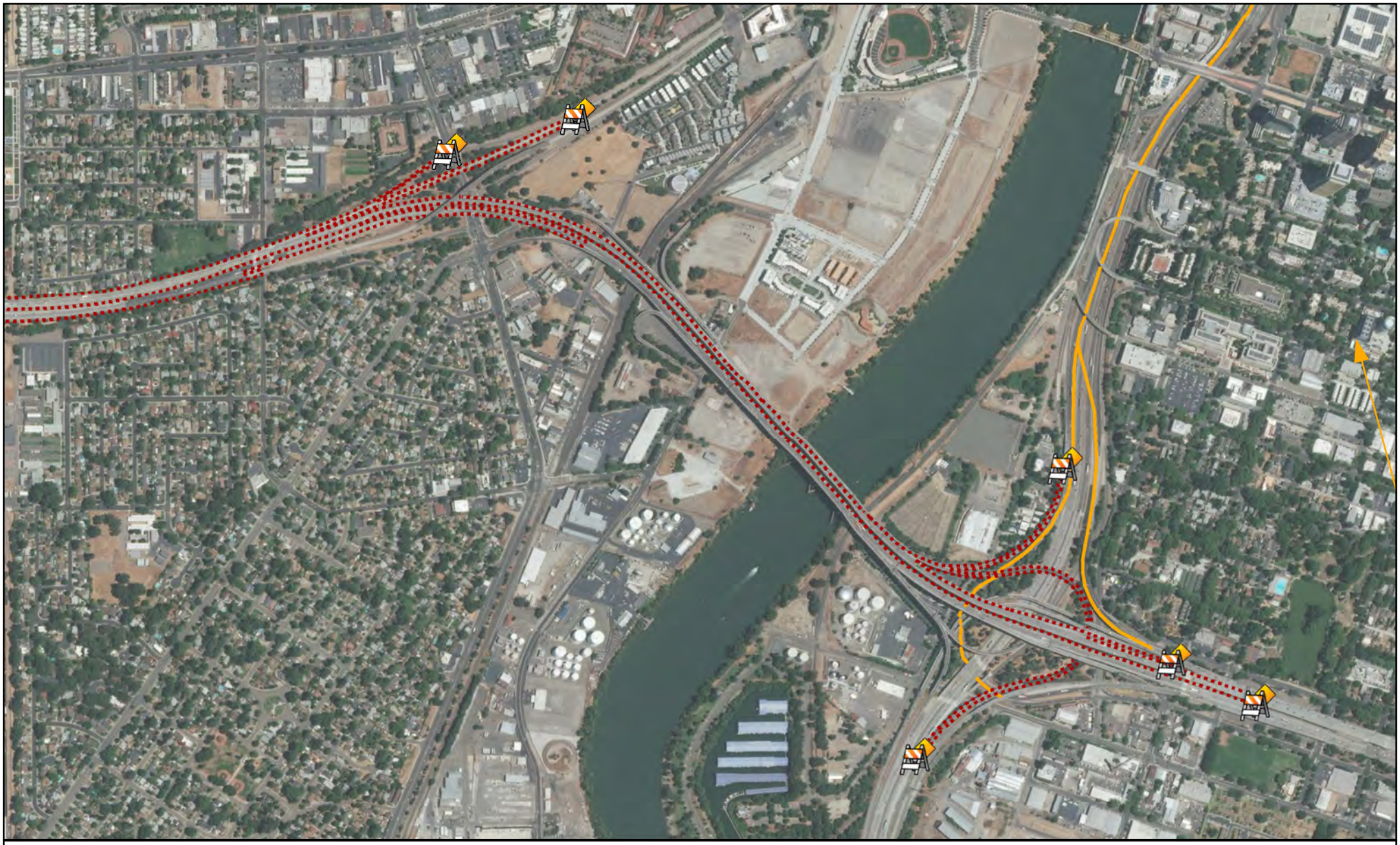
Source: ESRI Online Basemap, Aerial Imagery and Open Street Map, Yolo County Coordinate System NAD 83 State Plane California II FIPS 0402 Feet

Notes: This map was created for informational and display purposes only.

Sycamore Trail Phase II & III Extension Project
West Sacramento, CA

Project Detour Map

Figure 3c



Road Closure Signs

Roadway Closure

Detour Route

1,400 700 0 1,400 Feet

N

drake haglan AND ASSOCIATES

Source: ESRI Online Basemap, Aerial Imagery and Open Street Map, Yolo County Coordinate System NAD 83 State Plane California II FIPS 0402 Feet

Notes: This map was created for informational and display purposes only.

Sycamore Trail Phase II & III Extension Project
West Sacramento, CA

Project Detour Map

Figure 3d

2.3.5 New Bridge Construction

New bridge construction would involve placement of falsework to support the wet concrete of the superstructure, construction of bridge formwork, placing reinforcement, and then casting the bridge superstructure. A concrete curb with pedestrian fencing would be placed at the edge of the deck, as well as along the ramp approaches to the bridge. Temporary closure of the highway with a detour onto Interstate 80 (I-80) and Interstate 5 (I-5) would be required for the installation and removal of falsework **(**see **Figure 3).**

2.3.6 New Trail Extension

Trail extension construction would require excavation for a new structural section and placement of aggregate base and an asphalt surface.

2.3.7 New Retaining Wall Construction

Retaining walls would be constructed at both the north and south approaches to the bridge structure. It is anticipated that a mechanically-stabilized embankment with precast concrete panels would be used to retain the approach fill.

Table 2 provides a description of the type of equipment likely to be used during the construction of the proposed Project.

Table 2. Construction Equipment

Equipment	Construction Purpose
Hydraulic hammer	Demolition
Hoe ram	Demolition
Jack hammer	Demolition
Water truck	Earthwork construction, dust control
Bulldozer / loader	Earthwork construction, clearing and grubbing
Haul truck	Earthwork construction, clearing and grubbing
Front-end loader	Dirt or gravel manipulation
Grader	Ground grading and leveling
Dump truck	Fill material delivery
Bobcat	Fill distribution
Excavator	Soil manipulation
Compaction equipment	Earthwork
Roller / compactor	Earthwork and asphalt concrete construction
Backhoe	Soil manipulation, drainage work
Drill rig	Construction of drilled or driven pile foundations

Holding tanks	Slurry storage for pile installation
Crane	Placement of falsework beam, lifting of reinforcement cages
Concrete truck and pump	Placing concrete
Paver	Asphalt concrete construction
Truck with seed sprayer	Erosion control landscaping
Generators	Power hand tools

2.3.8 Construction Schedule and Timing

Construction is currently scheduled to start in the spring of 2019 and last for 8 to 12 months.

2.4 Permits and Approvals Needed

The following permits, reviews, and approvals are required for Project construction:

Table 3. Project Permits and Approvals

Agency	Permit/Approval	Status
Caltrans/FHWA	Approval of Categorical Exclusion under NEPA	Follows approval of technical studies
Caltrans	Construction Encroachment Permit	Application to follow release of IS/MND
Yolo-Solano Air Quality Management District	General permit to construct	Application to follow release of IS/MND
City of West Sacramento	Grading permit	Application to follow release of IS/MND
State Water Resource Control Board	General construction activity stormwater discharge permit	File Notice of Intent (NOI) and prepare Stormwater Pollution Prevention Plan (SWPPP) required prior to construction

3 ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The Project could potentially affect the environmental factor(s) checked below. The following pages present a more detailed checklist and discussion of each environmental factor.

- ☒ Aesthetics
- ☒ Biological Resources
- ☐ Greenhouse Gas Emissions
- ☐ Land Use and Land Use Planning
- ☐ Population and Housing
- ☒ Transportation and Traffic
- ☐ Agriculture and Forestry Resources
- ☒ Cultural Resources
- ☒ Hazards and Hazardous Materials
- ☐ Mineral Resources
- ☒ Public Services
- ☒ Tribal Resources
- ☐ Air Quality
- ☐ Geology, Soils and Seismicity
- ☐ Hydrology and Water Quality
- ☒ Noise
- ☐ Recreation
- ☐ Utilities/Service Systems

3.1 Determination (To be completed by Lead Agency)

On the basis of this initial study:

☐ I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.

☒ I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.

☐ I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.

☐ I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.

☐ I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, no further environmental documentation is required.

_____	_____
Signature	Date
_____	_____
Printed Name	For

4 ENVIRONMENTAL CHECKLIST

4.1 Aesthetics

Issues (and Supporting Information Sources)	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
Aesthetics – Would the project:				
a) Have a substantial adverse effect on a scenic vista?	☐	☐	☐	☒
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	☐	☐	☐	☒
c) Substantially degrade the existing visual character or quality of the site and its surroundings?	☐	☒	☐	☐
d) Create a new source of substantial light or glare which would adversely affect daytime or nighttime views in the area?	☐	☒	☐	☐

4.1.1 Setting

A Visual Impact Assessment (Minor Level) was prepared for the Project and is available for review at the City of West Sacramento Department of Public Works during business hours (Caltrans, 2018f). This Visual Impact Assessment follows the guidance and the definitions outlined in the publication *Guidelines for the Visual Impact Assessment of Highway Projects* published by the U.S. Department of Transportation Federal Highway Administration (FHWA) in January 2015.

Visual character is a description (not evaluation) of a site, and includes attributes such as form, line, color, and texture. Visual quality is the intrinsic appeal of a landscape or scene due to the combination of natural and built features in the landscape; and this analysis rates visual quality as high, moderate, or low. Visual sensitivity is the level of interest or concern that the public has for maintaining the visual quality of a particular aesthetic resource and is a measure of how noticeable proposed changes might be in a particular scene based on the overall clarity, distance, and relative dominance of the proposed changes in the view, as well as the duration that a particular view could be seen.

The landscape at the Project site is characterized by residential, commercial, and industrial land cover over valley landform. The land use within the Project site is primarily urban public/quasi-public/open space, but also includes areas of urban residential and industrial land uses. The existing visual quality of the Project site at the proposed trail is moderate and the existing visual quality at the proposed US 50 overcrossing is moderately low (Caltrans, 2018f). According to the Caltrans California Scenic Highway Mapping System and the FHWA database, there are no national scenic byways or state scenic highways within the Project vicinity (Caltrans, 2017f; FHWA, 2017).

Viewer groups include roadway users (people with views from the trail or US 50) and roadway neighbors (people with views of the trail or overcrossing structures from adjacent to the trail and US 50). Average overall viewer response to the Project is anticipated to be moderately low (Caltrans, 2017f).

4.1.2 Discussion

a) **No Impact.** There are no scenic vistas located within the Project site, and a scenic resource evaluation was not conducted because the Project is not within the limits of an officially designated state scenic highway or an officially designated scenic byway (Caltrans, 2017f, FHWA, 2017). The Project would be consistent with the current land uses within the Project vicinity (e.g., the LNWI sewer easement would still have the same General Plan designation and zoning), nor would the Project cause indirect changes to land uses within the Project vicinity.

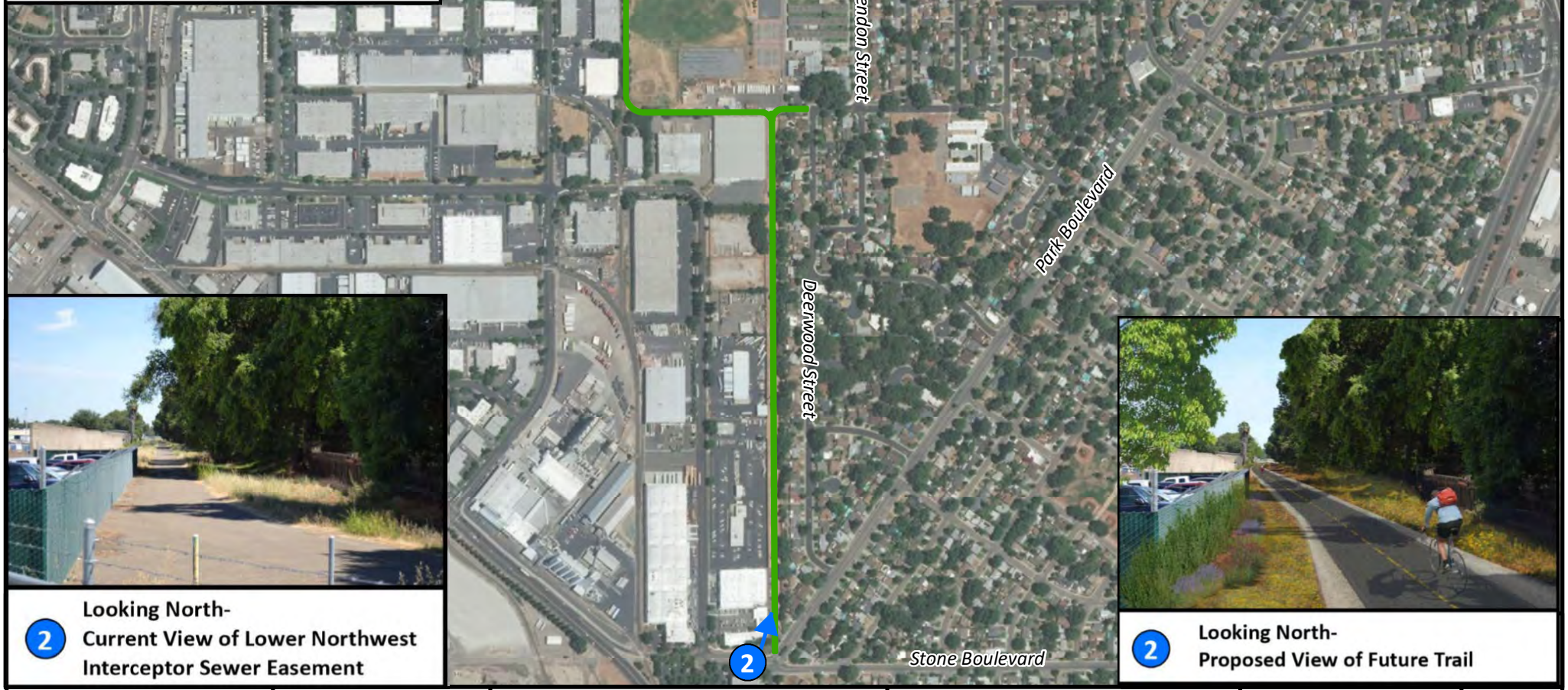
b) **No Impact.** The Project is not within the vicinity of a state scenic highway. There are no officially designated state scenic highways or scenic byways within the City (Caltrans, 2017c). State Route (SR) 16 is designated as an eligible state scenic highway; however, SR 16 is located approximately 15 miles west of the Project site. The nearest designated state scenic highway is SR 160, located approximately 8 miles south of the Project site. The nearest officially designated national scenic byway would be the Ebbetts Pass National Scenic Byway, approximately 68 miles southeast of the Project site.

c) **Less-than-Significant Impact with Mitigation.** Project construction would result in some changes to local visual conditions, but the changes to the Project site visual character would remain compatible to the existing visual corridor after construction and the alteration to visual quality that would be caused by the Project would not reduce visual quality within the Project corridor as a whole (**Figure 4 and Figure 5**). Implementation of **MM AES-1** would ensure that the Project would not substantially alter the visual quality of the trail segment or US 50 corridors and that change to the visual character of the trail segment and US 50 segment corridors would remain compatible to the corridor after Project construction.

d) **Less-than-Significant Impact with Mitigation.** The Project is located where street lighting is currently present. Roadway traffic and lighting from private properties are also sources of nighttime light. The bicycle-pedestrian overcrossing fencing, pedestrian scale lighting, and the structure could potentially introduce new sources of light and glare to the Project vicinity. Pedestrian scale lighting on both the overcrossing and the trail could introduce new sources of light to the Project vicinity. The Project is not a vehicular facility and would not increase roadway capacity, so a greater number of vehicles would not be introduced in the Project vicinity as a result of Project construction. Implementation of **MM AES-2** would ensure that potential impacts of lighting and glare are minimized and remain less than significant.

1 Looking Northwest-
Current View of US 50 at
Future Overcrossing Location

1 Looking Northwest-
Proposed View of US 50 at
Future Overcrossing Location



2 Looking North-
Current View of Lower Northwest
Interceptor Sewer Easement

2 Looking North-
Proposed View of Future Trail

drake haglan AND ASSOCIATES

Source: ESRI Online Basemap, Aerial Imagery,Yolo County Coordinate System NAD 83 State Plane California II FIPS 0402 Feet

Notes: This map was created for informational and display purposes only

0 300 600 1,200 Feet

N

Sycamore Trail Phase II & III Extension Project
West Sacramento, CA

Key Views and Proposed Views

Figure 4

Figure 5 - Site Photos

Sycamore Trail Phase II & III
Site Photos
Figure 5

Drone aerial photograph of the LNWI sewer easement at Evergreen Avenue, looking southbound.

Drone aerial photograph of the LNWI sewer easement at Evergreen Avenue, looking downward.

Drone aerial photograph of the LNWI sewer easement/northern project site connection to Joey Lopes Park taken slightly south of Evergreen Avenue, looking northbound.

Drone aerial photograph of Westmore Oaks Elementary School and the LNWI sewer easement slightly north of US 50, looking southbound.
Source: Drake Haglan & Associates, 2017

Figure 5

Drone aerial photograph of Westmore Oaks Elementary School at the athletic field, looking southbound.

Drone aerial photograph of Westmore Oaks Elementary School athletic field and School parking lot at the athletic field and the parking lot , looking downward.

Drone aerial photograph of the LNWI sewer easement at Grande Vista Avenue, looking southbound.

4.1.3 Mitigation Measures

MM AES-1: Implement the following into the Project to mitigate potential visual impacts:

- Incorporate designs, such as architectural form liners, concrete staining on the exterior girders and bridge railing, and aesthetic treatment on overcrossing fencing, to maintain aesthetic consistency with the surrounding residential and industrial community.
- Place buffer vegetation plantings directly adjacent to the trail segment fencing on the trail side to minimize the potential visual prominence and imbalance created by fencing.
- Choose a fencing design option for the southern trail segment that is aesthetically consistent with the surrounding residential community and minimizes the potential for pedestrians and cyclists to feel too enclosed, claustrophobic, or generally unsafe.

MM AES-2: Chose and develop pedestrian lighting in coordination with community involvement, that lessens the potential effects of sunrise, sunset, daytime, and nighttime glare on US 50 roadway users.

4.1.4 References

California Department of Transportation (Caltrans), 2017c. Yolo County scenic highways. Available at: http://www.dot.ca.gov/hq/LandArch/16_livability/scenic_highways/. Accessed February 8, 2018.

Caltrans, 2018f. Visual Impact Assessment (Minor Level): Sycamore Trail Phase II & III Extension Project. March 2018. Prepared for the City of West Sacramento by Drake Haglan & Associates.

City of West Sacramento, 2013 West Sacramento Parks Master Plan. September 2003. Prepared by Smith Group, JJR.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

U.S. Department of Transportation, Federal Aviation Administration (FAA), 2007. Advisory Circular AC 70/7460-1K: Obstruction Marking and Lighting. February 1, 2007.

U.S. Department of Transportation, Federal Highway Administration (FHWA), 2015. Guidelines for the Visual Impact Assessment of Highway Projects. Prepared by ICF International.

FHWA, 2017. America's Byways. Available at: https://www.fhwa.dot.gov/byways/states/CA. Accessed December 11, 2017.

4.2 Agricultural and Forest Resources

Issues (and Supporting Information Sources)	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
Agricultural and Forest Resources – In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board.				
Would the project:				
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	☐	☐	☐	☒
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	☐	☐	☐	☒
c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	☐	☐	☐	☒
d) Result in the loss of forest land or conversion of forest land to non-forest use?	☐	☐	☐	☒
e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to non-agricultural use or conversion of forest land to non-forest use?	☐	☐	☐	☒

4.2.1 Setting

According to the California Department of Conservation Farmland Mapping and Monitoring Program, land within the Project vicinity is categorized as urban and built up land (CDC, 2017b). The Project site is zoned as one-family residential (R-1-A), multifamily residential (R-3), public/quasi-public (PQP), public open space (POS), and light industrial (M-1) in the City Municipal Code; there are no lands zoned as forestland or timberland within the Project vicinity (City of West Sacramento, 2016a; City of West Sacramento, 2018b).

4.2.2 Discussion

a) **No Impact.** According to the California Department of Conservation Farmland Mapping and Monitoring Program, there are no lands designated as prime farmland, unique farmland, or farmland of statewide importance located within the Project vicinity (CDC, 2017b).

b) **No Impact.** According to California Department of Conservation, the Project site does not include agricultural land enrolled under the Williamson Act (CDC, 2012). Therefore, the Project would not result in any impacts to any lands covered by a Williamson Act contract.

c) **No Impact.** There are no land uses within the Project vicinity that are zoned as forestland or timberland (CDC, 2017b; City of West Sacramento, 2016a; City of West Sacramento, 2016b; City of West Sacramento, 2018b).

d) **No Impact.** There are no land uses within the Project vicinity that are zoned as forestland or timberland (CDC, 2017b; City of West Sacramento, 2016a; City of West Sacramento, 2016b; City of West Sacramento, 2018b). No forest conversion would occur as a result in the loss of forest land or conversion of forest land due to the Project.

e) **No Impact.** The Project does not propose any new land uses that would indirectly cause the permanent conversion of existing agricultural lands or result in any other actions that would adversely impact agricultural lands. Agricultural lands are not located within the Project vicinity (CDC, 2012; CDC, 2017b).

4.2.3 References

California Department of Conservation (CDC), 2012. Yolo County Williamson Act for Years 2010/2011. Available at: ftp://ftp.consrv.ca.gov/pub/dlrp/wa/yolo_10_11_WA.pdf. Accessed February 8, 2018.

CDC, 2017b. California Important Farmland Finder. Available at: https://maps.conservation.ca.gov/DLRP/CIFF/. Accessed February 8, 2018.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

4.3 Air Quality

Issues (and Supporting Information Sources)	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
Air Quality – Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations.				
Would the project:				
a) Conflict with or obstruct implementation of the applicable air quality plan?	☐	☐	☒	☐
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	☐	☐	☒	☐
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	☐	☐	☒	☐
d) Expose sensitive receptors to substantial pollutant concentrations?	☐	☐	☒	☐
e) Create objectionable odors affecting a substantial number of people?	☐	☐	☒	☐

4.3.1 Setting

The Project site is located in Yolo County within the Sacramento Valley Air Basin and is under the jurisdiction of the Yolo-Solano Air Quality Management District (YSAQMD) (ARB, 2012). The YSAQMD is one of 35 regional air quality districts in California and has jurisdiction over all of Yolo County and the northeast portion of Solano County. Air quality districts are public health agencies whose mission is to improve the health and quality of life for all residents through effective air quality management strategies. Yolo County is also a member of the Sacramento Area Council of Governments (SACOG), a regional planning association that also includes the counties of El Dorado, Placer, Sacramento, and Yuba and is located within the Sacramento Valley Air Basin (SACOG, 2017a). SACOG is responsible for regional transportation planning within its jurisdiction and preparing air quality conformity analyses, documents that are used to bring regional emissions into compliance with federal and state air quality standards pursuant to the Clean Air Act.

The Clean Air Act requires the U.S. Environmental Protection Agency (U.S. EPA) to set National Ambient Air Quality Standards (NAAQS) for major pollutants that could be detrimental to the environment and human health. The California Ambient Air Quality Standards (CAAQS) are the California state equivalent of the NAAQS. An air basin is in “attainment” (compliance) when the levels of the pollutant in that air basin are below NAAQS and CAAQS thresholds. **Table 4** provides information on the NAAQS and **Table 5** provides information on the CAAQS.

Table 4. NAAQS

Pollutant		Standard Type	Averaging Time	Concentration Threshold	Form
Carbon monoxide (CO)		Primary	8 hours	9 ppm	Not to be exceeded more than once per year
			1 hour	35 ppm	
Lead (Pb)		Primary and secondary	Rolling 3 month average	0.15 µg/m^3	Not to be exceeded
Nitrogen dioxide (NO_2)		Primary	1 hour	100 ppb	98th percentile of 1-hour daily maximum concentrations, averaged over 3 years
		Primary and secondary	1 year	53 ppb	Annual mean
Ozone (O_2)		Primary and secondary	8 hours	0.070 ppm	Annual fourth-highest daily maximum 8-hour concentration, averaged over 3 years
Particulate matter (PM)	$PM_{2.5}$	Primary	1 year	12.0 µg/m^3	Annual mean, averaged over 3 years
		Secondary	1 year	15.0 µg/m^3	Annual mean, averaged over 3 years
		Primary and secondary	24 hours	35 µg/m^3	98th percentile, averaged over 3 years
	PM_{10}	Primary and secondary	24 hours	150 µg/m^3	Not to be exceeded more than once per year on average over 3 years
Sulfur dioxide (SO_2)		Primary	1 hour	75 ppb	99th percentile of 1-hour daily maximum concentrations, averaged over 3 years
		Secondary	3 hours	0.5 ppm	Not to be exceeded more than once per year

Source: U.S. EPA, 2016

Table 5. CAAQS

Pollutant		Averaging Time	Concentration Threshold
Carbon monoxide (CO)		8 hours	0.09 ppm
		1 hour	0.070 ppm
Lead (Pb)		1.5	0.15 µg/m^3
Nitrogen dioxide (NO_2)		1 hour	0.18 ppm
		Annual arithmetic mean	0.030 ppm
Ozone (O_2)		8 hours	0.09 ppm
		1 hour	0.070 ppm
Particulate matter (PM)	$PM_{2.5}$	Annual arithmetic mean	12.0 µg/m^3
	PM_{10}	24 hours	50 µg/m^3
		Annual arithmetic mean	20 µg/m^3
Sulfur dioxide (SO_2)		1 hour	0.25 ppm
		24 hours	0.04 ppm
Visibility reducing particles		9 hours	Extinction of 0.23 per kilometer
Sulfates		24 hours	25 µg/m3
Hydrogen sulfide		1 hour	0.03 ppm
Vinyl chloride		24 hours	0.01 ppm

Source: California Air Resources Board (ARB), 2016

The Project site is located in an area that is currently in federal non-attainment for ozone (moderate) and $PM_{2.5}$ (U.S. EPA, 2018a). The Project site is also located in an area that is currently in state non-attainment for ozone and PM_{10} (ARB, 2017).

4.3.2 Discussion

a) **Less-than-Significant Impact.** The Project purpose is to increase the number of people in West Sacramento who bicycle or walk to work, to school, for errands, or for recreation. The Project would not increase roadway capacity or service capacities that would induce unplanned growth or remove an existing obstacle to growth. The Project is consistent with the General Plan air quality objectives, SACOG air quality objectives, and applicable federal and state air quality plans (City of West Sacramento, 2010; City of West Sacramento, 2016a; City of West Sacramento, 2016b; SACOG, 2017a; YSAQMD, 2007). The Project would not increase long-term traffic levels and there would be no operational impacts to air quality.

b) **Less-than-Significant Impact.** Since the Project would not add lanes or increase capacity, it would only affect local air pollutants during construction (approximately 8 to 12 months). The Project would not affect long-term air pollutant emissions in the area or stationary air pollutant sources.

Construction

The primary impact to local air quality during construction would be PM_{10} and $PM_{2.5}$ emissions from dust-generating activities.

The YSAQMD has adopted the following rule that is appropriate and applies to the proposed project, which is summarized below:

Rule 3.1: *General Permit Requirements*. Requires any project that includes the use of certain equipment capable of releasing emission to the atmosphere as part of project operation to obtain a permit from the YSAQMD prior to equipment operation. Portable construction equipment with an internal combustion engine over 50 horsepower are required to have an YSAQMD permit or an ARB portable equipment registration.

The YSAQMD sets the following thresholds of significance for criteria pollutants: 10 tons/year of reactive organic gases (ROG), 10 tons/year of NO_x, 80 lbs/day of PM_{10}, and the violation of a state ambient air quality standard for carbon monoxide (CO) (YSAQMD, 2007). The Roadway Construction Emissions Modeling of the Project, attached to this IS/MND as **Appendix A,** predicts that the Project would have the potential to emit an average of 13.90 lbs/day of NO_x, 6.44 lbs/day of ROG, and 80.84 lbs./day of PM_{10} (Huss, Karen and Grant, John, 2017). With the assumptions that the project construction would take place over 12 months and construction would take place 22 days a month, the project would generate approximately 1.66 tons of NOx over the year that construction would take place.

The assumptions used during modeling include: 1) the types and quantities of construction equipment typical of bridge projects would be used, 2) all on-road equipment used for the Project would be year 2010 or newer models, and 3) all construction equipment would meet ARB Tier 4 requirements.

Implementation of the following best management practices (BMPs), standard to construction projects and not accounted for in the Roadway Construction Emissions Model, would minimize construction dust emissions including PM_{10} emissions and ensure that impacts to air quality are

less than significant. BMPs include, but are not limited to, the following measures that will be implemented by the City construction contractor through the SWPPP and section 14-9 of the Caltrans Standard Specifications to minimize construction dust emissions:

- Water all active construction sites at least twice daily. Frequency should be based on the type of operation, soil, and wind exposure.
- Maintain at least 2 feet of freeboard for haul trucks.
- Cover all trucks hauling dirt, sand, or loose materials.
- Apply nontoxic binders (e.g., latex acrylic copolymer) to exposed areas after cut and fill operations and hydroseed area.
- Apply chemical soil stabilizers on inactive construction areas (disturbed lands within construction projects that are unused for at least four consecutive days).
- Plant vegetative groundcover in disturbed areas as soon as possible.
- Cover inactive storage piles.
- Sweep street visible soil material and ensure soil is carried out of the construction site.
- Restrict unnecessary vehicle idling to five minutes.

c) **Less-than-Significant Impact.** The Project would result in minimal air pollutant emissions during the short-term duration of construction and would not result in an increase in operational activities or emissions. The YSAQMD states in its 2007 "Handbook for Assessing and Mitigating Air Quality Impacts" that "any proposed project that would individually have a significant air quality impact [parentheses omitted] would also be considered to have a significant cumulative impact" (YSAQMD, 2007). YSAQMD guidance also states that CO impacts are to be considered cumulatively significant when modeling shows that combined emissions from the Project and other existing and planned projects will exceed air quality standards, using YSAQMD's cumulative impact screening methods (YSAQMD, 2007). Therefore, the Project would not result in a cumulatively considerable net increase of any criteria pollutant for which the Project region is non-attainment under an applicable federal or state ambient air quality standard (ozone and PM_{10}).

d) **Less-than-Significant Impact.** Construction activities would occur over a brief duration within the estimated 8 to 12 month construction timeline. Residents located adjacent to the Project site and within the vicinity would be exposed to construction contaminants only for the duration of construction. This brief exposure period would substantially limit exposure to hazardous emissions. This brief exposure period is less than the two-year exposure period typically assumed for health risk analysis for small construction projects, and the three-year exposure period assumed for PM_{10} and CO hotspot analysis (Caltrans, 2017a). In addition, Project operation would not result in increased levels of air pollutants. This impact would be less than significant and no mitigation measures are required.

e) **Less-than-Significant Impact.** Generally, the types of projects or activities that pose potential odor problems include refineries, chemical plants, wastewater treatment plants, landfills, composting facilities, and transfer stations. The types of materials used in bridge construction generally do not produce objectionable odors (vehicles emissions, cement, welding equipment, etc.). The bridge replacement project is located within an urban area and is not expected to create objectionable odors or complaints from a substantial number of nearby residents that live along

Sycamore Avenue, Park Boulevard, or Stone Boulevard, or from a substantial number of individuals that attend or work at the School.

4.3.3 References

California Air Resources Board (ARB), 2012. California Air District Map for District Rules. March 23, 2012. Available at: https://www.arb.ca.gov/drdb/dismap.htm. Accessed May 16, 2018.

ARB, 2016. Ambient Air Quality Standards.

ARB, 2017. Area Designation Maps / State and National. October 16, 2017. Available at: https://www.arb.ca.gov/desig/adm/adm.htm. Accessed February 7, 2018.

California Department of Transportation (Caltrans), 2017a. Standard Environmental Reference: Chapter 11 – Air Quality. Available at: http://www.dot.ca.gov/ser/vol1/sec3/physical/ch11air/chap11.htm. Accessed May 16, 2018.

City of West Sacramento, 2010. Draft Climate Action Plan. August 2010.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

Harris Miller & Hanson (HMM&H); 2006. Transit noise and vibration impact assessment (FTA-VA-90-1003-06). Prepared for the Federal Transit Administration.

Huss, Karen and Grant, John, 2017. Road Construction Emissions Model, Version 8.1.0. Prepared for the Sacramento Metropolitan Air Quality Management District (SMAQMD).

Sacramento Area Council of Governments (SACOG), 2017a. Air quality objectives. Available at: https://www.sacog.org/air-quality. Accessed on February 7, 2018.

U.S. Environmental Protection Agency (U.S. EPA), 2016. NAAQS Table. December 20, 2016. Available at: https://www.epa.gov/criteria-air-pollutants/naaqs-table. Accessed May 16, 2018.

U.S. EPA, 2018a. Current Nonattainment Counties for All Criteria Pollutants. January 31, 2018. Available at: https://www3.epa.gov/airquality/greenbook/ancl.html. Accessed February 7, 2018.

Yolo-Solano Air Quality Management District (YSAQMD), 2007. Handbook for Assessing and Mitigating Air Quality Impacts. July 11, 2007.

4.4 Biological Resources

Issues (and Supporting Information Sources)	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
Biological Resources – Would the project:				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special-status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	☐	☒	☐	☐
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	☐	☐	☐	☒
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	☐	☐	☐	☒
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	☐	☒	☐	☐
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	☐	☒	☐	☐
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	☐	☐	☐	☒

4.4.1 Setting

A Technical Memorandum for Biological Resources for the Sycamore Trail Phase II & III Extension Project [CML-5447(044)] was prepared for the Project and is available for review at the City Department of Public Works during business hours (Caltrans, 2018d). The Project is located in the Great Valley Ecological Section and Yolo-American Ecological Subsection (Caltrans, 2018d). Typical elevations within this ecological subsection range from 10 feet above sea level up to 40 feet above sea level. Historically, this region supported extensive marshes, riparian woodlands intermixed with oak woodland, vernal pools, and grasslands. Intensive agricultural and urban development has resulted in substantial habitat changes and conversions. Because most native habitats have been altered by changes in land use, native plant

communities are typically limited to areas along water courses and drainages, within designated reserves, or on untilled pasture lands.

An evaluation of biological resources was conducted to determine whether any special-status plant or wildlife species, or associated sensitive habitat occurs within the Project area. Data on the potential for special-status species and habitats known in the area was obtained from the California Department of Fish and Wildlife's (CDFW) California Natural Diversity Database (CNDDB), the United States Fish and Wildlife Service's (USFWS) Information for Planning and Consultation (IPaC) website, the NOAA Fisheries West Coast Region Species Lists, and the California Native Plant Society's (CNPS) Inventory of Rare Plants (Caltrans, 2018d). Maps and aerial photographs of the Project area and surrounding areas were reviewed. A field survey was conducted January 4, 2018. The biological study area is shown on **Figure 6**.

4.4.2 Regulatory Setting

4.4.2.1 *West Sacramento Tree Preservation Ordinance*

The City of West Sacramento Municipal Code (Municipal Code) Chapter 8.24 protects trees in order to promote public health, safety, and general welfare within the City. Municipal Code Section 8.24.040 states that it is unlawful for any person to perform any of the following acts with respect to street trees without a tree permit issued by the Tree Administrator:

a) Plant any tree within a tree maintenance strip or public right-of-way, other than those species that are designated in the West Sacramento Landscape Development Guidelines;
b) Move, remove, cut down, poison, set fire to or permit a fire to burn in proximity to, or perform or fail to perform any act which results in the unnatural death or destruction of a street tree;
c) Perform any activity that will interfere with or retard the natural growth of any street tree;
d) Perform any work or permit any work to be performed within the drip line area of a street tree which would endanger the tree; or
e) Trim or prune any branch of a street tree which is 5 inches in diameter or greater.

Municipal Code Section 8.24.060 also states regulations that protect landscape and heritage trees within the City. Landscape trees are defined as any tree or stand of trees that is especially prominent, stately, or which is of historical significance as designated by the City Council. Heritage trees are defined as any living tree with a trunk circumference of 75 inches or more or a native oak with a trunk circumference of 50 inches or more, both measured 4 feet 6 inches from ground level. The section states that it shall be unlawful to perform any of the following acts with respect to a landmark or heritage tree within the City limits without a tree permit issued by the Tree Administrator:

a) Move, remove, cut down, poison, set fire to or permit fire to burn in proximity to, or perform or fail to perform any act which results in the unnatural death or destruction of a landmark or heritage tree;
b) Perform any activity that will interfere with or retard the natural growth of any landmark or heritage tree;
c) Perform any work or permit any work to be performed within the drip line area of a landmark or heritage tree which would endanger the tree; or
d) Trim or prune any branch of a landmark or heritage tree which is 5 inches in diameter or greater.

Biological Study Area

Project Impact Area (PIA)

0 300 600 1,200 Feet

N

drake haglan AND ASSOCIATES

Source: ESRI Online Basemap, Aerial Imagery Map, Yolo County Coordinate System NAD 83 State Plane California II FIPS 0402 Feet

Notes: This map was created for informational and display purposes only

Sycamore Trail Phase II & III Extension Project
West Sacramento, CA

Project Impact Area and Biological Study Area

Figure 6

Municipal Code Section 8.24.070 states that during construction activities on any property upon which a landmark, heritage, or street tree is located, it is unlawful for any person to perform any of the following acts without a tree permit issued by the Tree Administrator, which permit shall not be denied if the activities are deemed necessary for the Project and proper care is taken to protect any landmark, heritage, or street tree:

a) Change the appropriate amount of irrigation or drainage water provided to any landmark, heritage, or street tree;
b) Trench, grade, pave, or otherwise damage or disturb any exposed roots within 1 foot outside the drip line area of any landmark heritage or street tree;
c) Park or operate any motor vehicle within 1 foot outside the drip line area of any landmark, heritage, or street tree;
d) Place or store any equipment or construction materials within one foot outside the dripline area of any landmark, heritage or street tree;
e) Place, apply, or attach any signs, ropes, cables or any other items to any landmark, heritage, or street tree;
f) Cut or trim any branch of any landmark, heritage or street tree that is 5 inches in diameter or greater; or
g) Place or allow to flow any oil, fuel, concrete mix, or other deleterious substance into or over within 1 foot outside the drip area of any landmark, heritage, or street tree.

4.4.2.2 *Migratory Raptors and Birds*

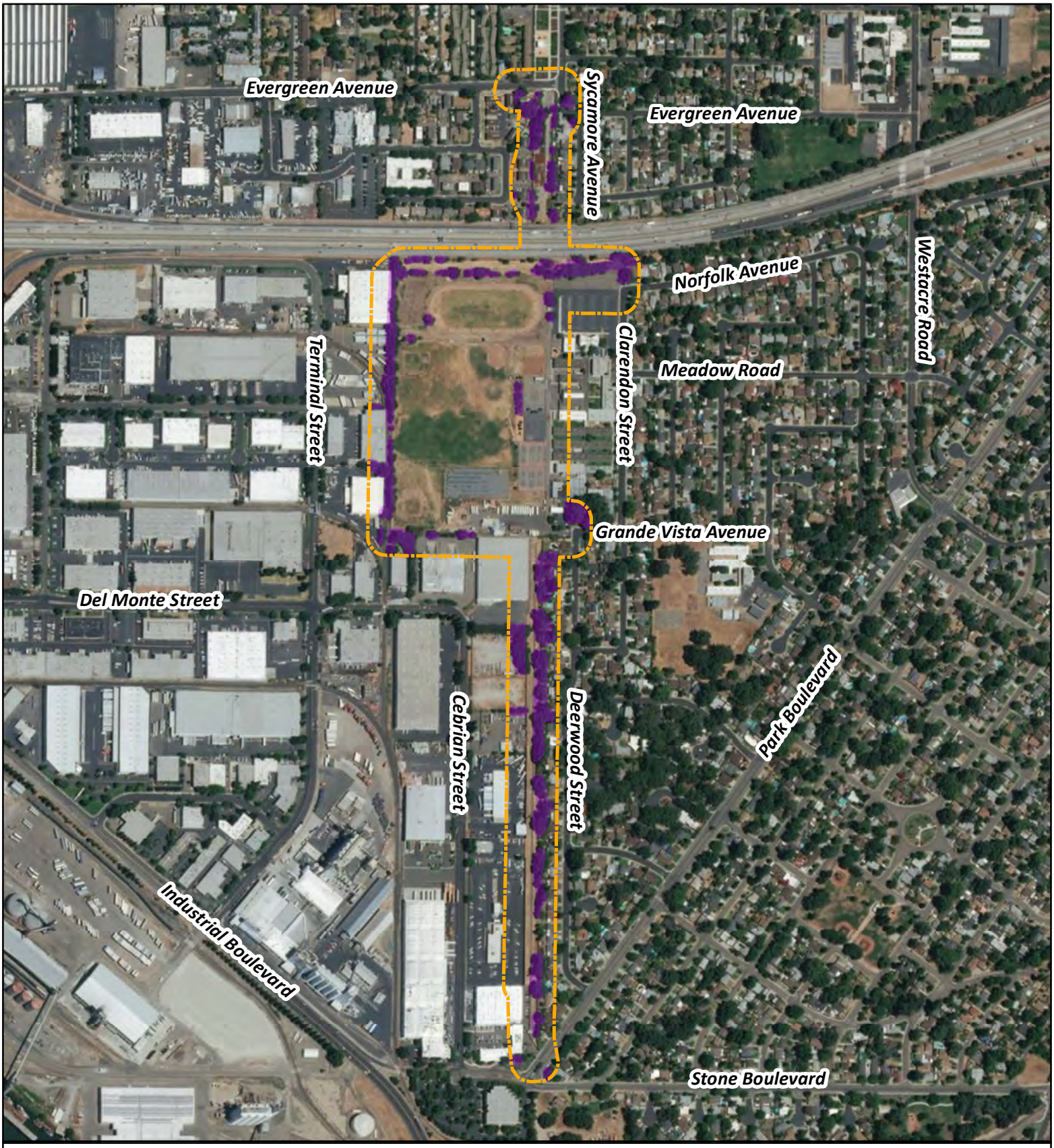
California Fish and Game Code (CFGC) (1 FGC §§ 3503, 3513, and 3800) protects migratory raptors and birds from harassment or harm and also protects their eggs and nestlings. Disturbance that causes nest abandonment and/or loss of reproductive effort is considered a "taking" by the CDFW.

Federal law also protects raptors, migratory birds, and their nests. The federal Migratory Bird Treaty Act (MTBA) (15 USC §§ 703-711; 16 USC §§ 7.3, Supp. 1989; 50 CFR § § 10, 21), prohibits killing, possessing, or trading of migratory birds. Executive Order 13186 (January 11, 2001) requires that any Project with federal involvement address the impact of federal actions on migratory raptors and birds. Potential nesting locations are shown on **Figure 7**.

4.4.2.3 *Invasive Species*

Executive Order 13112 (February 3, 1999) directs all federal agencies to refrain from authorizing funding or carrying out actions on projects that may spread invasive species. Other laws pertaining to the spread of noxious weeds include the Carlson-Foley Act of 1968 and the Federal Noxious Weed Act of 1974.

Executive Order 13112 further directs federal agencies to prevent the introduction of invasive species, to control and monitor existing invasive species populations, restore native species to invaded ecosystems, research and develop prevention and control methods for invasive species, and promote public education on invasive species.



Biological Study Area

Potential Nesting Locations

0 295 590 1,180 Feet

N

drake haglan AND ASSOCIATES

Source: ESRI Online Basemap, Aerial Imagery Map, Yolo County Coordinate System NAD 83 State Plane California II FIPS 0402 Feet

Notes: This map was created for informational and display purposes only

Sycamore Trail Phase II & III Extension Project
West Sacramento, CA

Potential Nesting Locations

Figure 7

4.4.3 Discussion

Less-than-Significant Impact with Mitigation. Noise associated with construction activities involving heavy equipment operation that occurs during the breeding season (generally between February 1 and August 31) could disturb nesting burrowing owls, Swainson's hawks, song sparrows, and other raptors and songbirds if an active nest is located near these activities. Potential impacts could include abandonment of nest sites and the mortality of young. Any disturbance that causes nest abandonment and subsequent loss of eggs or developing young at active nests located near the Project area would violate the California Endangered Species Act (CESA) (1 FGC §§ 2800, 3503, and 3503.5) and the MBTA. Implementation of **MM BIO-1** would ensure that the potential impacts to nesting birds are less than significant. The following minimization measures would ensure that the potential effects of invasive species on special status species and/or their habitat remains less than significant by preventing the introduction of invasive species into the Project site:

- Construction crew training about invasive plant identification and the importance of controlling and preventing the spread of invasive plant infestations;
- Surface disturbance in the construction work area will be minimized to the greatest extent possible;
- Where erosion control plantings are used, areas will be seeded with certified weed-free native mixes and, if appropriate, mulched with certified weed-free mulch to prevent invasive species from colonizing; and
- Revegetation of landscape areas will use native species, to the extent possible, and no invasive species will be used.

b.c) **No Impact.** There is no riparian habitat within the Project area or other habitat within the Project study area that would be considered sensitive as identified by local or regional plans, policies, and regulations or by the CDFG or USFWS (Caltrans, 2018d). According to the USFWS National Wetlands Inventory Mapper, are no federally protected wetlands within the Project study area (USFWS, 2018).

d) **Less-than-Significant Impact with Mitigation.** The Project would not substantially interfere with the movement of any wildlife species or with established native resident or migratory wildlife corridors or impede the use of native wildlife nursery sites. The Project area is not located within an established native resident or migratory wildlife corridor or wildlife nursery site (Caltrans, 2018d). Construction noise could temporarily alter foraging patterns of resident wildlife species and temporarily disrupt wildlife movement within the Project area. This disturbance would only occur during Project construction and the disruption of wildlife movement would be temporary in nature. Implementation of **MM BIO-1** would ensure that impacts to native resident wildlife species remain less than significant.

e) **Less-than-Significant Impact with Mitigation.** The Project includes the removal of multiple trees, which has the potential to conflict with the West Sacramento Tree Preservation Ordinance. The Project would result in the removal of up to six trees. No street trees would be removed, but many would be trimmed during construction activities. No heritage or landmark trees would be removed as a result of the Project but have the potential to be impacted by trimming and construction activities according to Section 8.24.070 of the tree preservation ordinance. All street,

landmark, and heritage trees within the City are protected by West Sacramento Tree Preservation Ordinance and all potential impacts to trees must be reviewed by a City Tree Administrator (West Sacramento, California, Municipal Code art. 8.24 § 040 [2018]). Implementation of **MM BIO-2** would ensure that conflicts with the West Sacramento Tree Ordinance remain less than significant.

f) **No Impact.** The Project is not within the jurisdiction of an adopted habitat conservation plan or natural community conservation plan (Caltrans, 2018a; Yolo Habitat Conservancy, 2018). Therefore, Project implementation would not conflict with the provisions of an adopted local, regional, or state habitat conservation plan. The closest habitat conservation plan jurisdiction to the Project site is the Natomas Basin Habitat Conservation Plan, which includes parts of southeastern Sutter County (City of Sacramento et al., 2003).

4.4.4 Mitigation Measures

MM BIO-1: *Conduct Preconstruction Tree Surveys.* To ensure there are no effects on nesting birds, a qualified biologist will conduct preconstruction tree surveys of the trees to be removed and within 250 feet of the Project construction area. Survey work will be done no more than two days prior to initiation of construction to minimize potential that nests are initiated after the survey and prior to the start of construction. If any occupied nests are detected, the tree will be flagged; a minimum buffer of 100 feet between the nest and construction zone will be established, unless determined otherwise by CDFW; and that area will be avoided until the qualified biologist has determined the nest is no longer active/occupied. Once the biologist has determined that young have fledged and the nest is no longer active, construction can resume in that area.

If no active nests are identified during the preconstruction survey, no further mitigation is necessary. If construction activities (i.e., vegetation and tree removal) are scheduled to begin during the nonbreeding season (September to January), preconstruction surveys would not be necessary.

MM BIO-2: *Replace and Protect Trees.* Implement the following measures prior to and during trail and overcrossing construction to mitigate potential impacts to the West Sacramento Tree Preservation Ordinance.

- Prior to removal of any trees, an International Society of Arboriculture-certified arborist shall conduct a tree survey in areas that may be impacted by construction activities. This survey shall document tree resources that may be adversely impacted by Project implementation. The survey will follow standard professional practices.
- Existing trees will be retained to extent feasible. A Tree Protection Zone (TPZ) shall be established around any tree or group of trees to be retained. The TPZ will be delineated by an International Society of Arboriculture-certified arborist. The TPZ shall be defined by the radius of the dripline of the tree(s) plus 1 foot. The TPZ of any protected trees shall be demarcated using fencing that will remain in place for the duration of construction activities.
- Construction-related activities shall be limited within the TPZ to those activities that can be done by hand. No heavy equipment or machinery shall be operated within the TPZ; if this is not possible, a 6 inch layer of bark mulch shall be placed where the equipment shall be under the dripline in order to protect the root system from too much compaction. Grading shall be prohibited within

the TPZ. No construction materials, equipment, or heavy machinery shall be stored within the TPZ.

- Prior to removal, all trees will be posted for public notice and a permit will be obtained (pursuant to City Code 8.24.040 and 8.24.060). The City will mitigate tree removal by replacing removed trees at a 1:1 ratio. All replacement trees will be oak or other native tree species for the area and would be planted with automatic irrigation to improve the survivability of the trees.

4.4.5 References

California Department of Transportation (Caltrans), 2018d. Technical Memorandum for Biological Resources for the Sycamore Trail Phase II & III Extension Project [CML-5447(044)]. February 27, 2018. Prepared by Drake Haglan & Associates.

City of Sacramento, Sutter County, Natomas Basin Conservancy, Reclamation District No. 1000, Natomas Central Mutual Water Company; 2003. Final Natomas Basin Habitat Conservation Plan. Sacramento and Yuba Cities, California. Prepared for United States Fish and Wildlife Service and the California Department of Fish and Game.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

U.S. Fish and Wildlife Service (USFWS), 2018. National Wetlands Inventory Mapper. May 1, 2018. Available at: https://www.fws.gov/wetlands/Data/Mapper.html. Accessed May 16, 2018.

Yolo Habitat Conservancy, 2017.Yolo Habitat Conservation Plan/Natural Community Conservation Plan: Volume 1, Public Review Draft. July 11, 2007. Prepared by ICF International.

Yolo Habitat Conservancy, 2018. Documents. Available at: https://www.yolohabitatconservancy.org/documents. Accessed May 16, 2018.

4.5 Cultural Resources

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Cultural Resources – Would the project:				
a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?	☐	☐	☒	☐
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	☐	☐	☒	☐
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	☐	☐	☒	☐
d) Disturb any human remains, including those interred outside of formal cemeteries?	☐	☒	☐	☐

4.5.1 Setting

A cultural resource is a broad term that includes prehistoric, historic, and traditional cultural properties that reflect the physical evidence of past human activity across the landscape. Cultural resources, along with prehistoric and historic human remains and associated grave-goods, must be considered under various federal, state, and local regulations including CEQA and the National Historic Preservation Act of 1966 (NHPA). Cultural resources that are listed on or eligible for inclusion in the National Register of Historic Places (NRHP) are also considered eligible for listing in the California Register of Historical Resources (CRHR). Those cultural resources that are listed in or eligible for inclusion in the CRHR are referred to as historical resources. To be considered a historical resource, or "historically significant," the resource must meet the criteria for listing in the CRHR, which include the following:

a) Is associated with the events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
b) Is associated with the lives of persons important to our past;
c) Embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic value; or
d) Has yielded, or may likely to yield, important information in prehistory or history.

A cultural resources investigation was conducted in 2017 for the Project that was in compliance with Section 106 of the NHPA, as it pertains to the administration of the federal-aid highway program in California (Section 106 PA) (Caltrans, 2017a). The investigation included a records search at the California Historical Resources Information System (CHRIS), background research, Native American consultation, and pedestrian survey. The investigation results show no known archaeological or historical resources in the Project area or within a half mile of the Project.

Paleontological resources are the fossilized evidence of organisms preserved in the geologic (rock) record. Fossils are considered nonrenewable resources that are protected by federal, state, and local environmental laws and regulations. Sedimentary rocks, and some volcanic and metamorphic rocks, have potential to yield significant fossiliferous deposits. According to the Society of Vertebrate Paleontology standards and guidelines, sedimentary rock units with a high potential for containing significant nonrenewable paleontological resources are those within which vertebrate or significant invertebrate fossils have been previously determined to be present, or likely to be present (SVP, 2010). The potential paleontological importance of the Project area can be assessed by identifying if the rock units are Pleistocene or older (older than 11,000 years) sedimentary deposits within the underlying landform. An individual vertebrate fossil specimen may be considered unique or significant if it is identifiable and well preserved, and it meets at least one of the following criteria:

- A type specimen (i.e., the individual from which a species or subspecies has been described);
- A member of a rare species;
- A species that is part of a diverse assemblage;
- A skeletal element different from, or a specimen more complete than, those now available for its species;
- A complete specimen; or
- At least 11,000 years or older.

4.5.2 Discussion

a,b) **Less-than-Significant Impact**. Background research and field surveys did not reveal any archaeological or historical resources in the Project area, and the Project area has a low to moderate potential to contain these resources (Caltrans, 2017a). Compliance with California Public Resources Code Sections 5097.5, 5097.9 et seq. and construction contract specifications, including halting construction in the vicinity of a potential cultural resources find and notifying the County to allow resource evaluation by a qualified archaeologist prior to resuming construction, would ensure any potential impacts on buried or previously undiscovered historical resources would be less than significant.

c) **Less-than-Significant Impact.** The underlying geologic landform in the Project area is Holocene-age (less than 10,000 years before present [BP]) alluvium (Jennings et. al, 1977), which is not considered a unique geologic feature but has the potential to contain buried paleontological resources. A search of the University of California Museum of Paleontology (UCMP) collections database identified 352 fossil occurrences in the County, 50 of which are primarily unidentified mammal from the Tertiary through Quaternary Period. One species of ground sloth (*P. harlani*) has been identified along Putah Creek and Cache Creek, in western Yolo County (UCMP, 2017). The majority of the recorded fossils is marine and includes several species of foraminifera, bony fish, and gastropods dating from 57 to 1.6 million years. Because no rare or complete fossil specimens are known to occur in the Project area, and the Holocene-age landform is not a unique geologic feature, there is a less-than-significant impact.

d) **Less-than-Significant Impact with Mitigation.** No formal cemeteries or human remains were identified during the field investigation and no burial sites are likely to be encountered during construction activities (Caltrans, 2017a). However, in the event of an unanticipated discovery of human remains, **MM CUL-1** would reduce the impact to less than significant with mitigation.

4.5.3 Mitigation Measures

MM CUL-1: *Follow Protocol for the Unanticipated Discovery of Cultural Resources or Human Remains*. If buried cultural materials are encountered during construction, stop work in that area until a qualified archaeologist can evaluate the find's nature and significance. In the event that human remains or associated funerary objects are encountered during construction, cease all work within the vicinity of the discovery. In accordance with CEQA and the California Health and Human Safety Code (14 CCR § 15064; 7 HSC § 7050.5), the County coroner will be contacted immediately. If the human remains are determined to be Native American, the coroner will notify the Native American Heritage Commission, who will notify and appoint a Most Likely Descendent (MLD). The MLD will work with a qualified archaeologist to decide the proper treatment of the human remains and any associated funerary objects.

4.5.4 References

California Department of Transportation (Caltrans), 2017a. Archaeological Survey Report: Sycamore Trail Phase II & III Extension Project, Federal Project No. CML5447(044). Prepared by LSA on behalf of Drake Haglan & Associates.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

The Society of Vertebrate Paleontology (SVP), 2010. Standard Procedures for the Assessment and Mitigation of Adverse Impacts to Paleontological Resources.

University of California Museum of Paleontology (UCMP), 2017. Collections. Available at: http://www.ucmp.berkeley.edu/science/collections.php. Accessed January 1, 2018.

4.6 Geology, Soils, and Seismicity

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Geology, Soils and Seismicity –Would the project:				
a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving: i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? (Refer to Division of Mines and Geology Special Publication 42.) ii) Strong seismic ground shaking? iii) Seismic-related ground failure, including liquefaction? iv) Landslides?	☐	☐	☒	☐
b) Result in substantial soil erosion or the loss of topsoil?	☐	☐	☒	☐
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?	☐	☐	☒	☐
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?	☐	☐	☒	☐
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?	☐	☐	☐	☒

4.6.1 Setting

The Project is located in the Sacramento Valley, within the Great Central Valley geomorphic province (CDC, 2017a). The Central Valley in not known to be seismically active; however, there are a number of Quaternary (from 1.6 million years old to the present) and pre-Quaternary (over 1.6 million years old) faults that are mapped within the County northwest of the Project site (CDC, 2015b). These faults include the East Valley fault, Dunnigan Hills fault, Capay fault, Sweitzer fault, West Valley fault, Wilson fault, Hunting Creek fault, and the Hunting fault (CDC, 2015b). The nearest fault is the Dunnigan Hills fault, over 18 miles northwest of the Project site. The Project site could experience ground shaking from regionally active faults.

The Soil Survey of Yolo County identified the following four soil map units or soil types within the Project site (USDA, 2015):

- Lang sandy loam (La), 0 to 1 percent slopes: This is a somewhat poorly drained soil formed in material from mixed rock sources, with rapid permeability. It is a partially hydric soil and has a moderate corrosion potential (USDA, 1993).
- Lang sandy loam, deep (Lb), 0 to 1 percent slopes: This is a somewhat poorly drained soil formed in material from mixed rock sources, with rapid permeability. It is a partially hydric soil and has a moderate corrosion potential (USDA, 1993).
- Valdez silt loam (Vb), 0 to 2 percent slopes: This is a poorly drained soil formed in recent alluvial material from mixed rock sources, with moderately slow permeability. It is a partially hydric soil and has high corrosion potential (USDA, 2003).
- Made land (Ma): This land is well drained, partially hydric, and does not have a reported corrosion potential.

4.6.2 Discussion

a) **Less-than-Significant Impact.** The Project area is not near any Alquist-Priolo faults, and the potential for seismic-related ground failure or landslides is considered low based on soil and geologic conditions (CDC, 2015a; CDC, 2015b). According to the California Department of Conservation Information Warehouse, no delineation earthquake fault zones, landslide and liquefaction zones, or tsunami hazard zones occur within the Project vicinity (CDC, 2015a). The closest earthquake fault zone to the Project is the Mount George quadrangle; the closest landslide and liquefaction zones would be the Jersey Island and Bouldin quadrangles southwest of the Project site. The closest area with landslide reports or maps to the Project site is the Allendale quadrangle in Solano County; the nearest areas with inundation maps are the Cuttings Wharf and the Benicia quadrangles southwest of the Project site. The Project would not expose people to seismic-related soil or geologic hazards.

   Borings were completed by WRECO and the Sacramento Regional County Sanitation District at the Project site. The subsurface data collected from the borings indicate that the following soil characteristic types occur at the project site: 15 to 25 feet of stiff to very stiff clays at the surface; 40 to 100 feet of loose to dense silty sands and sandy silts underneath the clays; and occasional discrete layers of very dense lenses (WRECO, 2018). Preliminary liquefaction evaluation and analysis of the Project site demonstrates that the medium dense sand and loose poorly graded sand layer is susceptible to seismically-induced liquefaction with approximately 2.6 inches of settlement (WRECO, 2018). However, the bicycle-pedestrian overcrossing is being designed to accommodate for the potential 2.6 inches of settlement. Therefore, there is little to no risk of loss, injury, or death due to potential liquefaction at the Project site.

b) **Less-than-Significant Impact.** The Project would extend the existing Sycamore Trail and create a new pedestrian overcrossing over US 50. Construction activities would involve earth moving activities. Construction activities involving soil disturbance, excavation, cutting/filling, demolition, paving, and grading activities have the potential for surface water runoff to carry sediment. Potential erosion impacts from construction activities would be less than significant with adherence to the California Building Code and the City of West Sacramento Grading Ordinance (West Sacramento Municipal Code Chapter 15.08).

c) **Less-than-Significant Impact.** According to the California Department of Conservation CGS Information Warehouse: Landslides (2015), very few landslides occur in the Project vicinity. The probability of landslides occurring on the Project site is very low. The Project site does not have loose sandy soil, nor does it contain soils that would be susceptible to lateral spreading, liquefaction, or collapse. With adherence to all applicable codes and regulations, including the 2016 California Building Code, the Project's impacts to the potential for on or off-site landslides would be minimized.

d) **Less-than-Significant Impact.** Expansive soils are those possessing clay particles that react to moisture changes by shrinking (when dry) or swelling (when wet). The extent of shrinking and swelling is influenced by the environment, including the extent of wet or dry cycles, and by the amount of clay in the soil. This physical change in the soils can react unfavorably with building foundations, concrete walkways, swimming pools, roadways, and masonry walls. The soil types at the Project site are loam and made land, which are not considered expansive soil types. Therefore, the Project would not expose life or properties to adverse effects associated with expansive soil.

e) **No Impact.** The Project does not involve the connection to sewer systems or septic tanks.

4.6.3 References

California Department of Conservation (CDC), 2015a. CGS Information Warehouse: Regulatory Maps. Available at: http://maps.conservation.ca.gov/cgs/informationwarehouse/index.html?map=regulatorymaps. Accessed February 8, 2018.

CDC, 2015b. Fault Activity Map of California (2010). Available at: http://maps.conservation.ca.gov/cgs/fam/. Accessed February 8, 2018.

CDC, 2017a. California Geotours: An Index to Online Geologic Field Trip Guides of California. Available at: http://www.conservation.ca.gov/cgs/Pages/Geotours.aspx. Accessed May 16, 2018.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

Jennings, C.W., Strand, R.G., and Rogers, T.H., 1977, Geologic map of California: California Division of Mines and Geology, scale 1:750,000.

Ontario Ministry of Agriculture, Food, and Rural Affairs. Universal Soil Loss Equation (USLE) Factsheet. Available at: http://www.omafra.gov.on.ca/english/engineer/facts/12-051.htm#t3a. Accessed February 8, 2018.

U.S. Department of Agriculture (USDA), 1993. Lang Series. Available at: https://soilseries.sc.egov.usda.gov/OSD_Docs/L/LANG.html. Accessed February 9, 2018.

USDA, 2003. Valdez Series. Available at: https://soilseries.sc.egov.usda.gov/OSD_Docs/V/VALDEZ.html. Accessed February 9, 2018.

USDA, 2013. Web Soil Survey. Available at: https://websoilsurvey.sc.egov.usda.gov. Accessed December 21, 2017.

U.S. Environmental Protection Agency (U.S. EPA), 2017. Rainfall Erosivity Factor Calculator. February 21, 2017. Available at: https://www.epa.gov/waterdata/rainfall-erosivity-factor-calculator. Accessed February 8, 2018.

4.7 Greenhouse Gas Emissions

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Greenhouse Gas Emissions –Would the project:				
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	☐	☐	☒	☐
b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?	☐	☐	☒	☐

4.7.1 Setting

California's primary legislation for reducing greenhouse gas emission is the California Global Warming Solutions Act (Assembly Bill [AB] 32). The City has not adopted a final Climate Action Plan, and neither the City nor YSAQMD have set significance thresholds for greenhouse gas emissions (City of West Sacramento, 2010; City of West Sacramento, 2016a; City of West Sacramento, 2016b; YSAQMD, 2007). In the case that significance thresholds for greenhouse gas emissions have not been set by a municipality or an air district and a Climate Action Plan has not been adopted by the municipality, it is standard to analyze greenhouse gas impacts by: 1) using data from the AB 32 Scoping Plan or 2) using an appropriate numeric threshold (*Center for Biological Diversity v. Department of Fish and Wildlife [2015]*).

4.7.2 Discussion

a.b) **Less-than-Significant Impact.** The purpose of this Project is to encourage more individuals within the City to walk or bike to work, school, errands, and for recreation by extending the Sycamore Trail and providing a pedestrian overcrossing on US 50. As the Project would not include additional through lanes, the Project would not increase roadway facilities or service capabilities that would induce unplanned growth or remove an existing obstacle to growth. The Project would not increase long-term traffic levels, and there would be no operational impacts associated with greenhouse gas emissions. Consequently, the Project construction is considered small, short-term in nature, and would not generate substantial air quality (including greenhouse gas emission) pollutant concentrations as discussed under the Air Quality section.

Road Construction Emissions Modeling of this project has projected that 2,028.69 lbs/day (370.11 tons/year) of carbon dioxide equivalent would be emitted during Project construction. Therefore, greenhouse gas emissions would not exceed the 25,000 metric tons of carbon dioxide equivalent per year threshold typically assumed when significance thresholds have not been set by a municipality or an air quality district (CEQ, 2014 [also applicable to CEQA projects]; Huss, Karen and Grant, John, 2017; *Center for Biological Diversity v. Department of Fish and Wildlife* [2015])). The assumptions were made during modeling that: 1) the types and quantities of construction equipment typical of bridge projects would be used, 2) all on-road equipment used for the Project would be year 2010 or newer models, and 3) all construction equipment would

meet California Air Resources Board (ARB) Tier 4 requirements. Road Construction Emissions Modeling of this Project is available in **Appendix A.**

4.7.3 References

Center for Biological Diversity v. Department of Fish and Wildlife (2015) 62 Cal. 4th 204.

City of West Sacramento, 2010. Draft Climate Action Plan. August 2010.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

Council on Environmental Quality (CEQ, 2014). Revised Draft Guidance for Federal Departments and Agencies on Consideration of Greenhouse and Emissions and the Effects of Climate Change in NEPA Reviews. December 24, 2014.

Huss, Karen and Grant, John, 2017. Road Construction Emissions Model, Version 8.1.0. Prepared for the Sacramento Metropolitan Air Quality Management District (SMAQMD).

Sacramento Area Council of Governments (SACOG), 2017a. Air quality objectives. Available at: https://www.sacog.org/air-quality. Accessed on February 7, 2018.

Yolo-Solano Air Quality Management District (YSAQMD), 2007. Handbook for Assessing and Mitigating Air Quality Impacts. July 11, 2007.

4.8 Hazards and Hazardous Materials

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Hazards and Hazardous Materials –Would the project:				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	☐	☐	☒	☐
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	☐	☒	☐	☐
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	☐	☒	☐	☐
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	☐	☐	☒	☐
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	☐	☐	☒	☐
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	☐	☐	☐	☒
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	☐	☒	☐	☐
h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	☐	☐	☐	☒

4.8.1 Setting

An Initial Site Assessment (ISA) was prepared on behalf of the City and is available for review at the City Department of Public Works during business hours (Caltrans 2018c). The ISA was performed in general conformance with the scope and limitations of ASTM Practice E1527-13. The ISA identifies Recognized Environmental Conditions (RECs) for the Project site that may adversely affect roadway and/or bridge construction or right-of-way acquisition. RECs are defined by the ASTM Practice E1527-13 as: "the

presence or likely presence of any hazardous substances or petroleum products in, on, or at a property: 1) due to any release to the environment; 2) under conditions indicative of a release to the environment; or 3) under conditions that pose a material threat of a future release to the environment." A database report was obtained from Environmental Database Resources, Inc. consisting of information compiled from various government records, such as Geotracker, National Priorities List, and EnviroStor, for information regarding the Project area. Based on the records review results, no potential RECs have been found in the Project site.

An ISA does not test for asbestos or lead-based paint within the Project site. The Occupational Safety & Health Administration (OSHA) requires that all thermal systems insulation, surfacing materials, and resilient flooring materials installed prior to 1981 be considered Presumed Asbestos Containing Materials (ACM) and treated accordingly. There are no existing structures to be demolished at the Project site, and other potential sources of ACMs were not observed during site reconnaissance. Lane stripping has the potential to contain lead-based paint if the road was constructed before 1978, and the existing soil at the Project site has the potential to be contaminated with elevated levels of aerially-deposited lead. Analysis and mitigation measures regarding lead containing materials and aerially-deposited lead are discussed in more detail below.

4.8.2 Discussion

a) **Less-than-Significant Impact.** Project construction would potentially require the use of various types and quantities of hazardous materials. Hazardous materials that are typically used during construction include, but are not limited to, hydraulic oil, diesel fuel, grease, lubricants, solvents, and adhesives. Although equipment used during construction activities could contain various hazardous materials, these materials would be used in accordance with the manufacturers specifications and all applicable regulations. Project operation would not involve the routine storage or use of hazardous materials.

b) **Less-than-Significant Impact with Mitigation.** No known RECs or other known contamination have been found on the Project site (Caltrans, 2018c). As stated above, the Project has the potential to use a variety of hazardous materials. Avoidance, minimization, and/or mitigation measures are proposed for potential lead-containing materials and aerially-deposited lead (ADL) that may be present at the Project site.

   Lead Containing Materials: Because of the length of time that the Project roadways have existed in their current alignment, there is the potential for lead-based paint to be present in the Project roadway pavement striping or the paint on utilities, or for other lead-containing materials to be encountered during ground-disturbing activities. During construction, building materials associated with paint or structures, paint on utilities, and pavement striping (white and yellow) paint would be abated by a California-licensed abatement contractor and disposed of as a hazardous waste.

   ADL: Lead was used as a gasoline additive prior to 1987. Therefore, ADL deposits are commonly located adjacent to heavily traveled roadways in service prior to 1987. Based on our review of air photos and topographical maps, US 50 has been in the same alignment since its construction in 1954 and is currently a heavily trafficked roadway (Caltrans, 2018c). Park Boulevard and West Capitol Avenue have remained in the same alignment since or before 1916 and 1948, respectively

(Caltrans, 2018c). Therefore, there is the potential for soils contaminated by elevated levels of ADL to be encountered during Project construction.

During construction, any existing hazardous materials that may be encountered would pose a hazard for construction workers and the environment. Construction workers typically are at the greatest risk for exposure to contaminated soil. Accidents or spills during transport of hazardous materials or wastes could have the potential to expose the public and the environment to these substances.

Implementation of **MM HAZ-1** and **HAZ-2** would be required to ensure there would not be a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment and reduce the impact to a less-than-significant level.

c) **Less-than-Significant Impact with Mitigation.** The School is located within the Project site. **MM HAZ-1** and **HAZ-2** would ensure that potential impacts to sensitive receptors at the School are less than significant.

d) **Less-than-Significant Impact.** This Project site is not included in the list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 (Caltrans, 2018c). The closest water body that may be affected is Washington Lake/the Sacramento River Deep Water Ship Channel south of the Project site. As discussed in 'b', avoidance, minimization, and/or mitigation measures are proposed for potential lead containing materials and ADL that may be present at the Project site.

e) **Less-than-Significant Impact.** The Project is located within the jurisdiction of the Airport Land Use Compatibility Plan (ALUCP) for Sacramento International Airport. Sacramento International Airport is the closest airport to the Project site and is located approximately 8.6 miles northwest (SACOG, 2017b). The Project is not located within the Sacramento International Airport Safety Zones, as defined by the SACOG Airport Land Use Commission, and does not pose any incompatibilities with the ALUCP (SACOG, 2017b; Caltrans, 2018a; SACOG, 2013).

f) **No Impact.** The Project is not located within the vicinity of a private air strip. The nearest private airstrip to the Project is the UC Davis Medical Center Life Flight Base/the UC Davis Medical Center Tower & Heliport, approximately 4.4 miles east of the Project site (SACOG, 2013; SACOG, 2017b).

g) **Less-than-Significant Impact with Mitigation.** During Project construction, there would be temporary short (less than 24 hours) closures of US 50 between Harbor Boulevard and Jefferson Boulevard to set up and take down the overcrossing falsework. A detour on I-80 and I-5 has been planned for the Project. **MM PUB-1** would ensure that any potential impacts to emergency response times caused by the temporary closures would be less than significant. Information about emergency response times is available in the Public Services section of this document.

h) **No Impact.** The Project would be constructed in an already urbanized and built residential and industrial environment. The Project site is not listed on a Sanborn Map as being a wildfire risk area (Caltrans, 2018c). The Project would not expose additional people or structures to the threat of fire.

4.8.3 Mitigation Measures

MM HAZ-1: *Develop a Health and Safety Plan (HASP).* Develop a HASP for the Project. The HASP shall describe appropriate procedures to follow in the event any contaminated soil or groundwater is encountered during construction activities. Any unknown substances shall be tested, handled, and disposed of in accordance with appropriate federal, state, and local regulations.

MM HAZ-2: *Follow Protocol for Handling Lead Containing Materials.* A California-licensed abatement contractor will conduct a survey for lead containing materials prior to demolition (including concrete elements) and the contractor will submit a National Emission Standard for Hazardous Air Pollutants (NESHAP) notification. Per the Asbestos NESHAP regulation, all "demolition activity" requires written notification even if there is no asbestos present. This notification shall be typewritten and postmarked or delivered no later than ten days prior to the beginning of the asbestos demolition or removal activity.

If lead-containing materials are found, the following will be required:

- Building materials associated with paint on structures and paint on utilities should be abated by a California-licensed abatement contractor and disposed of as a hazardous waste in compliance with 2015 Caltrans Standard Specification 14-11.12, 2015 Caltrans Standard Specification 14-11.13, and other federal and state regulations for hazardous waste.
- A Lead Compliance Plan should be prepared by the contractor for the disposal of lead-based paint. The grindings (which consist of the roadway material and the yellow and white color traffic stripes) shall be removed and disposed of in accordance with the appropriate 2015 Caltrans Standard Specifications.
- A California-licensed lead contractor should be required to perform all work that will disturb any lead-based paint as a result of planned or unplanned renovations in the Project area, including the presence of yellow traffic striping and pavement markings that may contain lead-based paint. All such material must be removed and disposed of as a hazardous material in compliance with 2015 Caltrans Standard Specification 14-11.12 and 14-11.13.

4.8.4 References

ATSM Standard E1527-13. "Standard Practice for Environmental Site Assessments: Phase I Environmental Site Assessment Process." ASTM International, West Conshohocken, PA, 2003.

California Department of Transportation (Caltrans), 2018a. Land Use and Community Impact Technical Memorandum for the Sycamore Trail Phase II & III Extension Project. February 28, 2018. Prepared for the City of West Sacramento by Drake Haglan & Associates.

Caltrans, 2018c. Sycamore Trail Phase II & III Extension Project [CML-5447(044)] Initial Site Assessment. February 2018. Prepared for the City of West Sacramento by Drake Haglan & Associates.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

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Sacramento Area Council of Governments (SACOG), 2013. Sacramento International Airport Land Use Compatibility Plan. December 12, 2013. Prepared by Mead & Hunt in association with Environmental Science Associates.

SACOG, 2017b. Airport Land Use Commission. Available at: https://www.sacog.org/post/airport-land-use-commission. Accessed February 7, 2018.

U.S. Department of Transportation, Federal Aviation Administration (FAA), 2007. Advisory Circular AC 70/7460-1K: Obstruction Marking and Lighting. February 1, 2007.

4.9 Hydrology and Water Quality

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Hydrology and Water Quality – Would the project:				
a) Violate any water quality standards or waste discharge requirements?	☐	☐	☒	☐
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	☐	☐	☒	☐
c) Substantially alter the existing drainage pattern of a site or area through the alteration of the course of a stream or river, or by other means, in a manner that would result in substantial erosion or siltation on- or off-site?	☐	☐	☒	☐
d) Substantially alter the existing drainage pattern of a site or area through the alteration of the course of a stream or river, or by other means, substantially increase the rate or amount of surface runoff in a manner that would result in flooding on- or off-site?	☐	☐	☒	☐
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	☐	☐	☒	☐
f) Otherwise substantially degrade water quality?	☐	☐	☒	☐
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	☐	☐	☐	☒
h) Place within a 100-year flood hazard area structures that would impede or redirect flood flows?	☐	☐	☐	☒
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	☐	☐	☐	☒
j) Expose people or structures to a significant risk of loss, injury or death involving inundation by seiche, tsunami, or mudflow?	☐	☐	☐	☒

4.9.1 Setting

The Project area is within the Lower Sacramento Watershed Hydrologic Unit (18020109) (U.S. EPA, 2018b). The Lower Sacramento Watershed encompasses 1,566 square miles and includes Yuba, Placer, Napa, Sacramento, Solano, Sutter, and Yolo counties (Montana State University, 2018). Flows in this watershed generally travel from the Sacramento Stone Corral, Lower Cache, Lower American, Upper Putah, and Upper Coon-Upper Auburn Hydrologic Units into the San Joaquin Delta Hydrologic Unit and terminates in the Pacific Ocean (Montana State University, 2018).

There are no water features within the Project site that could be considered waters of the U.S. or waters of the State (Caltrans, 2018d). There are no water features in general within the Project site, nor is the Project site actively used for groundwater recharge (City of West Sacramento, 2016b). According to the Federal Emergency Management Agency (FEMA) Flood Map Service Center, the Project site does not occur within a 100-year flood hazard zone or any other flood hazard zone (FEMA, 2018)(**Figure 8**).

The Project is subject to Construction General Permit (Order No. 2009-0009-DWQ [as amended by Order No. 2010-0014-DWQ and 2012-006-DWQ]) requirements, which requires preparation and implementation of a SWPPP.

4.9.2 Discussion

a,f) **Less-than-Significant Impact.** There are no water features on the Project site in which water discharge requirements or water quality standards could be violated (Caltrans, 2018d). Construction materials such as asphalt, concrete, and equipment fluids could be exposed to precipitation and subsequent runoff. If precautions are not taken to contain contaminants, construction could produce contaminated stormwater runoff (nonpoint source pollution), a major contributor to the degradation of water quality. Project construction is anticipated to take between eight and 12 months. Construction is scheduled for spring 2019.

The City will ensure that the Project contractor complies with the requirements of a National Pollution Discharge Elimination System (NPDES) permit from the RWQCB, Central Valley Region. As part of the permit, the contractor would be required to prepare and implement a SWPPP into their construction plans, prior to initiating construction activities, identifying BMPs to be used to avoid or minimize any adverse effects before, during, and after construction to surface waters. The following BMPs will be incorporated into the Project as part of the construction specifications:

- Implement appropriate measures to prevent debris, soil, rock, or other material from entering the water. Use a water truck or other appropriate measures to control dust on applicable access roads, construction areas, and stockpiles.
- Properly dispose of oil or other liquids.
- Fuel and maintain vehicles in a specified area that is designed to capture spills. All fueling and maintenance of vehicles and other equipment (including staging areas) will be located at least 65 feet any potential drainages on site.
- Fuels and hazardous materials would not be stored on site.
- Inspect and maintain vehicles and equipment to prevent the dripping of oil or other fluids.
- Schedule construction to avoid the rainy season as much as possible. Ground disturbance activities are expected to begin in the spring of 2019. If rains are forecasted during construction, additional erosion and sedimentation control measures would be implemented.

- Maintain sediment and erosion control measures during construction. Inspect the control measures before, during, and after a rain event.
- Train construction workers in stormwater pollution prevention practices.
- Revegetate disturbed areas in a timely manner to control erosion.

b) **Less-than-Significant Impact.** The Project area is not actively used for groundwater recharge (Caltrans, 2018d; City of West Sacramento, 2016b). The Project would not construct a significant amount of new impervious surfaces that would impede surface water drainage into the soil. No wells would be constructed; and construction activities would not intercept or alter groundwater recharge, discharge, or flow conditions.

c-e) **Less-than-Significant Impact.** The Project would not alter the course of a water body nor would it alter the existing site drainage pattern. The site drainage is not expected to result in substantial on or off-site siltation or erosion. The Project would not substantially increase the amount or rate of surface runoff such that on or off-site flooding would occur nor would it change the surrounding land use in such a way that would exceed the existing or planned storm drainage systems or provide substantial additional sources of polluted runoff.

g-j) **No Impact.** The Project would not construct housing or other structures that would result in the exposure of people or structures to 100-year flood hazards, and the Project would not place any structures that would redirect or impede flood flows.

The Project would not modify the surrounding levees and therefore would not expose people or structures to a significant risk of loss, injury, or death involving flooding, including flooding as a result of the failure of a levee or dam.

The Project site is located approximately 700 feet north from the Sacramento River Deep Water Ship Channel (Channel) and is separated from the Channel by urban development. The Channel would not be expected to be substantially affected by a tsunami, mudflow, or seiche (CDC, 2015a; CDC, 2015b). The Project would include a retaining wall on both the northern and the southern overcrossing approaches, limiting the possibility of a mudflow hazard to the Project site.

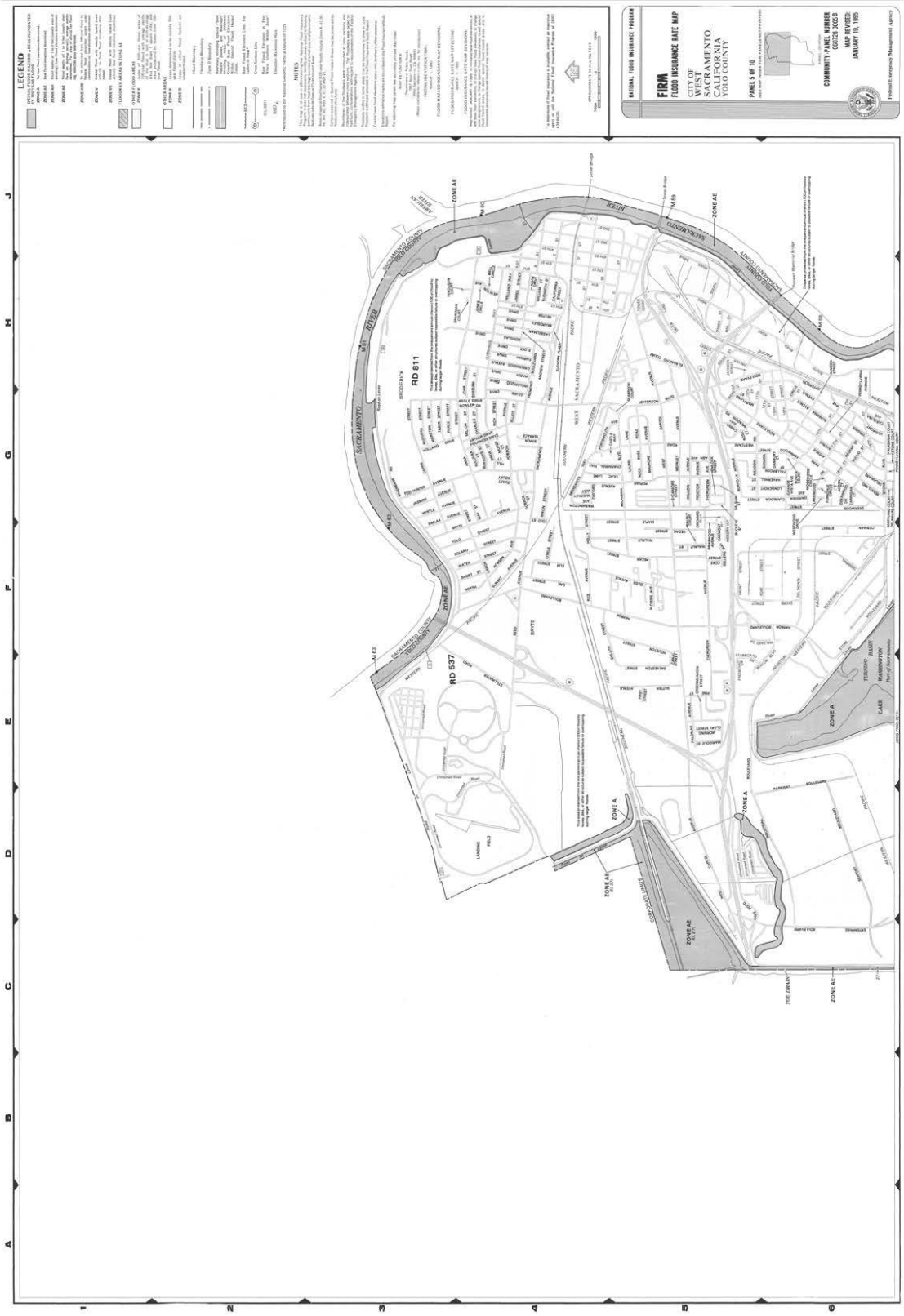

Figure 8 - FEMA Flood Map References

California Department of Conservation (CDC), 2015a. CGS Information Warehouse: Regulatory Maps. Available at: http://maps.conservation.ca.gov/cgs/informationwarehouse/index.html?map=regulatorymaps. Accessed February 8, 2018.

CDC, 2015b. Fault Activity Map of California (2010). Available at: http://maps.conservation.ca.gov/cgs/fam/. Accessed February 8, 2018.

California Department of Transportation (Caltrans), 2018d. Technical Memorandum for Biological Resources for the Sycamore Trail Phase II & III Extension Project [CML-5447(044)]. February 27, 2018.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

Montana State University, Environmental Statistics Group, 2018. Lower Sacramento – Cataloging Unit 18020109. Available at: http://www.esg.montana.edu/gl/huc/18020109.html. March 20, 2018.

U.S. Environmental Protection Agency (U.S. EPA), 2018b. Lower Sacramento Watershed – 18020109. Available at: https://cfpub.epa.gov/surf/huc.cfm?huc_code=18020109. Accessed March 20, 2018.

4.10 Land Use and Land Use Planning

Issues (and Supporting Information Sources)	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
Land Use and Land Use Planning – Would the project:				
a) Physically divide an established community?	☐	☐	☒	☐
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	☐	☐	☒	☐
c) Conflict with any applicable habitat conservation plan or natural community conservation plan?	☐	☐	☐	☒

4.10.1 Setting

A Land Use and Community Impact Technical Memorandum was prepared for the Project and is available for review at the City Department of Public Works during business hours.

The Project is located in the City of West Sacramento and is under the jurisdiction of the City's General Plan and the West Sacramento Municipal Code (Municipal Code). Yolo County is also a member of SACOG; therefore, SACOG regional plans are also applicable to the Project site. The Project is not within the jurisdiction of the Washington Specific Plan or any other specific plans within the City; and there are no local coastal programs, habitat conservation plans, or natural community conservation plans that have jurisdiction over the Project vicinity. The Yolo Habitat Conservancy has drafted but has not adopted the Yolo County Habitat Conservation Plan/Natural Community Conservation Plan. The Project is within the jurisdiction of the 2003 City of West Sacramento Parks Master Plan (Parks Master Plan).

4.10.2 Discussion

a) **Less-than-Significant Impact.** The Project would consist of extending an existing bicycle and pedestrian trail and constructing a new pedestrian and bicycle overcrossing over US 50. The Project would further reconnect the existing community that was bifurcated by US 50, and thus would not divide an established community (Caltrans, 2018a).

b) **Less-than-Significant Impact.** The existing trail extension and the overcrossing construction would not interfere with activity associated with any surrounding land uses. The Project does not propose any new land use designations for the Project site. The Project would not result in any land use conflicts and does not conflict with the General Plan, the Municipal Code, or any other applicable land use plan, policy, or regulations.

c) **No Impact.** The Project is not within the jurisdiction of an adopted habitat conservation plan or natural community conservation plan (Caltrans, 2018a; Yolo Habitat Conservancy, 2018). Therefore, Project implementation would not conflict with the provisions of an adopted local,

regional, or state habitat conservation plan. The closest habitat conservation plan jurisdiction to the Project site is the Natomas Basin Habitat Conservation Plan, which includes parts of southeastern Sutter County (City of Sacramento et al., 2003).

4.10.3 References

California Department of Transportation (Caltrans), 2018a. Land Use and Community Impact Technical Memorandum for the Sycamore Trail Phase II & III Extension Project. February 28, 2018. Prepared for the City of West Sacramento by Drake Haglan & Associates.

City of Sacramento, Sutter County, Natomas Basin Conservancy, Reclamation District No. 1000, Natomas Central Mutual Water Company; 2003. Final Natomas Basin Habitat Conservation Plan. Sacramento and Yuba Cities, California. Prepared for United States Fish and Wildlife Service and the California Department of Fish and Game.

City of West Sacramento, 1996. Washington Specific Plan: City of West Sacramento. May 15, 1996.

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City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

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City of West Sacramento, 2013. 2013 West Sacramento Bicycle, Pedestrian, and Trails Master Plan. May 2013. Prepared by Fehr & Peers.

Sacramento Area Council of Governments (SACOG), 2013. Sacramento International Airport Land Use Compatibility Plan. December 12, 2013. Prepared by Mead & Hunt in association with Environmental Science Associates.

City of West Sacramento., 2003. 2013 West Sacramento Parks Master Plan. September 2003. Prepared for the City of West Sacramento.

U.S. Department of Transportation, Federal Aviation Administration (FAA), 2015. Land Use Compatibility and Airports. Available at: https://www.faa.gov/about/office_org/headquarters_offices/apl/noise_emissions/planning_toolkit/media/III.B.pdf. Accessed March 6, 2018.

Yolo Habitat Conservancy, 2017. Yolo Habitat Conservation Plan/Natural Community Conservation Plan: Volume 1, Public Review Draft. July 11, 2007. Prepared by ICF International.

Yolo Habitat Conservancy, 2018. Documents. Available at: https://www.yolohabitatconservancy.org/documents. Accessed May 16, 2018.

4.11 Mineral Resources

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Mineral Resources – Would the project:				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	☐	☐	☒	☐
b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	☐	☐	☐	☒

4.11.1 Setting

The California Surface Mining and Reclamation Act (SMARA) was enacted by the California legislature to regulate activities related to mineral resource extraction. The act requires the prevention of adverse environmental effects caused by mining, the reclamation of mined lands for alternative land uses, and the elimination of public health and safety hazards from the effects of mining activities. The California Geological Survey (formerly California Division of Mines and Geology) classifies the regional significance of mineral resources in accordance with SMARA. Mineral Resource Zones (MRZs) have been designated to indicate the significance of mineral deposits. A classification of MRZ-1 signifies an area where adequate information indicates that no significant mineral deposits are present, or where it is judged that little likelihood exists for their presence; MRZ-2 signifies an area where adequate information indicates that significant mineral deposits are present, or where it is judged that a high likelihood for their presence exists; and MRZ-3 signifies an area where the significance of mineral deposits cannot be evaluated from existing data. These designations are intended to preserve known mineral resources for future mining and to prevent encroachment of urban development that would compromise the resource's value.

The Project site is located within the Sacramento-Fairfield Production-Consumption Region as defined by the California Department of Conservation (CDC, 1988). However, there are neither areas designated as MRZ-2 nor mines regulated under SMARA within the City (City of West Sacramento, 2016). The Project vicinity is designated as MRZ-1 (City of West Sacramento, 2016a; City of West Sacramento, 2016b).

4.11.2 Discussion

a) **Less-than-Significant Impact.** The Project would extend the Sycamore Trail and create a new pedestrian overcrossing over US 50 within an area designated as MRZ-1. Construction activities would be temporary and Project operation would not conflict with or limit access to mineral resources.

b) **No Impact.** There are no mineral resource recovery sites delineated on the General Plan or any other applicable land use plan within the Project vicinity (CDC, 1988; City of West Sacramento, 2016a; City of West Sacramento, 2016b).

4.11.3 References

California Department of Conservation (CDC), Division of Mines and Geology, 1988. Mineral Land Classification: Portland Cement Concrete-Grade Aggregate in the Sacramento-Fairfield Production Consumption Region. Special Report 156. Available at: ftp://ftp.consrv.ca.gov/pub/dmg/pubs/sr/SR_156/SR_156_Text.pdf. Accessed February 9, 2018.

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City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

4.12 Noise

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Noise – Would the project:				
a) Result in exposure of persons to, or generation of, noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	☐	☒	☐	☐
b) Result in exposure of persons to, or generation of, excessive groundborne vibration or groundborne noise levels?	☐	☒	☐	☐
c) Result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	☐	☐	☐	☒
d) Result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	☐	☒	☐	☐
e) For a project located within an airport land use plan area, or, where such a plan has not been adopted, in an area within two miles of a public airport or public use airport, would the project expose people residing or working in the area to excessive noise levels?	☐	☐	☐	☒
f) For a project located in the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	☐	☐	☐	☒

4.12.1 Setting

A Noise Technical Memorandum for the Project was prepared on behalf of City and is available for review at the City of West Sacramento Department of Public Works during business hours (Caltrans 2018b). Noise is defined as unwanted sound, and thus is a subjective reaction to characteristics of a physical phenomenon. A frequency weighting measure that simulates human perception is commonly used to describe noise environments and to assess impacts on noise-sensitive areas. It has been found that A-weighting of sound levels best reflects the human ear's reduced sensitivity to low frequencies, and correlates well with human perceptions of the annoying aspects of noise. Sensitive land uses include residences and a school (**Figure 9**) within the project area. The A-weighted decibel scale (dBA) is cited in most noise criteria. The decibel notation used for sound levels describes a logarithmic relationship of acoustical energy, for example, a doubling of acoustical energy results in an increase of 3 dB, which is considered barely perceptible. A ten-fold increase in acoustical energy equals a 10 dB change, which is subjectively like a doubling of loudness. **Table 6**, Typical Noise Levels, identifies decibel levels for common sounds heard in the environment.

Land use within and adjacent to the Project corridor consists of public/quasi-public, residential, industrial, commercial, and business park uses. During Project construction, noise from construction activities may intermittently dominate the noise environment in the immediate area of construction. Noise from construction activities generally attenuates at a rate of 6 dBA per doubling distance.

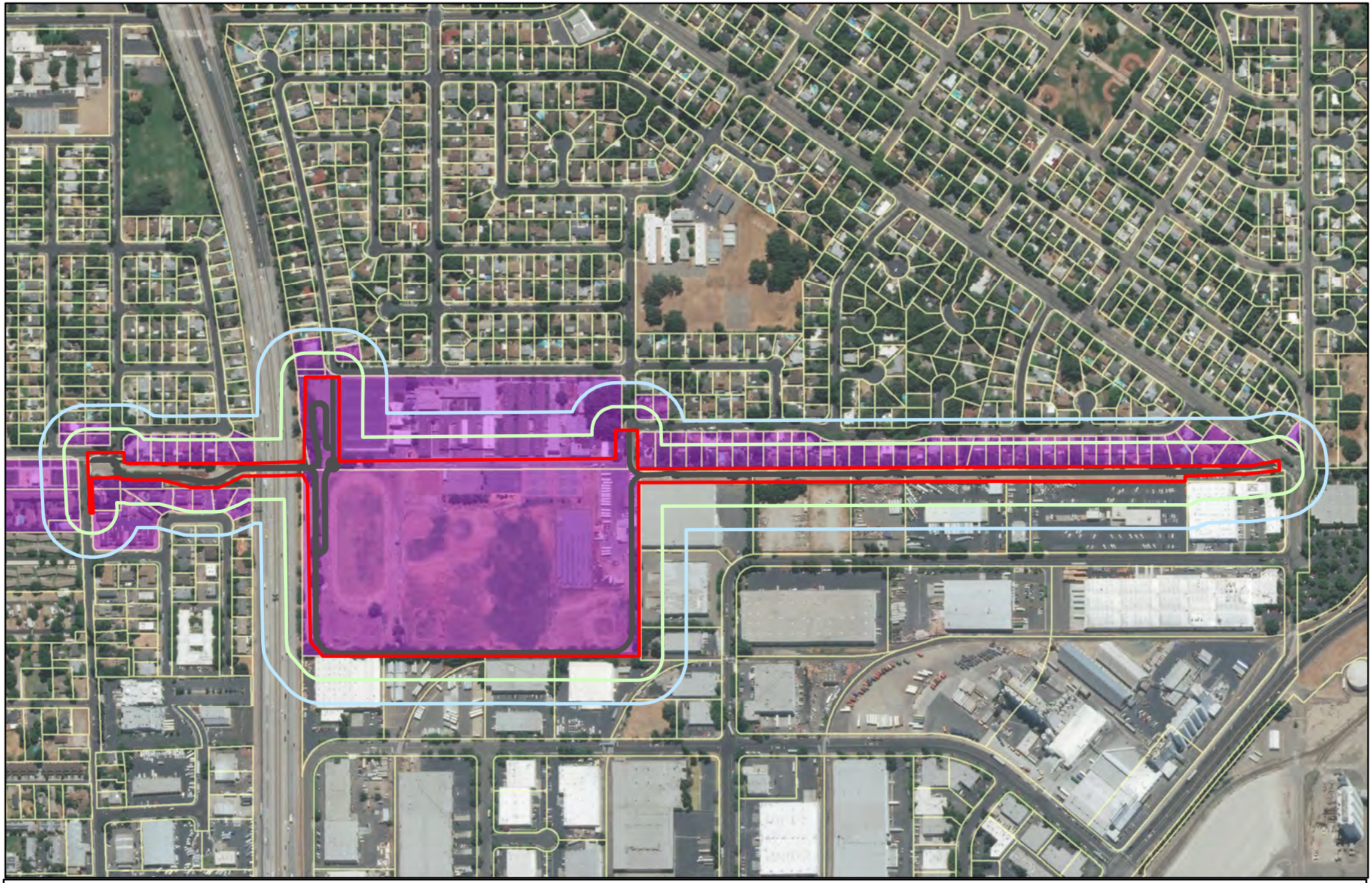
Table 6. Typical Noise Levels

Common outdoor activity	Noise level (dBA)	Common indoor activity
Jet flyover at 1,000 feet	110	Rock band
Gas lawnmower at three feet	100	
Diesel truck at 50 feet at 50 mph	90	Food blender at three feet
Noisy urban area, daytime	80	Garbage disposal at three feet
Gas lawnmower, 100 feet Commercial area	70	Vacuum cleaner at ten feet Normal speech at three feet
Heavy traffic at 300 feet	60	Large business office
Quiet urban daytime	50	Dishwasher next room
Quiet urban nighttime Quiet suburban nighttime	40	Theater, large conference room (background)
Quiet rural nighttime	30	Library Bedroom at night, concert hall (background)
	20	Broadcast/recording studio
	10	
Lowest threshold of human hearing	0	Lowest threshold of human hearing

Source: Caltrans, 2013

Several time-averaged scales represent noise environments and consequences of human activities. The most-commonly used noise descriptors are equivalent A-weighted sound level over a given time period (Leq); average day-night 24-hour average sound level with a nighttime increase of 10dBA to account for sensitivity to noise during the nighttime; and community noise equivalent level (CNEL), also a 24-hour average that includes both an evening and a nighttime weighting. Noise levels are generally considered low when ambient levels are below 45 dBA, moderate in the 45 to 60 dBA range, and high above 60 dBA. Although people often accept the higher levels associated with very noisy urban residential and residential-commercial zones, they nevertheless are considered to be adverse levels of noise with respect to public health because of sleep interference.

There are 77 noise-sensitive receptor parcels within 100 feet of the Project site boundary and an additional 50 noise-sensitive receptor parcels within 200 feet of the Project boundary. All sensitive receptors are residential dwellings except for the School and Joey Lopes Park.



Area of Direct Impact (ADI)
Proposed Work
Sensitive Receptors
100 Foot Buffer
200 Foot Buffer
Parcels

0 350 700 1,400 Feet

N

drake haglan AND ASSOCIATES

Source: ESRI Online Basemap, Aerial Imagery Map, Yolo County Coordinate System NAD 83 State Plane California II FIPS 0402 Feet

Notes: This map was created for informational and display purposes only

Sycamore Trail Phase II & III Extension Project
West Sacramento, CA

Sensitive Receptors Map

Figure 9

4.12.1.1 *Federal Noise Regulations*

Title 23, Part 772 of the Code of Federal Regulation (23 CFR 772) outlines procedures for noise studies that are required for approval of federal-aid highway projects. Under 23 CFR 772.7, projects are classified as Type 1 if they would construct a highway in a new location or substantially change the existing horizontal or vertical alignment of an existing highway, are classified as Type II if they involve construction on an existing highway with no changes to highway capacity or alignment or are classified as Type III if they do not fit the classification of a Type I or Type II project. Type III projects do not involve added capacity, construction of new through or auxiliary lanes, substantial changes in alignment, or exposure of noise sensitive land uses to new or existing highway noise source. 23 CFR 771.117(c)(3) lists construction of bicycle and pedestrian lanes, paths, and facilities as a potential Type III project, as long as they meet the other criteria for Type III project classification. Type III projects do not require a noise analysis, and the Project meets the criteria to be a Type III project. Therefore, an acoustical study is not required for the Project.

4.12.1.2 *Local Noise Regulations*

Noise within the City is regulated by Municipal Code Chapter 17.32. Chapter 17.32 does not specify an exemption for temporary daytime construction activity; therefore, the daytime and nighttime limits specified in the Municipal Code are considered to apply to all construction associated with the Project **(Table 7).** The Municipal Code sets noise level performance standards for non-transportation noise source, which it defines as including industrial operations, outdoor recreation facilities, HVAC units, loading docks, and construction equipment **(Table 8).**

Table 7. Noise Level Standards from Stationary Sources

Noise Level Descriptor	Daytime (7 AM to 10 PM)	Nighttime (10 PM to 7 AM)
Hourly Leq, dBA	55	45
Max. level, dBA	70	65

Source: City of West Sacramento, 2018.

Table 8. Noise Level Performance Standards for New Projects Affected by or Including Non-Transportation Noise Sources

Land Use	Noise Level Descriptor	Exterior Noise Levels		Interior Noise Levels	
		Daytime (7 AM to 10 PM)	Nighttime (10 PM to 7 AM)	Daytime (7 AM to 10 PM)	Nighttime (10 PM to 7 AM)
Residential	Hourly Leq, dBA	50	45	45	35
	Max. level, dBA	70	65	-	-
Transient lodging	Hourly Leq, dBA	-	-	45	35
Hospital, nursing home	Hourly Leq, dBA	-	-	45	35
Theaters, auditoriums, music halls	Hourly Leq, dBA	-	-	35	35
Churches, meeting halls	Hourly Leq, dBA	-	-	40	40
Office buildings	Hourly Leq, dBA	-	-	45	45
Schools, libraries, museums	Hourly Leq, dBA	-	-	45	45

Source: City of West Sacramento, 2018.

4.12.2 Discussion

a) **Less-than-Significant Impact with Mitigation.** Noise at the construction site would be intermittent and the intensity would vary. The degree of construction noise impacts may vary for different locations within the Project study area and also vary depending on the construction activities.

Roadway and/or bridge construction is accomplished in several different phases. General construction phases for typical roadway/highway projects and their estimated overall noise levels are summarized in **Table 9 .**

Table 9. Typical Construction Phases and Noise Levels

Construction Phase	Noise Level (dBA, Leq)
Ground clearing	84
Excavation	88/78
Foundations	88
Erection	79/78
Finishing	84

Source: U.S. EPA, 1971.

During Project construction, noise from construction activities may intermittently dominate the noise environment in the immediate area of construction and some sensitive receptors surrounding the Project study area may be temporarily affected. The majority of construction noise would be from Project site clearing along with the bridge abutments placement and structure. Pile driving is not proposed for the Project.

Table 10 summarizes noise levels produced by construction equipment that is commonly used on bridge replacement projects and is representative of the equipment necessary for Project construction. Construction equipment is expected to generate noise levels ranging from 80 to 90 dB at a distance of 50 feet and noise produced by construction equipment would be reduced over distance at a rate of about 6 dB per doubling of distance.

Table 10. Typical Construction Equipment Noise Levels

Construction Equipment	Noise Level (dBA, Leq at 50 feet)
Scrapers	85
Bulldozers	85
Heavy trucks	85
Pneumatic tools	85
Concrete pump	82
Backhoe	80

Source: HMM&H, 2013

No adverse noise impacts from construction are anticipated because construction would be conducted in accordance with Caltrans Standard Specifications Section 14-8.02 (revised with

special provisions to match the City's time window), Municipal Code Chapter 17.32, and control measures discussed below. Construction noise would be short-term and intermittent. Construction operations are anticipated to occur during daylight hours only (Monday to Friday, 7:00 AM to 7:00 PM), pursuant to the Municipal Code. This impact would be less than significant with implementation of **MM NO-1**.

b) **Less-than-Significant Impact with Mitigation.** Equipment associated with high vibration levels (pile drivers) would not be used for the Project. The Municipal Code states that no project shall be permitted to produce noticeable vibration beyond the property line through its construction or operation (City of West Sacramento, 2018b). Project construction would use bulldozers and other heavy tracked construction equipment, which may generate a groundborne vibration level of 90 "smoothed" root mean square vibration velocity level in decibels (VdB) at 50 feet from source. The majority of construction noise would be from Project site clearing along with the placement of the new bridge falsework, abutments, and structure. Project construction is expected to last 8 to 12 months. The Project is expected to have a less-than-significant impact with the implementation of **MM NO-1.**

c) **No Impact.** The Project would have no long-term effects on noise levels. Noise levels would return to levels similar to the existing noise environment upon Project completion.

d) **Less-than-Significant Impact with Mitigation.** During construction, the Project would temporarily increase ambient noise levels in the Project vicinity. See the discussion regarding construction noise under a) and b) above.

Based on the loudest equipment (jackhammer: 85 dBA at 50 feet), the sensitive receptors within 100 feet would experience maximum noise levels of approximately 73 dBA and at 200 feet the additional sensitive receptors would experience maximum noise levels of approximately 71 dBA. Since the Municipal Code does not establish standards for temporary daytime construction noise, the significance of noise impacts would be determined using Caltrans Standard Specifications Section 14-8.02. The noise levels produced by the Project would be in accordance with Caltrans Standard Specifications Section 14-8.02 but would exceed the maximum noise standards allowed by Municipal Code Chapter 17.32. Noise mitigation and abatement measures would be instated to reduce construction noise levels to be in compliance with City noise standards.

This impact would be less than significant with implementation of **MM NO-1.**

e) **No Impact.** Sacramento International Airport is the closest airport to the Project site and is located approximately 8.6 miles northwest. The Project site is not within 2 miles and is located outside the noise contours of the Sacramento International Airport or any other public-use airport. There would be no impact from airports upon people residing or working in the Project vicinity.

f) **No Impact.** There are no private airstrips within 2 miles of the Project. The nearest private airstrip to the Project is the UC Davis Medical Center Life Flight Base/the UC Davis Medical Center Tower & Heliport, approximately 4.4 miles east of the Project site. There would be no impact from airstrips upon people residing or working in the Project vicinity.

4.12.3 Mitigation Measures

MM NO-1: Implement the following control measures to minimize noise and vibration disturbances at sensitive receptors during construction:

- Use newer equipment with improved muffling and ensure that all equipment items have the manufacturers' recommended noise abatement measures, such as mufflers, engine enclosures, and engine vibration isolators intact and operational. Newer equipment will generally be quieter in operation than older equipment. All construction equipment shall be inspected at periodic intervals to ensure proper maintenance and presence of noise control devices (e.g., mufflers and shrouding, etc.).
- Utilize construction methods or equipment that provides the lowest level of noise and ground vibration impact.
- Turn off idling equipment.

4.12.4 References

California Department of Transportation (Caltrans), 2013. Technical Noise Supplement to the Traffic Noise Analysis Protocol. Caltrans. Sacramento, California. Caltrans, 2017.

Caltrans, 2018a. Land Use and Community Impact Technical Memorandum for the Sycamore Trail Phase II & III Extension Project. February 28, 2018. Prepared for the City of West Sacramento by Drake Haglan & Associates.

Caltrans, 2018b. Noise Technical Memorandum for the Sycamore Trail Phase II & III Extension Project [CML-5447(044)]. February 27, 2018. Prepared for the City of West Sacramento by Drake Haglan & Associates.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

Harris Miller & Hanson (HMM&H); 2006. Transit noise and vibration impact assessment (FTA-VA-90-1003-06). Prepared for the Federal Transit Administration.

Sacramento Area Council of Governments (SACOG), 2013. Sacramento International Airport Land Use Compatibility Plan. December 12, 2013. Prepared by Mead & Hunt in association with Environmental Science Associates.

U.S. Environmental Protection Agency (U.S. EPA), 1971. Noise from construction equipment, operations, building equipment, and home appliances. National Service Center for Environmental Publications (NSCEP). Washington, D.C.

4.13 Population and Housing

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Population and Housing – Would the project:				
a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	☐	☐	☒	☐
b) Displace substantial numbers of existing housing units, necessitating the construction of replacement housing elsewhere?	☐	☐	☐	☒
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	☐	☐	☐	☒

4.13.1 Setting

A Land Use and Community Impact Technical Memorandum has been prepared for the Project and is available for review at the City Department of Public Works during business hours.

According to the 2010 Census and the 2010 American Community Survey, Yolo County has a population of 168,660 individuals and a total of 61,587 housing units. The Project site is located within census tracts 102.03 and 102.04. Census tract number 102.03 has a population of 5,370 people and a total of 2,188 housing units (U.S. Census Bureau, 2010a; U.S. Census Bureau, 2010b). Census tract number 102.04 has a population of 4,905 people and a total of 2,156 housing units (U.S. Census Bureau, 2010a; U.S. Census Bureau, 2010b).

4.13.2 Discussion

a) **Less-than-Significant Impact.** The Project would not result in the permanent creation of new jobs or housing that would induce substantial population growth. The bridge would remain two lanes and would not extend or increase capacity on Kent Avenue, and therefore would not indirectly induce substantial population growth in the surrounding community.

b) **No Impact.** The Project would build a new pedestrian overcrossing over US 50 and extend the Sycamore Trail within mainly currently existing right-of-way. Temporary and permanent right-of-way acquisitions would not result in the displacement of housing (Caltrans, 2018a). Replacement housing would not be required.

c) **No Impact.** The Project would build a new pedestrian overcrossing over US 50 and extend the Sycamore Trail within mainly currently existing right-of-way. Temporary and permanent right-of-way acquisitions would not result in the displacement of people (Caltrans, 2018a). Replacement housing would not be required.

4.13.3 References

California Department of Transportation (Caltrans), 2018a. Land Use and Community Impact Technical Memorandum for the Sycamore Trail Phase II & III Extension Project. February 28, 2018. Prepared for the City of West Sacramento by Drake Haglan & Associates.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

U.S. Census Bureau, 2010a. 2006-2010 American Community Survey.

U.S. Census Bureau, 2010b. 2010 Census.

4.14 Public Services

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Public Services – Would the project:				
a) Result in substantial adverse physical impacts associated with the provision of, or the need for, new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for any of the following public services:				
i) Fire protection?	☐	☒	☐	☐
ii) Police protection?	☐	☒	☐	☐
iii) Schools?	☐	☐	☒	☐
iv) Parks?	☐	☐	☐	☒
v) Other public facilities?	☐	☐	☐	☒

4.14.1 Setting

A Land Use and Community Impact Technical Memorandum has been prepared for the Project and is available for review at the City Department of Public Works during business hours. The Project site and vicinity is served by the West Sacramento Police Department for law enforcement and is served by the West Sacramento Fire Department for fire and emergency services (City of West Sacramento, 2016a; City of West Sacramento, 2016b;). The WUSD serves the Project site and vicinity (WUSD, 2018). The Project site northern boundary connects to the southern extent of Joey Lopes Park. Fourteen other parks and recreation areas are located within the Project vicinity. For more information about parks and recreation facilities within the Project vicinity, please see the Recreation section of this document.

4.14.2 Discussion

i) **Less-than-Significant Impact with Mitigation.** Fire service is provided by the West Sacramento Fire Department. The Project vicinity is served by Fire Station #43 (1561 Harbor Boulevard), located approximately 0.8 miles northwest of the Project site (City of West Sacramento, 2018a). The West Sacramento Fire Department provides response to fire, medical, and hazardous material emergencies within the Project vicinity.

Project construction could result in accident or emergency incidents that would require emergency response, such as fire services; however, construction activities would be short-term and minimal. The Project would utilize existing public open space and vertical clearance above US 50, so would not create additional demands on the local fire district during operations.

Emergency access to the Project vicinity would continue during construction. US 50 between Harbor and Jefferson Boulevards would be temporarily closed (less than 24 hours at a time) to place and remove the falsework for the pedestrian-bicycle overcrossing; a detour is planned for

both westbound and eastbound traffic along I-80 and I-5. Potential impacts to fire emergency service times would be considered less than significant within implementation of **MM PUB-1.**

ii) **Less-than-Significant Impact with Mitigation.** The West Sacramento Police Department provides law enforcement services to the Project vicinity. The West Sacramento Police Department station is located at 550 Jefferson Boulevard, approximately 2.1 miles northeast of the Project site (City of West Sacramento, 2018d). Project construction could result in accident or emergency incidents that would require police services; however, construction activities would be short-term and minimal. Police, fire, and emergency protocols for accessing and traveling along the trail would be developed by the West Sacramento Police Department and the West Sacramento Fire Department as part of Project design.

Project design would incorporate Crime Prevention through Environmental Design (CPTED) principles that emphasize clear sightlines, defensible spaces, lighting, and good access to promote a safe environment and to decrease the necessity for policy patrols and intervention. CPTED improvements to the existing trail would add pedestrian-scale solar lighting where there is none currently, improving visibility and safety, minimizing dense vegetation to increase clear sightlines, and installing call boxes spaced out along the southern trail segment. These CPTED principle improvements would also have the effect of discouraging unauthorized camping along the Project site after Project completion (Caltrans, 2018a).

Emergency access to the Project vicinity would continue during construction. US 50 between Harbor Boulevard and Jefferson Boulevard would be temporarily closed (less than 24 hours at a time) to place and remove the falsework for the overcrossing; a detour is planned for both westbound and eastbound traffic along I-80 and I-5. Potential impacts to law enforcement emergency service times would be considered less than significant within implementation of **MM PUB-1.**

iii) **Less-than-Significant Impact**. The Project is located adjacent to the Westmore Oaks Elementary School (1100 Clarendon Street) and is within the WUSD. **Table 11** below describes the other schools located within the Project vicinity.

Table 11. Schools within the Project Vicinity.

Name	Address	Approximate Location from Project site (by driving)
Westmore Oaks Elementary School	1100 Clarendon Street	Adjacent
Westfield Village Elementary School	508 Poplar Avenue	0.7 miles north
Yolo High School	919 Westacre Road	0.4 miles west
Stonegate Elementary	2500 La Jolla Street	1.8 miles southeast
Elkhorn Village Elementary	750 Cummins Way	2.0 miles northeast

Source: WUSD, 2018.

The Project would not remove educational facilities or induce growth, and therefore would not generate any additional demand for schools. Traffic would not be significantly impacted by the short-term temporary construction detour along I-80 and I-5.

iv) **No Impact.** The Project site northern boundary would meet the southern boundary of the Sycamore Trail Phase I project and would be adjacent to Joey Lopes Park. The Project would not require temporary or permanent right-of-way from Joey Lopes Park or any park within the Project vicinity, and Project construction would not inhibit the use of any parks within the Project vicinity. A portion of the School land would be designated as a public park under a separate project; the Sycamore Phase II & III Extension Project would not create additional demands for the existence or use of parks and recreation facilities.

v) **No Impact.** The Project would not impact any other public services in the City.

4.14.3 Mitigation Measures

MM PUB-1: *Develop a Construction Period Emergency Access Plan.* Prior to the start of construction, the contractor shall coordinate with the City of West Sacramento Police and Fire departments and local public and private ambulance and paramedic providers in the area to prepare a Construction Period Emergency Access Plan. The Construction Period Emergency Access Plan shall identify Project phases and construction scheduling and shall identify appropriate alternative emergency access routes.

4.14.4 References

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018a. Fire Station Locations. Available at: http://www.cityofwestsacramento.org/city/depts/fire/deptdir.asp. Accessed March 6, 2018.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

City of West Sacramento, 2018c. West Sacramento Parks and Recreation. Available at: http://www.cityofwestsacramento.org/city/depts/pcs/parks/parks/default.asp. Accessed March 6, 2018.

City of West Sacramento, 2018d. West Sacramento Police Department. Available at: http://www.cityofwestsacramento.org/city/depts/police/. Accessed March 6, 2018.

City of West Sacramento, 2003. 2013 West Sacramento Parks Master Plan. September 2003. Prepared by Smith Group., JJR.

Washington Unified School District (WUSD), 2018. School Locator. Available at: http://www.schoolworksgis.com/WUSD/schoollocator.html. Accessed March 6, 2018.

West Sacramento Early College Prep Charter School, 2017. Home. Available at: http://www.westsacprep.org/. Accessed March 6, 2018.

4.15 Recreation

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Recreation – Would the project:				
a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facilities would occur or be accelerated?	☐	☐	☐	☒
a) Include recreational facilities or require the construction or expansion of recreational facilities that might have an adverse physical effect on the environment?	☐	☐	☐	☒

4.15.1 Setting

A Land Use and Community Impact Technical Memorandum has been prepared for the Project and is available for review at the City Department of Public Works during business hours.

The Project site northern boundary connects to the southern extent of Joey Lopes Park. Multiple other parks and recreation areas are located within the Project vicinity. Joey Lopes Park is approximately 4 acres and contains a half-court basketball court, a community garden, play equipment, and the southern extent of the Phase I Trail (City of West Sacramento, 2018c). Westacre Park is approximately 5 acres in size and has two backstops, a soccer/football field, a volleyball court, full-court basketball, and skateboarding elements as recreational facilities (City of West Sacramento, 2018c). San Combs Park is 4.5 aces and has horseshoe pits, a dog park, and walking paths as facilities (City of West Sacramento, 2018c).**Table 12** provides a comprehensive list of other public park facilities within the Project vicinity.

Table 12. Parks and Recreational Facilities within the Project Vicinity

Name	Address	Approximate Location from Project site (by Driving)
Joey Lopes Park	2101 West Capitol Avenue	Adjacent
Westacre Park	1755 Evergreen Avenue	0.2 miles east
Memorial Park	301 Regent Street	0.4 miles east
Sam Combs Park	155 Stone Boulevard	0.4 miles east
Fred and Leila Holmes Park	1650 Pennsylvania Avenue	0.5 miles east
Westfield Playground	504 Poplar Avenue	0.7 miles northeast
Barge Canal Recreational Access	2100 Jefferson Boulevard	0.8 miles south
Rotary Centennial Minipark	580 Jefferson Boulevard	1.0 miles northeast
Circle Park	1509 Circle Street	1.1 miles southeast
Jerome D. Barry	809 Ballpark Drive	1.2 miles east
Garden Park	564 Garden Street	1.3 miles northeast
Alyce Norman-Bryte Playfields	725 Todhunter Avenue	1.3 miles northwest
River Walk Park	651 2nd Street	1.9 miles east
Delta Gardens Park	1310 Cold Springs Road	1.9 miles southeast

Elkhorn Park	820 Cummins Way	2.0 miles northeast

Source: City of West Sacramento, 2018c.

Sycamore Trail is part of the City's Bicycle, Pedestrian and Trails Master Plan that will provide connections between central City neighborhoods, linking people with, parks, businesses and commercial centers. The Sycamore Trail is also a part of the Great California Delta Trail as of January 2016. The Great California Trail is the vision for a recreational trail system along the Sacramento-San Joaquin Delta connecting the greater Sacramento area to the San Francisco Bay Trail system and includes routes for bicycling and hiking, with interconnections to other trails, recreational facilities and public transportation.

4.15.2 Discussion

a) **No Impact.** The majority of the trail would be constructed on public/quasi-public land that is not currently accessible to the public and over an existing freeway, in which there is currently no bicycle-pedestrian crossing at the Project location; therefore, the Project would not increase demand on existing neighborhood streets, parks, and other facilities. A portion of the School athletic field may be turned into a public park under a separate project as part of the School-City Joint Use Agreement as suggested by WUSD's Letter of Intent (Caltrans, 2018a). No additional regional parks would be required to be created, and the Project would have no adverse impact on the use of the Joey Lopes Park or any other existing neighborhood and regional parks.

b) **No Impact.** A portion of the School athletic field may be turned into a public park under a separate project as part of the School-City Joint Use Agreement as suggested by WUSD's Letter of Intent (Caltrans, 2018a). The Project site does not include existing recreational facilities nor requires temporary or permanent right-of-way in any recreational facility.

4.15.3 References

City of West Sacramento, 2003. 2013 West Sacramento Parks Master Plan. September 2003. Prepared by Smith Group, JJR.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

City of West Sacramento, 2018c. West Sacramento Parks and Recreation. Available at: http://www.cityofwestsacramento.org/city/depts/pcs/parks/parks/default.asp. Accessed March 6, 2018.

City of West Sacramento, 2013. 2013 West Sacramento Bicycle, Pedestrian, and Trails Master Plan. May 2013. Prepared by Fehr & Peers.

Delta Protection Commission, 2018. The Great California Delta Trail. Available at: http://delta.ca.gov/recreation/delta_trail/. Accessed August 7, 2018.

4.16-Transportation and Traffic

Issues (and Supporting Information Sources)	Potentially Significant Impact	Less Than Significant with Mitigation Incorporation	Less Than Significant Impact	No Impact
Transportation and Traffic – Would the project:				
a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?	☐	☐	☒	☐
b) Conflict with an applicable congestion management program, including, but not limited to, level of service standards and travel demand measures, or other standards established by the City congestion management agency for designated roads or highways?	☐	☒	☐	☐
c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?	☐	☐	☐	☒
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	☐	☐	☒	☐
e) Result in inadequate emergency access?	☐	☒	☐	☐
f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?	☐	☐	☒	☐

4.16.1 Setting

A Traffic Technical Memorandum has been prepared for the Project and is available for review at the City Department of Public Works during business hours (Caltrans, 2018e).

Roadways are classified in a hierarchical manner, based on the level of traffic they are expected to carry. Local roads are intended to serve adjacent properties and carry minimal through traffic with low traffic volumes. Collector roads are intended to carry traffic from local roads to larger (arterial) roads; they serve adjacent properties and generally carry light to medium traffic. Minor arterial roads carry traffic from local and collector roads to major arterial roadways and freeways, often serve adjacent properties, and serve heavy volumes of traffic as part of the main intra-city circulation system. Major arterial roads carry heavy volumes of traffic from local, collector, and minor arterial roads for intra-city and regional travel. Freeways

and expressways serve intra-city and regional travel and are connected to the other roadway types via access ramps.

According to the 2016 General Plan, at the Project site Park Boulevard is classified as a residential collector street with an average daily traffic volume (ADT) of 4,030 vehicular trips. At the Project site, Stone Boulevard is also classified as a residential collector street and has an ADT of 2,459 vehicular trips. During A.M. peak hour, the US 50 has a level of service (LOS) of "B" in both westbound and eastbound directions at the Project site. This section of US 50 has a traffic volume of 4,945 vehicular trips in the eastbound direction and 3,555 vehicular trips in the westbound direction during the A.M. peak hour. During P.M. peak hour, eastbound US 50 has a traffic volume of 5,559 vehicular trips and a LOS of "C" while westbound US 50 has a traffic volume of 3,300 vehicular trips and a LOS of "B."

There is a detour route planned for this Project along I-80 and I-5. The detour route would move both east- and westbound traffic and is designed to account for both freeway capacity and trucks. The detour would be approximately 9.16 miles long in both the eastbound and the westbound direction. Along the planned detour route, I-80 and I-5 are both classified as freeway/highway segments within the City of West Sacramento General Plan and the City of Sacramento General Plan.

Along the planned detour route, I-80 and I-5 are both classified as freeway/highway segments. **Table 13** below provides details on the annual ADT of I-80 and I-5 segments along the planned detour route at different traffic surveying points. Annual averaged daily traffic volume (AADT) is collected by taking the total traffic volume for the year and dividing it by 365 days.

Table 13. Annual Average Daily Traffic Volume Along the Planned Detour Route

Traffic Surveying Point	AADT
Interstate 80	
West El Camino Avenue	89600
Junction with I-5	143900
Interstate 5	
Junction with I-80	157600
West El Camino Avenue	163500
Garden Highway	189300
Richards Boulevard	195300
I Street	190800
P/Q Street	193400
Junction with Route 50	189000

Source: Caltrans, 2016.

[1] Ahead AADT is displayed under AADT in this table. Ahead AADT usually represents traffic north or east of the traffic count location.

The Project site is under the jurisdiction of the 2013 West Sacramento Bicycle, Pedestrian, and Trails Master Plan (Trails Master Plan) and the SACOG Metropolitan Transportation Plan for 2035 for transportation planning and standards in addition to the General Plan.

4.16.2 Discussion

a) **Less-than-Significant Impact.** The Project would not create impacts that would impact vehicular, bicycle, or pedestrian circulation or access in a way that conflicts with the adopted General Plan; the Trails Master Plan; the SACOG Metropolitan Transportation Plan for 2035; or any other adopted plans, ordinances, or policies. The Sycamore Trail Phase II & III Extension Project is designated as a proposed bicycle network facility within the Trails Master Plan and is designated as part of the proposed recreation corridor 2 (RC2) within the adopted 2003 City of West Sacramento Parks Master Plan (Smith Group, 2003; City of West Sacramento, 2013). Project completion would satisfy both plans' objectives of creating a multi-modal trail/corridor within the Project site.

b) **Less-than-Significant Impact with Mitigation.** The Project purpose is to provide adequate and safe pedestrian and bicycle access from the intersection of Evergreen Avenue and Sycamore Avenue to the intersection of Park Boulevard and Stone Boulevard via the extension of a Class I bicycle facility and the creation of a pedestrian-bicycle overcrossing over US 50 as part of the Class I bicycle facility. The Project would not add capacity (e.g., create additional vehicular lanes to) nor affect vehicular traffic circulation on existing roadways within the Project facility; therefore, the ADT of these existing roadways is expected to remain consistent before and after Project construction. The Project would not conflict with any plan or policy established for measuring the performance of the circulation system and would not result in impacts to LOS along US 50, Park Boulevard, Stone Boulevard, I-80, I-5, Evergreen Avenue, or Sycamore Avenue. Impacts to level of service during Project construction would be considered less than significant with the implementation of **MM TRAF-1.**

c) **No Impact.** The Project does not include structures or uses that would affect air traffic patterns, nor is an airport located in proximity to the Project site. The projected vertical clearance of the pedestrian-bicycle overcrossing to be constructed, at its geographic distance from Sacramento International Airport as the nearest public or military airport to the Project site, does not meet the criteria to be considered an object that may affect navigable airspace (FAA, 2007; FAA, 2015). The Project would not cause changes to existing air traffic patterns.

d) **Less-than-Significant Impact.** One of the primary purposes of the Project is to provide a safer access route across US 50 to all City community members. The completed Phase II & III Trail would transition into the Phase I Trail at the Project northern boundary and would transition into the existing Park Boulevard and Stone Boulevard bicycle facilities at the southern Project boundary, thereby minimizing the potential of dangerous gaps in facility visibility and signage. Clear signage and pedestrian scale lighting would be placed on all trail sections to increase trail user visibility and minimize the potential for pedestrian, bicyclist, and/or motorist collisions. The new pedestrian and bicycle overcrossing is required to have fencing incorporated into its design to prevent individuals from falling or throwing objects off of the bridge onto US 50 below.

e) **Less-than-Significant Impact with Mitigation.** The 2035 City of West Sacramento General Plan Public Facilities and Services Element states in objective PFS-8.3 that the goal for average police response time for Priority 1 (emergency) calls shall be five minutes and states that in objective PFS-9.2 that the goal for average fire response time for Priority 1 calls shall be five minutes for 90 percent of calls (City of West Sacramento, 2016a). Objective PFS-9.14 of the Public Facilities

and Services Element states that the City shall maintain Fire Department response time standards for emergency medical service providers (City of West Sacramento, 2016a). There would be temporary (less than 12 hour) closures of US 50 during construction, but the detour route planned on I-80 and I-5 would not significantly impact emergency response times. Impacts to emergency service access during Project construction would be considered less than significant with the implementation of **MM PUB-1.**

f) **Less-than-Significant Impact.** The Project would increase safety for bicyclists and pedestrians within the City community by extending a Class I bicycle facility across US 50. Project completion would fulfill the goals of the Trails Master Plan and the Project would not conflict with the General Plan; Trails Master Plan; Parks Master Plan; SACOG Metropolitan Transportation Plan for 2035; or any other applicable adopted policy, plan, or program supporting alternative transportation.

4.16.3 Mitigation Measures

MM TRAF-1: *Develop a Standard Traffic Management Plan.* The construction contractor for the Project shall implement a standard traffic management plan to minimize traffic disruption and ensure adequate access is maintained to surrounding properties.

4.16.4 References

California Department of Transportation (Caltrans), 2016. 2016 Traffic Volumes. Available at: http://www.dot.ca.gov/trafficops/census/volumes2016/Route44-50.html. Accessed June 20, 2018.

Caltrans, 2018e. Traffic Technical Memorandum: Sycamore Trail Phase II & III Extension Project. February 14, 2018. Prepared for the City of West Sacramento by Drake Haglan & Associates.

City of Sacramento, 2015. Sacramento 2035 General Plan Background Report. Adopted March 3, 2015.

City of West Sacramento, 2013. 2013 West Sacramento Bicycle, Pedestrian, and Trails Master Plan. May 2013. Prepared by Fehr & Peers.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

Sacramento Area Council of Governments (SACOG), 2013. Sacramento International Airport Land Use Compatibility Plan. December 12, 2013. Prepared by Mead & Hunt in association with Environmental Science Associates.

City of West Sacramento, 2003. 2013 West Sacramento Parks Master Plan. September 2003. Prepared by Smith Group, JJR.

U.S. Department of Transportation, Federal Aviation Administration (FAA), 2007. Advisory Circular AC 70/7460-1K: Obstruction Marking and Lighting. February 1, 2007.

FAA, 2015. Land Use Compatibility and Airports. Available at: https://www.faa.gov/about/office_org/headquarters_offices/apl/noise_emissions/planning_toolkit/media/III.B.pdf. Accessed March 6, 2018.

4.17 Tribal Cultural Resources

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Tribal Cultural Resources – Would the project:				
a) Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resource Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:				
i) Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or	☐	☒	☐	☐
ii) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision C, of Public Resources Code Section 5024.1, the lead agency shall consider the significance of the resources to a California Native American tribe.	☐	☒	☐	☐

4.17.1 Setting

Assembly Bill 52 (AB 52) went into effect on July 1, 2015 and establishes a consultation process with all California Native American Tribes on the Native American Heritage Commission (NAHC) List for federal and non-federal tribes. Once the tribe is notified of the Project, the tribe has 30 days to request consultation. The consultation process ends when either the parties agree to mitigation measures or avoid a significant effect on tribal cultural resources or a party, acting in good faith and after reasonable effect concludes that mutual agreement cannot be reached.

A tribal cultural resource (TCR) is defined as a site, feature, place, cultural landscape, or sacred place or object that has cultural value to California Native American tribes. In order to be considered a TCR, the resource must be included in or determined eligible for inclusion in the CRHR or is in included in a local register of historical resources. To be considered a historical resource, for the purposes of a TCR, the resource must meet the following criteria for listing in the CRHR:

- is associated with the events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
- is associated with the lives of persons important to our past;
- embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic value; or
- has yielded, or may likely to yield, important information in prehistory or history.

An archaeological survey report was conducted by LSA on behalf of DHA for this project, which documents the cultural resource and tribal cultural resource identification efforts for the project (Caltrans, 2017a).

4.17.2 Discussion

a) **Less-than-Significant Impact with Mitigation.** Part of the effort to identify potentially significant historical and traditional cultural resources that may fall within the Project area, a letter was sent on October 10, 2017, to the NAHC requesting a search of the sacred lands files and contacts with Native American tribes and individuals who may have information of TCRs in the project vicinity. On October 19, 2017, the NAHC responded that their search of sacred land files indicated a potential presence of Native American resources in the immediate Project area. Native American individuals and entities identified by the NAHC were sent letters on October 20, 2017.

The City received a letter dated November 15, 2017, requesting AB 52 consultation to the Project from the United Auburn Indian Community (UAIC). UAIC requested a site visit on January 23, 2018, which was cancelled after further UAIC review of the Project impact map. UAIC requested two mitigation measures be incorporated into the IS/MND that include cultural resource awareness training of the construction crew prior to ground disturbing activities and the ability for tribes to visit the site after ground disturbing activities have commenced. With the implementation of those two mitigation measures, **MM TRC-1** and **MM TRC-2,** impacts to tribal cultural resources by the Project would be less than significant.

4.17.3 Mitigation Measures

MM TCR-1: *Conduct a Tribal Cultural Resource Construction Worker Awareness Training.* A consultant and construction worker tribal cultural resources awareness brochure and training program for all personnel involved in Project implementation will be developed in coordination with interested Native American tribes. Distribute the brochure and conduct the training in coordination with qualified cultural resources specialists and Native American representatives and monitors from culturally affiliated Native American tribes before any stages of Project implementation and construction activities begin on the Project site. The program will include relevant information regarding sensitive tribal cultural resources, including applicable regulations, protocols for avoidance, and consequences of violating State laws and regulations. The worker cultural resources awareness program will also describe the procedure to follow for resources that have the potential to be located on the Project site and will outline what to do and whom to contact if any potential archaeological resources or artifacts are encountered. The program will also underscore the requirement for confidentiality and culturally-appropriate treatment of any finds of significance to Native Americans and behaviors, consistent with Native American tribal values.

MM TCR-2: *Employ a Native American Monitor*. A minimum of seven days prior to beginning earthwork or other soil disturbance activities, the City shall contact concerned tribes of the construction schedule for ground disturbance activities. A tribal representative shall be invited to inspect the Project site, including any soil piles, trenches, or other disturbed areas, within the first five days of ground breaking activity. During this inspection, a site meeting of construction personnel shall also be held to afford the tribal representative the opportunity to provide tribal cultural resources awareness information. If any tribal cultural resources, such as structural features, unusual amounts of bone or shell, artifacts, human remains, or architectural remains are encountered during this initial inspection or during any subsequent construction activities, work shall be suspended within 100 feet of the find. The City shall coordinate any necessary investigation of the site with a tribal representative and a qualified archaeologist approved by the City; and as part of the site investigation and resource assessment, the archeologist shall consult with the tribe(s) and provide proper management recommendations should potential impacts to the resources be found by the City to be significant. A written report detailing the site assessment, coordination

activities, and management recommendations shall be provided to the City by the qualified archaeologist. The contractor shall implement the use of a Native American monitor whenever work occurs within 100 feet of the find.

4.17.4 References

California Department of Transportation (Caltrans), 2017a. Archaeological Survey Report: Sycamore Trail Phase II & III Extension Project, Federal Project No. CML5447(044). Prepared by LSA on behalf of Drake Haglan & Associates.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

University of California Museum of Paleontology (UCMP), 2017. Collections. Available at: http://www.ucmp.berkeley.edu/science/collections.php. Accessed January 1, 2018.

4.18 Utilities and Service Systems

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Utilities and Service Systems – Would the project:				
a) Conflict with wastewater treatment requirements of the applicable Regional Water Quality Control Board?	☐	☐	☐	☒
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	☐	☐	☐	☒
c) Require or result in the construction of new storm water drainage facilities, or expansion of existing facilities, the construction of which could cause significant environmental effects?	☐	☐	☐	☒
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	☐	☐	☒	☐
e) Result in a determination by the wastewater treatment provider that would serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	☐	☐	☐	☒
f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	☐	☐	☒	☐
g) Comply with federal, state, and local statutes and regulations related to solid waste?	☐	☐	☐	☒

4.18.1 Setting

Water treatment and delivery, wastewater collection and disposal, stormwater drainage, solid waste, and recycling facilities and services are all provided by the City within the Project site and vicinity (City of West Sacramento, 2016a). Electricity and natural gas services are provided by PG&E; telecommunication infrastructure is serviced by a variety of private providers (City of West Sacramento, 2016b).

4.18.2 Discussion

a) **No Impact**. The Project would not generate any wastewater.

b) **No Impact.** The Project would not require the construction of additional wastewater or water treatment facilities.

c) **No Impact.** The Project would not require construction of new stormwater facilities or require the expansion of existing facilities.

d) **Less-than-Significant Impact.** The Project would not require water supply. Some non-potable water use would be required for fugitive dust control during Project construction activities.

e) **No Impact.** The Project would not require wastewater treatment services.

f) **Less-than-Significant Impact.** The Project would generate waste from the temporary construction activities. Solid waste associated from construction activities would be handled by the Yolo County Central Landfill (44090 Co Road 28H), pursuant to the General Plan. The Yolo County Central Landfill has the capacity to accept waste generated by the Project, and the Project would not result in long-term demands for solid waste disposal services.

g) **No Impact.** The Project would comply with all federal, state, and local statutes and regulations related to solid waste.

4.18.3 References

California Department of Transportation (Caltrans), 2018a. Land Use and Community Impact Technical Memorandum for the Sycamore Trail Phase II & III Extension Project. February 28, 2018. Prepared for the City of West Sacramento by Drake Haglan & Associates.

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

4.19 Mandatory Findings of Significance

Issues (and Supporting Information Sources)	*Potentially Significant Impact*	*Less Than Significant with Mitigation Incorporation*	*Less Than Significant Impact*	*No Impact*
Mandatory Findings of Significance – Would the project:				
a) Have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory?	☐	☒	☐	☐
b) Have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?	☐	☐	☒	☐
c) Have environmental effects that would cause substantial adverse effects on human beings, either directly or indirectly?	☐	☐	☒	☐

4.19.1 Setting

Per CEQA regulations and guidelines, the Lead Agency must summarize the finding of significance from earlier sections and must consider potential cumulatively considerable effects for environmental impact reports (EIRs) and in the discussion section below. Even though this environmental document is an IS/MND and not an EIR, the potential for cumulatively considerable effects are analyzed below.

4.19.2 Discussion

a) **Less-than-Significant Impact with Mitigation**. Per the impact discussions in the Biological Resources section, the potential of the Project to substantially degrade the environment would be less than significant with incorporated mitigation measures.

b) **Less-than-Significant Impact.** The Project site is located within the City of West Sacramento. The Project purpose is to increase the number of individuals with bike or walk to school, work for errands or for recreation by extending an existing bicycle trail and building a bicycle-pedestrian overcrossing over US 50. The Project impacts are mitigated to a less-than-significant level, limited to the Project construction phase, and generally site specific. To the City's knowledge there are no other projects proposed in the area that would overlap or interact with the Project and prevent the Project from having independent utility.

c) **Less-than-Significant Impact.** The Project would not cause substantial adverse effects on human beings. Effects related to aesthetics, air quality, cultural resources, hazards and hazardous materials, noise, public services, and transportation are discussed above, and would not result in any significant and unavoidable impacts.

4.19.3 References

City of West Sacramento, 2016a. City of West Sacramento General Plan 2035 Policy Document. November 2016. Also prepared by ICF International, DKS Associations, and Mintier Harnish.

City of West Sacramento, 2016b. City of West Sacramento General Plan Update Draft Environmental Impact Report. August 2016. Prepared by ICF International.

City of West Sacramento, 2018b. West Sacramento Municipal Code. Available at: http://qcode.us/codes/westsacramento/view.php?topic=17&frames=off. Accessed February 6, 2018.

5 LIST OF PREPARERS AND REVIEWERS

This Final IS/MND was prepared by DHA in cooperation with the other members of the environmental study team. DHA was responsible for project management and Final IS/MND preparation. The Final IS/MND technical team and other environmental study team members provided technical expertise, as presented below.

CEQA Lead Agency: City of West Sacramento Public Works Department

Edgar Medina .....Senior Civil Engineer

Drake Haglan and Associates

Principal in Charge .....Dennis Haglan

Project Manager .....Brian Hansen, P.E.

Environmental Project Manager.....Leslie Haglan

Environmental Planner ....Amanda Dworkin

LSA Associates, Inc.

Archaeological Survey Report

6 COMMENTS

This Chapter summarizes the City's efforts to identify, address and resolve Project-related issues through early and continuing coordination.

Public circulation of the environmental document for the Project occurred from June 26, 2018 to July 26, 2018. All comments have been incorporated into the Initial Study/Mitigated Negative Declaration as Appendix B. Any additions or corrections to the IS/MND subsequent to public comments have been addressed within the document.

APPENDICES

Appendix A: Roadway Construction Emissions Modeling Data

Road Construction Emissions Model, Version 8.1.0

Daily Emission Estimates for -> Sycamore Trail Phase II & III Extension Project

Project Phases (Pounds)	ROG (lbs/day)	CO (lbs/day)	NOx (lbs/day)	Total PM10 (lbs/day)	Exhaust PM10 (lbs/day)	Fugitive Dust PM10 (lbs/day)	Total PM2.5 (lbs/day)	Exhaust PM2.5 (lbs/day)	Fugitive Dust PM2.5 (lbs/day)	SOx (lbs/day)	CO2 (lbs/day)	CH4 (lbs/day)	N2O (lbs/day)	CO2e (lbs/day)
Grubbing/Land Clearing	1.09	22.49	3.22	80.20	0.20	80.00	16.80	0.16	16.64	0.04	3,790.22	1.00	0.04	3,827.81
Grading/Excavation	6.44	122.27	13.90	80.84	0.84	80.00	17.33	0.69	16.64	0.22	21,422.33	6.28	0.20	21,640.30
Drainage/Utilities/Sub-Grade	4.51	88.35	10.50	80.64	0.64	80.00	17.16	0.52	16.64	0.16	15,404.02	3.71	0.14	15,538.76
Paving	1.13	26.89	3.18	0.22	0.22	0.00	0.17	0.17	0.00	0.04	3,870.97	1.02	0.04	3,908.60
Maximum (pounds/day)	6.44	122.27	13.90	80.84	0.84	80.00	17.33	0.69	16.64	0.22	21,422.33	6.28	0.20	21,640.30
Total (tons/construction project)	0.60	11.59	1.35	9.06	0.08	8.98	1.93	0.07	1.87	0.02	2,009.16	0.55	0.02	2,028.69

Notes:

Project Start Year ->	2019
Project Length (months) ->	12
Total Project Area (acres) ->	39
Maximum Area Disturbed/Day (acres) ->	8
Water Truck Used? ->	Yes

	Total Material Imported/Exported Volume (yd^3/day)		Daily VMT (miles/day)			
Phase	Soil	Asphalt	Soil Hauling	Asphalt Hauling	Worker Commute	Water Truck
Grubbing/Land Clearing	0	0	0	0	360	80
Grading/Excavation	0	0	0	0	1,560	80
Drainage/Utilities/Sub-Grade	0	0	0	0	1,160	40
Paving	0	0	0	0	560	40

PM10 and PM2.5 estimates assume 50% control of fugitive dust from watering and associated dust control measures if a minimum number of water trucks are specified.

Total PM10 emissions shown in column F are the sum of exhaust and fugitive dust emissions shown in columns G and H. Total PM2.5 emissions shown in Column I are the sum of exhaust and fugitive dust emissions shown in columns J and K.

CO2e emissions are estimated by multiplying mass emissions for each GHG by its global warming potential (GWP), 1 , 25 and 298 for CO2, CH4 and N2O, respectively. Total CO2e is then estimated by summing CO2e estimates over all GHGs.

Total Emission Estimates by Phase for -> Sycamore Trail Phase II & III Extension Project

Project Phases (Tons for all except CO2e. Metric tonnes for CO2e)	ROG (tons/phase)	CO (tons/phase)	NOx (tons/phase)	Total PM10 (tons/phase)	Exhaust PM10 (tons/phase)	Fugitive Dust PM10 (tons/phase)	Total PM2.5 (tons/phase)	Exhaust PM2.5 (tons/phase)	Fugitive Dust PM2.5 (tons/phase)	SOx (tons/phase)	CO2 (tons/phase)	CH4 (tons/phase)	N2O (tons/phase)	CO2e (MT/phase)
Grubbing/Land Clearing	0.01	0.30	0.04	1.06	0.00	1.06	0.22	0.00	0.22	0.00	50.03	0.01	0.00	45.84
Grading/Excavation	0.38	7.26	0.83	4.80	0.05	4.75	1.03	0.04	0.99	0.01	1,272.49	0.37	0.01	1,166.14
Drainage/Utilities/Sub-Grade	0.18	3.50	0.42	3.19	0.03	3.17	0.68	0.02	0.66	0.01	610.00	0.15	0.01	558.23
Paving	0.02	0.53	0.06	0.00	0.00	0.00	0.00	0.00	0.00	0.00	76.65	0.02	0.00	70.21
Maximum (tons/phase)	0.38	7.26	0.83	4.80	0.05	4.75	1.03	0.04	0.99	0.01	1272.49	0.37	0.01	1,166.14
Total (tons/construction project)	0.60	11.59	1.35	9.06	0.08	8.98	1.93	0.07	1.87	0.02	2009.16	0.55	0.02	1,840.41

PM10 and PM2.5 estimates assume 50% control of fugitive dust from watering and associated dust control measures if a minimum number of water trucks are specified.

Total PM10 emissions shown in column F are the sum of exhaust and fugitive dust emissions shown in columns G and H. Total PM2.5 emissions shown in Column I are the sum of exhaust and fugitive dust emissions shown in columns J and K.

CO2e emissions are estimated by multiplying mass emissions for each GHG by its global warming potential (GWP), 1 , 25 and 298 for CO2, CH4 and N2O, respectively. Total CO2e is then estimated by summing CO2e estimates over all GHGs.

The CO2e emissions are reported as metric tons per phase.

Appendix B: Comments Received and Response to Public Comments

RESPONSES TO PUBLIC COMMENTS

This section provides a summary of comments received during the public review period for the Initial Study and Mitigated Negative Declaration (IS/MND) for the Sycamore Trail Phase II & III. The public review period for this project was from June 27, 2018 to July 28, 2018. A total of two letters were received during the public review period. Section A provides a list of all written correspondences received during the public review period; Section B provides a written response to individual comments; and Section C contains a copy of each Correspondence that was received.

A. Agencies, Organizations, and Individuals Who Have Commented of the Draft Initial Study/ Mitigated Negative Declaration (IS/MND)

Letter 1: Delta Protection Commission, Erik Vink, Executive Director, July 26, 2018

Letter 2: Central Valley Regional Water Quality Board, Stephanie Tadlock, Environmental Scientist, July 19, 2018.

Letter 3: Governor's Office of Planning and Research, State Clearinghouse, Scott Morgan, Director, July 27, 2018

B. Responses to Written Comments

Response to comments submitted by Delta Protection Commission, Erik Vink, Executive Director, July 26, 2018 (Letter #1)

NO	Comment/ Recommendation	Response
Mr. Erik Vink Delta Protection Commission 2101 Stone Blvd., Suite 240 West Sacramento, CA 95691		
1	Comment letter recommends that the IS/MND discuss the Delta Trail in the recreation and transportation sections and describe how the Project can connect to current and future segments.	The recreation section of the IS/MND has been revised to include a discussion about how the Project is a part of the Bicycle, Pedestrian and Trails Master Plan as well as part of a larger regional trail network. The Transportation section discusses the City's Bicycle, Pedestrian and Trails Master Plan which discusses the Delta Trail Plan and therefore this section was not revised.

Response to comments submitted by Central Valley Regional Water Quality Board, Stephanie Tadlock, Environmental Scientist, July 19, 2018. (Letter #2)

NO	Comment/ Recommendation	Response
Ms. Stephanie Tadlock Central Valley Regional Water Quality Control Board 11020 Sun Center Drive, Ste. 200 Rancho Cordova, CA 95670		
1	Comment letter states that the Central Valley Regional Water Quality Control board is delegated the responsibility of protecting the quality of surface and groundwaters of the state. The letter indicates that the project may require various permits related to surface and	The City is currently in the final design phase of this project which includes the development of the construction documents and the preparation of the appropriate regulatory agency and jurisdictional permit applications.

	groundwaters of the state including Construction Storm Water General Permit, Phase I and II Municipal Separate Storm Sewer System Permits, Individual Storm Water General Permit, Clean Water Act Section 404 Permit, Clean Water Act Section 401 Permit-Water Quality Certification, Waste Discharge Requirement Permit, Dewatering Permit, Low or Limited Threat General NPDES Permit, and NPDES Permit.	The construction documents being prepared for this project include requirements for the contractor to prepare a Storm Water Pollution Prevention Plan (SWPPP) prior to starting construction activities and the use of best management practices during construction to prevent the runoff of pollutants from the work site. As discussed in the IS/MND, Section 2.4, *Permits and Approvals Needed*, The regulatory agency and jurisdictional permits that will be secured for this project prior to the start of construction include: • Construction Encroachment Permit • General Permit to construct • Grading permit General Construction activity stormwater discharge permit •

Response to comments submitted by Governor's Office of Planning and Research, State Clearinghouse, Scott Morgan, Director July 27, 2018 (Letter #3)

NO	Comment/ Recommendation	Response
Mr. Scott Morgan Governor's Office Of Planning and Research, State Clearinghouse 1400 10th Street Sacramento, CA 95812		
1	Comment letter states that the State Clearinghouse submitted the IS/MND to selected state agencies for review. Any comments received by the State Clearinghouse are enclosed with the letter (Central Valley Regional Water Quality Control Board letter [Letter #2] was enclosed). The letter acknowledges that the City has complied with the State Clearinghouse review requirements for draft environmental documents, pursuant to CEQA.	Refer to Response to comments submitted by the Central Valley Regional Water Quality Control Board (Letter #2), above. No further changes to the IS/MND are required.

*Synopsis of comment, for the full comment see copy of comment letter in Part C.

CITY OF WEST SACRAMENTO

August 8, 2018

Erik Vink, Executive Director
Delta Protection Commission
2101 Stone Blvd., Suite 240
West Sacramento, CA 95691

RE: **Mitigated Negative Declaration**
Sycamore Trail Phase II and Phase III Extension Project
SCH#2018062063, Yolo County

Dear Mr. Vink,

The City of West Sacramento (City) has reviewed the Delta Protection Commission's comments on the subject Mitigated Negative Declaration. The Final IS/MND has included a discussion of the Great California Delta Trail in the recreation section and a discussion of how the Project can connect to current and future trails.

The City of West Sacramento would like to thank you for your time in providing comments on this very important trail extension project. Please feel free to call me at (916) 617-4546 should you have any additional questions or concerns.

Respectfully Submitted,

Edgar Medina, P.E.
Senior Civil Engineer
Public Works Department

Public Works Department
1110 West Capitol Avenue
West Sacramento, CA 95691
(916) 617-4850

Letter #1

DELTA PROTECTION COMMISSION
2101 Stone Blvd., Suite 240
West Sacramento, CA 95691
(916) 375-4800 / FAX (916) 376-3962
www.delta.ca.gov

Skip Thomson, Chair
Solano County Board of Supervisors

Oscar Villegas, Vice Chair
Yolo County Board of Supervisors

Don Nottoli
Sacramento County Board of Supervisors

Chuck Winn
San Joaquin County Board of Supervisors

Diane Burgis
Contra Costa County Board of Supervisors

Ronald Kott
Cities of Contra Costa and Solano Counties

Christopher Cabaldon
Cities of Sacramento and Yolo Counties

Susan Lofthus
Cities of San Joaquin County

George Biagi, Jr.
Central Delta Reclamation Districts

Justin van Loben Sels
North Delta Reclamation Districts

Robert Ferguson
South Delta Reclamation Districts

Brian Annis
CA State Transportation Agency

Karen Ross
CA Department of Food and Agriculture

John Laird
CA Natural Resources Agency

Brian Bugsch
CA State Lands Commission

Ex Officio Members

Honorable Susan Talamantes Eggman
California State Assembly

Honorable Cathleen Galgiani
California State Senate

July 26, 2018

Edgar Medina
City of West Sacramento
Department of Public Works
1110 West Capitol Avenue, First Floor
West Sacramento, CA 95692

Subject: Sycamore Trail Phase II & III Extension Project Draft Initial Study/ Mitigated Negative Declaration

Dear Mr. Medina:

Thank you for providing the Delta Protection Commission (Commission) the opportunity to review the Draft Initial Study/Mitigated Negative Declaration (IS/MND) for the Sycamore Trail Phase II & III Extension Project (Project). The Project consists of construction of a pedestrian crossing over US 50 and extension of the existing Sycamore Trail from Joey Lopes Park to the intersection of Park Boulevard and Stone Boulevard.

Although the Project does not fall within the Commission's jurisdiction over "development" in the Primary Zone, we submit these comments under Public Resource Code Sections 5852-5855 (The Great California Delta Trail Act). These sections direct the Commission to develop and adopt a plan and implementation program for a continuous regional recreational corridor extending throughout the five Delta Counties linking the San Francisco Bay Trail system to the Sacramento River trails. The Commission is currently preparing the Great California Delta Trail Blueprint Report for Sacramento, San Joaquin, and Yolo counties.

The Commission adopted the existing Sycamore Trail as a segment of the Great California Delta Trail in January 2016. We hope that we can work with the City to incorporate Phases II and III into the Delta Trail as well. Given that the Sycamore Trail is already part of the Delta Trail, we recommend that the IS/MND discuss the Delta Trail in the recreation and transportation sections and describe how the Project can connect to current and future segments.

The Commission appreciates the City's consideration of these comments. Please contact Blake Roberts, Senior Environmental Planner, at (916) 375-4237 for any questions regarding the comments provided.

Sincerely,

Erik Vink
Executive Director

cc: Christopher Cabaldon, City of West Sacramento Mayor and Commission member

CITY OF WEST SACRAMENTO

August 8, 2018

Ms. Stephanie Tadlock
Central Valley Regional Water Quality Control Board
11020 Sun Center Drive, Ste.200
Rancho Cordova, CA 95670

RE: **Mitigated Negative Declaration**
Sycamore Trail Phase II and Phase III Extension Project
SCH#2018062063, Yolo County

Dear Ms. Tadlock,

The City of West Sacramento (City) has reviewed the Central Valley Flood Protection Board's comments on the subject Mitigated Negative Declaration. The City is currently in the final design phase of this project which includes the development of the construction documents and the preparation of the appropriate jurisdictional and regulatory agency permit applications.

The jurisdictional and regulatory agency permits that will be secured for this project prior to the start of construction include:

- Caltrans Construction Encroachment Permit
- General Permit to Construct – Yolo-Solano Air Quality Management District
- City of West Sacramento Grading Permit
- Construction Storm Water General Permit- State Water Resources Control Board

The City of West Sacramento would like to thank you for your time in providing comments on this very important trail extension project. Please feel free to call me at (916) 617-4546 should you have any additional questions or concerns.

Respectfully Submitted,

Edgar Medina, P.E.
Senior Civil Engineer
Public Works Department

Letter #2

California Water Boards

Edmund G. Brown Jr. Governor

Matthew Rodriquez Secretary for Environmental Protection

Central Valley Regional Water Quality Control Board

19 July 2018

Edgar Medina
City of West Sacramento
Department of Public Works
1110 West Capital Avenue
West Sacramento, CA 95692

CERTIFIED MAIL
91 7199 9991 7039 6992 5932

COMMENTS TO REQUEST FOR REVIEW FOR THE MITIGATED NEGATIVE DECLARATION, SYCAMORE TRAIL PHASE II AND PHASE III EXTENSION PROJECT, SCH# 2018062063, YOLO COUNTY

Pursuant to the State Clearinghouse's 27 June 2018 request, the Central Valley Regional Water Quality Control Board (Central Valley Water Board) has reviewed the *Request for Review for the Mitigated Negative Declaration* for the Sycamore Trail Phase II and III Extension Project, located in Yolo County.

Our agency is delegated with the responsibility of protecting the quality of surface and groundwaters of the state; therefore our comments will address concerns surrounding those issues.

I. Regulatory Setting

Basin Plan

The Central Valley Water Board is required to formulate and adopt Basin Plans for all areas within the Central Valley region under Section 13240 of the Porter-Cologne Water Quality Control Act. Each Basin Plan must contain water quality objectives to ensure the reasonable protection of beneficial uses, as well as a program of implementation for achieving water quality objectives with the Basin Plans. Federal regulations require each state to adopt water quality standards to protect the public health or welfare, enhance the quality of water and serve the purposes of the Clean Water Act. In California, the beneficial uses, water quality objectives, and the Antidegradation Policy are the State's water quality standards. Water quality standards are also contained in the National Toxics Rule, 40 CFR Section 131.36, and the California Toxics Rule, 40 CFR Section 131.38.

The Basin Plan is subject to modification as necessary, considering applicable laws, policies, technologies, water quality conditions and priorities. The original Basin Plans were adopted in 1975, and have been updated and revised periodically as required, using Basin Plan amendments. Once the Central Valley Water Board has adopted a Basin Plan amendment in noticed public hearings, it must be approved by the State Water Resources

Control Board (State Water Board), Office of Administrative Law (OAL) and in some cases, the United States Environmental Protection Agency (USEPA). Basin Plan amendments only become effective after they have been approved by the OAL and in some cases, the USEPA. Every three (3) years, a review of the Basin Plan is completed that assesses the appropriateness of existing standards and evaluates and prioritizes Basin Planning issues.

For more information on the *Water Quality Control Plan for the Sacramento and San Joaquin River Basins*, please visit our website:
http://www.waterboards.ca.gov/centralvalley/water_issues/basin_plans/.

Antidegradation Considerations

All wastewater discharges must comply with the Antidegradation Policy (State Water Board Resolution 68-16) and the Antidegradation Implementation Policy contained in the Basin Plan. The Antidegradation Policy is available on page IV-15.01 at:
http://www.waterboards.ca.gov/centralvalleywater_issues/basin_plans/sacsjr.pdf

In part it states:

> *Any discharge of waste to high quality waters must apply best practicable treatment or control not only to prevent a condition of pollution or nuisance from occurring, but also to maintain the highest water quality possible consistent with the maximum benefit to the people of the State.*
>
> *This information must be presented as an analysis of the impacts and potential impacts of the discharge on water quality, as measured by background concentrations and applicable water quality objectives.*

The antidegradation analysis is a mandatory element in the National Pollutant Discharge Elimination System and land discharge Waste Discharge Requirements (WDRs) permitting processes. The environmental review document should evaluate potential impacts to both surface and groundwater quality.

II. Permitting Requirements

Construction Storm Water General Permit

Dischargers whose project disturb one or more acres of soil or where projects disturb less than one acre but are part of a larger common plan of development that in total disturbs one or more acres, are required to obtain coverage under the General Permit for Storm Water Discharges Associated with Construction Activities (Construction General Permit), Construction General Permit Order No. 2009-009-DWQ. Construction activity subject to this permit includes clearing, grading, grubbing, disturbances to the ground, such as stockpiling, or excavation, but does not include regular maintenance activities performed to restore the original line, grade, or capacity of the facility. The Construction General Permit

requires the development and implementation of a Storm Water Pollution Prevention Plan (SWPPP).

For more information on the Construction General Permit, visit the State Water Resources Control Board website at:
http://www.waterboards.ca.gov/water_issues/programs/stormwater/constpermits.shtml.

Phase I and II Municipal Separate Storm Sewer System (MS4) Permits[1]
The Phase I and II MS4 permits require the Permittees reduce pollutants and runoff flows from new development and redevelopment using Best Management Practices (BMPs) to the maximum extent practicable (MEP). MS4 Permittees have their own development standards, also known as Low Impact Development (LID)/post-construction standards that include a hydromodification component. The MS4 permits also require specific design concepts for LID/post-construction BMPs in the early stages of a project during the entitlement and CEQA process and the development plan review process.

For more information on which Phase I MS4 Permit this project applies to, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/water_issues/storm_water/municipal_permits/.

For more information on the Phase II MS4 permit and who it applies to, visit the State Water Resources Control Board at:
http://www.waterboards.ca.gov/water_issues/programs/stormwater/phase_ii_municipal.shtml

Industrial Storm Water General Permit
Storm water discharges associated with industrial sites must comply with the regulations contained in the Industrial Storm Water General Permit Order No. 2014-0057-DWQ.

For more information on the Industrial Storm Water General Permit, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/water_issues/storm_water/industrial_general_permits/index.shtml.

Clean Water Act Section 404 Permit
If the project will involve the discharge of dredged or fill material in navigable waters or wetlands, a permit pursuant to Section 404 of the Clean Water Act may be needed from the United States Army Corps of Engineers (USACOE). If a Section 404 permit is required by

[1] Municipal Permits = The Phase I Municipal Separate Storm Water System (MS4) Permit covers medium sized Municipalities (serving between 100,000 and 250,000 people) and large sized municipalities (serving over 250,000 people). The Phase II MS4 provides coverage for small municipalities, including non-traditional Small MS4s, which include military bases, public campuses, prisons and hospitals.

the USACOE, the Central Valley Water Board will review the permit application to ensure that discharge will not violate water quality standards. If the project requires surface water drainage realignment, the applicant is advised to contact the Department of Fish and Game for information on Streambed Alteration Permit requirements.

If you have any questions regarding the Clean Water Act Section 404 permits, please contact the Regulatory Division of the Sacramento District of USACOE at (916) 557-5250.

Clean Water Act Section 401 Permit – Water Quality Certification

If an USACOE permit (e.g., Non-Reporting Nationwide Permit, Nationwide Permit, Letter of Permission, Individual Permit, Regional General Permit, Programmatic General Permit), or any other federal permit (e.g., Section 10 of the Rivers and Harbors Act or Section 9 from the United States Coast Guard), is required for this project due to the disturbance of waters of the United States (such as streams and wetlands), then a Water Quality Certification must be obtained from the Central Valley Water Board prior to initiation of project activities. There are no waivers for 401 Water Quality Certifications.

Waste Discharge Requirements – Discharges to Waters of the State

If USACOE determines that only non-jurisdictional waters of the State (i.e., "non-federal" waters of the State) are present in the proposed project area, the proposed project may require a Waste Discharge Requirement (WDR) permit to be issued by Central Valley Water Board. Under the California Porter-Cologne Water Quality Control Act, discharges to all waters of the State, including all wetlands and other waters of the State including, but not limited to, isolated wetlands, are subject to State regulation.

For more information on the Water Quality Certification and WDR processes, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/help/business_help/permit2.shtml.

Dewatering Permit

If the proposed project includes construction or groundwater dewatering to be discharged to land, the proponent may apply for coverage under State Water Board General Water Quality Order (Low Risk General Order) 2003-0003 or the Central Valley Water Board's Waiver of Report of Waste Discharge and Waste Discharge Requirements (Low Risk Waiver)
R5-2013-0145. Small temporary construction dewatering projects are projects that discharge groundwater to land from excavation activities or dewatering of underground utility vaults. Dischargers seeking coverage under the General Order or Waiver must file a Notice of Intent with the Central Valley Water Board prior to beginning discharge.

For more information regarding the Low Risk General Order and the application process, visit the Central Valley Water Board website at:

http://www.waterboards.ca.gov/board_decisions/adopted_orders/water_quality/2003/wqo/wqo2003-0003.pdf

For more information regarding the Low Risk Waiver and the application process, visit the Central Valley Water Board website at:

http://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/waivers/r5-2013-0145_res.pdf

Regulatory Compliance for Commercially Irrigated Agriculture

If the property will be used for commercial irrigated agricultural, the discharger will be required to obtain regulatory coverage under the Irrigated Lands Regulatory Program. There are two options to comply:

1. **Obtain Coverage Under a Coalition Group.** Join the local Coalition Group that supports land owners with the implementation of the Irrigated Lands Regulatory Program. The Coalition Group conducts water quality monitoring and reporting to the Central Valley Water Board on behalf of its growers. The Coalition Groups charge an annual membership fee, which varies by Coalition Group. To find the Coalition Group in your area, visit the Central Valley Water Board's website at: http://www.waterboards.ca.gov/centralvalley/water_issues/irrigated_lands/for_growers/apply_coalition_group/index.shtml or contact water board staff at (916) 464-4611 or via email at IrrLands@waterboards.ca.gov.

2. **Obtain Coverage Under the General Waste Discharge Requirements for Individual Growers, General Order R5-2013-0100.** Dischargers not participating in a third-party group (Coalition) are regulated individually. Depending on the specific site conditions, growers may be required to monitor runoff from their property, install monitoring wells, and submit a notice of intent, farm plan, and other action plans regarding their actions to comply with their General Order. Yearly costs would include State administrative fees (for example, annual fees for farm sizes from 10-100 acres are currently $1,084 + $6.70/Acre); the cost to prepare annual monitoring reports; and water quality monitoring costs. To enroll as an Individual Discharger under the Irrigated Lands Regulatory Program, call the Central Valley Water Board phone line at (916) 464-4611 or e-mail board staff at IrrLands@waterboards.ca.gov.

Low or Limited Threat General NPDES Permit

If the proposed project includes construction dewatering and it is necessary to discharge the groundwater to waters of the United States, the proposed project will require coverage under a National Pollutant Discharge Elimination System (NPDES) permit. Dewatering discharges are typically considered a low or limited threat to water quality and may be covered under the General Order for *Dewatering and Other Low Threat Discharges to Surface Waters* (Low Threat General Order) or the General Order for *Limited Threat Discharges of Treated/Untreated Groundwater from Cleanup Sites, Wastewater from*

Superchlorination Projects, and Other Limited Threat Wastewaters to Surface Water (Limited Threat General Order). A complete application must be submitted to the Central Valley Water Board to obtain coverage under these General NPDES permits.

For more information regarding the Low Threat General Order and the application process, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/general_orders/r5-2013-0074.pdf

For more information regarding the Limited Threat General Order and the application process, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/general_orders/r5-2013-0073.pdf

NPDES Permit

If the proposed project discharges waste that could affect the quality of surface waters of the State, other than into a community sewer system, the proposed project will require coverage under a National Pollutant Discharge Elimination System (NPDES) permit. A complete Report of Waste Discharge must be submitted with the Central Valley Water Board to obtain a NPDES Permit.

For more information regarding the NPDES Permit and the application process, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/help/business_help/permit3.shtml

If you have questions regarding these comments, please contact me at (916) 464-4644 or Stephanie.Tadlock@waterboards.ca.gov.

Stephanie Tadlock

Stephanie Tadlock
Senior Environmental Scientist

cc: State Clearinghouse unit, Governor's Office of Planning and Research, Sacramento

Letter #3

STATE OF CALIFORNIA

GOVERNOR'S OFFICE *of* PLANNING AND RESEARCH

EDMUND G. BROWN JR.
GOVERNOR

KEN ALEX
DIRECTOR

July 27, 2018

Edgar Medina
City of West Sacramento
1110 W. Capitol Avenue, 2nd Floor
West Sacramento, CA 95692

Subject: Sycamore Trail Phase II and III Extension Project
SCH#: 2018062063

Dear Edgar Medina:

The State Clearinghouse submitted the above named Mitigated Negative Declaration to selected state agencies for review. On the enclosed Document Details Report please note that the Clearinghouse has listed the state agencies that reviewed your document. The review period closed on July 26, 2018, and the comments from the responding agency (ies) is (are) enclosed. If this comment package is not in order, please notify the State Clearinghouse immediately. Please refer to the project's ten-digit State Clearinghouse number in future correspondence so that we may respond promptly.

Please note that Section 21104(c) of the California Public Resources Code states that:

> "A responsible or other public agency shall only make substantive comments regarding those activities involved in a project which are within an area of expertise of the agency or which are required to be carried out or approved by the agency. Those comments shall be supported by specific documentation."

These comments are forwarded for use in preparing your final environmental document. Should you need more information or clarification of the enclosed comments, we recommend that you contact the commenting agency directly.

This letter acknowledges that you have complied with the State Clearinghouse review requirements for draft environmental documents, pursuant to the California Environmental Quality Act. Please contact the State Clearinghouse at (916) 445-0613 if you have any questions regarding the environmental review process.

Sincerely,

Scott Morgan
Director, State Clearinghouse

Enclosures
cc: Resources Agency

1400 10th Street P.O. Box 3044 Sacramento, California 95812-3044
1-916-322-2318 FAX 1-916-558-3184 www.opr.ca.gov

Document Details Report
State Clearinghouse Data Base

SCH#	2018062063
Project Title	Sycamore Trail Phase II and III Extension Project
Lead Agency	West Sacramento, City of

Type	**MND** Mitigated Negative Declaration
Description	The city proposes to extend the Sycamore Trail from Evergreen Ave north of US Route 50, connecting Joey Lopes Park to the north with Westmore Oaks ES to the south including a new pedestrian and bicycle overcrossing over US 50. The project proposes to increase the number of people in the city who bicycle or walk to work, to school, for errands, or for recreation. The city proposes to extend the Sycamore Trail from Evergreen Ave north of US 50 to the intersection of Park and Stone Blvds near the Deep Water Shipping Channel. The project would generally follow the Sacramento Regional County Sanitation District Northwest Interceptor Alignment.

Lead Agency Contact

Name	Edgar Medina		
Agency	City of West Sacramento		
Phone	(916) 617-4546	*Fax*	
email			
Address	1110 W. Capitol Avenue, 2nd Floor		
City	West Sacramento	*State* CA	*Zip* 95692

Project Location

County	Yolo				
City	West Sacramento				
Region					
Lat / Long	38° 34' 30" N / 121° 32' 15" W				
Cross Streets	Park Blvd/Stone Blvd				
Parcel No.					
Township	8,9N	*Range* 4E	*Section*	*Base*	MD

Proximity to:

Highways	50
Airports	
Railways	
Waterways	Deep Water Shipping Channel
Schools	Westmore Oaks ES
Land Use	public OS, Light residential, high residential, light industrial

Project Issues	Aesthetic/Visual; Archaeologic-Historic; Biological Resources; Noise; Recreation/Parks; Toxic/Hazardous; Traffic/Circulation
Reviewing Agencies	Resources Agency; Department of Fish and Wildlife, Region 2; Department of Parks and Recreation; Caltrans, District 3 N; Regional Water Quality Control Bd., Region 5 (Sacramento); Air Resources Board, Transportation Projects; Native American Heritage Commission; Delta Protection Commission

Date Received	06/27/2018	*Start of Review*	06/27/2018	*End of Review*	07/26/2018

Note: Blanks in data fields result from insufficient information provided by lead agency.

CALIFORNIA
Water Boards

EDMUND G. BROWN JR.
GOVERNOR

MATTHEW RODRIQUEZ
SECRETARY FOR
ENVIRONMENTAL PROTECTION

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7/26/18
E

Central Valley Regional Water Quality Control Board

19 July 2018

Governor's Office of Planning & Research
JUL 20 2018
STATE CLEARINGHOUSE

Edgar Medina
City of West Sacramento
Department of Public Works
1110 West Capital Avenue
West Sacramento, CA 95692

CERTIFIED MAIL
91 7199 9991 7039 6992 5932

COMMENTS TO REQUEST FOR REVIEW FOR THE MITIGATED NEGATIVE DECLARATION, SYCAMORE TRAIL PHASE II AND PHASE III EXTENSION PROJECT, SCH# 2018062063, YOLO COUNTY

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In part it states:

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Industrial Storm Water General Permit

Storm water discharges associated with industrial sites must comply with the regulations contained in the Industrial Storm Water General Permit Order No. 2014-0057-DWQ.

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Clean Water Act Section 404 Permit

If the project will involve the discharge of dredged or fill material in navigable waters or wetlands, a permit pursuant to Section 404 of the Clean Water Act may be needed from the United States Army Corps of Engineers (USACOE). If a Section 404 permit is required by

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For more information on the Water Quality Certification and WDR processes, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/help/business_help/permit2.shtml.

Dewatering Permit

If the proposed project includes construction or groundwater dewatering to be discharged to land, the proponent may apply for coverage under State Water Board General Water Quality Order (Low Risk General Order) 2003-0003 or the Central Valley Water Board's Waiver of Report of Waste Discharge and Waste Discharge Requirements (Low Risk Waiver)
R5-2013-0145. Small temporary construction dewatering projects are projects that discharge groundwater to land from excavation activities or dewatering of underground utility vaults. Dischargers seeking coverage under the General Order or Waiver must file a Notice of Intent with the Central Valley Water Board prior to beginning discharge.

For more information regarding the Low Risk General Order and the application process, visit the Central Valley Water Board website at:

http://www.waterboards.ca.gov/board_decisions/adopted_orders/water_quality/2003/wqo/wqo2003-0003.pdf

For more information regarding the Low Risk Waiver and the application process, visit the Central Valley Water Board website at:

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Regulatory Compliance for Commercially Irrigated Agriculture

If the property will be used for commercial irrigated agricultural, the discharger will be required to obtain regulatory coverage under the Irrigated Lands Regulatory Program. There are two options to comply:

1. **Obtain Coverage Under a Coalition Group.** Join the local Coalition Group that supports land owners with the implementation of the Irrigated Lands Regulatory Program. The Coalition Group conducts water quality monitoring and reporting to the Central Valley Water Board on behalf of its growers. The Coalition Groups charge an annual membership fee, which varies by Coalition Group. To find the Coalition Group in your area, visit the Central Valley Water Board's website at: http://www.waterboards.ca.gov/centralvalley/water_issues/irrigated_lands/for_growers/apply_coalition_group/index.shtml or contact water board staff at (916) 464-4611 or via email at IrrLands@waterboards.ca.gov.

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Superchlorination Projects, and Other Limited Threat Wastewaters to Surface Water (Limited Threat General Order). A complete application must be submitted to the Central Valley Water Board to obtain coverage under these General NPDES permits.

For more information regarding the Low Threat General Order and the application process, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/general_orders/r5-2013-0074.pdf

For more information regarding the Limited Threat General Order and the application process, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/general_orders/r5-2013-0073.pdf

NPDES Permit

If the proposed project discharges waste that could affect the quality of surface waters of the State, other than into a community sewer system, the proposed project will require coverage under a National Pollutant Discharge Elimination System (NPDES) permit. A complete Report of Waste Discharge must be submitted with the Central Valley Water Board to obtain a NPDES Permit.

For more information regarding the NPDES Permit and the application process, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/help/business_help/permit3.shtml

If you have questions regarding these comments, please contact me at (916) 464-4644 or Stephanie.Tadlock@waterboards.ca.gov.

Stephanie Tadlock

Stephanie Tadlock
Senior Environmental Scientist

cc: State Clearinghouse unit, Governor's Office of Planning and Research, Sacramento

STATE OF CALIFORNIA

GOVERNOR'S OFFICE *of* PLANNING AND RESEARCH

EDMUND G. BROWN JR.
GOVERNOR

KEN ALEX
DIRECTOR

July 27, 2018

Edgar Medina
City of West Sacramento
1110 W. Capitol Avenue, 2nd Floor
West Sacramento, CA 95692

Subject: Sycamore Trail Phase II and III Extension Project
SCH#: 2018062063

Dear Edgar Medina:

The State Clearinghouse submitted the above named Mitigated Negative Declaration to selected state agencies for review. On the enclosed Document Details Report please note that the Clearinghouse has listed the state agencies that reviewed your document. The review period closed on July 26, 2018, and the comments from the responding agency (ies) is (are) enclosed. If this comment package is not in order, please notify the State Clearinghouse immediately. Please refer to the project's ten-digit State Clearinghouse number in future correspondence so that we may respond promptly.

Please note that Section 21104(c) of the California Public Resources Code states that:

> "A responsible or other public agency shall only make substantive comments regarding those activities involved in a project which are within an area of expertise of the agency or which are required to be carried out or approved by the agency. Those comments shall be supported by specific documentation."

These comments are forwarded for use in preparing your final environmental document. Should you need more information or clarification of the enclosed comments, we recommend that you contact the commenting agency directly.

This letter acknowledges that you have complied with the State Clearinghouse review requirements for draft environmental documents, pursuant to the California Environmental Quality Act. Please contact the State Clearinghouse at (916) 445-0613 if you have any questions regarding the environmental review process.

Sincerely,

Scott Morgan
Director, State Clearinghouse

Enclosures
cc: Resources Agency

1400 10th Street P.O. Box 3044 Sacramento, California 95812-3044
1-916-322-2318 FAX 1-916-558-3184 www.opr.ca.gov

Document Details Report
State Clearinghouse Data Base

SCH#	2018062063
Project Title	Sycamore Trail Phase II and III Extension Project
Lead Agency	West Sacramento, City of

Type	**MND** Mitigated Negative Declaration
Description	The city proposes to extend the Sycamore Trail from Evergreen Ave north of US Route 50, connecting Joey Lopes Park to the north with Westmore Oaks ES to the south including a new pedestrian and bicycle overcrossing over US 50. The project proposes to increase the number of people in the city who bicycle or walk to work, to school, for errands, or for recreation. The city proposes to extend the Sycamore Trail from Evergreen Ave north of US 50 to the intersection of Park and Stone Blvds near the Deep Water Shipping Channel. The project would generally follow the Sacramento Regional County Sanitation District Northwest Interceptor Alignment.

Lead Agency Contact

Name	Edgar Medina		
Agency	City of West Sacramento		
Phone	(916) 617-4546	*Fax*	
email			
Address	1110 W. Capitol Avenue, 2nd Floor		
City	West Sacramento	*State* CA	*Zip* 95692

Project Location

County	Yolo				
City	West Sacramento				
Region					
Lat / Long	38° 34' 30" N / 121° 32' 15" W				
Cross Streets	Park Blvd/Stone Blvd				
Parcel No.					
Township	8,9N	*Range* 4E	*Section*	*Base*	MD

Proximity to:

Highways	50
Airports	
Railways	
Waterways	Deep Water Shipping Channel
Schools	Westmore Oaks ES
Land Use	public OS, Light residential, high residential, light industrial

Project Issues	Aesthetic/Visual; Archaeologic-Historic; Biological Resources; Noise; Recreation/Parks; Toxic/Hazardous; Traffic/Circulation
Reviewing Agencies	Resources Agency; Department of Fish and Wildlife, Region 2; Department of Parks and Recreation; Caltrans, District 3 N; Regional Water Quality Control Bd., Region 5 (Sacramento); Air Resources Board, Transportation Projects; Native American Heritage Commission; Delta Protection Commission

Date Received	06/27/2018	*Start of Review*	06/27/2018	*End of Review*	07/26/2018

Note: Blanks in data fields result from insufficient information provided by lead agency.

CALIFORNIA
Water Boards

EDMUND G. BROWN JR.
GOVERNOR

MATTHEW RODRIQUEZ
SECRETARY FOR
ENVIRONMENTAL PROTECTION

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7/26/18
E

Central Valley Regional Water Quality Control Board

19 July 2018

Governor's Office of Planning & Research
JUL 20 2018
STATE CLEARINGHOUSE

Edgar Medina
City of West Sacramento
Department of Public Works
1110 West Capital Avenue
West Sacramento, CA 95692

CERTIFIED MAIL
91 7199 9991 7039 6992 5932

COMMENTS TO REQUEST FOR REVIEW FOR THE MITIGATED NEGATIVE DECLARATION, SYCAMORE TRAIL PHASE II AND PHASE III EXTENSION PROJECT, SCH# 2018062063, YOLO COUNTY

Pursuant to the State Clearinghouse's 27 June 2018 request, the Central Valley Regional Water Quality Control Board (Central Valley Water Board) has reviewed the *Request for Review for the Mitigated Negative Declaration* for the Sycamore Trail Phase II and III Extension Project, located in Yolo County.

Our agency is delegated with the responsibility of protecting the quality of surface and groundwaters of the state; therefore our comments will address concerns surrounding those issues.

I. Regulatory Setting

Basin Plan

The Central Valley Water Board is required to formulate and adopt Basin Plans for all areas within the Central Valley region under Section 13240 of the Porter-Cologne Water Quality Control Act. Each Basin Plan must contain water quality objectives to ensure the reasonable protection of beneficial uses, as well as a program of implementation for achieving water quality objectives with the Basin Plans. Federal regulations require each state to adopt water quality standards to protect the public health or welfare, enhance the quality of water and serve the purposes of the Clean Water Act. In California, the beneficial uses, water quality objectives, and the Antidegradation Policy are the State's water quality standards. Water quality standards are also contained in the National Toxics Rule, 40 CFR Section 131.36, and the California Toxics Rule, 40 CFR Section 131.38.

The Basin Plan is subject to modification as necessary, considering applicable laws, policies, technologies, water quality conditions and priorities. The original Basin Plans were adopted in 1975, and have been updated and revised periodically as required, using Basin Plan amendments. Once the Central Valley Water Board has adopted a Basin Plan amendment in noticed public hearings, it must be approved by the State Water Resources

Control Board (State Water Board), Office of Administrative Law (OAL) and in some cases, the United States Environmental Protection Agency (USEPA). Basin Plan amendments only become effective after they have been approved by the OAL and in some cases, the USEPA. Every three (3) years, a review of the Basin Plan is completed that assesses the appropriateness of existing standards and evaluates and prioritizes Basin Planning issues.

For more information on the *Water Quality Control Plan for the Sacramento and San Joaquin River Basins*, please visit our website:
http://www.waterboards.ca.gov/centralvalley/water_issues/basin_plans/.

Antidegradation Considerations

All wastewater discharges must comply with the Antidegradation Policy (State Water Board Resolution 68-16) and the Antidegradation Implementation Policy contained in the Basin Plan. The Antidegradation Policy is available on page IV-15.01 at:
http://www.waterboards.ca.gov/centralvalleywater_issues/basin_plans/sacsjr.pdf

In part it states:

> *Any discharge of waste to high quality waters must apply best practicable treatment or control not only to prevent a condition of pollution or nuisance from occurring, but also to maintain the highest water quality possible consistent with the maximum benefit to the people of the State.*
>
> *This information must be presented as an analysis of the impacts and potential impacts of the discharge on water quality, as measured by background concentrations and applicable water quality objectives.*

The antidegradation analysis is a mandatory element in the National Pollutant Discharge Elimination System and land discharge Waste Discharge Requirements (WDRs) permitting processes. The environmental review document should evaluate potential impacts to both surface and groundwater quality.

II. Permitting Requirements

Construction Storm Water General Permit

Dischargers whose project disturb one or more acres of soil or where projects disturb less than one acre but are part of a larger common plan of development that in total disturbs one or more acres, are required to obtain coverage under the General Permit for Storm Water Discharges Associated with Construction Activities (Construction General Permit), Construction General Permit Order No. 2009-009-DWQ. Construction activity subject to this permit includes clearing, grading, grubbing, disturbances to the ground, such as stockpiling, or excavation, but does not include regular maintenance activities performed to restore the original line, grade, or capacity of the facility. The Construction General Permit

requires the development and implementation of a Storm Water Pollution Prevention Plan (SWPPP).

For more information on the Construction General Permit, visit the State Water Resources Control Board website at:
http://www.waterboards.ca.gov/water_issues/programs/stormwater/constpermits.shtml.

Phase I and II Municipal Separate Storm Sewer System (MS4) Permits[1]

The Phase I and II MS4 permits require the Permittees reduce pollutants and runoff flows from new development and redevelopment using Best Management Practices (BMPs) to the maximum extent practicable (MEP). MS4 Permittees have their own development standards, also known as Low Impact Development (LID)/post-construction standards that include a hydromodification component. The MS4 permits also require specific design concepts for LID/post-construction BMPs in the early stages of a project during the entitlement and CEQA process and the development plan review process.

For more information on which Phase I MS4 Permit this project applies to, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/water_issues/storm_water/municipal_permits/.

For more information on the Phase II MS4 permit and who it applies to, visit the State Water Resources Control Board at:
http://www.waterboards.ca.gov/water_issues/programs/stormwater/phase_ii_municipal.shtml

Industrial Storm Water General Permit

Storm water discharges associated with industrial sites must comply with the regulations contained in the Industrial Storm Water General Permit Order No. 2014-0057-DWQ.

For more information on the Industrial Storm Water General Permit, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/water_issues/storm_water/industrial_general_permits/index.shtml.

Clean Water Act Section 404 Permit

If the project will involve the discharge of dredged or fill material in navigable waters or wetlands, a permit pursuant to Section 404 of the Clean Water Act may be needed from the United States Army Corps of Engineers (USACOE). If a Section 404 permit is required by

[1] Municipal Permits = The Phase I Municipal Separate Storm Water System (MS4) Permit covers medium sized Municipalities (serving between 100,000 and 250,000 people) and large sized municipalities (serving over 250,000 people). The Phase II MS4 provides coverage for small municipalities, including non-traditional Small MS4s, which include military bases, public campuses, prisons and hospitals.

the USACOE, the Central Valley Water Board will review the permit application to ensure that discharge will not violate water quality standards. If the project requires surface water drainage realignment, the applicant is advised to contact the Department of Fish and Game for information on Streambed Alteration Permit requirements.

If you have any questions regarding the Clean Water Act Section 404 permits, please contact the Regulatory Division of the Sacramento District of USACOE at (916) 557-5250.

Clean Water Act Section 401 Permit – Water Quality Certification

If an USACOE permit (e.g., Non-Reporting Nationwide Permit, Nationwide Permit, Letter of Permission, Individual Permit, Regional General Permit, Programmatic General Permit), or any other federal permit (e.g., Section 10 of the Rivers and Harbors Act or Section 9 from the United States Coast Guard), is required for this project due to the disturbance of waters of the United States (such as streams and wetlands), then a Water Quality Certification must be obtained from the Central Valley Water Board prior to initiation of project activities. There are no waivers for 401 Water Quality Certifications.

Waste Discharge Requirements – Discharges to Waters of the State

If USACOE determines that only non-jurisdictional waters of the State (i.e., "non-federal" waters of the State) are present in the proposed project area, the proposed project may require a Waste Discharge Requirement (WDR) permit to be issued by Central Valley Water Board. Under the California Porter-Cologne Water Quality Control Act, discharges to all waters of the State, including all wetlands and other waters of the State including, but not limited to, isolated wetlands, are subject to State regulation.

For more information on the Water Quality Certification and WDR processes, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/help/business_help/permit2.shtml.

Dewatering Permit

If the proposed project includes construction or groundwater dewatering to be discharged to land, the proponent may apply for coverage under State Water Board General Water Quality Order (Low Risk General Order) 2003-0003 or the Central Valley Water Board's Waiver of Report of Waste Discharge and Waste Discharge Requirements (Low Risk Waiver)
R5-2013-0145. Small temporary construction dewatering projects are projects that discharge groundwater to land from excavation activities or dewatering of underground utility vaults. Dischargers seeking coverage under the General Order or Waiver must file a Notice of Intent with the Central Valley Water Board prior to beginning discharge.

For more information regarding the Low Risk General Order and the application process, visit the Central Valley Water Board website at:

http://www.waterboards.ca.gov/board_decisions/adopted_orders/water_quality/2003/wqo/wqo2003-0003.pdf

For more information regarding the Low Risk Waiver and the application process, visit the Central Valley Water Board website at:

http://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/waivers/r5-2013-0145_res.pdf

Regulatory Compliance for Commercially Irrigated Agriculture

If the property will be used for commercial irrigated agricultural, the discharger will be required to obtain regulatory coverage under the Irrigated Lands Regulatory Program. There are two options to comply:

1. **Obtain Coverage Under a Coalition Group.** Join the local Coalition Group that supports land owners with the implementation of the Irrigated Lands Regulatory Program. The Coalition Group conducts water quality monitoring and reporting to the Central Valley Water Board on behalf of its growers. The Coalition Groups charge an annual membership fee, which varies by Coalition Group. To find the Coalition Group in your area, visit the Central Valley Water Board's website at: http://www.waterboards.ca.gov/centralvalley/water_issues/irrigated_lands/for_growers/apply_coalition_group/index.shtml or contact water board staff at (916) 464-4611 or via email at IrrLands@waterboards.ca.gov.

2. **Obtain Coverage Under the General Waste Discharge Requirements for Individual Growers, General Order R5-2013-0100**. Dischargers not participating in a third-party group (Coalition) are regulated individually. Depending on the specific site conditions, growers may be required to monitor runoff from their property, install monitoring wells, and submit a notice of intent, farm plan, and other action plans regarding their actions to comply with their General Order. Yearly costs would include State administrative fees (for example, annual fees for farm sizes from 10-100 acres are currently $1,084 + $6.70/Acre); the cost to prepare annual monitoring reports; and water quality monitoring costs. To enroll as an Individual Discharger under the Irrigated Lands Regulatory Program, call the Central Valley Water Board phone line at (916) 464-4611 or e-mail board staff at IrrLands@waterboards.ca.gov.

Low or Limited Threat General NPDES Permit

If the proposed project includes construction dewatering and it is necessary to discharge the groundwater to waters of the United States, the proposed project will require coverage under a National Pollutant Discharge Elimination System (NPDES) permit. Dewatering discharges are typically considered a low or limited threat to water quality and may be covered under the General Order for *Dewatering and Other Low Threat Discharges to Surface Waters* (Low Threat General Order) or the General Order for *Limited Threat Discharges of Treated/Untreated Groundwater from Cleanup Sites, Wastewater from*

Superchlorination Projects, and Other Limited Threat Wastewaters to Surface Water (Limited Threat General Order). A complete application must be submitted to the Central Valley Water Board to obtain coverage under these General NPDES permits.

For more information regarding the Low Threat General Order and the application process, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/general_orders/r5-2013-0074.pdf

For more information regarding the Limited Threat General Order and the application process, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/general_orders/r5-2013-0073.pdf

NPDES Permit

If the proposed project discharges waste that could affect the quality of surface waters of the State, other than into a community sewer system, the proposed project will require coverage under a National Pollutant Discharge Elimination System (NPDES) permit. A complete Report of Waste Discharge must be submitted with the Central Valley Water Board to obtain a NPDES Permit.

For more information regarding the NPDES Permit and the application process, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/help/business_help/permit3.shtml

If you have questions regarding these comments, please contact me at (916) 464-4644 or Stephanie.Tadlock@waterboards.ca.gov.

Stephanie Tadlock

Stephanie Tadlock
Senior Environmental Scientist

cc: State Clearinghouse unit, Governor's Office of Planning and Research, Sacramento

SYCAMORE TRAIL PHASE II & III EXTENSION PROJECT

MITIGATION MONITORING AND REPORTING PROGRAM

Yolo County, California

Submitted To:

City of West Sacramento

Department of Public Works

1110 West Capitol Ave.

West Sacramento, CA 95691

Submitted by:

Drake Haglan and Associates, Inc.

11060 White Rock Road, Suite 200

Rancho Cordova, CA 95670

916.363.4210

August 2018

SYCAMORE TRAIL PHASE II & III EXTENSION PROJECT
MITIGATION MONITORING AND REPORTING PROGRAM

1.1 INTRODUCTION

This document is the Mitigation Monitoring and Reporting Program (MMRP) for the Sycamore Trails Phase II & III Project. It has been prepared pursuant to Section 2108.6 of the California Public Resources Code, which requires public agencies to adopt a reporting and monitoring program for the changes made to the project or conditions of project approval, adopted to mitigate or avoid significant effects on the environment. This MMRP is required for the proposed project because the Initial Study/Mitigated Negative Declaration (IS/MND) has identified potentially significant adverse impacts, and measures have been identified to mitigate those impacts. Adoption of the MMRP would occur along with approval of the proposed project.

1.2 PURPOSE AND IMPLEMENTATION OF THE MITIGATION MONITORING AND REPORTING PROGRAM

This MMRP has been prepared to ensure that all required mitigation measures are implemented and completed according to schedule and maintained in a satisfactory manner during project implementation phases. The MMRP may be modified by City of West Sacramento (City) during project implementation, as necessary, in response to changing conditions or other refinements. The City would be responsible for the overall administration of the MMRP and for verifying that City staff members and/or contractors have completed the necessary actions for each measure. The City, at its discretion, may delegate implementation responsibility or portions thereof to contractors or other designated agent. The City will designate a project manager to oversee the implementation of the MMRP. Duties of the project manager include the following:

- Serving as the liaison between the City and the contractors regarding mitigation monitoring issues.
- Completing forms and maintaining any records and documents generated by the MMRP.
- Coordinating and ensuring that corrective actions or enforcement measures are taken, if necessary.

IS/MND or PERMIT MITIGATION NUMBER	MITIGATION MEASURE	TIMING	IMPLEMENTING PARTY	MONITORING PARTY	FREQUENCY AND DURATION OF MONITORING	PERFORMANCE CRITERIA	SPECIFICATION NOTES
AESTHETICS							
AES-1	**Implement measures to avoid and minimize potential adverse effects on the visual character of the site and vicinity.** • Incorporate designs, such as architectural form liners, concrete staining on the exterior girders and bridge railing, and aesthetic treatment on overcrossing fencing to maintain aesthetic consistency with the surrounding residential and industrial community. • Place buffer vegetation plantings directly adjacent to the trail segment fencing on the trail side to minimize the potential visual prominence and imbalance created by fencing. • Choose a fencing design option for the southern trail segment that is aesthetically consistent with the surrounding residential community and minimizes the potential for pedestrians and cyclists to feel too enclosed, claustrophobic, or generally unsafe.	Prior to and During Construction	The City	Construction Contractor	Prior and During construction	Comply with condition objective	
AES-2	**Light and Glare**. Choose and develop pedestrian lighting in coordination with community involvement that lessens the potential effects of sunrise, sunset, daytime, and nighttime glare on US 50 roadway users.	Prior to and during construction	The City	Construction Contractor	Prior and during construction	Comply with condition objective	
BIOLOGICAL RESOURCES							
BIO-1	**Conduct a Preconstruction Nesting Migratory Bird and Raptor Survey and Establish No-disturbance Buffers, if Necessary**. • Conduct preconstruction tree surveys of the trees to be removed, and within 250 feet of the Project construction area. Survey work will be done no more than two days prior to initiation of construction to minimize potential that nests are initiated after the survey and prior to the start of construction. • If any occupied nests are detected the tree will be flagged, a minimum buffer of 100 feet between the nest	February 1 – July 31	Qualified Biologist	The City	Prior to construction	Comply with condition objective	

IS/MND or PERMIT MITIGATION NUMBER	MITIGATION MEASURE	TIMING	IMPLEMENTING PARTY	MONITORING PARTY	FREQUENCY AND DURATION OF MONITORING	PERFORMANCE CRITERIA	SPECIFICATION NOTES
	and construction zone will be established, unless determined otherwise by CDFW, and that area will be avoided until the qualified biologist has determined the nest is no longer active/occupied. Once the biologist has determined that young have fledged and the nest is no longer active, construction can resume in that area. • If no active nests are identified during the preconstruction survey, no further mitigation is necessary. If construction activities (i.e. vegetation and tree removal) are scheduled to begin during the nonbreeding season (September to January), preconstruction surveys would not be necessary.						
BIO-2	**Replace and Protect Trees**. Implement the following measures prior to and during trail and overcrossing construction to mitigate potential impacts to the West Sacramento Tree Preservation Ordinance. • Prior to removal of any trees, an International Society of Arboriculture-certified arborist shall conduct a tree survey in areas that may be impacted by construction activities. This survey shall document tree resources that may be adversely impacted by Project implementation. The survey will follow standard professional practices. • Existing trees will be retained to extent feasible. A Tree Protection Zone (TPZ) shall be established around any tree or group of trees to be retained. The TPZ will be delineated by an International Society of Arboriculture-certified arborist. The TPZ shall be defined by the radius of the dripline of the tree(s) plus one foot. The TPZ of any protected trees shall be	Prior to and during construction	Certified Arborist	The City	Prior to and during construction	Comply with condition objective	

IS/MND or PERMIT MITIGATION NUMBER	MITIGATION MEASURE	TIMING	IMPLEMENTING PARTY	MONITORING PARTY	FREQUENCY AND DURATION OF MONITORING	PERFORMANCE CRITERIA	SPECIFICATION NOTES
	demarcated using fencing that will remain in place for the duration of construction activities. • Construction-related activities shall be limited within the TPZ to those activities that can be done by hand. No heavy equipment or machinery shall be operated within the TPZ; if this is not possible, a six inch layer of bark mulch shall be placed where the equipment shall be under the dripline in order to protect the root system from too much compaction. Grading shall be prohibited within the TPZ. No construction materials, equipment, or heavy machinery shall be stored within the TPZ. • Prior to removal, all trees will be posted for public notice and a permit will be obtained (pursuant to City Code 8.24.040 and 8.24.060). The City will mitigate tree removal by replacing removed trees at a 1:1 ratio. All replacement trees will be oak or other native tree species for the area and would be planted with automatic irrigation to improve tree survivability.						
CULTURAL RESOURCES							
CUL-1	**Discovery of Cultural Resources during Ground-Disturbing Activities.** If buried cultural materials are encountered during construction, stop work in that area until a qualified archaeologist can evaluate the find's nature and significance. In the event that human remains or associated funerary objects are encountered during construction, cease all work within the vicinity of the discovery. In accordance with CEQA and the California Health and Human Safety Code (14 CCR § 15064; 7 HSC § 7050.5), the County coroner will be contacted immediately. If the human remains are determined to be Native American, the coroner will notify the Native American Heritage Commission, who will notify and appoint a Most Likely Descendent (MLD). The MLD will work with a qualified	Prior to Construction, during Construction	Archaeologist / Construction Contractor	The City	Prior to and during construction	Comply with condition objective	

IS/MND or PERMIT MITIGATION NUMBER	MITIGATION MEASURE	TIMING	IMPLEMENTING PARTY	MONITORING PARTY	FREQUENCY AND DURATION OF MONITORING	PERFORMANCE CRITERIA	SPECIFICATION NOTES
	archaeologist to decide the proper treatment of the human remains and any associated funerary objects.						
HAZARDS AND HAZARDOUS MATERIALS							
HAZ-1	**Develop a Health and Safety Plan (HASP)** Develop a HASP for the Project. The HASP shall describe appropriate procedures to follow in the event that any contaminated soil or groundwater is encountered during construction activities. Any unknown substances shall be tested, handled and disposed of in accordance with appropriate federal, state and local regulations.	Prior to Construction	Construction Contractor	The City	Prior to construction	Comply with federal, state and local regulations	
HAZ-2	**Lead Containing Material**. A California-licensed abatement contractor will conduct a survey for lead containing materials prior to demolition (including concrete elements) and contractor will submit a National Emission Standard for Hazardous Air Pollutants (NESHAP) notification. Per the Asbestos NESHAP regulation, all "demolition activity" requires written notification even if there is no asbestos present. This notification shall be typewritten and postmarked or delivered no later than ten days prior to the beginning of the asbestos demolition or removal activity. If lead-containing materials are found, the following will be required: • Building materials associated with paint on structures, and paint on utilities should be abated by a California-licensed abatement contractor and disposed of as a hazardous waste in compliance with 2015 Caltrans Standard Specification 14-11.12, 2015 Caltrans Standard Specification 14-11.13, and other federal and state regulations for hazardous waste. • A Lead Compliance Plan should be prepared by the contractor for the disposal of lead-based paint. The	During PS&E	City Consultant	The City	Prior to construction	Comply with condition objectives	

IS/MND or PERMIT MITIGATION NUMBER	MITIGATION MEASURE	TIMING	IMPLEMENTING PARTY	MONITORING PARTY	FREQUENCY AND DURATION OF MONITORING	PERFORMANCE CRITERIA	SPECIFICATION NOTES
	grindings (which consist of the roadway material and the yellow and white color traffic stripes) shall be removed and disposed of in accordance with the appropriate 2015 Caltrans Standard Specifications. • A California- licensed lead contractor should be required to perform all work that will disturb any lead-based paint as a result of planned or unplanned renovations in the Project area, including the presence of yellow traffic striping and pavement markings that may contain lead-based paint. All such material must be removed and disposed of as a hazardous material in compliance with 2015 Caltrans Standard Specification 14-11.12 and 14-11.13.						
NOISE							
NO-1	**Elevated Noise Level during Construction.** Implement the following control measures to minimize noise and vibration disturbances at sensitive receptors during construction: • Use newer equipment with improved muffling and ensure that all equipment items have the manufacturers' recommended noise abatement measures, such as mufflers, engine enclosures, and engine vibration isolators intact and operational. Newer equipment will generally be quieter in operation than older equipment. All construction equipment shall be inspected at periodic intervals to ensure proper maintenance and presence of noise control devices (e.g., mufflers and shrouding, etc.). • Utilize construction methods or equipment that provides the lowest level of noise and ground vibration impact.	During Construction	Construction Contractor	The City	During construction	Comply with condition objective	

IS/MND or PERMIT MITIGATION NUMBER	MITIGATION MEASURE	TIMING	IMPLEMENTING PARTY	MONITORING PARTY	FREQUENCY AND DURATION OF MONITORING	PERFORMANCE CRITERIA	SPECIFICATION NOTES
	• Turn off idling equipment.						
PUBLIC SERVICES							
PUB-1	**Emergency Access Plan**. Prior to the start of construction, the contractor shall coordinate with the City of West Sacramento Police and Fire departments and local public and private ambulance and paramedic providers in the area to prepare a Construction Period Emergency Access Plan. The Emergency Access Plan shall identify Project phases and construction scheduling and shall identify appropriate alternative emergency access routes.	Prior to Construction	Construction Contractor	The City	Prior to construction	Comply with condition objective	
TRANSPORTATION AND TRAFFIC							
TRAF-1	**Traffic Management Plan**. The construction contractor for the Project shall implement a standard traffic management plan to minimize traffic disruption and ensure adequate access is maintained to surrounding properties.	Prior to and during Construction	Construction Contractor	The City	Prior to and during construction	Comply with condition objective	
TRIBAL CULTURAL RESOURCES							
TCR-1	**Tribal Cultural Resource Construction Worker Awareness Training**. A consultant and construction worker tribal cultural resources awareness brochure and training program for all personnel involved in Project implementation will be developed in coordination with interested Native American tribes. Distribute the brochure and conduct the training in coordination with qualified cultural resources specialists and Native American representatives and monitors from culturally affiliated Native American tribes before any stages of Project implementation and construction activities begin on the Project site. The program will include relevant information regarding sensitive tribal cultural resources, including applicable regulations, protocols for avoidance, and consequences of violating State laws and regulations. The	Prior to Construction	Archaeologist	The City	Prior to Construction	Comply with condition objective	

IS/MND or PERMIT MITIGATION NUMBER	MITIGATION MEASURE	TIMING	IMPLEMENTING PARTY	MONITORING PARTY	FREQUENCY AND DURATION OF MONITORING	PERFORMANCE CRITERIA	SPECIFICATION NOTES
	worker cultural resources awareness program will also describe the procedure to follow for resources that have the potential to be located on the Project site and will outline what to do and whom to contact if any potential archaeological resources or artifacts are encountered. The program will also underscore the requirement for confidentiality and culturally-appropriate treatment of any finds of significance to Native Americans and behaviors, consistent with Native American tribal values.						
TCR-2	**Native American Monitor**. A minimum of seven days prior to beginning earthwork or other soil disturbance activities, the City shall contact concerned tribes of the construction schedule for ground disturbance activities. A tribal representative shall be invited to inspect the Project site, including any soil piles, trenches, or other disturbed areas, within the first five days of ground breaking activity. During this inspection, a site meeting of construction personnel shall also be held to afford the tribal representative the opportunity to provide tribal cultural resources awareness information. If any tribal cultural resources, such as structural features, unusual amounts of bone or shell, artifacts, human remains, or architectural remains are encountered during this initial inspection or during any subsequent construction activities, work shall be suspended within 100 feet of the find. The City shall coordinate any necessary investigation of the site with a tribal representative and a qualified archaeologist approved by the City; and as part of the site investigation and resource assessment, the archeologist shall consult with the tribe(s) and provide proper management recommendations should potential impacts to the resources be found by the City to be significant. A written report detailing the site assessment, coordination activities, and management recommendations shall be provided to the City by the qualified archaeologist.	Prior to and during Construction	The City	The City	Prior to and during Construction	Comply with condition objective	

IS/MND or PERMIT MITIGATION NUMBER	MITIGATION MEASURE	TIMING	IMPLEMENTING PARTY	MONITORING PARTY	FREQUENCY AND DURATION OF MONITORING	PERFORMANCE CRITERIA	SPECIFICATION NOTES
	The contractor shall implement the use of a Native American monitor whenever work occurs within 100 feet of the find.						

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 10

SUBJECT:

CONSIDERATION OF PROJECT APPROVAL AND ADOPTION OF RESOLUTION 18-36 ADOPTING THE FINAL INITIAL STUDY/MITIGATED NEGATIVE DECLARATION AND, MITIGATION MONITORING PLAN FOR THE LINDEN ACRES WATER MAIN REPLACEMENT PROJECT

INITIATED OR REQUESTED BY:

[ ] Council [X] Staff
[ ] Other

REPORT COORDINATED OR PREPARED BY:

Mauricio Meza-Pedraza, Assistant Engineer

Denix Anbiah, Director of Public Works

ATTACHMENT [X] Yes [ ] No [ ] Information [ ] Direction [X] Action

OBJECTIVE

The objective of this report is to obtain Project Approval from the City Council for the Linden Acres Water Main Replacement Project. and for the City Council to adopt Resolution 18-36 adopting the Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program in compliance with CEQA.

RECOMMENDED ACTION

Staff respectfully recommend that the City Council:

1) Adopt Resolution 18-36 adopting the Linden Acres Water Main Replacement Project final Initial Study and Mitigated Negative Declaration (IS/MND), that the IS/MND (Attachment 2) based on findings that it has been prepared in accordance with the California Environmental Quality Act (CEQA); that the Council has considered the Mitigated Negative Declaration and all comments received during the comment period; and, that there is no substantial evidence in the record that the Project will have a significant impact on the environment; and identifying the City Clerk and the Public Works Department as the location and custodian of the documents or other material that constitute the record of proceedings upon which this decision is based; and
2) Provide Project Approval for the Linden Acres Water Main Replacement Project as described in the environmental documents.

BACKGROUND

The Linden Acres water main replacement project is located in the Southport area within the Linden Acres and the Linden West Subdivisions. These two neighborhoods have different characteristics. The Linden West subdivision was built in the early 1980s. In this neighborhood the horizontal alignment of the existing water distribution system is located directly in the front lawns of homes and the existing water mains are undersized (6 inches). Additionally, the existing pipe material includes welded steel and asbestos cement (AC). The Linden Acres subdivision was built in the late 1940s and the lots are approximately half-acre in size. The work included in this project will consist of replacing the existing welded steel backyard mains with Polyvinyl Chloride pressure pipes to be located in the City's right-of-way. This will reduce the frequent maintenance of the existing water distribution system of the Linden Acres subdivision. These lots are currently served by sewer septic systems (rather than being connected to the City sewer system) and the septic systems need to be avoided when abandoning the old water lines and installing new water service lines to the residents.

The project will benefit over 200 residents by providing new water mains and water services and by correcting existing fire flow deficiencies in the area. The work will include replacing existing asbestos-cement and welded steel water mains, abandoning in place existing backyard mains, placing new mains in the city streets, and installing new water meters. The existing welded steel and substandard water mains (16 inches, 12 inches, 8 inches, and 6 inches in diameter) will be abandoned in place after the new mains are placed. The Project anticipates extending a water main to the south of Redwood Avenue, then east to reconnect at the east end of Tamarack Road to complete the water distribution system. It should be noted that this Project will extend into the boundaries of the privately-owned proposed Liberty Specific Plan to the east. If approved, the Liberty

Specific Plan area will eventually connect to the proposed Project at Tamarack Road and at the north water line coming east from Bastone Court to provide for a water distribution loop system. The water main associated with the proposed Project that extends into the proposed Liberty Specific Plan area will be temporary and abandoned if and when development of the Liberty Specific Plan commences.
The 2005 Water Master Plan identified the need to replace leaky welded steel and undersized water mains, and replacement of backyard water mains with new mains in the City roadway. These backyard replacement projects have been prioritized and systematically completed except in the Linden area. Most recently, the 2015 Water Master Plan also identified replacement of existing undersized backyard mains as a needed improvement to mitigate fire flow deficiencies and improve the water distribution system capacity in the area.

The Linden Acres Water Main Replacement Project scope is a part of the Water Meter implementation Program approved by Council in March 2007. The Water Meter Implementation Program is scheduled to be substantially complete by December 2018. The only exceptions are meters associated with the Linden Acres and Westfield Village Water Main Replacement Projects. The pipes are too old and fragile for meter retrofit at these two locations, thereby requiring water main replacement.

Since the location of the project is in an environmental sensitive area and with recent environmental regulation changes to the California Environmental Quality Act (CEQA), the City elected to seek professional services from environmental firms to assist with the determination of level of compliance and documentation needed for CEQA. On May 10, 2017, Council awarded the contract for environmental services to LSA Consulting for the Linden Acres Water Main Replacement Project. City staff with assistance from the consultant prepared an Initial Study and Mitigated Negative Declaration (IS/MND) for the project in accordance with CEQA.

ANALYSIS

The Project improvements are designed to improve capacity in the water distribution system. Attachment 4 shows a schematic design of the proposed water distribution system. The project design includes installation of approximately 11,000 linear feet of 16-inch, 12-inch and 8-inch pipes with the addition of fire hydrants and service lines (laterals) that will include installation of new water meters. The new mains will be placed in the existing roadway sections within City right-of-way (ROW) and dedicated easements and will be accessible for future City maintenance. This will eliminate the need to access the residents' backyards by maintenance crews and public inconvenience will be reduced.

In coordinating project design, staff met with individual property owners of the Linden Acres Subdivision between the months of April – May 2018, informing residents about the proposed project and collecting field data for completing the project design. Staff plans to advertise the project for construction by November and anticipates that the project will begin construction in early spring 2019.

Environmental Consideration

An Initial Study and Mitigated Negative Declaration (IS/MND) was prepared for the project and was circulated for review through the State Clearinghouse. A Notice of Availability, which meets the requirements of a Notice of Intent, was distributed to responsible agencies and published in the West Sacramento News Ledger on May 23, 2018, and a Notice of Completion of the Proposed Mitigated Negative Declaration was filed with the State Clearinghouse on May 1, 2018. Copies of the document were also circulated to federal and state agencies, City departments, County departments, local agencies, and private agencies. The agency review period commenced on May 2, 2018 and ended May 31, 2018. The public commenting period began May 23, 2018 and ended June 22, 2018. During the public review period, a copy of the draft Mitigated Negative Declaration was made available for review at City Hall.

The City of West Sacramento has examined the potential environmental impacts to the study area that is associated with improvements within the entire project. Pursuant to the CEQA all required environmental research and analyses have been performed. All environmental surveys, resource agency coordination, and contacting of interested parties have been conducted.

The Initial Study with Proposed Mitigated Negative Declaration summarizes the findings of recently conducted field and literature research along with record searches. The analysis provided in the environmental document found that the project's potential impacts on air quality, biological resources, cultural resources, noise, and transportation/traffic could be reduced to less than significant with the adoption and implementation of mitigation measures. These measures can be found in the Mitigation Monitoring and Reporting Program (Attachment 3). Staff believes the environmental document adequately addresses and requires measures to mitigate the project's impact on the environment.

Commission Recommendation
N/A

Strategic Plan Integration
The construction of the Linden Acres Water Main Replacement Project is consistent with a continuation of the Council adopted "West Sacramento Vision 2029 - Principles to Vibrant Neighborhood" by enhancing the following Principals: Quality Municipal Service, Quality City Infrastructure and Facilities, Inspire Community Improvements, Vibrant Neighborhoods, and Comfortable Life Style and Living.

Alternatives
1) It is respectfully recommended that the City Council:
   a. Adopt Resolution 18-36 certifying that it has reviewed and considered the information in the Linden Acres Water Main Replacement Project final Initial Study and Mitigated Negative Declaration (IS/MND), that the IS/MND (Attachment 2) has been prepared in accordance with the California Environmental Quality Act (CEQA); and that the IS/MND reflects the City of West Sacramento's independent judgement and analysis.
   b. Provide Project Approval for the Linden Acres Water Main Replacement Project as described in the environmental documents.
2) Council may choose to make adjustments or modifications to the Project and/or in accepting the environmental documents; however, such actions could delay construction.

Coordination and Review
This report has been coordinated between the City's Public Works Department and the Planning Division of the Community Development Department.

Budget/Cost Impact
Providing Project Approval and Adoption of Resolution No. 18-36 certifying the Mitigated Negative Declaration approving the Mitigation Monitoring and Reporting Program will not have direct fiscal impacts.

ATTACHMENTS
1) Resolution 18-36
2) Final CEQA Initial Study and Mitigated Negative Declaration for the Linden Acres Water Main Replacement Project
3) Mitigation Monitoring and Reporting Program
4) Proposed water distribution system
5) Vicinity Map

RESOLUTION 18-36

RESOLUTION OF THE CITY COUNCIL OF THE CITY OF WEST SACRAMENTO ADOPTING THE INITIAL STUDY/MITIGATED NEGATIVE DECLARTION AND MITIGATION MONITORING AND REPORT PROGRAM IN COMPLIANCE WITH CEQA FOR THE LINDEN ACRES WATER MAIN REPLACEMENT PROJECT

WHEREAS, on October 18, 2017, the City Council adopted the Water Master Plan Update; and

WHEREAS, the City of West Sacramento ("City") proposes to construct improvements to its water distribution system in the Linden Acres and Linden West Subdivisions consistent with the Water Master Plan recommendations including: improvements that will mitigate capacity deficiencies in the existing water distribution system; and

WHEREAS, construction of the improvements will improve capacity in the water distribution system; and

WHEREAS, the project is in furtherance of the goals and recommendations contained within the Water Master Plan Update; and

WHEREAS, the City has prepared an Initial Study/Mitigated Negative Declaration (IS/MND) in accordance with the California Environmental Quality Act, CEQA Guidelines, and all other applicable laws and regulations; and

WHEREAS, the City has considered all public comments received on the Mitigated Negative Declaration and that adequate mitigation will be applied to the project; and

WHEREAS, the City finds that the public health, safety and general welfare warrant the adoption of the IS/MND for the Linden Acres Water Main Replacement Project, and

WHEREAS, the City has reviewed the Linden Acres Water Main Replacement project final Initial Study and Mitigated Negative Declaration and finds that the public health, safety and general welfare warrant the adopting of the final IS/MND.

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of West Sacramento finds that:

1. The Linden Acres Water Main Replacement Project Initial Study and Mitigated Negative Declaration and Mitigation Monitoring and Reporting Program (MMRP) has been completed in compliance with the California Environmental Quality Act; and
2. The Linden Acres Water Main Replacement project final IS/MND and MMRP has been reviewed and considered by the City Council prior to any action by the City Council on the Project; and
3. The Linden Acres Water Main Replacement project final IS/MND identified all potentially significant impacts to the environment which can and will be avoided or mitigated to less than significant levels through adoption and implementation of the mitigation measures proposed as part of the Project and through implementation of the MMRP; and
4. The Linden Acres Water Main Replacement project final IS/MND and MMRP reflect the City of West Sacramento's independent judgement and analysis; and
5. There is no substantial evidence in the record that the Project, as mitigated, will have a significant effect on the environment; and
6. The administrative record is located in the Office of the City Clerk and Public Works Department located at 1110 West Capitol Avenue, West Sacramento, California, 95691; and,

7. The City Clerk and the Public Works Department are collectively designated as the location and custodian of the documents and other material constituting the record of proceedings upon which this decision is based; and
8. The City Council of the City of West Sacramento has considered and hereby adopts the Findings and Statements of the final IS/MND; and
9. The City Council hereby adopts the Linden Acres Water Main Replacement project final IS/MND and MMRP.

PASSED AND ADOPTED by the City Council of the City of West Sacramento this 22nd day of August 2018, by the following vote:

AYES:
NOES:
ABSENT:

Christopher L. Cabaldon, Mayor

ATTEST:

Kryss Rankin, City Clerk

FINAL

CEQA INITIAL STUDY AND MITIGATED NEGATIVE DECLARATION

LINDEN ACRES WATER MAIN REPLACEMENT PROJECT

WEST SACRAMENTO, CALIFORNIA

STATE CLEARINGHOUSE NUMBER: 2018052001

LSA

July 2018

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FINAL

CEQA INITIAL STUDY AND MITIGATED NEGATIVE DECLARATION

LINDEN ACRES WATER MAIN REPLACEMENT PROJECT

WEST SACRAMENTO, CALIFORNIA

STATE CLEARINGHOUSE NUMBER: 2018052001

Submitted to:

Mauricio Meza-Pedraza
City of West Sacramento
1110 West Capitol Avenue
West Sacramento, California 95691

Prepared by:

LSA
201 Creekside Ridge Court, Suite 250
Roseville, California 95678
(916) 772-7450

Project No. WSA1701

LSA

July 2018

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LIST OF ABBREVIATIONS AND ACRONYMS

AB	Assembly Bill
AFY	acre-feet per year
BMP	best management practices
CAA	Clean Air Act
CARB	California Air Resources Board
CCR	California Code of Regulations
CDFW	California Department of Fish and Wildlife
CEQA	California Environmental Quality Act
CGS	California Geological Survey
CH_4	Methane
CHMIRS	California Hazardous Material Incident Reporting Systems
City	City of West Sacramento
cm	centimeters
CNDDB	California Natural Diversity Database
CNEL	community noise equivalent level
CNPS	California Native Plant Society
CO	carbon monoxide
CO_2	carbon dioxide
CO_2e	CO2 equivalents
CVRWQCB	Central Valley Regional Water Quality Control Board
dB	decibel(s)
dBA	A-weighted decibels
DOGGR	Division of Oil, Gas, and Geothermal Resources
DTSC	California Department of Toxic Substances Control
DWR	Department of Water Resources
EDR	Environmental Data Resources
EIR	Environmental Impact Report
FEMA	Federal Emergency Management Agency
FMMP	Farmland Mapping and Monitoring Program
GHG	greenhouse gases
GWP	Global Warming Potential
HFC	Hydrofluorocarbons
IS/MND	Initial Study/Mitigated Negative Declaration
L_{dn}	day-night average level
L_{eq}	equivalent continuous sound level

L_{max}	maximum sound level
LOS	level of service
LRA	Local Responsibility Area
MMT	million metric tons
MMRP	Mitigation and Monitoring Reporting Program
MRZ	Mineral Resource Zone
N_2O	nitrous oxide
NAAQS	National Ambient Air Quality Standards
NAHC	Native American Heritage Commission
NHTSA	National Highway Traffic Safety Administration
NO_2	nitrogen dioxide
NO_X	nitrogen oxides
NPDES	National Pollutant Discharge Elimination System
O_3	ozone
PFC	Perfluorocarbons
PM_{10}	particulate matter less than or equal to 10 microns in diameter
$PM_{2.5}$	particulate matter less than or equal to 2.5 microns in diameter
PPV	Peak Particle Velocity
PRC	Public Resources Code
RoadMod	Road Construction Emissions Model
ROG	reactive organic gases
ROW	right-of-way
RR	rural residential
SF_6	Sulfur Hexafluoride
SIP	Sacramento Regional State Implementation Plan
SMAQMD	Sacramento Metropolitan Air Quality Management District
SMARA	Surface Mining and Reclamation Act of 1975
SO_2	Sulfur dioxide
SWPPP	Storm Water Pollution Prevention Plan
SWRCB	State Water Resources Control Board
TAC	toxic air contaminant
UAIC	United Auburn Indian Community
UCMP	University of California, Museum of Paleontology
USEPA	United States Environmental Protection Agency
USFWS	United States Fish and Wildlife Service
VdB	vibration velocity decibels
WSFD	West Sacramento Fire Department

WUSD	Washington Unified School District
YDWN	Yocha Dehe Wintun Nation
YSAQMD	Yolo-Solano Air Quality Management District

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1.0 INTRODUCTION

The City of West Sacramento is proposing to install water mains and laterals in the Linden neighborhood of the Southport Planning Area. As part of the Linden Acres Water Main Replacement Project (herein referred to as "proposed Project" or "Project"), the new water mains will be installed in existing road right-of-way and will be replacing aging backyard water mains that currently exist within residential parcels. New laterals will also be installed as part of the proposed Project allowing a connection to the existing residential units and the new water mains that will be installed.

1.1 ENVIRONMENTAL REVIEW

The proposed Project constitutes a "Project" in accordance with California Environmental Quality Act (CEQA). Prior to approving the proposed Project, the City of West Sacramento must provide environmental review in accordance with CEQA to assess the potential impacts of the proposed Project, including mitigation where necessary.

The City of West Sacramento has prepared this Initial Study to provide agencies and the public with information about the potential impacts of the proposed Project on the local and regional environment. This document has been prepared in compliance with CEQA of 1970 as amended, and the State CEQA Guidelines, California Administrative Code, Title 14, Division 6, Chapter 3 (CEQA Guidelines). In anticipation of determining that all potentially significant impacts resulting from the proposed Project can be mitigated to less than significant levels, a Mitigated Negative Declaration is being considered to provide environmental clearance for the proposed Project.

1.2 CLARIFICATIONS AND CORRECTIONS

During the agency review period (between May 2, 2018 to May 31, 2018) the following three agency comment letters were received: one from the United Auburn Indian Community (UAIC); one from the Central Valley Regional Water Quality Control Board (CVRWQCB); and one from the State Clearinghouse (SCH). The comment letter from UAIC requested that standard mitigation measures, developed by UAIC, be incorporated into the environmental document. The CVRWQCB letter was a standard letter discussing regulatory information and permitting requirements. The SCH letter indicated that the IS/MND for the Project complied with the State Clearinghouse review requirements for draft environmental documents, pursuant to CEQA. Based on the comment letter received from UAIC, Mitigation Measure MM-CUL-1 in Section 3.5 of this environmental document was revised to add "<u>and invite a geographically-affiliated Native American Representative"</u> to respond to UAIC's comment. The text that was added was double underlined to indicate a revision/addition was made to this Final IS/MND based on comments received.

During the public review period (between May 23, 2018 to June 22, 2018), no comment letters from the public were received.

On the Cover and Title Pages of this document the word "Draft" has been deleted, the word "Final" has been added, and the State Clearinghouse number has been added. Sections 1.0 "Introduction"; 1.1 "Environmental Review"; 1.2 "Clarifications and Corrections"; 1.3 "Public Comments"; 1.4 "Responses to Comments Format"; and, 1.5 "Additional Documentation" have been added to this

Final IS/MND. Section 5.0 "Response to Comments" has been added to this Final IS/MND and provides response to comments that were receiving during the agency and public review periods. Section 6.0 "Mitigation and Monitoring Program" which provides a matrix of the mitigation measures that would be implemented, the mitigation milestones (timing of when the measure is to be implemented/completed) and agencies/entities responsible for implementing/overseeing the measures, has also been added to this Final IS/MND.

1.3 AGENCY AND PUBLIC REVIEW COMMENTS

The City of West Sacramento circulated the Draft IS/MND of the proposed Project for agency review and public review, for 30 days each. The agency review period commenced on May 2, 2018 and ended on May 31, 2018 while the public comment period commenced on May 23, 2018 and ended on June 22, 2018. The following comment letters were received on the Draft IS/MND (it should be noted that three agency letters and no public comment letters were received):

- United Auburn Indian Community Comment Letter (5/21/18)
- Central Valley Regional Water Quality Control Board (5/24/18)
- State Clearinghouse Review Requirement Completion Letter (6/1/18)

1.4 RESPONSE TO COMMENTS FORMAT

Section 5.0 Response to Comments is organized in the following way:

- The comment letters are included and labeled with a comment code that corresponds to the responses; and,
- A response to each relevant comment follows, organized by comment code.

1.5 ADDITIONAL DOCUMENTATION

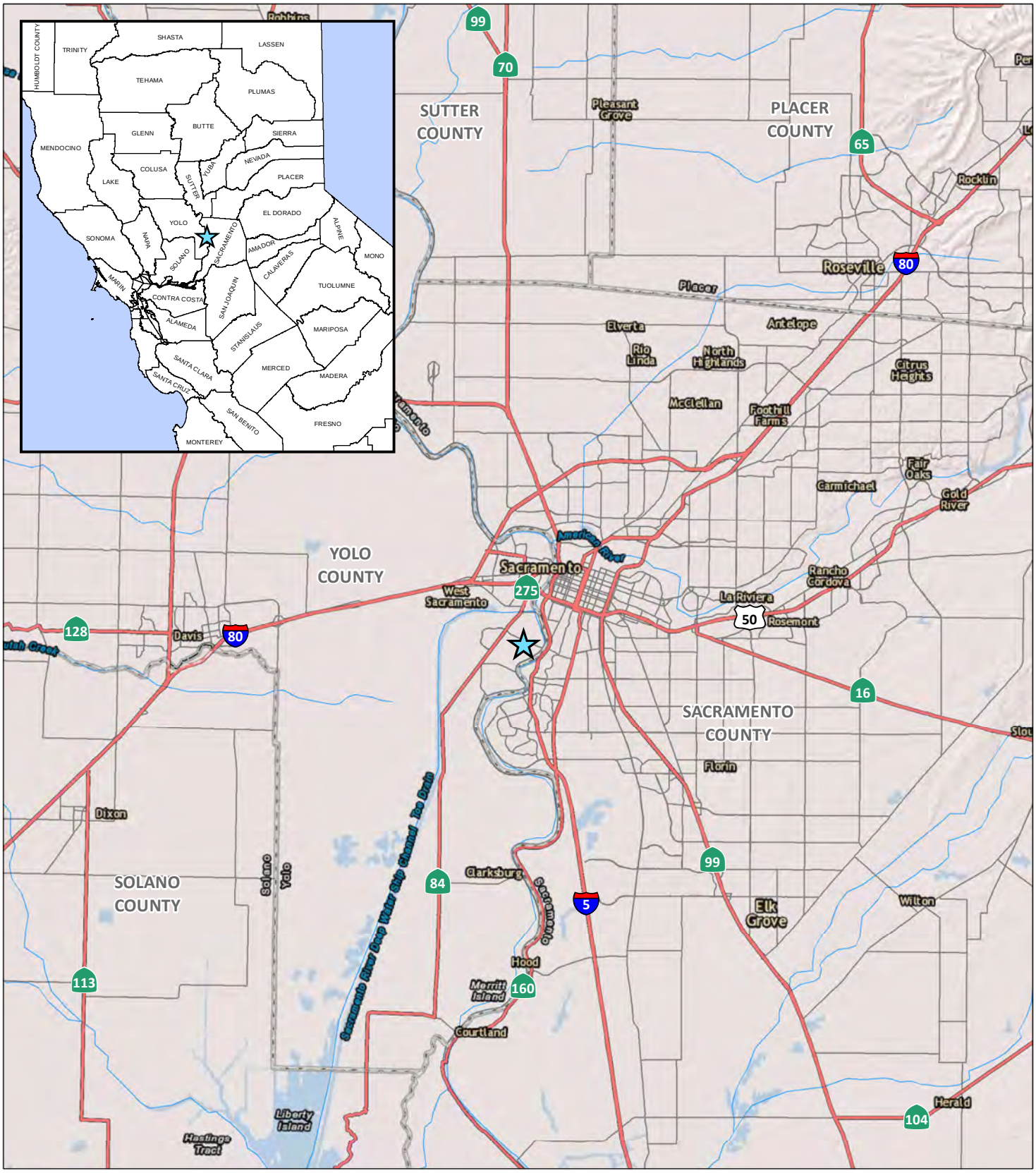
The Final IS/MND includes additional documentation for the public record, including:

- Notice of Completion **(Appendix A)**;
- Notice of Determination **(Appendix B)**;
- State Clearinghouse Compliance Letter Date June 1, 2018 **(Appendix C)**.

2.0 INITIAL STUDY

Project Title:	Linden Acres Water Main Replacement Project
Lead Agency Name and Address:	City of West Sacramento, 1110 West Capitol Avenue, West Sacramento, California 95691
Contact Person and Phone Number:	Mauricio Meza-Pedraza, City of West Sacramento, (916) 617-4645
Project Location:	The Linden Acres Water Main Replacement Project (herein referred to as the proposed Project) is located in West Sacramento, within the Southport Planning Area. The Southport Planning Area lies in the southern half of West Sacramento and is bounded by the Deep Water Ship Channel to the north and west, the Sacramento River to the east, and the city limits to the south. The Project site consists of right-of-way (ROW) (roadway) work and is bounded by Linden Road to the north, Mojave Drive to the west, Trinity Way, and the proposed Liberty Specific Plan to the south, and the proposed Liberty Specific Plan to the east. **Figure 2-1: Regional Location** and **Figure 2-2: Project Location** shows the location of the Project site on a regional and local scale. **Figure 2-2** also shows the location of the proposed Project in comparison to the Liberty Specific Plan area.
Project Sponsor's Name and Address:	City of West Sacramento 1110 West Capitol Avenue West Sacramento, CA 95691
General Plan and Zoning Designation:	Roadway (City Right-of-Way)
Description of Project (Described the whole action involved, including but not limited to later phases of the project, and any secondary support, or off-site features necessary for its implementation):	The purpose of the Project is to replace the existing backyard water mains with new mains in the City of West Sacramento (City) owned roadway within ROW and dedicated easements. The existing water mains (16 inches, 12 inches, 8 inches, and 6 inches in diameter) will be abandoned in place at average depths ranging from 4 to 8 feet deep. The Project anticipates extending a water main to the south of Redwood Avenue, then east to reconnect at the east end of Tamarack Road to complete the water distribution system. It should be noted that this Project would extend into the boundaries of the privately owned Liberty Specific Plan to the east. The Liberty Specific Plan will eventually connect to the proposed Project at Tamarack Road and at the north water line coming east from Bastone Court in order to maintain the water distribution loop system. The water main associated with the proposed Project that extends into the Liberty Specific Plan area will be temporary and abandoned once development of the Liberty Specific Plan commences. The new water main will be placed in the existing roadway sections and will be accessible for future City maintenance. The replacement of water mains will include approximately 11,000 linear feet of 16- and 8-inch pipe with the addition of fire hydrants and service lines (laterals) that will include new water meters. The service laterals consist of 111 pipes that are 0.75 inch in size and 94 pipes that are 1 inch in size. Most of the pipe excavations will be approximately 4 feet deep, with a small section that will be as deep as 10 feet to connect to the existing system. **Figure 2-3: Project Design** shows the design of the proposed Project. The Project will be implemented on the following roads: Linden Road, Mojave Drive, Merced Way, Shasta Way, Carmel Court, Rubicon Way, Ironwood Way, Spruce Street, Redwood Avenue, Alder Way, Tamarack Road, and Cedar Street. Construction of the proposed Project is anticipated to commence in spring 2019 and will last 8 months (170 working days). During construction, affected roadways may be closed to through traffic; however, access by residents will still be permitted. The City of West Sacramento would direct the construction

	contractor to locate construction equipment at staging areas as far from residential units as possible. The construction equipment anticipated to be used includes: Hydrovac vacuum trucks, tractors, a backhoe, a trencher, a loader, a haul truck, an excavator, a compressor, a concrete saw, a concrete mixer truck, jack hammers, an asphalt planer, a trench paver, compactors, rollers, heavy-duty trucks, dump trucks, a street sweeper, a heavy equipment transport truck, a forklift, pickups, and chipping guns.
Surrounding Land Uses and Setting; Briefly Describe the Project's Surroundings:	The Project site is located in a portion of West Sacramento that is urbanized and consists of mostly residential units. The Project area is located approximately 0.42 mile to the east of the Sacramento River and 0.97 mile south of the Deep Water Ship Channel. There is existing agricultural and vacant land to the south and east of the Project site, respectively; however, this area is currently entitled and is part of the proposed Liberty Specific Plan that has currently undergone environmental review.
Other Public Agencies Whose Approval Is Required (i.e., permits, financial approval, or participation agreements):	The City of West Sacramento is the Lead Agency under CEQA and has the primary authority for Project approval. At this time, no regulatory permits are anticipated for the proposed Project.
Have California Native American tribes traditionally and culturally affiliated with the project area requested consultation pursuant to Public Resource Code section 21080.3.1? If so, has consultation begun?	Tribal consultation commenced on June 12, 2017.



LSA

LEGEND

Project Area

FIGURE 2-1

Linden Acres Water Main Replacement Project
West Sacramento, Yolo County, California
LSA Project No. WSA1701

0 2.5 5
MILES

Regional Location

SOURCE: ESRI World Street Map (2016)

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LSA

LEGEND

Project Area (109.7 acres)

Liberty Specific Plan Area

N

0 500 1000
FEET

FIGURE 2-2

Linden Acres Water Main Replacement Project
West Sacramento, Yolo County, California
LSA Project No. WSA1701

Project Location

SOURCE: NAIP (2016); City of West Sacramento (2017)

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LSA

FIGURE 2-3

LEGEND

Project Area (109.7 acres)

Parcels

Design

Existing Water Accessory

Right-of-Way

Existing Water Main

Water Fittings

Proposed Water Main

N

0 250 500

FEET

Linden Acres Water Main Replacement Project
West Sacramento, Yolo County, California
LSA Project No. WSA1701

Project Design

SOURCE: Esri World Imagery (2014); Design - City of West Sacramento (07/2017); Mapping - LSA (07/2017)

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2.1 ENVIRONMENTAL FACTORS AFFECTED

The environmental factors checked below would be potentially affected by this Project, involving at least one impact that could be lessened to a level of significance through implementation of mitigation measures.

☐	Aesthetics	☐	Agricultural and Forest Land Resources	☒	Air Quality
☒	Biological Resources	☒	Cultural Resources	☐	Geology and Soils
☐	Greenhouse Gas Emissions	☒	Hazards and Hazardous Materials	☐	Hydrology and Water Quality
☐	Land Use/Planning	☐	Mineral Resources	☒	Noise
☐	Population/Housing	☐	Public Services	☐	Recreation
☒	Transportation/Traffic	☐	Tribal Cultural Resources	☐	Utilities and Service Systems
☐	Mandatory Findings of Significance				

2.2 DETERMINATION

On the basis of this initial evaluation:

☐	I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
☒	I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
☐	I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
☐	I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
☐	I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required

Signature: Mauricio Meza **Date:** 4/23/2018

Printed Name: MAURICIO MEZA-PEDRAZA **For:**

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3.0 EVALUATION OF ENVIRONMENTAL IMPACTS

The following sections of this document provide discussions of the possible environmental impacts of the proposed Project for specific issue areas that have been identified in the *CEQA Statute and Guidelines Appendix G Environmental Checklist Form*. For each issue area, potential impacts are discussed and analyzed in order to determine a level of significance.

As defined by Section 15382 of the *CEQA Guidelines*, a "significant effect" is a "substantial, or potentially substantial, adverse change in any of the physical conditions within the area affected by a project, including land, air, water, minerals, flora, fauna, ambient noise, and objects of historic or aesthetic significance." According to the *CEQA Guidelines,* "an economic or social change by itself shall not be considered a significant effect on the environment, but may be considered in determining whether the physical change is significant".

For resource topics that are determined to be potentially significant, a list of mitigation measures is provided at the conclusion of each evaluation. Residual impacts or levels of significance remaining after implementation of the mitigation measures are also provided.

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3.1 AESTHETICS

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Have a substantial adverse effect on a scenic vista?	☐	☐	☐	☒
b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	☐	☐	☐	☒
c. Substantially degrade the existing visual character or quality of the site and its surroundings?	☐	☐	☒	☐
d. Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	☐	☐	☐	☒

3.1.1 Environmental Setting

West Sacramento is composed mostly of suburban and rural development and agricultural open space, with some light commercial and industrial development, educational facilities, and riparian corridors. The natural topography of West Sacramento is mostly flat with raised levees and is composed of vegetation in the form of residential landscaping, agricultural crops and hedgerows, and riparian vegetation along the Sacramento River and in swales and drainages throughout rural residential (RR) lands. Residential landscaping and riparian vegetation, when combined with development in West Sacramento, act to restrict views largely to the foreground.

The proposed Project is located in West Sacramento's Northeast Village neighborhood, a well-established urbanized area consisting of single-family residential units, City-owned neighborhood streets, and residential landscaping. The Project site is bounded by Linden Road to the north, Mojave Drive to the west, Trinity Way and the proposed Liberty Specific Plan area to the south, and the proposed Liberty Specific Plan area to the east. Specifically, the Project will be implemented within City-owned right-of-way (ROW) and within dedicated easements along the following roads: Merced Way, Rubicon Way, Shasta Way, Trinity Way, Ironwood Way, Spruce Street, Linden Road, Redwood Avenue, Alder Way, Bastone Court, Tamarack Road, and Cedar Court. The Project site is topographically flat and the only nearby scenic feature is the Sacramento River to the east; however, views of the river are blocked by a levee.

The Project site is not located near a scenic roadway, freeway, or highway as designated by local or State jurisdictions.

3.1.2 Discussion

a. Would the Project have a substantial effect on a scenic vista?

No Impact. West Sacramento, in which the Project is located, is topographically flat and is not located near a scenic vista. Implementation of the proposed Project would not block views of scenic

vistas, as there are none in the area. As such, **no impact** would occur under this threshold. No mitigation would be required.

b. Would the Project substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?

No Impact. The Project is located in an urbanized portion of West Sacramento and is not located within, adjacent to, or near a State scenic highway. There are no federal, State, or locally designated scenic roadways within West Sacramento; therefore, implementation of the proposed Project would not have an impact on scenic resources within a designated scenic highway. **No impact** would occur, and no mitigation would be required.

c. Would the Project substantially degrade the existing visual character or quality of the site and its surroundings?

Less than Significant Impact. The proposed Project consists of abandonment of the existing water mains along Merced Way, Rubicon Way, Shasta Way, Trinity Way, Ironwood Way, Spruce Street, and Linden Road and installation of new water mains and service laterals. During construction, the visual character or quality of the Project site may temporarily change compared to existing conditions due to trenching in the roadway ROW to install the new water mains and laterals. Implementation of the proposed Project would not require the removal of existing trees or natural vegetation as work would be completed in the street ROWs. The portion of the Project that is located within the Liberty Specific Plan would not require the removal of existing trees (as none exist) where the new water main would be placed. Once completed, the Project site will resemble the existing visual character and quality of the site. The area where the Project site is located is urbanized and does not have outstanding visual character or quality from an aesthetic viewpoint. As such, implementation of the Project would not substantially degrade the existing visual character or quality of the site and surroundings, and impacts would be **less than significant**. No mitigation would be required.

d. Would the Project create a new source of substantial light or glare which would adversely affect day or nighttime views in the area.

No Impact. The proposed Project includes the abandonment of deteriorating water mains and installation of a new water mains, service laterals, and meters to serve existing residential units. During construction and operation, the Project would not generate increased light or glare in the area above what is currently experienced under existing conditions. Neither the materials used for implementation of the Project nor the new lighting fixtures associated with the Project would be reflective. The Project would not create a new source of substantial light or glare that would adversely affect daytime or nighttime views in the area; as such, **no impact** would occur. No mitigation would be required.

3.1.3 Mitigation Measures

No mitigation is required.

3.2 AGRICULTURAL AND FORESTRY RESOURCES

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment Project; and the forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:				
a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	☐	☐	☐	☒
b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?	☐	☐	☐	☒
c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	☐	☐	☐	☒
d. Result in the loss of forest land or conversion of forest land to non-forest use?	☐	☐	☐	☒
e. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?	☐	☐	☐	☒

3.2.1 Environmental Setting

The proposed Project would be implemented within an urbanized area of West Sacramento. The California Department of Conservation Farmland Mapping and Monitoring Program (FMMP) website was accessed for Yolo County (California Department of Conservation 2016) to determine if Important Farmland (defined by the FMMP as Prime Farmland, Farmland of Statewide Importance, Unique Farmland, or Farmland of Local Importance) existed within the Project site. The California Department of Conservation defines these Important Farmland categories as follows:

- **Prime Farmland:** Farmland with the best combination of physical and chemical features able to sustain long-term agricultural production. This land has the soil quality, growing season, and moisture supply needed to produce sustained high yields. The land must have been used for irrigated agricultural production at some time during the 4 years prior to the mapping date.

- **Farmland of Statewide Importance:** Farmland similar to Prime Farmland but with minor shortcomings, such as greater slopes or less ability to store soil moisture. The land must have

been used for irrigated agricultural production at some time during the 4 years prior to the mapping date.

- **Unique Farmland:** Farmland of lesser-quality soils used for the production of the State's leading agricultural crops. This land is usually irrigated, but it may include non-irrigated orchards or vineyards as found in some climatic zones in California. The land must have been cropped at some time during the 4 years prior to the mapping date.

- **Farmland of Local Importance:** Land of importance to the local agricultural economy as determined by each county's board of supervisors and a local advisory committee. The County of Yolo defines this category as cultivated farmland with soils that meet the criteria for Prime Farmland or Farmland of Statewide Importance, except that the land is not presently irrigated, and other non-irrigated farmland.

The FMMP also defines the following categories that are not considered Important Farmland:

- **Grazing Land**: Land on which the existing vegetation is suited to the grazing of livestock. This category was developed in cooperation with the California Cattlemen's Association, the University of California Cooperative Extension, and other groups interested in the extent of grazing activities.

- **Urban and Built-Up Land:** Land occupied by structures with a building density of at least one unit to 1.5 acres, or approximately six structures to a 10-acre parcel. This land is used for residential, industrial, commercial, construction, institutional, public administration, railroad and other transportation yards, cemeteries, airports, golf courses, sanitary landfills, sewage treatment, water control structures, and other developed purposes.

- **Other Land:** Land not included in any other mapping category. Common examples include low-density rural developments; brush, timber, wetland, and riparian areas not suitable for livestock grazing; confined livestock, poultry, or aquaculture facilities; strip mines; borrow pits; and water bodies smaller than 40 acres. Vacant and nonagricultural land surrounded on all sides by urban development and greater than 40 acres is mapped as Other Land.

The FMMP designates the Project site as Urban and Built-Up Land. Farmland of Local Importance is located to the east of the Project site; however, the eastern extent of the Project (the water main extending into the Liberty Specific Plan) would not be located in land designated as Farmland of Local Importance by the FMMP.

There is currently no active agricultural production within the Project site. The Liberty Specific Plan area located to the east and south of the Project site is currently under agricultural production with alfalfa/hay crops. Additionally, none of the land within or near the Project site is zoned for agricultural use.

The California Land Conservation Act of 1965, commonly referred to as the Williamson Act, allows local governments to enter into contracts with private landowners for the purpose of restricting specific parcels of land to agricultural, or related open space use. In return, landowners receive

property tax assessments that are much lower than normal because they are based on farming and open space uses as opposed to full market value. Review of the Williamson Act land within Yolo County and West Sacramento indicates there are no parcels within the Project site under a Williamson Act contract.

The City of West Sacramento and specifically the Project site do not have land designated as forest land resources.

3.2.2 Discussion

a. Would the Project convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland) as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

No Impact. The Project would be implemented in an urbanized portion of West Sacramento that is developed with a long-established neighborhood. The California Department of Conservation Farmland Mapping and Monitoring Program designated the land on the Project site as Urban and Built-Up Land. Farmland of Local Importance (defined as Prime or Statewide soils which are presently not irrigated or cultivated) are located to the south and east of the Project site where the Liberty Specific Plan will be developed; however, the proposed Project would not extend into areas that are designated as Farmland of Local Importance. As such, **no impacts** to Important Farmland would occur with implementation of the proposed Project. No mitigation measures would be required.

b. Would the Project Conflict with existing zoning for agricultural use, or a Williamson Act contract?

No Impact. The proposed Project will be developed within the roadway ROWs not zoned for agricultural use. The Project site is currently zoned Residential One Family (R-1-B) and the portion where the Project extends into the Liberty Specific Plan area is zoned as RR. There is no land within the Project site that is designated as Williamson Act Land. **No impacts** to existing zoning for agricultural use or Williamson Act Contracted land would occur with implementation of the proposed Project. As such, no mitigation measures would be required.

c. Would the Project conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?

No Impact. The Project site is currently zoned as Residential One Family (R-1-B), and the portion where the Project extends into the Liberty Specific Plan area is zoned as RR. As such, the proposed Project would not conflict with or cause rezoning of forest land, timberland, or timberland zoned Timberland Production. **No impacts** would occur and mitigation measures would not be required.

d. Would the Project result in the loss of forest land or conversion of forest land to non-forest use?

No Impact. The proposed Project is in an urbanized area of West Sacramento. Forest land does not exist within or near the Project site; therefore, implementation of the Project would not result in the loss of forest land or conversion of forest land to nonforest use. **No impacts** would occur and mitigation measures would not be required.

e. Would the Project involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?

No Impact. The proposed Project is located in an urbanized portion of West Sacramento and would not result in the conversion of forest land to nonforest use. The portion of the Project site that extends into the Liberty Specific Plan area is currently under agricultural production and occupied by an alfalfa/hay field. Implementation of the proposed Project would require temporary disruption to this area during trench digging and water main installation, but the area would be returned to pre-construction conditions after construction activities are complete. Additionally, the loss of this land has already been analyzed in the Liberty Specific Plan Environmental Impact Report (EIR), and the land is slated to become zoned for development. As such, the proposed Project would not change the zoning or land use status of agricultural land and **no impacts** would occur. Mitigation measures would not be required.

3.2.3 Mitigation Measures

No mitigation is required.

3.3 AIR QUALITY

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:				
a. Conflict with or obstruct implementation of the applicable air quality plan?	☐	☐	☒	☐
b. Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	☐	☒	☐	☐
c. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non- attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	☐	☐	☒	☐
d. Expose sensitive receptors to substantial pollutant concentrations?	☐	☐	☒	☐
e. Create objectionable odors affecting a substantial number of people?	☐	☐	☒	☐

3.3.1 Environmental Setting

Both State and federal governments have established health-based ambient air quality standards for six criteria air pollutants:[1] carbon monoxide (CO), ozone (O_3), nitrogen dioxide (NO_2), sulfur dioxide (SO_2), lead, and suspended particulate matter. In addition, the State has set standards for sulfates, hydrogen sulfide, vinyl chloride, and visibility-reducing particles. These standards are designed to protect the health and welfare of the populace with a reasonable margin of safety. Two criteria pollutants, O_3 and NO_2, are considered regional pollutants because they (or their precursors) affect air quality on a regional scale. Pollutants such as CO, SO_2, and lead are considered local pollutants that tend to accumulate in the air locally.

The primary pollutants of concern in the Project area are O_3, CO, and particulate matter. Significance thresholds established by an air district are used to manage total regional and local emissions within an air basin based on the air basin's attainment status for criteria pollutants. These emission thresholds were established for individual development projects that would contribute to regional and local emissions and could adversely affect or delay the air basin's projected attainment target goals for non-attainment criteria pollutants.

Because of the conservative nature of the significance thresholds and the basin-wide context of individual development project emissions, there is no direct correlation between a single project and localized air quality-related health effects. One individual project that generates emissions

[1] Criteria pollutants are defined as those pollutants for which the federal and State governments have established ambient air quality standards, or criteria, for outdoor concentrations in order to protect public health.

exceeding a threshold does not necessarily result in adverse health effects for residents in the project vicinity. This condition is especially true when the criteria pollutants exceeding thresholds are those with regional effects, such as O_3 precursors like nitrogen oxides (NO_X) and reactive organic gases (ROG).

Occupants of facilities such as schools, day-care centers, parks and playgrounds, hospitals, and nursing and convalescent homes are considered to be more sensitive than the general public to air pollutants because these population groups have increased susceptibility to respiratory disease. Persons engaged in strenuous work or exercise also have increased sensitivity to poor air quality. Residential areas are considered more sensitive to air quality conditions than commercial and industrial areas because people generally spend longer periods of time at their residences, with greater associated exposure to ambient air quality conditions. Recreational uses are also considered sensitive compared to commercial and industrial uses due to greater exposure to ambient air quality conditions associated with exercise.

3.3.2 Regulatory Setting

The Project is located in West Sacramento and is within the jurisdiction of the Yolo-Solano Air Quality Management District (YSAQMD), which regulates air quality throughout Yolo County and the northeast portion of Solano County. As shown in **Table 3.3-1: Yolo-Solano Air Quality Management District Air Quality Attainment Status**, the YSAQMD is under State nonattainment status for the O_3 and particulate matter less than or equal to 10 microns in diameter (PM_{10}) standards. The YSAQMD is classified as nonattainment for the federal O_3 8-hour and 1-hour standards and nonattainment for the federal particulate matter less than or equal to 2.5 microns in diameter ($PM_{2.5}$) standard.

Table 3.3-1: Yolo-Solano Air Quality Management District Air Quality Attainment Status

Pollutant	Federal	State
Carbon Monoxide	Attainment	Attainment
Lead	Attainment	Attainment
Nitrogen Dioxide	Attainment	Attainment
Particulate Matter (PM_{10})	Attainment	Non-attainment
Particulate Matter ($PM_{2.5}$)	Non-attainment	Non-attainment
Ozone (8-hour)	Non-attainment	Non-attainment
Ozone (1-hour)	Non-attainment	Non-attainment
Sulfur Dioxide	Attainment	Attainment

Source: Yolo-Solano Air Quality Management District (2016).

Air quality standards for the proposed Project are regulated by the YSAQMD's *Handbook for Assessing and Mitigating Air Quality Impacts* (YSAQMD 2007). **Table 3.3-2**: **Thresholds of Significance for Criteria Pollutants of Concern** shows the project-level thresholds of significance as established by the YSAQMD for PM_{10}, CO, ROG, and NO_X. The thresholds apply to both construction and operational impacts.

Table 3.3-2: Thresholds of Significance for Criteria Pollutants of Concern

Pollutant	Thresholds of Significance
ROG	10 tons per year
NO_X	10 tons per year
PM_{10}	80 pounds per day
CO	Violation of a state ambient air quality standard for CO

Source: Yolo-Solano Air Quality Management District (2007).
CO = carbon monoxide
NO_X = nitrogen oxides
PM_{10} = particulate matter less than or equal to 10 microns in diameter
ROG =reactive organic gases

3.3.3 Discussion

a. Would the Project conflict with or obstruct implementation of the applicable air quality plan?

Less than Significant Impact. The Project is located within the jurisdiction of the YSAQMD, which is part of the Sacramento Federal Non-Attainment Area as designated by the United States Environmental Protection Agency (USEPA). Accordingly, the City of West Sacramento is included in the Sacramento Regional State Implementation Plan (SIP), which was prepared by the Sacramento Metropolitan Air Quality Management District (SMAQMD) in conjunction with the YSAQMD. The Air Quality Attainment Plans applicable to the Project site are the SMAQMD's 8-Hour Ozone Plan (SMAQMD 2013a) and $PM_{2.5}$ Implementation/Maintenance Plan (SMAQMD 2013b).

The 8-Hour Ozone Plan demonstrates how existing and new control strategies will provide the necessary future emission reductions to meet the federal Clean Air Act requirements for reasonable further progress and attainment of the 1997–1998 O_3 National Ambient Air Quality Standards (NAAQS) for the Sacramento region. The $PM_{2.5}$ Plan shows that the region has met the redesignation requirements and requests that the USEPA redesignate the area to attainment. The plan also analyzes measures that were implemented to achieve attainment and that will provide for maintenance of the $PM_{2.5}$ NAAQS.

As indicated in the analysis that follows, the proposed Project would result in less than significant operational and construction-period emissions. Therefore, the proposed Project supports the goals of the applicable AQAPs and SIP and would not conflict with any measures identified in the plans or designed to bring the region into attainment. The proposed Project would not hinder the region from attaining the goals outlined in the AQAPs or SIP. The proposed Project would not hinder or disrupt implementation of the applicable AQAPs or SIP; as such, impacts would be **less than significant.**

b. Would the Project violate any air quality standard or contribute substantially to an existing or projected air quality violation?

Less than Significant Impact with Mitigation. The following provides a discussion on potential construction and operational impacts to air quality due to implementation of the proposed Project.

Construction Impacts. The proposed Project could result in temporary air quality impacts as ground disturbance occurs from installation of the new water main, laterals, and water meters. Given the nature of the proposed Project and anticipated activities, daily emissions from construction equipment operation, vehicles transporting equipment and workers, and hauling materials would be minimal. These emissions would be temporary and limited to the immediate area around the Project site, and would not be anticipated to result in an exceedance of construction-level thresholds established by the YSAQMD. Off-road equipment used at the Project site would be required to adhere to the statewide In-Use Off-Road Diesel-Fueled Fleets Regulation, including limits on idling of all construction equipment to 5 minutes or less. Additionally, all portable equipment with internal combustion engines over 50 horsepower would obtain a YSAQMD Permit to Operate or a valid statewide Portable Equipment Registration Program issued by the California Air Resources Board (CARB).

A quantitative analysis of construction emissions for the proposed Project using the Road Construction Emissions Model (RoadMod), Version 8.1.0, has been conducted. Emissions are compared to the YSAQMD's ROG, NO_X, and PM10 thresholds. **Table 3.3-3: Project Construction Emissions** shows that emissions of ROG, NO_X, and PM_{10} would not be exceeded due to contribution of construction emissions associated with the proposed Project.

Table 3.3-3: Project Construction Emissions

	ROG (tons/year)	NO_X (tons/year)	PM_{10} (maximum lbs/day)	$PM_{2.5}$ (maximum lbs/day
Projected Construction Emissions	0.45	4.68	24.29	8.05
YSAQMD Thresholds of Significance	10.0	10.0	80.0	NA
Exceeds Threshold?	**No**	**No**	**No**	**No**

Source: LSA June 2017.
lbs/day = pounds per day
NO_X = nitrogen oxides
$PM_{2.5}$ = particulate matter less than or equal to 2.5 microns in diameter
PM_{10} = particulate matter less than or equal to 10 microns in diameter
ROG =reactive organic gases
YSAQMD = Yolo-Solano Air Quality Management District

In addition to exhaust emissions, the effects of construction activities would include increased dust and locally elevated levels of particulate matter downwind of construction areas. According to the YSAQMD's *CEQA Handbook* (YSAQMD 2007), even projects not exceeding district thresholds should implement best management practices (BMP) to reduce fugitive dust emissions and avoid localized health impacts. Common measures include watering, chemical stabilization of soils or stockpiles, and reducing surface wind speeds with windbreaks. Implementation of **Mitigation Measure MM AQ-1**, as identified below, would ensure compliance with YSAQMD-recommended BMPs for fugitive dust control and would reduce impacts. As such, with implementation of **Mitigation Measure MM AQ-1**, impacts would be **less than significant**.

Operational Impacts. Long-term air emission impacts are associated with stationary and mobile sources. Stationary source emissions typically result from the consumption of natural gas and

electricity. Mobile source emissions typically result from vehicle trips and result in air pollutant emissions affecting the entire air basin. As discussed above, the proposed Project includes the installation of new water mains, laterals, and water meters. Once operational, the Project would not result in an increase in the generation of vehicle trips that would increase air pollutant emissions. The Project would not be a source of stationary source emissions. Therefore, operation of the proposed Project would not result in any long-term operational emissions. Impacts related to operation of the proposed Project would therefore be **less than significant**.

Localized CO Impacts. According to the YSAQMD's *CEQA Handbook,* a screening-level approach originally developed by the San Joaquin Valley Air Pollution Control District can be used to estimate whether or not a project's traffic impact would cause a potential CO hotspot at any given intersection. If either of the following criteria is true of any intersection affected by project traffic, the project can be said to have the potential to create a violation of the CO standard:If a traffic study prepared for a project indicates that the peak-hour level of service (LOS) on one or more streets or at one or more intersections in the vicinity of said project would be reduced to an unacceptable LOS (typically LOS E or F).

- A traffic study prepared for a project indicates that the project would substantially worsen an existing peak-hour LOS F on one or more streets or at one or more intersections in the project vicinity. "Substantially worsen" includes situations where delay would increase by 10 seconds or more when project-generated traffic is included.

As discussed above, the proposed Project would include the installation of water mains, laterals, and water meters. Once operational, the proposed Project would not result in an increase in vehicle trips and therefore would not exceed the YSAQMD's CO hotspot screening criteria. The proposed Project would not result in localized impacts, including localized CO impacts, and impacts would therefore be **less than significant**.

c. Would the Project result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non- attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?

Less Than Significant Impact. CEQA defines a cumulative impact as two or more individual effects that, when considered together, are considerable or compound or increase other environmental impacts. According to the YSAQMD, project emissions that are not consistent with the AQAPs or SIP, or that exceed YSAQMD thresholds of significance will have a significant cumulative impact unless offset.

As described above in Thresholds a) and b), construction and operation of the proposed Project would not conflict with the applicable AQAPs or SIP, or result in significant levels of criteria pollutants or pollutant precursors. Therefore, the Project would not result in a cumulatively considerable contribution to regional air quality impacts, and this impact would be **less than significant**.

d. Would the Project expose sensitive receptors to substantial pollutant concentrations?

Less Than Significant Impact. According to the YSAQMD's *CEQA Handbook* (YSAQMD 2007), a sensitive receptor is generically defined as a location where human populations (especially children, seniors, or sick persons) are found, and where there is a reasonable expectation of continuous human exposure according to the averaging period for the NAAQS (e.g., 24-hour, 8-hour, 1-hour). Examples of sensitive receptors include residences, hospitals, and schools.

In 1998, the CARB identified particulate matter from diesel-fueled engines as a toxic air contaminant (TAC). The CARB has completed a risk management process that identifies potential cancer risks for a range of activities using diesel-fueled engines (CARB 2000). High-volume freeways, stationary diesel engines, and facilities attracting heavy and constant diesel vehicle traffic (e.g., distribution centers and truck stops) were identified as having the highest associated risk.

Health risks from TACs are a function of both concentration and duration of exposure. Exposure of receptors to substantial concentrations of TACs and $PM_{2.5}$ could occur from the following situations:

- Siting a new TAC and/or $PM_{2.5}$ source (e.g., diesel generator, truck distribution center, or freeway) near existing or planned receptors; and
- Siting a new receptor near an existing source of TAC and/or $PM_{2.5}$ emissions.

Construction Impacts. During construction, various diesel-powered vehicles and equipment would be in use. Unlike freeways and stationary sources, construction diesel emissions are temporary, affecting an area for a period of days or perhaps weeks, whereas health risks are based on a 70-year risk duration. Additionally, construction-related emissions sources are mobile and transient in nature, and the emissions occur within the Project site. Utility replacement projects are also typically linear in nature, with construction only occurring at one location for a few days before moving on to another location, thereby exposing each receptor to a fraction of the construction duration.

The proposed Project is located in a developed neighborhood of West Sacramento with single-family residential units located directly adjacent to the roads where Project implementation will occur. Construction activities occurring on the road ROW within the Project site may expose these residents to airborne particulates and fugitive dust, as well as a small quantity of pollutants associated with the use of construction equipment (e.g., diesel-fueled vehicles and equipment). However, given the short duration of construction activities relative to the 70-year health risk exposure analysis period and the minimal nature of construction activities for the proposed Project, these sensitive receptors would not be exposed to substantial pollutant concentrations. In addition, construction contractors would be required to implement the BMPs required in **Mitigation Measure MM AQ-1**, which would reduce construction emissions. Therefore, health risks associated with construction of the proposed Project would be **less than significant** with implementation of mitigation.

Operational Impacts. The proposed Project includes the installation of new water mains, laterals, and water meters (the existing water infrastructure will be abandoned in place). Once operational,

the Project would not increase emissions of TACs or expose new sensitive receptors to TAC or $PM_{2.5}$ emissions. Therefore, health risks associated with operation of the proposed Project would be **less than significant**.

e) Would the Project create objectionable odors affecting a substantial number of people?

Less Than Significant Impact. Some objectionable odors may be generated from the operation of diesel-powered construction equipment and/or vehicles during the Project construction period. Additionally, during the short-term construction period, odors may occur related to decaying organic material disturbed during the excavation process to install the new water mains, laterals, and meters. However, these odors would be short-term in duration, would disperse quickly, and would not result in long-term impacts to the nearby sensitive receptors. Long-term operation of the proposed Project would not generate any new vehicle trips or be a permanent source of odors; therefore, increases in permanent odors would not result from Project operation. Impacts would be **less than significant**.

3.3.4 Mitigation Measures

The following mitigation measure would be implemented to reduce impacts under Threshold b), above:

MM AQ-1: The City of West Sacramento (City) or construction contractor shall implement the following measures at the Project site:

- Water all active construction sites at least twice daily. The frequency shall be based on the type of operation, soil, and wind exposure.
- Haul trucks shall maintain at least 2 feet of freeboard.
- Cover all trucks hauling dirt, sand, or loose materials.
- Apply nontoxic binders (e.g., latex acrylic copolymer) to exposed areas after cut and fill operations and hydroseed the area as applicable.
- Apply chemical soil stabilizers on inactive construction areas (disturbed lands within construction projects that are unused for at least 4 consecutive days).
- Plant tree windbreaks on the windward perimeter of construction projects if adjacent to open land.
- Plant vegetative ground cover in disturbed areas as soon as possible.
- Cover inactive storage piles.
- Sweep streets if visible soil material is carried out from the construction site.
- Treat accesses to a distance of 100 feet from the paved road with a 6- to 12-inch layer of wood chips or mulch or with a 6-inch layer of gravel.

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3.4 BIOLOGICAL RESOURCES

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	☐	☒	☐	☐
b. b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or US Fish and Wildlife Service?	☐	☐	☐	☒
c. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	☐	☐	☐	☒
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	☐	☐	☐	☒
e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	☐	☐	☐	☒
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	☐	☐	☐	☒

3.4.1 Environmental Setting

The Project site is located in an established neighborhood (Northeast Village) of West Sacramento. A portion of the proposed Project extends into the Liberty Specific Plan area, which has already been environmentally cleared through approval of an EIR in August 2017. As such, the Biological Resources section of the Liberty Specific Plan EIR was used to evaluate the portion of the Project that would extend into the site.

3.4.1.1 Habitat/Land Cover

The Project site is located in California's Central Valley, which is characterized by large flat areas of agricultural farmland and development. The proposed Project site consists primarily of an existing urban neighborhood but also includes a narrow sliver of adjacent agricultural land.

The Project site is located in an existing neighborhood and is mainly developed, including established roadways (Linden Road and all neighborhood roads), sidewalks, parking areas, and one

park. There is an abundance of trees within the Project site. The narrow area of agricultural land within the Project site consists of row crops.

3.4.1.2 Sensitive Species

A list of sensitive wildlife and plant species potentially occurring within the Project site was compiled to evaluate the potential impacts resulting from Project construction. Sources used to compile the list include the California Natural Diversity Database (CNDDB), the United States Fish and Wildlife Service (USFWS) online special-status species list, and the California Native Plant Society (CNPS) Online Edition. The species lists obtained from the CNDDB, CNPS, and USFWS, as well as the Biological Resources section of the Liberty Specific Plan EIR, were reviewed to determine which species could potentially occur on the Project site.

No special-status plants are expected to occur on the Project site, and the Project site has no suitable habitat for special-status fish species.

The following 13 special-status wildlife species were identified as having a moderate to high potential to occur within the Liberty Specific Plan study area and were thus reviewed for their potential to occur within the Project site as a whole: valley elderberry longhorn beetle (*Desmocerus californicus dimorphus*), western pond turtle (*Actinemys marmorata*), giant garter snake (*Thamnophis gigas*), tricolored blackbird (*Agelaius tricolor*), western burrowing owl (*Athene cunicularia hypugaea*), Swainson's hawk (*Buteo swainsoni*), northern harrier (*Circus cyaneus*), white-tailed kite (*Elanus leucurus*), loggerhead shrike (*Lanius ludovicianus*), song sparrow (Modesto population) (*Melospiza melodia*), purple martin (*Progne subis*), pallid bat (*Antrozous pallidus*), and western red bat (*Lasiurus blossevillii*).

There is no suitable habitat on the Project site for three of the species listed above:

- valley elderberry longhorn beetle (there are no elderberry shrubs present within the site)
- western pond turtle (there are no aquatic features present within the site)
- giant garter snake (there are no aquatic features present within the site)

For four of the bird species listed above, suitable foraging habitat is located on the Project site, but there is no suitable nesting habitat present.

- tricolored blackbird (there are no emergent wetlands or thorny brambles near water within the site)
- northern harrier (there are no emergent wetlands or tall grasslands within the site)
- song sparrow (Modesto population) (there are no emergent wetlands or thorny brambles near water within the site)
- purple martin (there are no old-growth trees or freeway overpasses within the site)

The remaining six special-status wildlife species could potentially nest or roost on the Project site:

- western burrowing owl (agricultural fields and ruderal areas with small mammal burrows provide suitable nesting and foraging habitat within the Project site)
- Swainson's hawk (agricultural fields with scattered trees provide suitable nesting and foraging habitat within the Project site)
- white-tailed kite (agricultural fields with scattered trees provide suitable nesting and foraging habitat within the Project site)
- loggerhead shrike (agricultural fields with scattered trees provide suitable nesting and foraging habitat within the Project site)
- pallid bat (trees throughout the Project site provide potential roosting habitat)
- western red bat (trees throughout the Project site provide potential roosting habitat)

3.4.1.3 Wildlife Movement Corridors

Wildlife movement corridors are linear habitats that function to connect two or more areas of significant wildlife habitat. These corridors may function on a local level as links between small habitat patches (e.g., streams in urban settings) or may provide critical connections between regionally significant habitats (e.g., deer movement corridors). No evidence of a substantial wildlife movement corridor was identified within the Project site.

3.4.1.4 Aquatic Resources

There are no aquatic resources within the Project site. The Sacramento River is within 0.25 mile of the Project site, but there are no aquatic features connecting the river to the site. The Project will not encroach into the Sacramento River corridor.

3.4.2 Discussion

a. Would the Project have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Less than Significant Impact with Mitigation. The Project activities would occur primarily within existing roadways located in the Northeast Village subdivision, except for an approximately 700-foot section of water main located in the adjacent agricultural field at the southeast corner of the subdivision. Per the Liberty Specific Plan EIR (City of West Sacramento Community Development Department 2017), suitable burrows for burrowing owl were identified in the agricultural field adjacent to Northeast Village; however, it is unknown whether suitable burrows occur at or near the location of the 700-foot section of water main associated with the proposed Project. Consequently, it cannot be concluded that this species could not occur within the Project site; therefore, this

species could potentially be affected by the Project if present. Implementation of **Mitigation Measure MM-BIO-1** would reduce potential impacts to burrowing owl to **less than significant.**

Installation of the approximately 700-foot water line in the agricultural field would result in a temporary loss of potential foraging habitat for several bird species; however, since the impact would be temporary, no mitigation is proposed. No trees would be removed during construction; therefore, nesting birds and roosting bats would not be affected.

b. Would the Project have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or US Fish and Wildlife Service?

No Impact. There are no sensitive natural communities within the Project site. The Sacramento River, located to the east of the Project site, provides both sensitive terrestrial and aquatic habitats; however, the Project would not encroach into the Sacramento River corridor. The primary habitats in the Project area, not including paved surfaces or structures, consist of urban residential landscaping, agricultural, and ruderal/disturbed areas. **No impact** would occur.

c. Would the Project have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

No Impact. There are no aquatic features within the Project site. As such, **no impact** would occur with implementation of the proposed Project.

d. Would the Project interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

No Impact. No evidence of substantial wildlife movement corridors was identified in the Project site. The Project area is heavily impacted by human activity (existing urban development, agriculture, traffic, etc.) and provides no connectivity with natural habitat in the vicinity. As such, **no impact** would occur with implementation of the proposed Project.

e. Would the Project conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?

No Impact. The only local policy that may be applicable to the proposed Project is the West Sacramento Tree Preservation Ordinance described in Chapter 8.24 of the West Sacramento Municipal Code. The Project would not remove any trees during construction of the Project; as such, **no impact** would occur.

f. Would the Project conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?

No Impact. There are no Habitat Conservation Plans or Natural Community Conservation Plans applicable to the Project. As such, implementation of the proposed Project would not conflict with any Habitat Conservation Plans, Natural Community Conservation Plans, or local/regional/State Habitat Conservation Plans. No impacts would occur.

3.4.3 Mitigation Measures

The following mitigation measure shall be implemented to reduce potential impacts to burrowing owls during Project construction.

MM-BIO-1: The measures listed below shall be implemented to mitigate potential impacts to western burrowing owl:

- Preconstruction surveys for western burrowing owl shall be conducted by a qualified biologist in accordance with the California Department of Fish and Wildlife's (CDFW) 2012 Staff Report on Burrowing Owl Mitigation.
- If burrowing owls are identified during the preconstruction survey, passive exclusion shall be implemented per CDFW's 2012 Staff Report on Burrowing Owl Mitigation (including avoidance of occupied burrows during the breeding season [February 1 to August 31]).
- Following construction, all areas temporarily impacted during Project construction shall be restored to pre-construction contours (if necessary) and revegetated with native species as specified in the table below:

Native Species Mix

Scientific Name	Common Name	Rate (lbs/acre)	Minimum Percent Germination
Artemisia douglasiana	Mugwort	2.0	50
Bromus carinatus carinatus	California brome	5.0	85
Elymus trachycaulus	Slender wheatgrass	2.0	60
Elymus X triticum	Regreen	10.0	80
Eschscholzia californica	California poppy	2.0	70
Hordeum brachyantherum	California barley	2.0	80
Lupinus bicolor	Bicolored lupine	4.0	80

lbs/acre = pounds per acre

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3.5 CULTURAL RESOURCES

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?	☐	☒	☐	☐
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	☐	☒	☐	☐
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	☐	☒	☐	☐
d) Disturb any human remains, including those interred outside of formal cemeteries?	☐	☒	☐	☐

3.5.1 Environmental Setting

CEQA applies to all discretionary projects undertaken or subject to approval by the State's public agencies. CEQA states that it is the policy of the State of California to "take all action necessary to provide the people of this state with… historic environmental qualities… and preserve for future generations examples of the major periods of California history". Under the provisions of CEQA, "A project with an effect that may cause a substantial adverse change in the significance of an historical resource is a project that may have a significant effect on the environment."

CEQA requires that historical resources and unique archaeological resources be taken into consideration during the CEQA planning process. If feasible, adverse effects to the significance of historical resources must be avoided or mitigated. CEQA requires that all feasible mitigation be undertaken even if it does not mitigate impacts to a less than significant level.

3.5.1.1 Literature Review

Publications, maps, and aerial photographs were reviewed for archaeological, ethnographic, historical, and environmental information about the Project site and vicinity. The purpose of this review was to: (1) identify cultural resources within the Project site, and (2) identify the potential for the Project site to contain such resources.

Paleontological and geological literature relevant to the Project site and vicinity were also reviewed. The Project site is situated on a Late Holocene Basin (2,000 to 150 years old). Holocene Basin deposits are made up of thick deposits of mixed alluvium put in place during seasonal flooding episodes and stream deposition from the Sacramento River. These deposits include terrace, valley, and floodplain deposits and are made up of mixed sediments including sand, silt, and clay derived from sedimentary, igneous, and metamorphic sources. Holocene-aged deposits are considered to have very high sensitivity for buried archaeological sites. The Project site is further underlain by Pliocene Orinda Formation deposits (2.6 to 5.3 million years old) and Pleistocene to Late Pliocene Santa Clara Formation deposits (10,000 to 5.3 million years old). The Orinda Formation and Santa Clara Formation are known to contain fossils.

Literature and archival review identified two previously recorded cultural resources within the Project site: one prehistoric archaeological site (CA-YOL-18) and one historic-period built environment cultural resource (P-57-001144). See below for a description of these resources.

Field Review. A field review of the Project site was conducted on February 1, 2018, to identify any cultural resources in the Project site and also to ensure that the most current baseline conditions of the one built-environment historical resource within the Project site was considered.

LSA archaeologists conducted a presence/absence excavation to support this study on February 1, and 8, 2018. The purpose of the excavation was to determine whether the site, CA-YOL-18, extended into the Project's Area of Direct Impact. Excavation included six trenches and six hand augers. Trenches were excavated by backhoe equipment with a 0.6-meter (24-inch) bucket. The trenches were distributed within the Area of Direct Impact closest to the estimated location of CA-YOL-18. All trenching activity was monitored by two archaeologists and one to two tribal cultural monitors. Soil was removed 10 to 20 centimeters (cm) at a time, dispersed on the ground surface for inspection by the archaeologists and tribal monitor(s), and portion-sampled through 0.6 cm (0.25-inch) mesh screens. LSA also conducted hand auger excavations within the City-owned road ROW within the Project site. A total of six hand auger excavations were distributed along the proposed water main and laterals in those areas most likely to contain deposits associated with CA-YOL-18. Each auger bore measured 10 cm (3.9 inches) in width and was excavated to a depth of 140 cm below surface. From 140 to 157 cm below surface, auger bores measured 5 cm (1.9 inches) in width. Soil was removed 10 to 20 cm at a time and was screened and inspected by tribal cultural monitors. One trench identified a butchered cow bone and a basalt flake within mixed fill. One auger identified a butchered pig bone fragment that may be modern or historic. No intact cultural resource deposits or archaeological features were identified during the archaeological testing.

3.5.1.2 Consultation and Outreach

Native American Heritage Commission. On June 6, 2017, LSA emailed a letter describing the Project and a map depicting the Project site to the Native American Heritage Commission (NAHC) in Sacramento requesting a review of its Sacred Lands File for any Native American cultural resources that might be affected by the proposed Project. The NAHC is the official State repository of Native American sacred site location records in California. On June 12, 2017, NAHC Staff Services Analyst Sharaya Souza responded via email, saying that "Archaeological sites and Tribal Cultural Resources were identified in the project areas provided" and suggesting that LSA contact the Native American tribes listed in the attached response letter for more information about the resources.

Native American Tribal Organizations. The City conducted initial tribal outreach for the Project on June 12, 2017. The purpose of this outreach was to identify sites of Native American interest or concern that may be impacted by the proposed Project and to solicit opinions for avoiding or mitigating potential impacts to such sites. The City maintains a notification list for the Project area of all tribes that have requested to be consulted pursuant to Public Resources Code (PRC) 21080.3.1(b)(1) and Chapter 532 Statutes of 2014 (i.e., Assembly Bill [AB] 52). Two Native American tribal groups are currently on the City's Notice List. LSA, on behalf of the City, mailed a letter describing the Project, the Project location, known cultural and/or historic records, City contact information, and a map indicating the approximate location of the Project site.

On June 27, 2017, LSA made follow-up telephone calls to those tribes since no response was received. The following is a summary of the Native American coordination conducted to date:

- **Gene Whitehouse, Chairperson, United Auburn Indian Community of the Auburn Rancheria (UAIC):** LSA called Chairperson Whitehouse on June 27, 2017. The UAIC receptionist, Pam, took a message and said she would provide the information to his assistant, who would then relay the information to Chairperson Whitehouse. On July 6, 2017, Cherilyn Neider called LSA, saying that UAIC's records did not show receipt of the original letter and requesting a Project schedule and geographic information system shapefile of the Project area. LSA emailed her the original letter sent on June 12, 2017, to Chairperson Whitehouse and relayed Ms. Neider's requests to the City. UAIC, the City, and LSA met on August 23, 2017, to conduct a formal consultation meeting. UAIC provided a sensitivity map of the area and requested the Archaeological Work Plan, which LSA emailed to Melodi McAdams the next day. Ms. McAdams provided comments on the work plan on September 21, 2017. LSA continued to coordinate with UAIC regarding the archaeological excavation. UAIC provided a monitor during the excavation conducted on February 1 and 8, 2018.

- **Leland Kinter, Chairperson, Yocha Dehe Wintun Nation (YDWN):** LSA called Mr. Kinter on June 27, 2017. The YDWN receptionist, Susan, answered the phone and patched LSA to the voicemail of Deb Jones, where LSA left a message for Chairperson Kinter. On September 5, 2017, LSA coordinated with YDWN's Cultural Resources Manager, James Sarmento, via email and telephone, and confirmed a date for a consultation meeting. YDWN, the City, and LSA met on September 21, 2017, to conduct a formal consultation meeting. LSA continued to coordinate with YDWN regarding the archaeological test excavation conducted on February 1 and 8, 2018, for which YDWN provided a monitor.

West Sacramento Historical Society. On June 21, 2017, LSA sent a letter describing the Project and maps depicting the Project site to the West Sacramento Historical Society (Historical Society) to solicit information or concerns about cultural resources in the Project site. LSA conducted a follow-up telephone call with the Historical Society on June 27, 2017, and left a voicemail message, again asking for any information or concerns it may have regarding the Project. Tom Lewis returned LSA's telephone call on June 30, 2017. Mr. Lewis said the Historical Society had received the letter and maps from LSA and indicated that it was being kept abreast of the Project through one of its members who is now working for UAIC. Mr. Lewis thanked LSA for keeping the Historical Society "in the loop" during this process.

City of West Sacramento. LSA coordinated with the City to determine the extent of investigations conducted for the Liberty Specific Plan, which is adjacent to the Project site. A small portion of the Project site was previously investigated by Peak and Associates (Peak and Associates 2016). This firm's study included a field survey and extensive trenching efforts to identify whether CA-YOL-18 extended into its project site. Its findings were negative.

Paleontological Resources. Project plans, geologic maps of the project site, and relevant geological and paleontological literature were reviewed to determine which geologic units are present within the project site and whether fossils have been recovered within the project site or from those or similar geologic units elsewhere in the region. A search for known fossil localities was also

conducted through the online collections database of the University of California, Museum of Paleontology (UCMP) at the University of California, Berkeley, in order to determine the status and extent of previously recorded paleontological resources within and surrounding the project site (UCMP 2017).

Paleontological Sensitivity. Geologic mapping by Gutierrez (2011) indicates the project site contains Holocene (less than 11,700 years ago) Alluvium and late Holocene (Basin Deposits. The Holocene Alluvium consists of poorly to moderately sorted gravel, sand, and silt deposited on fans, on terraces, or in basins (Gutierrez 2011). The Holocene Basin Deposits consist of fine-grained material (e.g., fine-grained sand, silt, and clay) that was deposited by standing or slow moving water (Gutierrez 2011). Although Holocene deposits can contain remains of plants and animals, only those from the middle to early Holocene (4,200 to 11,700 years ago; Walker et al. 2012) are considered scientifically important (Society of Vertebrate Paleontology 2010). Scientifically important fossils from middle to early Holocene deposits are not very common, and the UCMP has no records of vertebrate fossil localities from Holocene deposits within or surrounding the project site. However, Pleistocene (11,700–2.588 million years ago) sediments, which may be encountered beneath the Holocene Alluvium and the Holocene Basin Deposits at depths of approximately 20 feet or more, have produced a variety of scientifically important fossils elsewhere in the County and the region. These fossils include large and small mammals, reptiles, fish, invertebrates, and plants (Jefferson 1991a, 1991b). According to the locality search through the UCMP online collections database, there are five known localities from Pleistocene deposits within the County. These localities have produced 126 fossil specimens, including large and small mammals, such as mammoth (*Mammuthus columbi*), bison (*Bison*), horse (*Equus*), giant ground sloth (*Glossotherium harlani*), camel (*Camelops hesternus*), dire wolf (*Canis dirus*), coyote (*Canis latrans*), rabbits (*Sylvilagus*), and various rodents (*Thomomys, Neotoma, Microtus, Spermophilus, Reithrodontomys, Scapanus latimus*), as well as snakes (*Thamnophis*), amphibians (*Rana*, *Scaphiopus*), and birds (Neornithes). Because there is a potential to find these types of fossils in the older sediments beneath the Holocene Alluvium and Holocene Basin Deposits at depths of approximately 20 feet or more, the deposits within the project site are considered to have low paleontological sensitivity from the surface to a depth of 20 feet and a high sensitivity below that mark.

The project is located in a previously disturbed area, and ground disturbance is not expected to extend below a depth of 10 feet. Therefore, the project is unlikely to impact scientifically important paleontological resources. In the unlikely event that fossil remains are encountered, paleontological mitigation will need to be developed. This mitigation would include paleontological monitoring; collection of observed resources; preservation, stabilization, and identification of collected resources; curation of resources into a museum repository; and preparation of a monitoring report of findings.

3.5.2 Regulatory Setting

CEQA, relevant sections of the PRC and Section 7050.5 of the California Health and Safety Code make up the regulatory framework for cultural resources on the Project site.

3.5.2.1 California Environmental Quality Act

CEQA applies to all discretionary projects undertaken or subject to approval by the State's public agencies (California Code of Regulations [CCR] Title 14, §15022(i)). CEQA states that it is the policy of the State of California to "take all action necessary to provide the people of this state with… historic environmental qualities… and preserve for future generations examples of the major period of California history" (PRC §21001(b),(c)). Under the provisions of CEQA, "A project with an effect that may cause a substantial adverse change in the significance of an historical resource is a project that may have a significant effect on the environment" (14 CCR §15126.4 (a)(1)).

CEQA requires that historical resources and unique archaeological resources be taken into consideration during the CEQA planning process (14 CCR §15064.5; PRC §21083.2). If feasible, adverse effects to the significance of historical resources must be avoided or mitigated (14 CCR §15064.5(b)(4)). CEQA requires that all feasible mitigation be undertaken even if it does not mitigate impacts to a less than significant level (14 CCR §15126.4 (a)(1)).

Historical Resources. The term CEQA uses for significant cultural resources is "historical resource," which is defined as any resource that meets one or more of the following criteria:

- Listed in, or eligible for listing in, the California Register of Historical Resources (California Register);
- Listed in a local register of historical resources (as defined at PRC §5020.1(k));
- Identified as significant in a historical resource survey meeting the requirements of PRC §5024.1(g); or
- Determined to be a historical resource by a project's Lead Agency (14 CCR §15064.5(a)).

A historical resource consists of "Any object, building, structure, site, area, place, record or manuscript which a Lead Agency determines to be historically significant or significant in the architectural, engineering, scientific, economic, agricultural, educational, social, political, military, or cultural annals of California."

Unique Archeological Resources. As defined in PRC §21083.2(g), a unique archaeological resource is defined as "an archaeological artifact, object, or site about which it can be clearly demonstrated that, without merely adding to the current body of knowledge, there is a high probability that it meets any of the following criteria:

1. Contains information needed to answer important scientific research questions and that there is a demonstrable public interest in that information.
2. Has a special and particular quality such as being the oldest of its type or the best available example of its type.
3. Is directly associated with a scientifically recognized important prehistoric or historic event or person."

3.5.2.2 Tribal Cultural Resources

AB 52, which became law on January 1, 2015, provides for consultation with California Native American tribes during the CEQA process and equates significant impacts to "tribal cultural resources" with significant environmental impacts. PRC §21074 states that "tribal cultural resources" are:

- Sites, features, places, cultural landscapes, sacred places, and objects with cultural value to a California Native American tribe and are one of the following:

    A. Included or determined to be eligible for inclusion in the California Register of Historical Resource.

    B. Included in a local register of historical resources as defined in subdivision (k) of PRC §5020.1.

    C. A resource determined by the Lead Agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of PRC §5024.1. In applying the criteria set forth in subdivision (c) of PRC §5024.1 for the purposes of this paragraph, the Lead Agency shall consider the significance of the resource to a California Native American tribe.

    D. The consultation provisions of the law require that within 14 days of determining that a project application is complete, or a decision by a public agency to undertake a project, the Lead Agency must notify tribes of the opportunity to consult on the project. California Native American tribes must be recognized by the Native American Heritage Commission as traditionally and culturally affiliated with the project site, and must have previously requested that the Lead Agency notify them of projects. Tribes have 30 days following notification of a project to request consolation with the Lead Agency.

The purpose of consultation is to inform the Lead Agency in its identification and determination of the significance of tribal cultural resources. Consultation may also include a discussion of project alternatives, significant effects, and mitigation measures, and should be undertaken in good faith by both the tribe and Lead Agency. If a project is determined to result in a significant impact to an identified tribal cultural resource, the consultation process must occur and conclude prior to adoption of a Negative Declaration or MND, or the certification of an EIR (PRC §21080.3.1, §21080.3.2, and §21082.3).

3.5.2.3 Public Resources Code 5024.1: California Register of Historical Resources

Section 5024.1 of the PRC established the California Register. Generally, a resource is considered by the Lead Agency to be "historically significant" if the resource meets the criteria for listing on the California Register (14 CCR §15064.5(a)(3)). For a cultural resource to qualify for listing in the California Register, it must be significant under one or more of the following criteria:

Criterion 1: Associated with events that has made a significant contribution to the broad patterns of California's history and cultural heritage;

Criterion 2: Associated with the lives of persons important in our past;

Criterion 3: Embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of an important creative individual, or possesses high artistic values; or

Criterion 4: Has yielded, or may be likely to yield, information important in prehistory or history.

In addition to being significant under one or more criteria, a resource must retain enough of its historic character and appearance to be recognizable as a historical resource and also be able to convey the reasons for its significance (14 CCR §4852(c)). Generally, a cultural resource must be 50 years old or older to qualify for the California Register.

3.5.2.4 California Health and Safety Code 7050.5

Section 7050.5 of the California Health and Safety Code states that in the event of discovery or recognition of any human remains in any location other than a dedicated cemetery, there shall be no further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent remains until the coroner of the county in which the remains are discovered has determined whether or not the remains are subject to the coroner's authority. If the human remains are of Native American origin, the coroner must notify the NAHC within 24 hours of this identification.

3.5.3 Discussion

a. Would the Project cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?

Less Than Significant Impact with Mitigation. One eligible historical resource was identified through field surveys, background research, tribal consultation, and excavation:

- P-57-000021/CA-YOL-18 is a pre-contact period burial mound site.

For purposes of this Project, the City as the Lead CEQA Agency considers the CA-YOL-18 site to be eligible for listing in the California Register under Criterion 4 due to its ability to yield information important in prehistory. This assessment is based on previous archaeological surveys conducted at this site that have identified flaked stone, stone tools, shell beads, and human remains. These materials and human remains have the potential to provide information on ancestral Patwin lifeways. This information can address questions related to ancestral Patwin chronology and cultural history; subsistence and settlement behaviors; technology; and social interaction and exchange. The location of CA-YOL-18 was delineated based on information compiled from the original site record, archival research, a review of historic aerials and maps, and communications with local residents.

Although no intact archaeological features or deposits associated with CA-YOL-18 were identified in the Area of Direct Impact during the archaeological testing completed by LSA, due to the archaeological sensitivity of the area, previously unidentified archaeological resources and human remains may be encountered during Project construction. Implementation of **Mitigation Measure MM-CUL-1** would reduce potential impacts to CA-YOL-18 to **less than significant**.

b. Would the Project cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?

Less Than Significant Impact with Mitigation. As discussed above, the City as the Lead CEQA Agency considers the CA-YOL-18 site to be eligible for listing in the California Register under Criterion 4 for the purposes of this Project. Although no intact archaeological features or deposits associated with CA-YOL-18 were identified during the archaeological testing completed by LSA, due to the archaeological sensitivity of the area, previously unidentified archaeological resources and human remains may be encountered during Project construction. Implementation of Mitigation Measure **MM-CUL-1** would reduce potential impacts to CA-YOL-18 to **less than significant**.

c. Would the Project directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

Less Than Significant with Mitigation Incorporated. No paleontological resources or unique geologic features are known to exist within or near the project site. The site is underlain by Holocene Alluvium and Holocene Basin Deposits, both of which are considered to have low paleontological sensitivity from the surface to a depth of 20 feet and high sensitivity below that mark. However, the project site is located in a previously disturbed area and will have ground disturbance that extends to a maximum depth of 10 feet. Therefore, the project is unlikely to impact scientifically important paleontological resources. Should undiscovered paleontological resources be found during project construction, **Mitigation Measure PALEO-1** shall be implemented to reduce potential impacts to paleontological resources.

d. Would the Project disturb any humans remains, including those interred outside of formal cemeteries?

Less Than Significant Impact with Mitigation. Native American skeletal remains have been identified in portions of the Project site, and it is possible that human remains could be encountered during Project construction. In the event that human remains are encountered during construction activities, the proper authorities would be notified and standard procedures for the respectful handling of human remains during the earthmoving activities would be implemented, as specified in **Mitigation Measure MM-CUL-1.** Implementation of **Mitigation Measure MM-CUL-1** would reduce the potential for impacts on unknown buried human remains to **less than significant**.

3.5.4 Mitigation Measures

The following mitigation measure shall be implemented to reduce impacts to cultural and paleontological resources:

MM-CUL-1: **Construction Monitoring.** Prior to construction, the City shall retain a professional archaeologist and invite a geographically-affiliated Native American Representative to provide a pre-construction briefing to supervisory personnel of any excavation contractor to alert them to the possibility of exposing significant historic or prehistoric archaeological resources within the project area. The briefing shall discuss any resources that could be exposed, the need to stop excavation at the discovery site, and the procedures to follow regarding discovery protection and notification. The City will notify geographically-affiliated tribal groups 7 days prior to excavation, grading, and other earthmoving activities within 100 feet of the current site boundary of CA-YOL-18. During construction, a qualified archaeologist and a tribal representative from a geographically-affiliated tribe shall be present to monitor Project excavation, grading, and other earthmoving activities within 100 feet of the current site boundary of CA-YOL-18. Monitoring shall continue until excavation, grading, and other earthmoving activities within 100 feet of the current site boundary have been completed.

Discovery of Unidentified Archaeological Resources. If deposits of prehistoric or historical archaeological materials are encountered during Project construction activities, all work within 50 feet of the discovery shall be redirected and a qualified archaeologist should be contacted (if one is not already on site) to assess the situation and make recommendations regarding the treatment of the discovery, and to develop proper mitigation measures required for the discovery (California Code of Regulations [CCR] Title 14, § 15064.5(f)). The City of West Sacramento shall also be notified. The archaeologist should prepare a report documenting the methods and results of the investigation, and provide recommendations for the treatment of the archaeological materials discovered. The report should be submitted to the City of West Sacramento and the Northwest Information Center.

Discovery of Human Remains. During construction, consistent with the requirements outlined by CEQA Guidelines, Section 15064.5(e)(1), and in accordance with Section 7050.5 of the Health and Safety Code, Section 5097.98 of the PRC (Chapter 1492, Statutes of 1982, Senate Bill 297), as relevant, should be followed and no further disturbance shall occur until the Yolo County Coroner can evaluate them. If the human remains are of Native American origin, the coroner must notify the NAHC within 24 hours of identification. Pursuant to Section 5097.9 and 5097.993 of the PRC, the NAHC shall identify a "Native American Most Likely Descendent" to inspect the site and provide recommendations for the proper treatment of the remains and any associated grave goods.

MM-PALEO-1: If paleontological resources are encountered during project excavation and no monitor is present, all ground-disturbing activities within 50 feet of the find shall be redirected to other areas until a qualified paleontologist can be retained to evaluate the find and make recommendations for additional paleontological mitigation, which may include paleontological monitoring; collection of observed resources; preservation, stabilization, and identification of collected resources; curation of

resources into a museum repository; and preparation of a final report documenting the monitoring methods and results to be submitted to the museum repository and the City.

3.6 GEOLOGY AND SOILS

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:				
i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42?	☐	☐	☒	☐
ii. Strong seismic ground shaking?	☐	☐	☒	☐
iii. Seismic-related ground failure, including liquefaction?	☐	☐	☒	☐
iv. Landslides?	☐	☐	☐	☒
b. Result in substantial soil erosion or the loss of topsoil?	☐	☐	☒	☐
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	☐	☐	☒	☐
d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?	☐	☐	☒	☐
e. Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	☐	☐	☐	☒

3.6.1 Environmental Setting

The Project is located in the western portion of the Great Valley geomorphic province of California. The Central Valley of California is generally considered to be an elongated sedimentary trough, approximately 450 miles long and 50 miles wide. Rock units within the Great Valley geomorphic province consist of Mesozoic to Cenozoic marine and nonmarine sedimentary rocks. The site is underlain by Holocene basin deposits, which consist of Holocene-aged (less than 11,700 years old) fine-grained sediments derived from the same sources as modern alluvium (City of West Sacramento 2016c).

The Natural Resources Conservation Service (NRCS) Soil Survey for Yolo County describes surface soils across the Project site as sandy or silty loams (NRCS 2018). **Table 3.6-1: Soils in the Project Area** lists the soils found in the Project area as well as common building-related soil issues.

Table 3.6-1: Soils in the Project Area

Soil Series	K Factor[1]	Linear Extensibility[2]	Corrosiveness[3]		Hydrologic Soil Group[4]
			Concrete	Steel	
Lang silt loam	0.24 (Low)	1.5 (Low)	Moderate	High	B (Moderate)
Lang sandy loam, deep	0.24 (Low)	1.5 (Low)	Moderate	High	C (Slow)
Merritt silty clay loam	0.37 (Moderate)	4.5 (Moderate)	Low	High	C/D (Slow/Very slow)
Sacramento silty clay loam	0.28 (Low)	4.5 (Moderate)	Low	High	C/D (Slow/Very slow)
Sycamore silt loam	0.43 (High)	4.5 (Moderate)	Low	High	C (Slow)
Tyndall very fine sandy loam, deep	0.43 (High)	1.5 (Low)	Moderate	High	B (Moderate)
Valdez silt loam	0.49 (High)	1.5 (Low)	Low	High	C (Slow)

Source: Natural Resources Conservation Service (2018); City of West Sacramento (2016c)

1 K factor is a measure of the susceptibility of a soil to erosion by water. Values of K in Yolo County range from 0.02 to 0.69, where the higher the K value, the more susceptible the soil is to erosion by water. K factor values ranging from 0.02 to 0.2 are considered low, from 0.25 to 0.4 are considered moderate, and greater than 0.4 are considered high.

2 Linear extensibility is a measure of how much a soil expands and contracts with changes in moisture content. Soils with a moderate to high linear extensibility, also known as expansive soils, do not provide a suitable substrate for construction without modification. Expansive soils generally have high clay content. Values of linear extensibility range from 0 to 30, where 0–3 is low, 3–6 is moderate, 6–9 is high, and 9–30 is very high.

3 Risk of corrosion is a measure of the potential soil-induced electrochemical or chemical action that corrodes or weakens concrete or uncoated steel. This measure is based mainly on the sulfate and sodium content, texture, moisture content, and acidity of the soil for concrete and the soil moisture, particle-size distribution, acidity, and electrical conductivity for steel. Concrete or steel that is installed across soil boundaries or soil layers is more susceptible to corrosion than concrete or steel installed in one kind of soil.

4 Hydrologic soil class is a measure of infiltration rates. Soils in hydrologic group A have a high rate of infiltration when thoroughly wet and low runoff potential. Hydrologic group B soils have a moderate rate of infiltration when wet. Hydrologic group C soils have a slow rate of infiltration. Hydrologic group D soils have a very slow infiltration rate and high runoff potential. For dual hydrologic soil groups such as C/D, the first letter applies to the drained condition and the second to the undrained condition.

The topography across the Project site is relatively flat, with elevations ranging from about 13 to 20 feet above mean sea level. Based on the relatively flat topography of the site and the lack of slopes in the vicinity of the site, the potential for landslides is nonexistent.

Land subsidence as a result of compaction and oxidation of peat soils or hydrocompaction as a result of groundwater overdraft are not significant concerns in the Project vicinity (City of West Sacramento 2016c).

The Project site is not located within or adjacent to an Alquist-Priolo Earthquake Fault Zone; consequently, ground rupture resulting from seismic activity is unlikely. The site is located in proximity to several surface faults that are presently zoned as active or potentially active by the California Geological Survey (CGS), pursuant to the guidelines of the Alquist-Priolo Earthquake Fault Zoning Act (CGS 2018). Therefore, the Project could potentially be susceptible to ground shaking during a maximum momentum magnitude earthquake on faults in proximity to the Project area. The probabilistic peak horizontal ground acceleration values for the Project site have been estimated between 0.21 to 0.23g (where g equals the acceleration speed of gravity), which is considered relatively low for California. The composition of the soils in the Project vicinity—primarily unconsolidated alluvium sediments with minimal clay—indicates that there is some potential for liquefaction (City of West Sacramento 2016c).

3.6.2 Discussion

a. Would the Project expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:

i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42?

Less Than Significant Impact. Surface rupture occurs when the ground surface is broken due to fault movement during an earthquake. The location of surface rupture generally can be assumed to be along an active or potentially active major fault trace. The Alquist-Priolo Earthquake Fault Zoning Act requires the State Geologist to delineate "Earthquake Fault Zones" along faults that are "sufficiently active" and "well defined". The proposed Project is not located within a designated Alquist-Priolo Earthquake Fault Zone (CGS 2018). No active or potentially active faults have been mapped on the Project site; therefore, the potential for fault rupture at the Project site is low. This impact would be **less than significant**. No mitigation is required.

ii. Strong seismic ground shaking?

Less Than Significant Impact. Although the probability is low, there is a potential for the Project to be subject to ground shaking during a maximum momentum magnitude earthquake on one of the active faults in the Project vicinity. Although the proposed Project could be exposed to ground shaking, it would be designed and constructed consistent with City standards that are required to adhere to the State seismic design parameters identified in the California Building Standards Code, which minimize risks by requiring new structures (and utilities) to be designed to withstand seismic activity. Therefore, this impact would be **less than significant**. No mitigation is required.

iii. Seismic-related ground failure, including liquefaction?

Less Than Significant Impact. Soil liquefaction is a phenomenon primarily associated with the saturated soil layers located close to the ground surface. These soils lose strength during ground shaking. Due to the loss of strength, the soil acquires "mobility" sufficient to permit both horizontal and vertical movements. Soils that are most susceptible to liquefaction are clean, loose, uniformly graded, saturated, fine-grained sands that lie relatively close to the ground surface. However, loose sands or sediment that contains a significant amount of fines (minute silt and clay fraction) may also liquefy.

While the potential is low, alluvial soils at the Project site could be susceptible to liquefaction as a result of seismic shaking. As described in (ii) above, the proposed Project would be designed and constructed in accordance with applicable standards addressing the potential risk of liquefaction as a result of seismic activity. Therefore, this impact would be **less than significant**. No mitigation is required.

iv. Landslides?

No Impact. The Project is not located in an area of known landslides or on steep terrain that would be prone to landslide activity. Consequently, implementation of the proposed Project would not impact persons or structures because of landslides. **No impact** would occur.

b. Would the Project result in substantial soil erosion or the loss of topsoil?

Less Than Significant Impact. As shown in **Table 3.6-1**, some soils at the Project site have high potential for runoff and erosion. Other soils at the Project site exhibit a low potential for runoff and erosion. Ground-disturbing activities during Project construction could result in soil erosion as associated topsoil loss, particularly during strong rain events. However, construction contractors would be required to comply with federal, State, and local regulations and guidelines to minimize the potential for soil erosion, including the National Pollutant Discharge Elimination System (NPDES) General Permit for Storm Water Discharges Associated with Construction and Land Disturbance Activities, 2009-0009-DWQ, as amended by 2010-0014-DWQ (General Construction Permit), during construction.

Erosion control measures and BMPs would also be identified in required grading plans, and a Storm Water Pollution Prevention Plan (SWPPP) would be prepared in accordance with the NPDES General Construction Permit. Therefore, as part of Project construction, erosion control measures and BMPs would be implemented to manage sediment and prevent discharge of sediment from the Project site to storm drains and surface waterways, and to prevent wind and water erosion from the beginning through conclusion of construction activities. Implementation of required erosion control measures and BMPs would minimize the potential for soil erosion and the loss of topsoil to a less than significant level. The impact would be **less than significant**. No mitigation is required.

c. Would the Project be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?

Less Than Significant Impact. As described above, the potential for hazard from landslide and liquefaction would be low or nonexistent. Therefore, the potential for liquefaction-induced lateral spreading also would be low. Subsidence is the sudden sinking or gradual downward settling of the earth's surface with little or no horizontal motion. Subsidence is caused by a variety of activities, including microbial oxidation of soil organic carbon, withdrawal of groundwater, extraction of oil and natural gas resources, liquefaction, and hydrocompaction. The Project site is not currently experiencing subsidence, and land subsidence in the vicinity of the proposed Project is not considered to be a significant concern (City of West Sacramento 2016c). Furthermore, the proposed Project would be designed and constructed to adhere to applicable building and related codes that include requirements related to minimizing potential for soil instability, settlement, liquefaction, and collapse. Therefore, this impact would be **less than significant**. No mitigation is required.

d. Would the Project be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

Less Than Significant Impact. Expansion and contraction of volume can occur when expansive soils undergo alternating cycles of wetting (swelling) and drying (shrinking), and can cause damage to foundations and infrastructure unless properly treated during construction. As shown in **Table 3.6-1**, the expansive properties of soils at the Project site are considered to be low to moderate. Infrastructure designed and constructed as part of the proposed Project would comply with applicable building and related codes that include requirements related to expansive soils to minimize potential risks to life and property. The impact would be **less than significant**.

e. Would the Project site have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?

No Impact. No septic tanks or alternative wastewater disposal systems are proposed as part of the Project. Therefore, Project implementation would not result in impacts on soils associated with the use of such wastewater treatment systems. **No impact** would occur.

3.6.3 Mitigation Measures

No mitigation is required.

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3.7 GREENHOUSE GAS EMISSIONS

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	☐	☐	☒	☐
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?	☐	☐	☒	☐

3.7.1 Environmental Setting

Greenhouse gases (GHG) are present in the atmosphere naturally, are released by natural sources, or are formed from secondary reactions taking place in the atmosphere. The gases that are widely seen as the principal contributors to human-induced global climate change are:

- Carbon dioxide (CO_2);
- Methane (CH_4);
- Nitrous oxide (N_2O);
- Hydrofluorocarbons (HFC);
- Perfluorocarbons (PFC); and
- Sulfur Hexafluoride (SF_6).

Over the last 200 years, humans have caused substantial quantities of GHGs to be released into the atmosphere. These extra emissions are increasing GHG concentrations in the atmosphere and enhancing the natural greenhouse effect, which is believed to be causing global warming. While man-made GHGs include naturally occurring GHGs such as CO_2, CH_4, and N_2O, some gases, like HFCs, PFCs, and SF_6, are completely new to the atmosphere.

Certain gases, such as water vapor, are short-lived in the atmosphere. Others remain in the atmosphere for significant periods of time, contributing to climate change in the long term. Water vapor is excluded from the list of GHGs above because it is short-lived in the atmosphere and its atmospheric concentrations are largely determined by natural processes, such as oceanic evaporation.

These gases vary considerably in terms of Global Warming Potential (GWP), which is a concept developed to compare the ability of each GHG to trap heat in the atmosphere relative to another gas. The GWP is based on several factors, including the relative effectiveness of a gas in absorbing infrared radiation and the length of time that the gas remains in the atmosphere ("atmospheric lifetime"). The GWP of each gas is measured relative to CO_2, the most abundant GHG; the definition of GWP for a particular GHG is the ratio of heat trapped by one unit mass of the GHG to the ratio of heat trapped by one unit mass of CO_2 over a specified time period. GHG emissions are typically measured in terms of pounds or tons of "CO_2 equivalents" (CO_2e).

3.7.2 Regulatory Setting

The United States has historically had a voluntary approach to reducing GHG emissions. However, on April 2, 2007, the United States Supreme Court ruled that the USEPA has the authority to regulate CO_2 emissions under the federal Clean Air Act (CAA).

On April 1, 2010, the USEPA and the United States Department of Transportation's National Highway Traffic Safety Administration (NHTSA) announced a final joint rule to establish a national program consisting of new standards for model year 2012 through 2016 light-duty vehicles that will reduce GHG emissions and improve fuel economy. The USEPA GHG standards require these vehicles to meet an estimated combined average emissions level of 250 grams of CO_2 per mile in model year 2016, which is equivalent to 35.5 miles per gallon. These standards mark the first-ever national GHG emissions standards under the CAA. Additionally, the Heavy-Duty National Program was finalized in August 2011, by the USEPA and the NHTSA, and addresses medium- and heavy-duty vehicles.

In June 1, 2005, then-Governor Schwarzenegger established California's GHG emissions reduction targets in Executive Order S-3-05. This Executive Order established the following goals for the State of California: GHG emissions should be reduced to 2000 levels by 2010, to 1990 levels by 2020, and to 80 percent below 1990 levels by 2050.

California's major initiative for reducing GHG emissions is outlined in AB 32, the "Global Warming Solutions Act," passed by the State Legislature on August 31, 2006. This effort aims at reducing GHG emissions to 1990 levels by 2020. The CARB established the level of GHG emissions in 1990 at 427 million metric tons (MMT) of CO_2e. The emissions target of 427 MMT requires the reduction of 169 MMT from the State's projected business-as-usual 2020 emissions of 596 MMT. AB 32 requires the CARB to prepare a Scoping Plan that outlines the main State strategies for meeting the 2020 deadline and to reduce GHGs that contribute to global climate change. The Scoping Plan was approved by the CARB on December 11, 2008, and includes measures to address GHG emission reduction strategies related to energy efficiency, water use, and recycling and solid waste, among other measures. Emission reductions that are projected to result from the recommended measures in the Scoping Plan are expected to total 174 MMT of CO_2e, which would allow California to attain its emissions goal of 427 MMT of CO_2e by 2020. The Scoping Plan includes a range of GHG reduction actions that may include direct regulations, alternative compliance mechanisms, monetary and nonmonetary incentives, voluntary actions, and market-based mechanisms such as a cap-and-trade system. The measures in the Scoping Plan will not be binding until after they are adopted through the normal rulemaking process and are therefore only recommendations at this time. The CARB rulemaking process includes preparation and release of each of the draft measures, and public input through workshops and a public comment period, followed by a CARB Board hearing and rule adoption.

The City of West Sacramento completed a Draft Climate Action Plan in August 2010. While the plan has not been finalized, it outlines the program the community will follow to reduce GHG emissions. None of the proposed measures in the Draft Climate Action Plan apply to the proposed Project, as they include either actions the City will take to reduce GHG emissions community-wide or actions to be taken by new development projects.

The proposed Project would generate direct and indirect GHG emissions that contribute to global warming and climate change impacts. Although the contribution from an individual project may be minor, the cumulative impact can be substantial. The YSAQMD has not established any specific thresholds of significance for GHG emissions, but it recommends that proposed projects include a qualitative discussion of GHGs in air quality analyses for sizable projects.

3.7.3 Discussion

a. Would the project generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

Less than Significant Impact. The proposed Project includes the abandonment in place of an existing water main in an established West Sacramento neighborhood and the installation of a new water main, service laterals, and water meters. Emissions associated with implementation of the proposed Project would occur from construction activities.

> Short-Term Construction-Related Greenhouse Gas Emissions. Construction activities, such as site preparation, site grading, on-site heavy-duty construction vehicles, equipment hauling materials to and from the site, and motor vehicles transporting the construction crew would produce combustion emissions from various sources. During construction of the proposed Project, GHGs would be emitted through the transport of workers to the site and the use of backhoes during the construction period, both of which use fossil-based fuels to operate. The combustion of fossil-based fuels creates GHGs such as CO_2, CH_4, and N_2O. Furthermore, CH_4 is emitted during the fueling of heavy equipment.

The YSAQMD does not have an adopted threshold of significance for construction-related GHG emissions. However, Lead Agencies are encouraged to quantify and disclose GHG emissions that would occur during construction. Using RoadMod, it is estimated that construction of the proposed Project would generate approximately 553.6 metric tons of CO_2e. However, other air quality management districts, such as the South Coast Air Quality Management District and SMAQMD, recommend accounting for construction emissions by amortizing them over a 30-year project life. The total amortized construction emissions for the Project would be 18.5 metric tons of CO_2e per year. Potential impacts would be limited to the duration of construction activities and GHG generation would halt once the Project is completed. Therefore, Project construction impacts associated with GHG emissions would be considered **less than significant**.

Long-Term Operational GHG Emissions. As discussed above, the proposed Project includes the in-place abandonment of existing water mains and the installation of new water mains, laterals, and water meters. Once completed, the proposed Project would not generate any GHG emissions or result in any new vehicle trips that would contribute to an increase in GHG emissions. Therefore, GHG emissions generated by the proposed Project would be **less than significant**. No mitigation is required.

b. Would the project conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

Less than Significant Impact. The regulatory plans and policies discussed above are intended to reduce federal, State, and local GHG emissions by targeting the largest emitters of GHGs: the transportation and energy sectors. The proposed Project includes the in-place abandonment of existing water mains and the installation of new water mains, laterals, and water meters in an existing established neighborhood in West Sacramento. The proposed Project would not generate any new vehicle trips during operation and would not conflict with these transportation reduction measures. In addition, the proposed Project does not propose any development that would increase energy demand. The proposed Project would not conflict with the State goal of reducing GHG emissions and would not conflict with the AB 32 Scoping Plan or any other plan or policy. The proposed Project would be subject to all applicable permit and planning requirements in place or adopted by the County of Yolo and the City of West Sacramento. Therefore, the proposed Project would not conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing emissions of GHGs. Impacts would be **less than significant**.

3.7.4 Mitigation Measures

No mitigation is required.

3.8 HAZARDS AND HAZARDOUS MATERIALS

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	☐	☐	☒	☐
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	☐	☐	☒	☐
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	☐	☒	☐	☐
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	☐	☐	☒	☐
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	☐	☐	☒	☐
f. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	☐	☐	☐	☒
g. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	☐	☐	☒	☐
h. Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	☐	☐	☒	☐

3.8.1 Environmental Setting

The Project is bounded by Linden Road to the north, Mojave Drive to the west, Trinity Way and the proposed Liberty Specific Plan site to the south, and the proposed Liberty Specific Plan site to the east. Specifically, the Project will be implemented on City-owned ROW and within dedicated easements along the following roads: Merced Way, Rubicon Way, Shasta Way, Trinity Way, Ironwood Way, Spruce Street, Linden Road, Redwood Avenue, Alder Way, Bastone Court, Tamarack Road, and Cedar Court.

To evaluate the potential presence of hazardous materials within the Project or surrounding areas, a comprehensive search of environmental records and databases was performed and historical aerial photographs, topographic maps, and city directories were provided by Environmental Data Resources (EDR) to identify permitted hazardous materials facilities and potential sites that may be

contaminated with hazardous substances in the Project vicinity (EDR 2017a, 2017b, 2017c, and 2017d). The database search conforms to the ASTM International Standard Practice for Phase 1 Environmental Site Assessments (ESA) (E 1527-13). In March 2018, an updated supplemental search of the Hazardous Waste and Substances Sites List (Cortese List) (California Environmental Protection Agency 2018) and search for sites with reported hazardous materials spills, leaks, ongoing investigations, and/or remediation near the Project was performed using the California Department of Toxic Substances Control (DTSC) online EnviroStor and State Water Resources Control Board (SWRCB) GeoTracker databases (DTSC 2018; SWRCB 2018).

A reconnaissance-level site visit was also performed by AECOM in July 2017. The reconnaissance-level visual assessment of the Project site and its vicinity, with authorization from the Yolo County Division of Environmental Health (Gallagher 2017), did not reveal any businesses or companies that appear to generate, use, store, or dispose of large quantities of hazardous materials that may impact the Project.

Results of the EDR records search are discussed further below. Small-quantity users, generators, and user/generators of hazardous wastes with no violations or regulatory agency corrective actions pending were not considered to be a significant hazard to the public or the environment and are therefore excluded from further discussion.

3.8.1.1 Environmental Record Search Findings

Listed Sites. Seven sites within the Project area are listed on regulatory databases researched by EDR and are discussed in further detail:

- Two sites, Newport Estates Unit 5 and Linden West Construction, are listed on the NPDES or Waste Discharge System databases for permitted storm water discharge to public waterways. These discharges appear to be related to the initial residential development that the Project area encompasses.

- One site, David L. Woody, is listed on the Pesticide Licensing Registration database maintained by the California Department of Pesticide Regulation for the licensing of commercial pesticide users or sellers.

- One site, Lewis Cleaners, is listed on the EDR Historical Cleaner database for a home-based garment pressing business (dry cleaners).

- One site, Chris C. Vitanov, is listed on the Haznet database maintained by the DTSC for disposal of a small quantity of hazardous waste in 1998.

- Two sites, 3111 Alder Place and 3040 Bastone Court, are listed on the California Hazardous Material Incident Reporting Systems (CHMIRS) database for releases of small quantities of hazardous substances that did not appear to require subsequent regulatory enforcement.

None of these sites are either expected to impact the proposed Project as the sites are listed on databases of an administrative nature, or based on documentation resulting from releases of small

quantities of hazardous substances or wastes with no apparent subsequent enforcement. All of the sites are in areas potentially adjacent to, but not directly on, the City-owned ROW or within dedicated Project easements.

Off-Site Listings. Based on information obtained from the SWRCB GeoTracker database, local depth to groundwater is approximately 19 feet below ground surface and the groundwater flow direction is to the northwest (West Environmental Services [West] 2017). Therefore, sites southwest of the Project area are considered to be topographically hydrologically upgradient in general (SWRCB 2018).

The goal of reviewing the EDR database report was to identify facilities that have known environmental issues that may negatively impact the Project. The Project site listings are discussed below. Other facilities listed in the EDR database report as "open" or "active," are located hydrologically upgradient of the Project area, are within the ASTM International search distances, and are listed on a database indicating a release of hazardous materials (e.g., Leaking Underground Storage Tank Sites, Spill, Leaks, Investigations, and Clean-ups, CHMIRS); these sites are discussed below. Sites that are listed as receiving regulatory closure are not anticipated to pose an environmental concern for the Project and are therefore excluded from this discussion. Any other sites that are listed in the EDR database report but not on databases indicating a release are also excluded from this discussion as they are not expected to represent an environmental concern to the Project based on the aforementioned criteria.

The Project would include the installation of approximately 9,000 linear feet of 16-inch and 8-inch-diameter pipe with the addition of fire hydrants and service lines (laterals) to residents that will include new water meters. Most of the pipe excavations completed during Project implementation will be relatively shallow (approximately 4-feet deep, with a small section that will be as deep as 10 feet to connect to the existing system).

- **Database Listings Within 1 Mile of the Project Area:**
    - **EnviroStor:** Five sites are listed on the DTSC EnviroStor database (DTSC 2018). Four of these are school sites, which required robust environmental sampling under DTSC's Schools Investigation Program prior to development. All four sites received a No Further Action determination; thus, these sites are not expected to negatively impact the Project.

      The remaining site, Richfield Oil Company, located in Sacramento, is hydrologically separated from the Project area by the Sacramento River and is not expected to impact the Project area.

      No other database listings within 1 mile of the Project area were identified by EDR.
- **Database Listings Within 0.5 Mile of the Project Area:**
    - **Spills, Leaks, Investigation, and Cleanup:** One site, Former Time Oil West Sacramento, at 1155 Linden Road, is listed on the Spills, Leaks, Investigation, and Cleanup database for an unauthorized release of petroleum hydrocarbons into soil and groundwater. Based on the

site's Fourth Quarter 2016 Groundwater Monitoring Report (West 2017), the groundwater plume does not appear to extend off site. This site is also listed on the Clandestine Drug Lab database for operation of an illegal drug lab in 2004.

The Project would require most excavation to extend to 4 feet below grade but as deep as 10 feet in certain areas to connect to the existing water system. Given the relatively shallow depth of excavation, the Project is unlikely to intercept contaminated groundwater originating from the Former Time Oil site; thus, this site is not expected to impact the Project.

No other database listings within 0.5 mile of the Project area were identified by EDR.

3.8.1.2 California Department of Conservation, Division of Oil, Gas, and Geothermal Resources

The California Department of Conservation, Division of Oil, Gas, and Geothermal Resources (DOGGR) maps reviewed for this report do not indicate that oil, gas, or geothermal wells are present within the Project area (DOGGR 2017).

Radon is a colorless, odorless, naturally occurring, radioactive, inert, gaseous element formed by radioactive decay of radium atoms. The USEPA has prepared a map to assist federal, State, and local organizations in targeting their resources and implementing radon-resistant building codes. The USEPA recommends site-specific testing to determine radon levels at a specific location. However, the map does give a valuable indication of the propensity of radon gas accumulation in structures, especially below-grade structures and basements. Review of the USEPA Map of Radon Zones places the Project area in Zone 3 (USEPA 2017), where average predicted radon levels are less than 2.0 picocuries per liter. Based on the radon zone classification for the Project area (Zone 3), radon is not expected to be an environmental concern for the Project.

The nearest schools—Our Lady of Grace (1990 Linden Road) and River City High School (1 Raider Lane)—are located within 0.25 mile west of the proposed Project.

The nearest airport or private airstrip is the Sacramento Executive Airport, which is located approximately 2 miles southeast of the Project site. However, the proposed Project is not located within the airport's safety zones or land use plan (SACOG 1999).

According to the California Department of Forestry and Fire Protection (CAL FIRE) Hazard Severity Zone map for Yolo County, the Project site is in a Local Responsibility Area (LRA) unzoned fire hazard severity zone (CAL FIRE 2007).

The West Sacramento Fire Department (WSFD) responds to all calls for emergency services within city limits that include fires, emergency medical incidents, public assistance, traffic and vehicle accidents, and other emergency situations. The closest fire station to the Project area is Station 45, approximately 0.5 mile to the northwest.

The City's Emergency Operations Plan, approved in January 2017, addresses the City's planned response to extraordinary emergency situations associated with any type of natural, technological, or human-caused hazard (City of West Sacramento 2016b).

3.8.2 Discussion

a. Would the Project create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?

Less Than Significant Impact. Project construction would involve the incidental transport and use of common hazardous materials, such as oils, lubricants, and fuels, as well as specific materials for building construction, such as concrete.

DTSC has primary regulatory authority for enforcing hazardous materials regulations. State hazardous waste regulations are contained primarily in Title 22 of the CCR. The California Occupational Health and Safety Administration has developed rules and regulations regarding worker safety around hazardous and toxic substances. If used and stored properly, these materials do not pose a substantial risk to the public or the environment.

The proposed Project would use only a limited amount of hazardous materials during construction, and potential impacts associated with the routine transport, use, or disposal of hazardous materials would be minimized with adherence to applicable regulations. Once operational, the proposed Project would not transport or generate hazardous materials. The impact would be **less than significant**. No mitigation is required.

b. Would the Project create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

Less Than Significant Impact. The proposed Project would involve abandoning in place approximately 11,200 linear feet of asbestos concrete pipe and approximately 5,650 linear feet of welded steel pipe. Because the pipe would be abandoned in place without excavation or surface exposure, there would be no accidental upset or release of old asbestos or welded piping. As discussed under Threshold a), the proposed Project would involve the use of hazardous materials during construction. During Project construction, hazardous materials would be stored, labeled, and disposed of in accordance with applicable federal, State, and local regulations. Construction contractors would be responsible for reporting any accidental discharges of hazardous materials or other similar substances (where amounts are above the threshold for reportable quantities). As a condition of Project approval, if threshold limits are exceeded for fuel storage, a spill prevention control and countermeasures plan would be required for the storage of flammable fuel hydrocarbons at the Project site. The impact would be **less than significant**. No mitigation is required.

c. Would the Project emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?

Less Than Significant Impact with Mitigation. The proposed Project site is located within 0.25 mile of Our Lady of Grace (1990 Linden Road) and River City High School (1 Raider Lane). As discussed in the response to Threshold a), the incidental use and handling of potentially hazardous materials would not pose a substantial risk to the public or the environment. The proposed Project would

involve abandoning in place approximately 11,200 linear feet of asbestos concrete pipe and approximately 5,650 linear feet of welded steel pipe. Because the pipe would be abandoned in place without excavation or surface exposure, there would be no hazardous waste generated by the Project or impacts associated with exposure to old asbestos or welded piping. The proposed Project has the potential to emit dust during excavation activities and construction emissions typical of heavy machinery, such as diesel exhaust. These emissions would be regulated in accordance with air quality standards (discussed further in Section 3.3, Air Quality) designed to reduce migratory dust and hazardous construction equipment emissions. Implementation of Mitigation Measure **MM AQ-1** would further reduce any potential impacts. The impact would be **less than significant with mitigation**.

d. Would the Project be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

Less Than Significant Impact. The Cortese List is compiled by the DTSC in accordance with Section 65962.5 of the California Government Code. A comprehensive search of the Cortese List and other environmental databases was conducted. These searches did not identify any Cortese List sites with potential hazardous contamination within approximately 2 miles of the proposed Project.
The search did identify seven sites within the proposed Project; however, a detailed evaluation of these sites indicated that none of these sites are expected to impact the proposed Project. The sites are either listed on databases of an administrative nature or based on documentation resulting from minor releases of hazardous substances or wastes in areas located outside the ROW where Project construction disturbance would occur. Based on a review of available information, the proposed Project would not pose a hazard to the public or the environment during construction of the new water mains and laterals. The impact would be **less than significant**. No mitigation is required.

e. Would the Project be located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?

Less Than Significant Impact. The proposed Project is located approximately 2 miles from Sacramento Executive Airport; however, it is located outside the airport's safety zone and land use planning area. Consequently, the airport would not cause a safety hazard for people residing or working in the Project area. Therefore, the impact would be **less than significant**. No mitigation is required.

f. For a project within the vicinity of a private airstrip, would the Project result in a safety hazard for people residing or working in the project area?

No Impact. The Project site is not located within an existing airport land use plan area or within 2 miles of a private airstrip. The Project would not result in a safety hazard for people residing or working in the Project area. **No impact** would occur.

g. Would the Project impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

Less Than Significant Impact. The proposed Project would install new water mains within existing road ROWs. As part of the Project, an encroachment permit with a traffic control plan approved by the City of West Sacramento Community Development Division would be required. The traffic control plan would conform to the current edition of the California Department of Transportation (Caltrans) *Manual of Traffic Controls for Construction and Maintenance Work Zones.* The Project would also be required to comply with all local regulations, provide advanced notification to emergency services of planned construction within the Project area, and identify and publicly communicate an alternate route for emergency vehicles through the Project area if a road closure is planned; therefore, the impact would be **less than significant**. No mitigation is required.

h. Would the Project expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?

Less Than Significant Impact. CAL FIRE classifies land in California based on fire hazard severity. An area that is not located within CAL FIRE jurisdiction is designed as an LRA responsibility. CAL FIRE has designated the Project area as "LRA Unzoned" (CAL FIRE 2007). This is considered an area of low fire risk. The WSFD would be responsible for responding to all fires in the Project area. All equipment would be located at staging areas that have been previously disturbed or have been cleared of vegetation, which minimizes any potential for increased risk of grassfires associated with Project construction. In the unlikely event of grassfires on undeveloped lands to the south of the proposed Project, adequate access would be maintained to accommodate firefighting crews and equipment as needed. Therefore, this impact would be **less than significant**. No mitigation is required.

3.8.3 Mitigation Measures

As discussed above under Threshold c), **Mitigation Measure MM-AQ-1** (provided in Section 3.3, Air Quality) shall be implemented to reduce impacts.

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3.9 HYDROLOGY AND WATER QUALITY

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Violate any water quality standards or waste discharge requirements?	☐	☐	☒	☐
b. Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	☐	☐	☒	☐
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?	☐	☐	☒	☐
d. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?	☐	☐	☒	☐
e. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	☐	☐	☒	☐
f. Otherwise substantially degrade water quality?	☐	☐	☒	☐
g. Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	☐	☐	☐	☒
h. Place within a 100-year flood hazard area structures which would impede or redirect flood flows?	☐	☐	☒	☐
i. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	☐	☐	☒	☐
j. Inundation by seiche, tsunami, or mudflow?	☐	☐	☒	☐

3.9.1 Environmental Setting

The Project is located in an area that has a Mediterranean climate and typically experiences hot, dry summers and temperate rainy winters, averaging between 20 and 24 inches of rainfall per year (Central Valley Regional Water Quality Control Board [CVRWQCB] 2016a). Annual precipitation generally falls from October to April, with the majority falling between the months of November and March.

The Project is located within the Yolo Subbasin of the Sacramento hydrologic region. As discussed in Section 3.8, Hazards and Hazardous Materials, groundwater below the Project site is expected to be encountered at depths of approximately 19 feet above mean sea level. Groundwater levels at the site should be expected to fluctuate throughout the year based on variations in seasonal precipitation, time of year, and local groundwater pumping.

There are no natural drainages present on the Project site. Storm water runoff collects in a series of storm water collection system inlets and is redirected to an existing storm water detention basin located to the southwest of the Project site. The receiving water for storm water runoff in the vicinity of the Project site is the Sacramento River. The reach of the Sacramento River (Knights Landing to the Delta) in the vicinity of the Project site is listed as impaired for mercury, dieldrin, dichlorodiphenyltrichloroethane, chlordane, polychlorinated biphenyls (PCB), and toxicity (CVRWQCB 2016b).

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Study Map for the City of West Sacramento, last updated in 1995, all areas within West Sacramento located outside of the main waterways are mapped as Zone X—areas protected from the 1 percent (1%) chance (100-year) flood by levee, dike, or other structures subject to possible failure of overlapping during longer floods. Extensive studies have been conducted since 1995 that identify deficiencies in the City of West Sacramento's levee system and likely inability to truly provide 100-year or 200-year flood protection. As such, new draft revised FEMA maps anticipated to be issued in the near future are expected to show that all or parts of West Sacramento may meet neither the 100-year flood standards nor the 200-year level of flood protection required by the Central Valley Flood Protection Plan for urban areas. It is expected that FEMA will eventually change West Sacramento's flood zone designations from Flood Zone X to a Special Flood Hazard Area (City of West Sacramento 2016b).

A catastrophic failure of any of the six dams (Monticello, Indian Valley, Shasta, Oroville, Folsom, and Nimbus) in the vicinity of the Project site would cause some degree of inundation (County of Yolo 2012). In 2017, erosion was discovered on the lower chute of the main flood control spillway at Lake Oroville. With an onslaught of winter storms, releases down the damaged main spillway were unable to prevent the reservoir from overtopping and water cascaded down the emergency spillway, triggering the evacuation of more than 180,000 people downstream of Lake Oroville (Department of Water Resources [DWR] 2018a). Appropriate site maintenance, continuous inspection and monitoring, and implementation of periodic site improvements improve the safety of most dam facilities (County of Yolo 2012). Following the Lake Oroville incident, the Division of Safety of Dams established the Spillway Re-evaluation Project in 2017 to assess dam appurtenant structures, including spillways, to confirm they meet minimum safety standards. Safety evaluations include the assessment of the spillway's design and construction, geologic attributes, maintenance and inspection programs, the spillway's historical performance, and any previous spillway repairs (DWR 2018b).

The Project site is not located in a volcanic hazard zone or tsunami inundation zone. A maximum momentum magnitude earthquake on one of the active faults in the Project vicinity could potentially produce oscillations or waves in the Sacramento River, which could overtop and damage levees. The danger of seiches during seismic events is limited to those periods when the Sacramento River is full during the flood season (City of West Sacramento 2016c).

3.9.2 Discussion

a. Would the Project violate any water quality standards or waste discharge requirements?

Less Than Significant Impact. Construction activities associated with the Project would include minor earth-disturbing activities (i.e., cut and fill, vegetation removal, grading, trenching, and movement of soil) that could expose disturbed areas and stockpiled soils to winter rainfall and storm water runoff. Areas of exposed or stockpiled soils could be subject to wind or water erosion, allowing temporary discharges of sediment into the storm drain system and ultimately to the Sacramento River. Accidental spills of construction-related contaminants (e.g., fuels, oils, paints, solvents, cleaners, and concrete) or nonstorm water discharges from activities such as construction dewatering also could occur during Project construction, resulting in releases to nearby surface water and thereby degrading water quality. If not managed properly, water used for dust suppression during Project construction could also enter the storm drain system.

Because the area of disturbance associated with Project construction would be more than 1 acre, the Project would be subject to the requirements of the NPDES General Permit for Storm Water Discharges Associated with Construction and Land Disturbance Activities, 2009-0009-DWQ, as amended by 2010-0014-DWQ and 2012-0006-DWQ (General Construction Permit, BMPs) and a SWPPP would be prepared (as a condition of Project approval) in accordance with the NPDES General Construction Permit to minimize the potential for degradation of storm water quality during construction activities. Post-construction runoff would be conveyed into the existing storm water collection system. Although not anticipated for the proposed Project due to the depth to groundwater, if dewatering is required, a general NPDES Permit for short-term discharges of small volumes of wastewater from certain construction-related activities (General Dewatering Permit) would be obtained. Permit conditions for the discharge of these types of wastewater to surface waters are specified in the General Order for Dewatering and Other Low Threat Discharges to Surface Waters (Order No. R5-2013-0074, NPDES No. CAG995001) and are designed to minimize impacts to water quality during dewatering activities. The General Dewatering Permit also specifies standards for testing, monitoring, and reporting; receiving water limitations; and discharge prohibitions.

Because BMPs, a SWPPP, and dewatering provisions would be implemented (as applicable as a condition of Project approval) and post-construction runoff would be conveyed into the existing storm water collection system, water quality impacts would be **less than significant**. Mitigation is not required.

b. Would the Project substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?

Less Than Significant Impact. Installation of new water lines (water mains and laterals) associated with the Project will require the removal and replacement of the impermeable roadway surfaces within the existing ROW. Any change in the amount of impervious surface would be negligible;

therefore, the Project would not result in a change in infiltration. Groundwater is expected to be encountered at a depth of approximately 19 feet below ground surface and is not anticipated to be encountered during construction activities. Because of the small amount of excavation required, in the event that dewatering is required as part of the Project, it would not result in a substantial depletion of groundwater sources such that there would be a net deficit in aquifer volume at the Project site, as construction activities would be temporary. Consequently, this impact would be **less than significant**. No mitigation is required.

c. Would the Project substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?

Less Than Significant Impact. The Project would not alter any natural waterways or drainages. Storm water would be collected and redirected into the existing storm water system and on-site storm water detention pond. No substantial change in the volume of storm water discharged into the storm water system is expected with implementation of the Project. See the response to Threshold a) regarding potential water quality impacts associated with Project construction. The impact would **be less than significant**. No mitigation is required.

d. Would the Project substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?

Less Than Significant Impact. See the response to Threshold c). No substantial change in the volume of storm water discharged into the storm water system is expected with implementation of the Project. The impact would be **less than significant.** No mitigation is required.

e. Would the Project create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?

Less Than Significant Impact. See the responses to Thresholds a) and c). No substantial change in the volume of storm water discharged into the storm water system or sources of runoff are expected with implementation of the proposed Project. Impacts would be **less than significant** and no mitigation is required.

f. Would the Project otherwise substantially degrade water quality?

Less Than Significant Impact. Water quality effects are described above in the response to Threshold a). The Project would not degrade water quality beyond the conditions described previously. The impact would be **less than significant**. No mitigation is required.

g. Would the Project place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?

No Impact. The Project would not involve the construction of housing; therefore, no housing would be placed within a 100-year flood zone. **No impact** would occur.

h. Would the Project place within a 100-year flood hazard area structures which would impede or redirect flood flows?

Less Than Significant Impact. The Project involves installing new underground water mains and laterals within the existing road ROW. Although the Project is located entirely within an area protected by levees that may be designated as a special flood hazard area in the future, the Project would be designed to allow drainage patterns to remain largely as they currently occur on site, directing runoff to existing storm water collection systems. In addition, the Project would not result in the encroachment of aboveground structures that could impede or redirect flood flows. As a result, this impact would be **less than significant**. No mitigation is required.

i. Would the Project expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?

Less Than Significant Impact. The Project involves installing new underground water mains and laterals within the existing road ROW. Although the Project is located entirely within a dam inundation area and an area protected by levees that may be designated as a special flood hazard area in the future, the Project would be designed to allow drainage patterns to remain largely as they currently occur on site, directing runoff to existing storm water collection systems. In addition, the Project would not result in the encroachment of aboveground structures that could impede or redirect flood flows as a result of the failure of a levee or dam. Consequently, this impact would be **less than significant**. No mitigation is required.

j. Would the Project be inundated by seiche, tsunami, or mudflow?

Less Than Significant Impact. The Project is not located in a tsunami inundation zone and would not be affected by mudflow because of its relatively flat topography. Seiches are earthquake-generated waves within enclosed or restricted bodies of water. The proposed Project could potentially be inundated by a seiche if a maximum momentum magnitude earthquake on one of the active faults in the Project vicinity generated oscillations or waves in the Sacramento River that overtopped and damaged levees; however, the potential is considered to be low and limited to those periods when the Sacramento River is full during the flood season. Because the Project involves installing new underground water mains and laterals within the existing road ROW, it would not contribute to inundation depth if a seiche event were to occur. Therefore, the impact is **less than significant**. No mitigation is required.

3.9.3 Mitigation Measures

No mitigation is required.

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3.10 LAND USE AND PLANNING

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Physically divide an established community?	☐	☐	☐	☒
b. Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	☐	☐	☐	☒
c. Conflict with any applicable habitat conservation plan or natural community conservation plan?	☐	☐	☐	☒

3.10.1 Environmental Setting

The Project site is located in the Northeast Village neighborhood of West Sacramento, in a long-established urbanized area. The City of West Sacramento General Plan designates the Project site as a Low-Density Residential land use with a zoning designation of Residential One Family (R-1-B). A portion of the proposed Project will extend east into the Liberty Specific Plan area, which has a current land use and zoning designation of Rural Residential (RR and RRA, respectively). The proposed Project does not include or require the redesignation of existing land uses or zoning within West Sacramento.

3.10.2 Discussion

a. Would the Project physically divide an established community?

No Impact. The Project includes the in-place abandonment of existing water mains within an established West Sacramento neighborhood and the installation of new water mains, service laterals and meters. The Project would not physically divide this established community as construction activities would be temporary and, once operational, the design features would be located underground in City-owned ROW and prescriptive easements. Consequently, **no impacts** would occur under this threshold.

b. Would the Project conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?

No Impact. The proposed Project would not involve a change in land use and would continue to comply with the West Sacramento General Plan Land Use Element, Land Use Map, and Zoning Ordinance. The Project would also comply with applicable development standards for utilities as set forth by the City of West Sacramento Public Works Department. The Project would not conflict with applicable land use plans, policies, or regulations. **No impact** would occur.

c. Would the Project conflict with any applicable habitat conservation plan or natural community conservation plan?

No Impact. The Project is not located within the jurisdiction of an applicable Habitat Conservation Plan or Natural Community Conservation Plan. The Project is located in an urbanized neighborhood in West Sacramento. Consequently, **no impact** would occur under this threshold.

3.10.3 Mitigation Measures

No mitigation is required.

3.11 MINERAL RESOURCES

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	☐	☐	☐	☒
b. Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	☐	☐	☐	☒

3.11.1 Environmental Setting

The principal legislation addressing mineral resources in California is the Surface Mining and Reclamation Act of 1975 (SMARA) (PRC Sections 2710–2719), which was enacted in response to land use conflicts between urban growth and essential mineral production. The stated purpose of SMARA is to provide a comprehensive surface mining and reclamation policy that will encourage the production and conservation of mineral resources while ensuring that adverse environmental effects of mining are prevented or minimized; that mined lands are reclaimed and residual hazards to public health and safety are eliminated; and that consideration is given to recreation, watershed, wildlife, aesthetic, and other related values. SMARA governs the use and conservation of a wide variety of mineral resources, although some resources and activities are exempt from its provisions, including excavation and grading conducted for farming, construction, and recovery from flooding or other natural disaster.

SMARA provides for the evaluation of an area's mineral resources using a system of Mineral Resource Zone (MRZ) classifications that reflect the known or inferred presence and significance of a given mineral resource. The MRZ classifications are based on available geologic information, including geologic mapping and other information on surface exposures, drilling records, and mine data, and on socioeconomic factors such as market conditions and urban development patterns. The MRZ classifications are defined as follows:

- **MRZ-1**—Areas where adequate information indicates that no significant mineral deposits are present, or where it is judged that little likelihood exists for their presence.
- **MRZ-2**—Areas where adequate information indicates that significant mineral deposits are present, or where it is judged that a high likelihood exists for their presence.
- **MRZ-3**—Areas containing mineral deposits, the significance of which cannot be evaluated from available data.
- **MRZ-4**—Areas where available information is inadequate for assignment into any other MRZ.

Although the State of California is responsible for identifying areas containing mineral resources, the county or city is responsible for SMARA implementation and enforcement by providing annual mining inspection reports and coordinating with the CGS.

The proposed Project is located in an area designated as MRZ-1 (no significant mineral deposits), and no areas within the Project boundary are designated as MRZ-2 (likelihood of significant mineral deposits). There is a band of land designated as MRZ-3 (unknown); however, this land is near the Sacramento River and is not within the Project boundary.

3.11.2 Discussion

a. Would the Project result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

No Impact. No historical or current commercial mining operations are known to have occurred in West Sacramento. The California Division of Mines and Geology has classified most of the Project area as MRZ-1 (information indicates that no significant mineral deposits are present). The small area bordering the Sacramento River is classified as MRZ-3 (aggregate deposits of undetermined significance occur there). MRZ-1 and MRZ-3 zones are not subject to State policies that guarantee maintenance of access to regionally significant mineral deposits under SMARA. The Project would therefore have **no impact** on mineral resources.

b. Would the Project result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

No Impact. The proposed Project is not located in an area where a locally important mineral resource recovery site is delineated in the West Sacramento General Plan, or in any specific plan or other land use plan of the City. As such, implementation of the proposed Project would not result in the loss of availability of a locally important mineral recovery site. **No impact** would occur.

3.11.3 Mitigation Measures

No mitigation is required.

3.12 NOISE

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	☐	☒	☐	☐
b. Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	☐	☐	☒	☐
c. A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	☐	☐	☐	☒
d. A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	☐	☒	☐	☐
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	☐	☐	☒	☐
f. For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	☐	☐	☒	☐

3.12.1 Environmental Setting

The following provides an overview of the characteristics of sound and vibration and the regulatory framework that applies to both within the vicinity of the Project site. The existing noise environment in and around the Project site is also described.

3.12.1.1 Characteristics of Sound

Noise is usually defined as unwanted sound. Noise consists of any sound that may produce physiological or psychological damage and/or interfere with communication, work, rest, recreation, or sleep. Several noise measurement scales exist that are used to describe noise in a particular location. A decibel (dB) is a unit of measurement that indicates the relative intensity of a sound. Sound levels in dB are calculated on a logarithmic basis. An increase of 10 dB represents a tenfold increase in acoustic energy, while 20 dB is 100 times more intense and 30 dB is 1,000 times more intense. Each 10 dB increase in sound level is perceived as approximately a doubling of loudness; similarly, each 10 dB decrease in sound level is perceived as half as loud. Sound intensity is normally measured in A-weighted decibels (dBA). This scale gives greater weight to the frequencies of sound to which the human ear is most sensitive. The A-weighted sound level is the basis for 24-hour sound measurements, which better represent how humans are more sensitive to sound at night.

As noise spreads from a source, it loses energy, so that the farther away the noise receiver is from the noise source, the lower the perceived noise level would be. Geometric spreading causes the

sound level to attenuate or be reduced, resulting in a 6 dB reduction in the noise level for each doubling of distance from a single point source of noise to the noise-sensitive receptor of concern.

There are many ways to rate noise for various time periods, but an appropriate rating of ambient noise affecting humans also accounts for the annoying effects of sound. The equivalent continuous sound level (L_{eq}) is the total sound energy of time varying noise over a sample period. However, the predominant rating scales for human communities in the State of California are the L_{eq}, the community noise equivalent level (CNEL), and the day-night average level (L_{dn}) based on dBA. CNEL is the time varying noise over a 24-hour period, with a 5 dBA weighting factor applied to the hourly L_{eq} for noises occurring from 7:00 p.m. to 10:00 p.m. (defined as relaxation hours) and a 10 dBA weighting factor applied to noise occurring from 10:00 p.m. to 7:00 a.m. (defined as sleeping hours). L_{dn} is similar to the CNEL scale, but without the adjustment for events occurring during the evening relaxation hours. CNEL and L_{dn} are within 1 dBA of each other and are normally interchangeable. The noise adjustments are added to the noise events occurring during the more sensitive hours. Terms used in this noise analysis are defined as follows:

- **Decibel (dB):** A unitless measure of sound on a logarithmic scale that indicates the squared ratio of sound pressure amplitude with respect to reference sound pressure amplitude. The reference pressure is 20 micropascals.

- **A-weighted decibel (dBA):** An overall frequency-weighted sound level in decibels that approximates the frequency response of the human ear.

- **C-weighted decibel (dBC):** The sound pressure level in decibels as measured using the C-weighting filter network. The C-weighting is very close to an unweighted or flat response. C-weighting is used only in special cases (i.e., when low-frequency noise is of particular importance). A comparison of measured A- and C-weighted levels gives an indication of low-frequency content.

- **Maximum sound level (L_{max}):** The maximum sound level measured during the measurement period.

- **Minimum sound level:** The minimum sound level measured during the measurement period.

- **Equivalent sound level (L_{eq}):** The equivalent steady-state sound level that in a stated period of time would contain the same acoustical energy.

- **Percentile-exceeded sound level:** The sound level exceeded xx percent of a specific time period. L_{10} is the sound level exceeded 10 percent of the time, and L_{90} is the sound level exceeded 90 percent of the time. L_{90} is often considered to be representative of the background noise level in a given area.

- **Day-night level:** The energy average of the A-weighted sound levels occurring during a 24-hour period, with 10 dB added to the A-weighted sound levels occurring during the period from 10:00 p.m. to 7:00 a.m.

- **Community Noise Equivalent Level (CNEL)**: The energy average of the A-weighted sound levels occurring during a 24-hour period, with 5 dB added to the A-weighted sound levels occurring during the period from 7:00 p.m. to 10:00 p.m. and 10 dB added to the A-weighted sound levels occurring during the period from 10:00 p.m. to 7:00 a.m.

- **Vibration Velocity Level (or vibration velocity decibels [VdB])**: The root-mean-square velocity amplitude for measured ground motion expressed in dB.

- **Peak Particle Velocity (peak velocity, or PPV)**: A measurement of ground vibration, defined as the maximum speed (measured in inches per second) at which a particle in the ground is moving relative to its inactive state. PPV is usually expressed in inches per second.

- **Frequency: Hertz:** The number of complete pressure fluctuations per second above and below the atmosphere pressure.

3.12.1.2 Existing Noise Conditions

The proposed Project is located in West Sacramento, within the Southport Planning Area. The Southport Planning Area lies in the southern half of West Sacramento and is bounded by the Deep Water Ship Channel to the north and west, the Sacramento River to the east, and the city limits to the south. Surrounding land uses are occupied mostly by residential units; however, there is vacant land to the east and south of the Project site. This vacant land is currently entitled and is anticipated to be developed as part of the Liberty Specific Plan. Noise monitoring was conducted for the City's General Plan Update Draft EIR (City of West Sacramento 2016a), including a long-term noise monitoring site within the Project boundary at the end of the Shasta Way cul-de-sac. The long-term noise-monitoring site referenced in the General Plan Update Draft EIR indicated that noise levels ranged between 59.5 and 60.8 dBA $L_{dn.}$ The dominant noise sources in the Project vicinity are motor vehicles. The amount of noise varies according to many factors, such as volume of traffic, vehicle mix (percentage of cars and trucks), average traffic speed, and distance from the observer.

3.12.1.3 Fundamentals of Vibration

Vibration is a form of noise with energy carried through structures and the earth, whereas noise is simply carried through the air. Thus, vibration is generally felt rather than heard. Some vibration effects can be caused by noise (e.g., the rattling of windows from passing trucks). This phenomenon is related to the coupling of the acoustic energy at frequencies that are close to the resonant frequency of the material being vibrated. Typically, ground-borne vibration generated by man-made activities attenuates rapidly as distance from the source of the vibration increases. Vibration, which spreads through the ground rapidly, diminishes in amplitude with distance from the source.

The ground motion caused by vibration is measured as PPV in inches per second. PPV is the speed at which a particle of earth moves and is expressed in units of inches per second. Vibration also is measured as the root-mean-square amplitude of a motion over a 1-second period. For ease, the logarithmic dB scale is used to describe the vibration velocity level relative to a reference level of 10^6 inches per second and is expressed as VdB.

The vibration velocity level threshold of perception for humans is approximately 65 VdB. A vibration velocity of 75 VdB is considered the approximate threshold between barely and distinctly perceptible levels for many humans. Most perceptible indoor vibration is caused by sources within buildings such as operation of mechanical equipment, movement of people, or slamming of doors. Typical sources of perceptible ground-borne vibration include construction equipment, steel-wheeled trains, and traffic on rough roads. Ground-borne vibration from traffic is barely perceptible if a roadway is smooth.

Analysis regarding ground-borne vibration for utility projects is typically focused on construction activities. Once operational, these projects do not generate ground-borne vibration. Construction activities can result in varying degrees of ground vibration, depending on the equipment and methods employed. Operation of construction equipment causes vibrations that spread through the ground and diminish in strength with distance. Structures built on the soil in the vicinity of the construction site respond to these vibrations, with varying results ranging from no perceptible effects at the lowest levels to low rumbling sounds and perceptible vibrations at moderate levels, and slight damage at the highest levels.

Ground vibrations from construction activities do not often reach the levels that can damage structures, but they can achieve the audible and feelable ranges in buildings very close to the site. A possible exception is the case of fragile buildings, many of them old, where special care must be taken to avoid damage. The construction activities that typically generate the most severe vibrations are blasting and impact pile-driving. Various types of construction equipment have been measured under a wide variety of construction activities, with an average of source levels reported in terms of velocity. **Table 3.12-1: Vibration Source Levels for Construction Equipment** shows the vibration levels of various types of construction equipment measured in PPV and VdB at a distance of 25 feet from the equipment.

Table 3.12-1: Vibration Source Levels for Construction Equipment

Construction Equipment		PPV at 25 feet (in/sec)	Approximate VdB at 25 feet
Pile Driver (impact)	Upper Range	1.518	112
	Typical	0.644	104
Pile Driver (sonic)	Upper Range	0.734	105
	Typical	0.170	93
Clam Shovel Drop (slurry wall)		0.202	94
Hydromill (slurry wall)	In Soil	0.008	66
	In Rock	0.017	75
Vibratory Roller		0.210	94
Hoe Ram		0.089	87
Large Bulldozer		0.089	87
Caisson Drilling		0.089	87
Loaded Trucks		0.076	86
Jackhammer		0.035	79
Small Bulldozer		0.003	58

Source: United States Department of Transportation/Federal Transit Administration (2006).
in/sec = inches per second VdB = vibration velocity decibels
PPV = peak particle velocity

3.12.2 Regulatory Setting

The City of West Sacramento addresses noise in the Health and Safety Element of its General Plan (City of West Sacramento 2016a) and in Chapter 17.32 of its Municipal Code (Performance Standards of Noise). The primary purpose of the noise portion of the element is to protect West Sacramento residents from the harmful effects of excessive noise. The noise guidance serves to set land use compatibility standards for new developments.

The City's performance standards for noise, found in Chapter 17.32 of the City's Municipal Code, are the primary enforcement tool for the operation of locally regulated noise sources, such as construction activity or outdoor recreation facilities. This section of the Municipal Code sets noise level performance standards for nontransportation noise sources, which are summarized in **Table 3.12-2: City of West Sacramento Noise Level Standards of Nontransportation Uses**. Examples of nontransportation noise sources are construction equipment; industrial operations; outdoor recreation facilities; heating, ventilation, and air-conditioning units; and loading docks. The City's performance standards do not specify an exemption for temporary daytime construction activity, so the daytime and nighttime limits specified in the City's performance standards for noise would apply to all construction activities in West Sacramento (City of West Sacramento 2008). The City's Municipal Code also sets noise level performance standards for transportation sources; however, the proposed Project would not generate transportation noise above what already exists under ambient conditions, as the Project only includes the removal of a water main and the installation of a replacement water main and service laterals.

In addition, the City's Municipal Code prohibits the installation of any operation that consistently produces noticeable construction- or operation-related vibration beyond the property line. **Table 3.12-3: Groundborne Vibration Impact Criteria for General Assessment** shows the City of West Sacramento's General Plan Vibration Impact Criteria for frequent events, occasional events, and infrequent events.

Table 3.12-2: City of West Sacramento Noise Level Standards for Nontransportation Uses

Land Use	Noise Level Descriptor (dBA)	Exterior Noise Levels		Interior Noise Levels	
		Daytime[1]	Nighttime[2]	Daytime[1]	Nighttime[2]
Residential	Hourly L_{eq}	50	45	45	35
	Maximum level	70	65	-	-
Transient lodging	Hourly L_{eq}	-	-	45	35
Hospitals, nursing homes	Hourly L_{eq}	-	-	45	35
Theaters, auditoriums, music halls	Hourly L_{eq}	-	-	35	35
Churches, meeting halls	Hourly L_{eq}	-	-	40	40
Office buildings	Hourly L_{eq}	-	-	45	45
Schools, libraries, museums	Hourly L_{eq}	-	-	45	45

Source: City of West Sacramento (2008).
Note: Each noise level specified above will be lowered by 5 dB for simple-tone noises, noises consisting primarily of speech or music, or recurring impulsive noises. These noise level standards do not apply to residential units established in conjunction with industrial or commercial uses (e.g., caretaker dwellings).
[1] Daytime is defined as 7:00 a.m. to 10:00 p.m.
[2] Nighttime is defined as 10:00 p.m. to 7:00 a.m.
dB = decibels
dBA = A-weighted decibels
L_{eq} = equivalent continuous sound level

Table 3.12-3: Groundborne Vibration Impact Criteria for General Assessment

Land Use	Impact Levels (VdB)		
	Frequent Events[a]	Occasional Events[b]	Infrequent Events[c]
Category 1: Building where vibration would interfere with interior operations.	65[d]	65[d]	65[d]
Category 2: Residences and buildings where people normally sleep	72	75	80
Category 3: Institutional land uses with primarily daytime uses	75	78	83

Source: City of West Sacramento (2016a).

[a] "Frequent Events" is defined as more than 70 vibration events of the same source per day.

[b] "Occasional Events" is defined as between 30 and 70 vibration events of the same source per day.

[c] "Infrequent Events" is defined as fewer than 30 vibration events of the same source per day.

[d] This criterion limit is based on levels that are acceptable for most moderately sensitive equipment, such as optical microscopes. Vibration-sensitive manufacturing or research will require detailed evaluation to define the acceptable vibration levels.

VdB = vibration velocity decibels

3.12.3 Discussion

a. Would the Project result in exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

Less than Significant Impact with Mitigation. The following provides a discussion on operational noise impacts and construction noise impacts.

3.12.3.1 Operational Noise Impacts

The proposed Project would remove an existing water main and replace it with a new water main and new service laterals. Once operational, the Project would not result in noise-generating sources. Therefore, once operational, the Project would not result in the exposure of noise-sensitive land uses to noise levels in excess of the City of West Sacramento standards, as shown above in **Table 3.12-2**. Operational impacts would be less than significant.

3.12.3.2 Construction Noise Impacts

Construction often generates community noise complaints, even when it takes place over a limited timeframe. Noise impacts from construction may vary greatly depending on the proximity, duration, and complexity of the project. The noise levels generated by construction equipment would vary depending on the type of equipment, the specific model, the operation being performed, and the condition of the equipment. The L_{eq} of the construction activity also depends on the fraction of time that the equipment is operated over the time period of construction. The dominant source of noise from most construction equipment is the engine, which is usually a diesel engine and often lacks sufficient muffling. In a few cases, such as impact pile driving or pavement breaking, noise generated by the process dominates. Construction equipment can operate in two modes—stationary and mobile. Stationary equipment operates in one location for 1 or more days at a time, with either a fixed power operation (e.g., pumps, generators, and compressors) or a variable noise operation (e.g., pile drivers and pavement breakers). Mobile equipment moves around the construction site

with power applied in cyclic fashion (e.g., bulldozers and loaders), or to and from the site (e.g., trucks). Variation in power imposes additional complexity in characterizing the noise source level from a piece of construction equipment. This variation is handled by describing the noise at a reference distance from the equipment operating at full power and adjusting it based on the duty cycle of the activity to determine the L_{eq} of the operation.

During construction of the proposed Project, noise from construction activities may intermittently dominate the noise environment in the immediate area of construction. Two types of short-term noise impacts would occur during Project construction. The first type would be from construction crew commutes and the transport of construction equipment and materials to the Project site, which would incrementally increase noise levels on the existing roadways leading to the site.

The transport of heavy equipment would occur only a few times (to the Project construction areas prior to construction commencement and from the Project construction areas upon construction completion) and would not add to the daily traffic volume along roadways surrounding the Project site. During arrival and departure of this heavy equipment, there is a potential for a high single-event noise exposure at a maximum level of 87 dBA L_{max} from trucks passing, as measured from a distance of 50 feet. However, the projected construction traffic would be minimal when compared to existing traffic volumes on surrounding roadways, including truck traffic, and the noise levels along these roadways would not be increased permanently. Therefore, short-term construction-related worker commutes and equipment noise impacts would be less than significant.

The second type of short-term noise impact is related to noise generated during excavation, construction, and paving. The proposed Project would include construction of roadway improvements in the Washington district of West Sacramento. Construction activities would include the use of earthmovers such as backhoes, front-end loaders, graders, dump trucks, and water trucks, as well as paving equipment such as cement/asphalt trucks and compactors. Such construction equipment would be used intermittently throughout the duration of the Project construction period. **Table 3.12-4: Anticipated Construction Equipment Used for the Project and Their Maximum Noise Levels, L_{max}** shows the construction equipment types anticipated to be used during Project construction and their maximum sound levels.

Construction of the proposed Project will occur within existing ROW along the following neighborhood roads: Linden Road, Mojave Drive, Merced Way, Shasta Way, Carmel Court, Rubicon Way, Ironwood Way, Spruce Street, Redwood Avenue, Alder Way, Tamarack Road, and Cedar Street. Sensitive receptors (single-family residential units) are located along these roads and would be as close as 40 feet from active construction areas and equipment. Despite the variety in the type and size of construction equipment, similarities in the dominant noise sources and patterns of operation allow construction-related noise ranges to be categorized by work phase. **Table 3.12-4**, below, lists typical construction equipment noise levels recommended for noise impact assessments of the construction equipment that will be used during Project development. Excavation and construction activities at the Project site are expected to require the use of earth movers such as backhoes, front-end loaders, graders, dump trucks, and water trucks, as well as paving equipment such as cement/asphalt trucks and compactors.

Table 3.12-4: Anticipated Construction Equipment Used for the Project and Their Maximum Noise Levels, L_{max}

Type of Equipment	Impact Device? (Yes/No)	Specification L_{max} for Analysis (dBA at 50 feet)
Hydrovac Vacuum Trucks	No	85
Tractors	No	84
Backhoes	No	80
Trencher	No	80
Loader	No	80
Haul Trucks	No	55
Excavator	No	85
Compressor	No	80
Concrete Saw	No	90
Concrete Mixer Truck	No	85
Jack Hammer	Yes	85
Asphalt Planer	No	85
Trench Paver[1]	No	109
Compactors	No	80
Rollers	No	85
Street Sweeper	No	80
Forklift[1]	No	93
Chipping Guns	Yes	85

Source: Federal Highway Administration (2006).
[1] 116 dBA at 7 meters = 109.2 dBA at 50 feet (Berger et al. 2015).
dBA = A-weighted decibels
L_{max} = maximum sound level

Typical operating cycles for these types of construction equipment may involve 1 or 2 minutes of full-power operation followed by 3 or 4 minutes at lower power settings. Impact equipment, such as jack hammers and chipping guns, would be used during construction of this Project. As shown in **Table 3.12-4**, the typical maximum noise level generated by these types of equipment is assumed to be 109 dBA L_{max} at 50 feet. Each doubling of the sound sources with equal strength would increase the noise level by 3 dBA. Assuming the noisiest pieces of construction equipment operates (forklift [50 percent usage factor], trench paver [20 percent usage factor], and concrete saw [20 percent usage factor]) at some distance apart from the other equipment, the worst-case combined noise level during this phase of construction would be 109 dBA L_{max} and 102 dBA L_{eq} at a distance of 50 feet from multiple pieces of heavy construction equipment operating at full power simultaneously.

As stated above, the nearest residential land uses to the Project site would be the single-family residences located directly adjacent to the roadway ROWs where construction would occur, with the nearest building façades located as close as 40 feet from the nearest construction areas. At a distance of 10 feet, intermittent noise levels could reach approximately 111 dBA L_{max} and 105 dBA L_{eq}.

As noted previously, typical operating cycles for heavy construction equipment involve 1 or 2 minutes of full-power operation followed by 3 or 4 minutes at lower power settings. Additionally,

construction at the Project site would be limited to a duration of no more than a few months. Therefore, although there would be a relatively high single-event noise exposure potential causing intermittent noise nuisance, the effect on longer-term (hourly or daily) ambient noise levels would be limited. However, the intermittent noise levels could exceed the maximum exterior noise level of 70 dBA for residential receptors, which is the City's standard as described above in **Table 3.12-4**, resulting in a significant noise impact during construction. However, implementation of **Mitigation Measure MM NOI-1,** as presented below, would reduce short-term construction-related noise impacts to a **less than significant** level.

b. Would the Project result in exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?

Less Than Significant Impact. Construction activities associated with implementation of the proposed Project are not expected to result in excessive groundborne vibration or groundborne noise levels. Jackhammers will be used during Project construction and would generate the highest amount of vibration. Jackhammers that are operating as close as 40 feet to sensitive receptors would generate vibration levels of approximately 72.7 VdB, which is below the City of West Sacramento's vibration threshold for category 2 land uses (residences and buildings where people normally sleep). Additionally, groundborne vibration during construction activities is temporary and would cease to occur after Project construction is complete. Once the water main and laterals are installed and operational, groundborne vibrations would not be generated. As such, impacts would be **less than significant**.

c. Would the Project result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?

No Impact. The proposed Project includes the installation of a new water main and new laterals for residential connection to the water system. Once operational, the Project would not result in noise-generating sources. Therefore, the proposed Project would not result in a substantial permanent increase in ambient noise levels in the Project vicinity above levels existing without the Project, and there would be **no impact.**

d. Would the Project result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?

Less Than Significant Impact with Mitigation. Temporary intermittent noise from short-term construction activities associated with installation of the new water main and laterals would increase ambient noise levels during the construction period. However, the increased noise levels would be temporary and intermittent, and would occur in association with excavation, earthwork, and paving activities. Additionally, implementation of **Mitigation Measure MM-NOI-1** would further minimize the short-term noise increase generated during construction activities on the Project site to a **less than significant** level.

e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the Project expose people residing or working in the project area to excessive noise levels?

Less Than Significant Impact. The proposed Project is approximately 1.8 miles northwest of Sacramento Executive Airport (located at 6151 Freeport Boulevard, Sacramento). The Sacramento Executive Airport Comprehensive Land Use Plan (Sacramento Area Council of Governments 1998) was reviewed to determine if the proposed Project is located within the airport's land use plan or noise contours where construction personnel on the Project site could be exposed to excessive noise levels. The proposed Project is not located within the boundary of the Sacramento Executive Airport Land Use Plan and is not within the 65 dBA CNEL noise contour boundary of the airport. As such, implementation of the Project would not expose construction workers to excessive noise levels associated with operation of Sacramento Executive Airport. Impacts would be **less than significant.**

f. For a project within the vicinity of a private airstrip, would the Project expose people residing or working in the project area to excessive noise levels?

Less Than Significant Impact. The proposed Project is not located within the vicinity of a private airstrip. The closest private airstrip to the proposed Project is California Highway Patrol Academy Airport (60CL), located approximately 3.9 miles northwest of the Project site. As such, the proposed Project would not expose people residing or working in the Project area to excessive noise levels. This impact would be **less than significant.**

3.12.4 Mitigation Measures

Prior to commencement of Project construction, the following mitigation measure shall be implemented:

MM-NOI-1: Prior to initiating construction, the Project proponent shall complete a noise reduction plan. The noise reduction plan shall identify the type and quantity of construction equipment to be operated, the expected noise levels of each piece of equipment, and the duration of operation at each area of construction. The noise reduction plan shall include measures to ensure construction of the Project will meet the standards of Chapter 17.32 of the City of West Sacramento Municipal Code (Performance Standards of Noise).

With implementation of **Mitigation Measure MM-NOI-1**, short-term construction noise impacts would be **less than significant.**

3.13 POPULATION AND HOUSING

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	☐	☐	☐	☒
b. Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?	☐	☐	☐	☒
c. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	☐	☐	☐	☒

3.13.1 Environmental Setting

In 1987, the year West Sacramento was incorporated, the City had an estimated population of 28,195 residents. Currently (Year 2016 as this is the most recent data available), the American Community Survey 5-year demographic and housing estimates reports the population is 51,386 persons, with a total of 18,860 housing units. Because population data for the Project area is analyzed at the census tract level (Census Tract 103.02, Yolo County, California), population estimates for the Project area are limited to the information collected in the decennial Census. The 2010 Census estimated 2,566 occupied households distributed among a total population of 7,270 persons within the Project area, of which 77.6 percent of households are owner-occupied and 22.4 percent are renter-occupied, with an average household size of 2.83 persons (United States Census Bureau 2010).

3.13.2 Discussion

a) Would the Project induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?

No Impact. The proposed Project includes the installation of a new water main and laterals to connect to residential units in an established neighborhood. The proposed Project does not include design features that would promote direct or indirect population growth, as the Project is being constructed in an established neighborhood in West Sacramento. As such, the proposed Project would not promote population growth and **no impact** would occur.

b) Would the Project displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?

No Impact. The proposed Project would include the installation of a new water main and laterals to existing residential units within an established neighborhood in West Sacramento. The installation of the water main and laterals would occur within City-owned ROW in the roads of the neighborhood. A portion of the Project will extend into the Liberty Specific Plan area; however, this area is currently unoccupied, and the Project would not displace existing housing. As such, **no**

impacts would occur with implementation of the proposed Project on existing housing stock in West Sacramento.

c) Would the Project displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?

No Impact. The proposed Project would not displace residents, as the Project includes replacement of a water main and installation of laterals in road ROWs in an established West Sacramento neighborhood. The proposed Project would not necessitate the construction of replacement housing elsewhere in West Sacramento. As such, **no impacts** would occur with implementation of the proposed Project.

3.13.3 Mitigation Measures

No mitigation is required.

3.14 PUBLIC SERVICES

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
i. Fire protection?	☐	☐	☒	☐
ii. Police protection?	☐	☐	☒	☐
iii. Schools?	☐	☐	☒	☐
iv. Parks?	☐	☐	☒	☐
v. Other public facilities?	☐	☐	☒	☐

3.14.1 Environmental Setting

The proposed Project is located in an area of West Sacramento that is served by the WSFD for fire protection services. The WSFD is divided into three major units that provide a wide range of services for the community: Fire Administration, Emergency Operations, and Fire Prevention/Hazardous Materials. There are five fire stations located within West Sacramento that operate 24 hours per day, 7 days per week, with a combined staffing of 17 personnel on duty. The personnel on duty include a battalion chief who responds to all structure fires and major emergencies, providing incident command and scene management. Fire Station #45 (located at 2040 Lake Washington Boulevard in West Sacramento) is the closest station to the Project site, approximately 0.37 mile to the northwest. The City, through its General Plan, is required to maintain an average response time to emergency calls (Priority 1) of 5 minutes for 95 percent of Priority 1 calls. The average response time for the WSFD overall is 4 minutes, 42 seconds, from time of dispatch to arrival on scene. Therefore, the WSFD is currently meeting the General Plan goals and policies of response times.

Law enforcement services in West Sacramento and at the Project site are provided by the West Sacramento Police Department, which comprises three main divisions: Administration, Support Services, and Field Operations. Field Operations is under the command of two watch commanders and eight police sergeants who provide direct supervision to 65 sworn officers and 4 community services officers. This staff is assigned to five patrol shifts and two specialty units. The average response time for law enforcement services in 2015 ranged from 0.02 minute for Priority 0 Calls (life-threatening emergencies and crimes in progress) to 7.6 minutes for Priority 5 Calls (non-emergency). Response times for Priority 2 and 4 calls (the most abundant in 2015) were 5.96 minutes and 11.81 minutes, respectively. The West Sacramento Police Department station is located at 550 Jefferson Boulevard in West Sacramento, approximately 2.7 miles north of the Project site.

Education services in West Sacramento are provided by the Washington Unified School District (WUSD). The WUSD provides primary, secondary, and high school education services to the residents of West Sacramento. As of 2014, WUSD had an enrollment of 7,421 students and a staff of 400 certificated employees and 350 classified employees. River City High School (located at 1 Raider Lane in West Sacramento) is the closest school to the Project site and is located approximately 0.3 mile to the west.

The West Sacramento Parks and Recreation division oversees the maintenance of 145+ acres of developed City parks. The City currently has an inventory of 33 parks, plazas, and playfields totaling 152.6 acres. Based on the City of West Sacramento's population of 51,386 residents in 2016 and its park acreage to population ratio requirements of 5:1, the City is deficient in parkland for the existing population. The closest park to the Project site, Southport Gateway Park, is approximately 0.6 mile to the north.

The Yolo County Library provides library services to the City of West Sacramento. The Arthur F. Turner Community Library is located at 1212 Merkley Avenue and is 2.3 miles north of the Project site. This library is approximately 18,000 square feet in size and provides West Sacramento residents with access to books and other materials, including DVDs, CDs, magazines, and newspapers.

3.14.2 Discussion

a) Would the Project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

i. Fire protection?

ii. Police protection?

iii. Schools?

iv. Parks?

v. Other public facilities?

Less Than Significant Impact. The proposed Project includes the installation of a new water main and laterals connecting to existing residential units in a neighborhood in West Sacramento. The Project is not growth-inducing and would therefore not increase the population of West Sacramento, the need for new schools, or the use of parks or other public facilities. During construction of the Project, road detours may be needed, which may temporarily affect response times of fire and law enforcement staff in the neighborhood where the Project will be located. However, a Traffic Management Plan (as discussed in Section 3.16) would be implemented as a condition of approval for the Project, and thus would reduce potential impacts to temporary increased response times for fire and law enforcement staff. As such, impacts would be **less than significant**.

3.14.3 Mitigation Measures

No mitigation is required.

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3.15 RECREATION

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	☐	☐	☐	☒
b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?	☐	☐	☐	☒

3.15.1 Environmental Setting

As discussed above in Section 3.13, Public Services, the West Sacramento Parks and Recreation division oversees parks and recreational facilities within West Sacramento. The closest park to the Project site, Southport Gateway Park, is approximately 0.6 mile to the north. The Sacramento River, which provides recreational opportunities on a regional and local basis, is approximately 0.41 mile east of the Project site.

3.15.2 Discussion

a. Would the Project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

No Impact. The proposed Project would not generate population growth as it includes the installation of a water main and laterals in an existing neighborhood within West Sacramento. As such, implementation of the proposed Project would not generate an increased use of existing neighborhood parks, regional parks or recreational facilities above and beyond the amount of use currently occurring. **No impacts** would occur to recreational facilities due to Project implementation.

b. Does the Project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?

No Impact. The Project includes the installation of a water main and laterals in an established residential neighborhood of West Sacramento. The design elements of the proposed Project do not include the construction of recreational facilities that may have an adverse physical effect on the environment. As such, **no impact** would occur due to Project implementation.

3.15.3 Mitigation Measures

No mitigation is required.

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3.16 TRANSPORTATION/TRAFFIC

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?	☐	☐	☒	☐
b. Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?	☐	☐	☐	☒
c. Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location which results in substantial safety risks?	☐	☐	☐	☒
d. Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	☐	☐	☐	☒
e. Result in inadequate emergency access?	☐	☒	☐	☐
f. Conflict with adopted policies, plans or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?	☐	☐	☐	☒

3.16.1 Environmental Setting

The proposed Project is located in the Northeast Village neighborhood of West Sacramento in an area with local roads. Local roads are intended to serve adjacent properties. They carry minimal through traffic and generally carry very low traffic volumes. Many of West Sacramento's local roads are arranged in a grid (similar to the roads in the neighborhood where the Project would be implemented), making through travel possible but not desirable because of slow speeds, traffic control or calming, and competing roadway users. The speed limits on the roads where the Project would be implemented do not exceed 25 miles per hour. Collector roads are also near the proposed Project and are intended to convey traffic from local roads to larger roads. Collector roads also serve adjacent properties and generally carry light to moderate traffic volumes, with typical speed limits of 25 to 35 miles per hour.

An Environmental Impact Report for the Liberty Specific Plan (adjacent to the Project site) was released for public review in August 2017. The EIR for the Liberty Specific Plan provided data for existing intersection LOS and roadway segment volumes near the proposed Project. The intersection

of Stonegate Drive/Linden Road currently operates at LOS B in the a.m. and p.m. peak hours, and the intersection of Mojave Drive/Linden Road currently operates at LOS A in the a.m. and p.m. peak hours. These are acceptable LOS when compared to the City's LOS C threshold for these intersections. Linden Road between Stonegate Drive and Mojave Drive has an existing daily traffic volume of 4,400 vehicles; Linden Road between Mojave Drive and Santiam Street has an existing daily traffic volume of 2,800 vehicles; and Linden Road between Alder Way and Bastone Court has an existing daily traffic volume of 1,200 vehicles. All of these road segments are considered two-lane arterial moderate access control with a maximum desirable volume of 14,400 vehicles. As such, these roadway segments operate below desirable volumes under existing conditions. Other roadway segments and their existing volumes within the Project site include Tamarack Road between Redwood Avenue and Cedar Court, with an existing volume of 150 vehicles; Trinity Way between Shasta Way and the cul-de-sac, with an existing volume of 310 vehicles; Mojave Drive between Trinity Way and Merced Way, with an existing volume of 670 vehicles; and Redwood Avenue between Alder Way and Linden Road, with an existing volume of 330 vehicles. All of these roadway segments operate below maximum desirable volumes under existing conditions.

There are existing bicycle and pedestrian facilities within the proposed Project site. A Class II Bike Lane is located along Linden Road between Mojave Drive and Spruce Street and along Mojave Drive between Linden Road and Merced Way. Sidewalks exist within the Project site, providing pedestrian access throughout the neighborhood.

The Yolo County Transportation District provides public transportation through Yolobus, which offers fixed-route and special services in Yolo County and West Sacramento. Bus service is provided near the Project site through Yolobus Routes 35 and 39. Both of these routes travel along Stonegate Drive and Linden Road, with the closest bus stops at the intersection of Stonegate Drive/Linden Road.

3.16.2 Discussion

a. Would the Project conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?

Less Than Significant Impact. The proposed Project includes the installation of a water main and laterals in an existing neighborhood within West Sacramento. During construction, vehicle trips by construction crews accessing the Project area would be added to the local circulation system. However, this increase would be nominal compared to existing vehicle volumes and would occur temporarily for the duration of construction. This nominal temporary vehicle volume increase would not be enough to degrade existing LOS performance standards as set forth by the City of West Sacramento in its 2035 General Plan. Once operational, the proposed Project would not generate vehicle trips as the features of the Project include water main and lateral installation. As such, the proposed Project would not conflict with an applicable plan, ordinance, or policy establishing measures of effectiveness for the performance of circulation, and impacts would be **less than significant**.

b. Would the Project conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?

No Impact. The proposed Project would not generate an increase in vehicle miles traveled, as the Project is a utility replacement that would require the installation of a new water main and laterals in an established West Sacramento neighborhood. As such, the proposed Project would not conflict with an applicable congestion management program and established LOS standards or travel demand measures. **No impact** would occur with implementation of the proposed Project.

c. Would the Project result in a change in air traffic patterns, including either an increase in traffic levels or a change in location which results in substantial safety risks?

No Impact. The proposed Project would install water mains and laterals that would be buried in road ROWs in an established West Sacramento neighborhood. The Project does not include tall design features that would penetrate areas of air traffic patterns and therefore would not generate substantial safety risks to aircraft. **No impact** would occur with implementation of the proposed Project.

d. Would the Project substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

No Impact. The proposed Project would install a new water main and laterals underground in existing road ROW. The proposed Project does not include hazardous design features or components that are incompatible with the residential uses located in the neighborhood where installation would occur. As such, **no impact** would occur with implementation of the proposed Project.

e. Would the Project result in inadequate emergency access?

Less Than Significant Impact with Mitigation. Construction of the proposed Project could temporarily result in inadequate emergency access due to temporary closures and/or road detours. The City of West Sacramento published an emergency evacuation map in 2009, detailing routes near the Project site designated as Surface Evacuation Streets. Linden Road, Jefferson Boulevard, and Village Parkway are the closest Surface Evacuation Streets to the proposed Project. During construction, **Mitigation Measure MM-TRANS-1** (discussed below) would be implemented to reduce construction impacts to emergency access within the Project area. With implementation of this mitigation measure, impacts would be **less than significant**.

f. Would the Project conflict with adopted policies, plans or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?

No Impact. The proposed Project includes the installation of a new water main and lateral lines in an established neighborhood in West Sacramento. This type of project would not conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, nor would it

decrease the performance or safety of such facilities. As such, **no impact** would occur with implementation of the proposed Project.

3.16.3 Mitigation Measures

The following mitigation measure would be implemented to reduce impacts to emergency access during Project construction:

MM-TRANS-1: A Traffic Management Plan/Emergency Services Plan shall be prepared by the Project proponent following the Yolo County Multi-Hazard Functional Plan recommendations. This plan shall be implemented during construction of the proposed Project to ensure that emergency access to and from the Project site is provided during construction activities and to ensure that emergency responders (i.e., fire, law enforcement, and paramedics/emergency medical technicians) are aware of potential detours and road closures to help in reducing emergency response times to the Project site.

Implementation of this mitigation measure during Project construction would reduce impacts to **less than significant.**

3.17 TRIBAL CULTURAL RESOURCES

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:				
a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)	☐	☒	☐	☐
b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.	☐	☒	☐	☐

3.17.1 Environmental Setting

Please refer to Section 3.5, Cultural Resources, for the environmental setting.

3.17.2 Regulatory Setting

CEQA, relevant sections of the PRC, and Section 7050.5 of the California Health and Safety Code make up the regulatory framework for cultural resources on the Project site.

3.17.2.1 California Environmental Quality Act

CEQA applies to all discretionary projects undertaken or subject to approval by the State's public agencies (14 CCR §15022(i)). CEQA states that it is the policy of the State of California to "take all action necessary to provide the people of this state with… historic environmental qualities… and preserve for future generations examples of the major period of California history" (PRC §21001(b), (c)). Under the provisions of CEQA, "A project with an effect that may cause a substantial adverse change in the significance of an historical resource is a project that may have a significant effect on the environment" (14 CCR §15126.4 (a)(1)).

3.17.2.2 Tribal Cultural Resources

AB 52, which became law on January 1, 2015, provides for consultation with California Native American tribes during the CEQA process, and equates significant impacts to "tribal cultural resources" with significant environmental impacts. PRC §21074 states that "tribal cultural resources" are:

- Sites, features, places, cultural landscapes, sacred places, and objects with cultural value to a California Native American tribe and are one of the following:

  A. Included or determined to be eligible for inclusion in the California Register.

  B. Included in a local register of historical resources as defined in subdivision (k) of PRC §5020.1.

  C. A resource determined by the Lead Agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of PRC §5024.1. In applying the criteria set forth in subdivision (c) of PRC §5024.1 for the purposes of this paragraph, the Lead Agency shall consider the significance of the resource to a California Native American tribe.

  D. The consultation provisions of the law require that within 14 days of determining that a project application is complete, or a decision by a public agency to undertake a project, the Lead Agency must notify tribes of the opportunity to consult on the project. California Native American tribes must be recognized by the NAHC as traditionally and culturally affiliated with the project site, and must have previously requested that the Lead Agency notify them of projects. Tribes have 30 days following notification of a project to request consultation with the Lead Agency.

The purpose of consultation is to inform the Lead Agency in its identification and determination of the significance of tribal cultural resources. Consultation may also include a discussion of project alternatives, significant effects, and mitigation measures, and should be undertaken in good faith by both the tribe and the Lead Agency. If a project is determined to result in a significant impact to an identified tribal cultural resource, the consultation process must occur and conclude prior to adoption of a Negative Declaration or MND, or certification of an EIR (PRC §21080.3.1, §21080.3.2, and §21082.3).

Please refer to Section 3.5, Cultural Resources, for the complete regulatory setting.

3.17.3 Discussion

Would the Project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k).

Less Than Significant Impact with Mitigation. A preliminary records search of the Project site and a 0.5-mile buffer was conducted on February 7, 2017, at the Northwest Information Center of the California Historical Resources Information System at Sonoma State University. A subsequent records search was done at the Northwest Information Center on June 22, 2017, to identify cultural resource investigations and recorded archaeological resources, as well as supplemental ethnographic information, historical literature, historical maps, local inventories, and General Land

Office and Rancho Plat maps relevant to the Project. The records search did not identify any listed or eligible tribal cultural resources within the Project site.

Consultation with the NAHC was initiated by LSA on June 6, 2017. The NAHC responded stating that tribal cultural resources were present in the Project area. Consultation with Native American tribal organizations was initiated by LSA on June 12, 2017, and followed by formal meetings with the City as described in the Environmental Setting discussion in Section 3.5, Cultural Resources, where it was determined that the Project site is sensitive for tribal cultural resources. None of the tribal cultural resources identified were listed in the California Register or a local register; however, eligibility for listing was not evaluated. As such, the Project has the potential to result in impacts to tribal cultural resources that are assumed eligible for listing in the California Register or a local register. Implementation of **Mitigation Measure MM-CUL-1** will reduce the potential for impacts to **less than significant.**

b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision c of Public Resources Code Section 5024.1. In applying the criteria set forth in subdivision c of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.

Less Than Significant Impact with Mitigation. As stated above, tribal cultural resources are present in the Project site. On August 23, 2017, representatives of the UAIC met with LSA and the City for a formal consultation. UAIC provided a sensitivity map, requested an Archaeological Work Plan, and requested that a tribal monitor be present during archaeological testing. On September 21, 2017, representatives of the YDWN met with LSA and the City for a formal consultation. YDWN requested that a tribal monitor be present during archaeological testing as well.

Mitigation Measure MM-CUL-1 requires that a qualified archaeologist and a tribal representative shall be present to monitor Project excavation, grading, and other earthmoving activities within 100 feet of the current site boundary of CA-YOL-18. With implementation of **Mitigation Measure MM-CUL-1,** the potential for impacts to tribal cultural resources would be reduced to **less than significant.**

3.17.4 Mitigation Measures

Please refer to Section 3.5, Cultural Resources, for **Mitigation Measure MM-CUL-1**.

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3.18 UTILITIES AND SERVICE SYSTEMS

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
Would the project:				
a. Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	☐	☐	☐	☒
b. Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	☐	☐	☒	☐
c. Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	☐	☐	☐	☒
d. Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	☐	☐	☒	☐
e. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	☐	☐	☐	☒
f. Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	☐	☐	☒	☐
g. Comply with federal, state, and local statutes and regulations related to solid waste?	☐	☐	☐	☒

3.18.1 Environmental Setting

The potable water supply for West Sacramento is sourced from the Sacramento River and water available under contract with the United States Bureau of Reclamation. The combined maximum available water supply from these sources is 23,600 acre-feet per year (AFY) in an average year and 5,900 AFY during a single dry year. Based on projected City of West Sacramento population growth, between 2015 and 2030, the demand for water is expected to range between 16,418 and 20,123 AFY. In 2035, the City of West Sacramento would require 23,920 AFY to adequately supply the estimated population; as such, the water supply would be deficient by 320 AFY in 2035 based on population projections.

Collection and conveyance of wastewater is currently provided by the City of West Sacramento. The Sacramento Regional County Sanitation District provides wastewater treatment and disposal through its Sacramento Regional Wastewater Treatment Plant. The average dry-weather flow for wastewater is estimated at 11.56 million gallons per day, while the peak wet-weather flow is estimated at 35.20 million gallons per day. The Sacramento Regional County Sanitation District and Sacramento Regional Wastewater Treatment Plant currently have daily capacity to receive wastewater flows from West Sacramento.

Storm water is currently managed in West Sacramento primarily by Reclamation District Number 900 and the City, and to a smaller extent by Reclamation District 537.

3.18.2 Discussion

a. Would the Project exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?

No Impact. The proposed Project includes the abandonment of existing deficient water mains in an established neighborhood and the installation of new water mains and laterals to provide efficient water supply to existing residential units. The portion of the proposed Project that extends into the Liberty Specific Plan area will be abandoned in the future, as it is temporary. The developer of the Liberty Specific Plan will be directed by the City (per a condition of approval) to connect that proposed water distribution system to the proposed Project at Tamarack Road and at the north water line coming east from Bastone Court. The proposed Project itself does not include an increase in population that would cause the City of West Sacramento to exceed the wastewater treatment requirements of the CVRWQCB. As such, **no impacts** would occur and no mitigation measures would be required.

b. Would the Project require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?

Less than Significant Impact. The proposed Project would not construct new residential or commercial uses that could generate increases in water demand or the need for wastewater service disposal. As such, the construction of new water or wastewater treatment facilities or the expansion of existing facilities would not be required. The proposed Project would abandon the existing 16-inch, 12-inch, 8-inch, and 6-inch water mains in the neighborhood. The new water mains being installed would be 8 inches, 16 inches, and 12 inches in size; 111 of the new laterals will be 0.75 inch in size and 94 new laterals will be 1 inch in size. Short-term service disruptions of up to a few hours would be unavoidable while connecting each property to the new water infrastructure. However, each property owner will be notified at least 48 hours and the City will direct the construction contractor not to exceed four hours of service interruption for any individual property.

It should be noted that the Liberty Specific Plan will eventually connect its water system to the proposed Project at Tamarack Road and the north water line coming east from Bastone Court to maintain the water distribution loop system. The proposed Project's water main that will extend into the Liberty Specific Plan will be abandoned in the future once development of the specific plan area commences. An EIR was prepared for the Liberty Specific Plan, and utilities and services were analyzed in the document. The environmental document concluded that the existing water treatment plant had adequate treatment capacity and would not require expansion as a result of the Liberty Specific Plan build out. Additionally, it is noted that if the water infrastructure is not updated or expanded, the City's projected population increases and associated increased demand could lead to a deficit in potable and fire service water supplies whether or not the Liberty Specific Plan is developed.

As such, implementation of the proposed Project, as well as connecting to the Liberty Specific Plan area to complete the water system, would not require the construction or expansion of new or existing water and wastewater facilities. Impacts would be **less than significant**.

c. Would the Project require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?

No Impact. Please see the discussion under Threshold b). The proposed Project would not require construction or expansion of storm water drainage facilities, as the Project involves replacement of existing water delivery infrastructure to an established neighborhood. As such, **no impact** would occur and no mitigation measures would be required.

d. Would the Project have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?

Less than Significant Impact. Please see the discussion under Threshold b). The proposed Project includes the replacement of water mains and laterals in an established West Sacramento neighborhood. The existing water conveyance system is old and deficient, and system maintenance/interruption of service occurs frequently. The proposed Project is not growth-inducing; as such, the existing water supplies to the neighborhood would continue to be sufficient as under existing conditions. The proposed Project would connect to the future Liberty Specific Plan area; however, analysis has previously been conducted and a determination was made that such a connection would not in itself require new or expanded water supply entitlements. Impacts would be **less than significant** and no mitigation measures are required.

e. Would the Project result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

No Impact. Please see the discussion under Thresholds b), c), and d). Because the proposed Project would include installation of water mains and laterals to replace existing infrastructure in an established neighborhood, wastewater would not be generated due to Project implementation. As such, the existing wastewater treatment provider would continue to have adequate capacity. **No impact** would occur and no mitigation measures are required.

f. Would the Project be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?

Less than Significant. Solid waste disposal is provided and governed by the City of West Sacramento General Plan in close consultation with the Yolo County Department of Public Works. This plan defines the projects for recycling and reuse, resource recovery, and disposal. Solid waste currently is disposed of at the Yolo County Central Landfill in Woodland. In fall 2009, the remaining capacity for the Yolo County Central Landfill was 37,108,000 cubic yards.

Installing water mains and laterals would generate some quantities of earth and concrete material that would require disposal. Solid waste materials such as asphalt, concrete, pipes, and gravel would be removed from the Project site and disposed of at the Yolo County Central Landfill. The current Yolo County Central Landfill closure projection is in 2070, which takes into account the disposal growth rate. The landfill therefore has sufficient capacity to serve the solid waste disposal needs of the proposed Project during construction. It should be noted that the existing water conveyance infrastructure system in the neighborhood would be abandoned in place and disposal of infrastructure components would not be required. Impacts would be **less than significant** and mitigation measures would not be required.

g. Would the Project comply with federal, state, and local statutes and regulations related to solid waste?

No Impact. Construction of the proposed Project would comply with all federal, State, and local statutes and regulations related to solid waste. As such, **no impact** would occur and mitigation measures would not be required.

3.18.3 Mitigation Measures

No mitigation is required.

3.19 MANDATORY FINDINGS OF SIGNIFICANCE

	Potentially Significant Impact	Less Than Significant Impact with Mitigation	Less Than Significant Impact	No Impact
a. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	☐	☒	☐	☐
b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?	☐	☒	☐	☐
c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	☐	☐	☒	☐

3.19.1 Discussion

a. Does the Project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

Less Than Significant Impact with Mitigation. The proposed Project would include the installation of a water main and laterals connecting to existing residential units in an established West Sacramento neighborhood. As described throughout this document, implementation of the proposed Project would have the potential to impact air quality, biological resources, cultural resources, geology and soils, hazards and hazardous materials, hydrology and water quality, noise, transportation/traffic, and tribal cultural resources. With implementation of the mitigation measures recommended in this document, compliance with County of Yolo and City of West Sacramento requirements, and application of standard practices, the proposed Project would not degrade the quality of the environment, substantially reduce the habitat of fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, or reduce the number or restrict the range of a rare or endangered plant or animal.

b. Does the Project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?

Less Than Significant Impact with Mitigation. The impacts of the proposed Project described in this environmental document would be individually limited and would not be cumulatively considerable. All environmental impacts of the proposed Project would be reduced to a less than significant level with implementation of the mitigation measures recommended throughout this document. When viewed in conjunction with other closely related past, present, or reasonably foreseeable future projects, development of this Project would not cumulatively contribute to impacts.

c. Does the Project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

Less Than Significant Impact. As described in this document, implementation of the proposed Project could result in temporary impacts to air quality, cultural resources, hazards and hazardous materials, noise, and transportation/traffic during the construction period. Implementation of the mitigation measures recommended in this document, compliance with County of Yolo and City of West Sacramento regulations, application of standard construction practices, and conditions of Project approval would ensure that the proposed Project would not result in environmental impacts that would cause substantial direct or indirect adverse impacts on human beings. Impacts would be **less than significant.**

3.19.2 Mitigation Measures

No additional mitigation measures would be required beyond what is presented in this document.

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5.0 RESPONSE TO COMMENTS

Letter A: UAIC Comment Letter (5-21-18)

CITY OF WEST SACRAMENTO

RECEIVED
Marcos
MAY 03, 2018
UNITED AUBURN INDIAN COMMUNITY

Letter A

NEGATIVE DECLARATION
AND REQUEST FOR COMMENTS

DATE: May 2, 2018

TO: Interested Agencies and Individuals

FROM: Kathy Allen, Associate Planner

PROJECT NAME: Linden Acres Water Main Replacement Project

PROJECT LOCATION: Linden Road to the north, Mojave Drive to the west, Trinity Way and the proposed Liberty Specific Plan to the south, and the proposed Liberty Specific Plan to the east.

This Negative Declaration is being sent to you for your review and comment. Please advise this office within thirty days (30) days of the date above, of any comments you may have on the Negative Declaration. Failure to respond will be considered acceptance of the draft statements.

GENERAL QUESTIONS

1. Do you have jurisdiction by law over this project? X YES ___ NO

2. Are you a responsible agency? ___ YES X NO

   If yes, indicate required permits: _____

3. Do you recommend:
   ___ Approval of the draft as submitted.
   X Have the following comments summarized below.

4. Recommended conditions of approval (Use additional pages if necessary).
   Based on sensitivity of area and know burial sites in close proximity, UAIC recommends a tribal monitor.

A-1

1110 West Capitol Avenue
West Sacramento, CA 95691
(916) 617-4500
www.cityofwestsacramento.org

CITY OF WEST SACRAMENTO

5. For each mitigation measure or condition of approval, specify how your agency intends to monitor the measure. Please clearly describe the monitoring program, responsible individual(s), the timing for monitoring, and submittal of reports. (Use additional pages if necessary.)

see mitigation measures.

6. Indicate areas of environmental concern and availability of appropriate technical data: (Use additional pages if necessary.)

Call for map.

A-1 Con.

7. Have you previously reviewed an application on any portion of this project?

___ YES (X) NO

Name of Respondent: Marcos Guerrero

Title: Cultural Resources Manager

Telephone Number: 916-300-8792

E-mail: mguerrero@auburnrancheria.com

Name of Reviewing Agency: UAIC

Tribal Cultural Resource Avoidance Mitigation Measure

Avoidance and preservation in place is the preferred manner of mitigating impacts to tribal cultural resources and will be accomplished by several means, including:

- Planning construction to avoid tribal cultural resources, archaeological sites and/ or other resources; incorporating sites within parks, green-space or other open space; covering archaeological sites; deeding a site to a permanent conservation easement; or other preservation and protection methods agreeable to consulting parties and regulatory authorities with jurisdiction over the activity. Recommendations for avoidance of cultural resources will be reviewed by the CEQA lead agency representative, interested Native American Tribes and the appropriate agencies, in light of factors such as costs, logistics, feasibility, design, technology and social, cultural and environmental considerations, and the extent to which avoidance is consistent with project objectives. Avoidance and design alternatives may include realignment within the project area to avoid cultural resources, modification of the design to eliminate or reduce impacts to cultural resources or modification or realignment to avoid highly significant features within a cultural resource. Native American Representatives from interested Native American Tribes will be allowed to review and comment on these analyses and shall have the opportunity to meet with the CEQA lead agency representative and its representatives who have technical expertise to identify and recommend feasible avoidance and design alternatives, so that appropriate and feasible avoidance and design alternatives can be identified.

A-2

- If the resource can be avoided, the construction contractor(s), with paid Native American Monitors from culturally affiliated Native American Tribes present, will install protective fencing outside the site boundary, including a buffer area, before construction restarts. The construction contractor(s) will maintain the protective fencing throughout construction to avoid the site during all remaining phases of construction. The area will be demarcated as an "Environmentally Sensitive Area". Native American Representatives from interested Native American Tribes and the CEQA lead agency representative will also consult to develop measures for long term management of the resource and routine operation and maintenance within culturally sensitive areas that retain resource integrity, including tribal cultural integrity, and including archaeological material, Traditional Cultural Properties and cultural landscapes, in accordance with state and federal guidance including National Register Bulletin 30 (*Guidelines for Evaluating* and *Documenting Rural Historic Landscapes*), Bulletin 36 (*Guidelines for Evaluating and Registering Archaeological Properties*), and Bulletin 38 (*Guidelines for Evaluating and Documenting Traditional Cultural Properties*); National Park Service Preservation Brief 36 (*Protecting Cultural Landscapes: Planning, Treatment and Management of Historic Landscapes*) and using the Advisory Council on Historic Preservation (ACHP) *Native American Traditional Cultural Landscapes Action Plan* for further guidance. Use of temporary and

A-2
Con.

permanent forms of protective fencing will be determined in consultation with Native American Representatives from interested Native American Tribes.

Native American Monitoring Mitigation Measure

To minimize the potential for destruction of or damage to existing or previously undiscovered archaeological and Cultural resources and to identify any such resources at the earliest possible time during project-related earthmoving activities, THE PROJECT PROPONENT and its construction contractor(s) will implement the following measures:

A-3

- Paid Native American Monitors from culturally affiliated Native American Tribes will be invited to monitor the vegetation grubbing, stripping, grading or other ground-disturbing activities in the project area to determine the presence or absence of any cultural resources. Native American Representatives from cultural affiliated Native American Tribes act as a representative of their Tribal government and shall be consulted before any cultural studies or ground-disturbing activities begin.

- Native American Representatives and Native American Monitors have the authority to identify sites or objects of significance to Native Americans and to request that work be stopped, diverted or slowed if such sites or objects are identified within the direct impact area. Only a Native American Representative can recommend appropriate treatment of such sites or objects.

Inadvertent Discoveries Mitigation Measures

Develop a standard operating procedure, points of contact, timeline and schedule for the project so all possible damages can be avoided or alternatives and cumulative impacts properly accessed.

If potential tribal cultural resources, archaeological resources, other cultural resources, articulated, or disarticulated human remains are discovered by Native American Representatives or Monitors from interested Native American Tribes, qualified cultural resources specialists or other Project personnel during construction activities, work will cease in the immediate vicinity of the find (based on the apparent distribution of cultural resources), whether or not a Native American Monitor from an interested Native American Tribe is present. A qualified cultural resources specialist and Native American Representatives and Monitors from culturally affiliated Native American Tribes will assess the significance of the find and make recommendations for further evaluation and treatment as necessary. These recommendations will be documented in the project record. For any recommendations made by interested Native American Tribes which are not implemented, a justification for why the recommendation was not followed will be provided in the project record.

A-4

If adverse impacts to tribal cultural resources, unique archeology, or other cultural resources occurs, then consultation with UAIC regarding mitigation contained in the Public Resources Code sections 21084.3(a) and (b) and CEQA Guidelines section 15370 should occur, in order to coordinate for compensation for the impact by replacing or providing substitute resources or environments.

Tribal Cultural Resource – Awareness Training - Mitigation Measure

A-5

A consultant and construction worker tribal cultural resources awareness brochure and training program for all personnel involved in project implementation will be developed in coordination with interested Native American Tribes. The brochure will be distributed and the training will be conducted in coordination with qualified cultural resources specialists and Native American Representatives and Monitors from culturally affiliated Native American Tribes before any stages of project implementation and construction activities begin on the project site. The program will include relevant information regarding sensitive tribal cultural resources, including applicable regulations, protocols for avoidance, and consequences of violating State laws and regulations. The worker cultural resources awareness program will also describe appropriate avoidance and minimization measures for resources that have the potential to be located on the project site and will outline what to do and whom to contact if any potential archaeological resources or artifacts are encountered. The program will also underscore the requirement for confidentiality and culturally-appropriate treatment of any find of significance to Native Americans and behaviors, consistent with Native American Tribal values.

Response to Comment A-1: This comment is based on a questionnaire the City of West Sacramento sent out to interested agencies and individuals requesting comments on the IS/MND. As this comment is a questionnaire, it does not have any effect on the information prepared for the IS/MND. As such, revisions to the environmental document are not required and no further response is required based on this comment.

Response to Comment A-2: The commenter requests that "Tribal Cultural Resource Avoidance Mitigation Measure" is implemented as part of the Project. Examples of measures suggested by the commenter include, but are not limited to, "planning construction to avoid tribal cultural resources, archaeological sites, and/or other resources; incorporating sites within parks, green-space or other open space; covering archaeological sites; deeding a site to a permanent conservation easement; or other preservation and protection methods agreeable to consulting parties and regulatory authorities with jurisdiction over the activity" and "if resource can be avoided, the construction contractor(s), with paid Native American Monitors from culturally affiliated Native American Tribes present, will install protective fencing outside the boundary, including a buffer area, before construction starts." The IS/MND, in Section 3.5 Cultural Resources, provides Mitigation Measure CUL-1 (MM-CUL-1), which implements similar strategies as requested by the commenter. MM-CUL-1 indicates procedures will be followed prior to and during construction if sensitive tribal cultural resources are discovered and indicates a qualified archaeologist and a geographically-affiliated tribal member will be present to monitor Project excavation near known resources. MM-CUL-1 also stipulates that if a cultural resource is inadvertently encountered during construction, all work will stop within 50 feet of the discovery until recommendations regarding the treatment of the discovery and proper mitigation measures are developed. Since the language of MM-CUL-1 is similar to the commenter's request of implementing their specific measures and has similar mitigation intention, revisions of MM-CUL-1 to include specific language as presented by the commenter is not warranted. As such, no revisions to the environmental document have been made.

Response to Comment A-3: The commenter requests that "Native American Monitoring Mitigation Measures" are implemented as part of the Project. The commenter requests that measures such as "paid Native American Monitors from culturally affiliated Native American Tribes will be invited to monitor vegetation grubbing, stripping, grading, or other ground-disturbing activities in the project area to determine the presence or absence of any cultural resources" and "Native American Representatives and Native American Monitors have the authority to identify sites or objects of significance to Native American and to request that work be stopped, diverted or slowed if such sites or objects are identified within the direct impact area" be included in the environmental document. Mitigation Measure MM- CUL-1, in Section 3.5, of this environmental document includes similar strategies that will be implemented as part of the Project. Mitigation Measure CUL-1 indicates that a qualified archaeologist and a tribal representative from a geographically-affiliated tribe shall be present to monitor Project excavation, grading, and other earthmoving activities within 100 feet of the current site boundary of CA-YOL-18. The Mitigation Measure also provides measures for work stoppage and or avoidance if prehistoric or historical archaeological materials are inadvertently encountered. Since the language of MM-CUL-1 is similar to the commenter's request of implementing their specific measures and has similar mitigation intention, revisions of MM-CUL-1 to include specific language as presented by the commenter is not warranted. As such, no revisions to the environmental document have been made.

Response to Comment A-4: The commenter requests that "Inadvertent Discoveries Mitigation Measures" are implemented as part of the Project. The commenter requests that if potential tribal cultural resources, archaeological resources, other cultural resources, articulated, or disarticulated human remains are discovered that work will cease in the immediate vicinity of the find whether or not a Native American Monitor or Native American Tribe is present. Mitigation Measure MM-CUL-1 provides similar provisions requiring construction stoppage if such resources are inadvertently encountered during construction and also requires notification of geographically-affiliated Native American tribal members, archaeologists, and other qualified personal to determine the significance of such finds. Since the language of MM-CUL-1 is similar to the commenter's request of implementing their specific measures and has similar mitigation intention, revisions of MM-CUL-1 to include specific language as presented by the commenter is not warranted. As such, no revisions to the environmental document have been made.

Response to Comment A-5: The commenter requests that "Tribal Cultural Resource – Awareness Training – Mitigation Measures" are implemented as part of the Project. Specifically, the commenter requests that language be added into the environmental document that requires "a consultant and construction worker tribal cultural resource awareness brochure and training program for all personnel involved in project implementation will be developed in coordination with interested Native American Tribes" Mitigation Measure CUL-1 includes provisions that a pre-construction briefing will be held by a professional archaeologist to alert construction staff to the possibility of exposing significant historic or prehistoric archaeological resources (including Native American resources) within the Project area. The briefing will also discuss any resources that could be exposed, the need to stop excavation at the discovery site, and procedures to follow regarding discovery protection and notification. The language presented in Mitigation Measure MM-CUL-1 is similar in nature and intent as the commenter's request; however, a revision has been made to state that a geographically-affiliated Native American Representative will be invited to assist in the training.

Letter B: Central Valley Regional Water Quality Control Board (5/24/18)

CALIFORNIA
Water Boards

EDMUND G. BROWN JR.
GOVERNOR

MATTHEW RODRIQUEZ
SECRETARY FOR
ENVIRONMENTAL PROTECTION

Letter B

Central Valley Regional Water Quality Control Board

Governor's Office of Planning & Research
MAY 31 2018
STATE CLEARINGHOUSE

Clear
5-31-18
E

24 May 2018

Mauricio Meza-Pedraza
City of West Sacramento
1110 West Capitol Avenue
West Sacramento, CA 95691

CERTIFIED MAIL
91 7199 9991 7039 6992 3990

COMMENTS TO REQUEST FOR REVIEW FOR THE MITIGATED NEGATIVE DECLARATION, LINDEN ACRES WATER MAIN REPLACEMENT PROJECT, SCH# 2018052001, YOLO COUNTY

B-1

Pursuant to the State Clearinghouse's 2 May 2018 request, the Central Valley Regional Water Quality Control Board (Central Valley Water Board) has reviewed the *Request for Review for the Draft Mitigated Negative Declaration* for the Linden Acres Water Main Replacement Project, located in Yolo County.

Our agency is delegated with the responsibility of protecting the quality of surface and groundwaters of the state; therefore our comments will address concerns surrounding those issues.

I. Regulatory Setting

B-2

Basin Plan

The Central Valley Water Board is required to formulate and adopt Basin Plans for all areas within the Central Valley region under Section 13240 of the Porter-Cologne Water Quality Control Act. Each Basin Plan must contain water quality objectives to ensure the reasonable protection of beneficial uses, as well as a program of implementation for achieving water quality objectives with the Basin Plans. Federal regulations require each state to adopt water quality standards to protect the public health or welfare, enhance the quality of water and serve the purposes of the Clean Water Act. In California, the beneficial uses, water quality objectives, and the Antidegradation Policy are the State's water quality standards. Water quality standards are also contained in the National Toxics Rule, 40 CFR Section 131.36, and the California Toxics Rule, 40 CFR Section 131.38.

The Basin Plan is subject to modification as necessary, considering applicable laws, policies, technologies, water quality conditions and priorities. The original Basin Plans were adopted in 1975, and have been updated and revised periodically as required, using Basin Plan amendments. Once the Central Valley Water Board has adopted a Basin Plan amendment in noticed public hearings, it must be approved by the State Water Resources Control Board (State Water Board), Office of Administrative Law (OAL) and in some cases,

the United States Environmental Protection Agency (USEPA). Basin Plan amendments only become effective after they have been approved by the OAL and in some cases, the USEPA. Every three (3) years, a review of the Basin Plan is completed that assesses the appropriateness of existing standards and evaluates and prioritizes Basin Planning issues.

B-2 Con.

For more information on the *Water Quality Control Plan for the Sacramento and San Joaquin River Basins*, please visit our website: http://www.waterboards.ca.gov/centralvalley/water_issues/basin_plans/.

Antidegradation Considerations

All wastewater discharges must comply with the Antidegradation Policy (State Water Board Resolution 68-16) and the Antidegradation Implementation Policy contained in the Basin Plan. The Antidegradation Policy is available on page IV-15.01 at: http://www.waterboards.ca.gov/centralvalleywater_issues/basin_plans/sacsjr.pdf

In part it states:

B-3

Any discharge of waste to high quality waters must apply best practicable treatment or control not only to prevent a condition of pollution or nuisance from occurring, but also to maintain the highest water quality possible consistent with the maximum benefit to the people of the State.

This information must be presented as an analysis of the impacts and potential impacts of the discharge on water quality, as measured by background concentrations and applicable water quality objectives.

The antidegradation analysis is a mandatory element in the National Pollutant Discharge Elimination System and land discharge Waste Discharge Requirements (WDRs) permitting processes. The environmental review document should evaluate potential impacts to both surface and groundwater quality.

II. Permitting Requirements

Construction Storm Water General Permit

Dischargers whose project disturb one or more acres of soil or where projects disturb less than one acre but are part of a larger common plan of development that in total disturbs one or more acres, are required to obtain coverage under the General Permit for Storm Water Discharges Associated with Construction Activities (Construction General Permit), Construction General Permit Order No. 2009-009-DWQ. Construction activity subject to this permit includes clearing, grading, grubbing, disturbances to the ground, such as stockpiling, or excavation, but does not include regular maintenance activities performed to restore the original line, grade, or capacity of the facility. The Construction General Permit requires the development and implementation of a Storm Water Pollution Prevention Plan

B-4

(SWPPP).

For more information on the Construction General Permit, visit the State Water Resources Control Board website at:
http://www.waterboards.ca.gov/water_issues/programs/stormwater/constpermits.shtml.

B-4 Con.

Phase I and II Municipal Separate Storm Sewer System (MS4) Permits[1]

The Phase I and II MS4 permits require the Permittees reduce pollutants and runoff flows from new development and redevelopment using Best Management Practices (BMPs) to the maximum extent practicable (MEP). MS4 Permittees have their own development standards, also known as Low Impact Development (LID)/post-construction standards that include a hydromodification component. The MS4 permits also require specific design concepts for LID/post-construction BMPs in the early stages of a project during the entitlement and CEQA process and the development plan review process.

B-5

For more information on which Phase I MS4 Permit this project applies to, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/water_issues/storm_water/municipal_permits/.

For more information on the Phase II MS4 permit and who it applies to, visit the State Water Resources Control Board at:
http://www.waterboards.ca.gov/water_issues/programs/stormwater/phase_ii_municipal.shtml

Industrial Storm Water General Permit

Storm water discharges associated with industrial sites must comply with the regulations contained in the Industrial Storm Water General Permit Order No. 2014-0057-DWQ.

B-6

For more information on the Industrial Storm Water General Permit, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/water_issues/storm_water/industrial_general_permits/index.shtml.

Clean Water Act Section 404 Permit

If the project will involve the discharge of dredged or fill material in navigable waters or wetlands, a permit pursuant to Section 404 of the Clean Water Act may be needed from the United States Army Corps of Engineers (USACOE). If a Section 404 permit is required by the USACOE, the Central Valley Water Board will review the permit application to ensure

B-7

[1] Municipal Permits = The Phase I Municipal Separate Storm Water System (MS4) Permit covers medium sized Municipalities (serving between 100,000 and 250,000 people) and large sized municipalities (serving over 250,000 people). The Phase II MS4 provides coverage for small municipalities, including non-traditional Small MS4s, which include military bases, public campuses, prisons and hospitals.

that discharge will not violate water quality standards. If the project requires surface water drainage realignment, the applicant is advised to contact the Department of Fish and Game for information on Streambed Alteration Permit requirements.

B-7 Con.

If you have any questions regarding the Clean Water Act Section 404 permits, please contact the Regulatory Division of the Sacramento District of USACOE at (916) 557-5250.

Clean Water Act Section 401 Permit – Water Quality Certification

If an USACOE permit (e.g., Non-Reporting Nationwide Permit, Nationwide Permit, Letter of Permission, Individual Permit, Regional General Permit, Programmatic General Permit), or any other federal permit (e.g., Section 10 of the Rivers and Harbors Act or Section 9 from the United States Coast Guard), is required for this project due to the disturbance of waters of the United States (such as streams and wetlands), then a Water Quality Certification must be obtained from the Central Valley Water Board prior to initiation of project activities. There are no waivers for 401 Water Quality Certifications.

B-8

Waste Discharge Requirements – Discharges to Waters of the State

If USACOE determines that only non-jurisdictional waters of the State (i.e., "non-federal" waters of the State) are present in the proposed project area, the proposed project may require a Waste Discharge Requirement (WDR) permit to be issued by Central Valley Water Board. Under the California Porter-Cologne Water Quality Control Act, discharges to all waters of the State, including all wetlands and other waters of the State including, but not limited to, isolated wetlands, are subject to State regulation.

B-9

For more information on the Water Quality Certification and WDR processes, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/help/business_help/permit2.shtml.

Dewatering Permit

If the proposed project includes construction or groundwater dewatering to be discharged to land, the proponent may apply for coverage under State Water Board General Water Quality Order (Low Risk General Order) 2003-0003 or the Central Valley Water Board's Waiver of Report of Waste Discharge and Waste Discharge Requirements (Low Risk Waiver)
R5-2013-0145. Small temporary construction dewatering projects are projects that discharge groundwater to land from excavation activities or dewatering of underground utility vaults. Dischargers seeking coverage under the General Order or Waiver must file a Notice of Intent with the Central Valley Water Board prior to beginning discharge.

B-10

For more information regarding the Low Risk General Order and the application process, visit the Central Valley Water Board website at:

http://www.waterboards.ca.gov/board_decisions/adopted_orders/water_quality/2003/wqo/wqo2003-0003.pdf

For more information regarding the Low Risk Waiver and the application process, visit the Central Valley Water Board website at:

http://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/waivers/r5-2013-0145_res.pdf

B-10 Con.

Regulatory Compliance for Commercially Irrigated Agriculture

If the property will be used for commercial irrigated agricultural, the discharger will be required to obtain regulatory coverage under the Irrigated Lands Regulatory Program. There are two options to comply:

1. **Obtain Coverage Under a Coalition Group.** Join the local Coalition Group that supports land owners with the implementation of the Irrigated Lands Regulatory Program. The Coalition Group conducts water quality monitoring and reporting to the Central Valley Water Board on behalf of its growers. The Coalition Groups charge an annual membership fee, which varies by Coalition Group. To find the Coalition Group in your area, visit the Central Valley Water Board's website at: http://www.waterboards.ca.gov/centralvalley/water_issues/irrigated_lands/for_growers/apply_coalition_group/index.shtml or contact water board staff at (916) 464-4611 or via email at IrrLands@waterboards.ca.gov.

B-11

2. **Obtain Coverage Under the General Waste Discharge Requirements for Individual Growers, General Order R5-2013-0100**. Dischargers not participating in a third-party group (Coalition) are regulated individually. Depending on the specific site conditions, growers may be required to monitor runoff from their property, install monitoring wells, and submit a notice of intent, farm plan, and other action plans regarding their actions to comply with their General Order. Yearly costs would include State administrative fees (for example, annual fees for farm sizes from 10-100 acres are currently $1,084 + $6.70/Acre); the cost to prepare annual monitoring reports; and water quality monitoring costs. To enroll as an Individual Discharger under the Irrigated Lands Regulatory Program, call the Central Valley Water Board phone line at (916) 464-4611 or e-mail board staff at IrrLands@waterboards.ca.gov.

Low or Limited Threat General NPDES Permit

If the proposed project includes construction dewatering and it is necessary to discharge the groundwater to waters of the United States, the proposed project will require coverage under a National Pollutant Discharge Elimination System (NPDES) permit. Dewatering discharges are typically considered a low or limited threat to water quality and may be covered under the General Order for *Dewatering and Other Low Threat Discharges to Surface Waters* (Low Threat General Order) or the General Order for *Limited Threat Discharges of Treated/Untreated Groundwater from Cleanup Sites, Wastewater from Superchlorination Projects, and Other Limited Threat Wastewaters to Surface Water*

B-12

(Limited Threat General Order). A complete application must be submitted to the Central Valley Water Board to obtain coverage under these General NPDES permits.

For more information regarding the Low Threat General Order and the application process, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/general_orders/r5-2013-0074.pdf

For more information regarding the Limited Threat General Order and the application process, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/board_decisions/adopted_orders/general_orders/r5-2013-0073.pdf

B-12 Con.

NPDES Permit

If the proposed project discharges waste that could affect the quality of surface waters of the State, other than into a community sewer system, the proposed project will require coverage under a National Pollutant Discharge Elimination System (NPDES) permit. A complete Report of Waste Discharge must be submitted with the Central Valley Water Board to obtain a NPDES Permit.

For more information regarding the NPDES Permit and the application process, visit the Central Valley Water Board website at:
http://www.waterboards.ca.gov/centralvalley/help/business_help/permit3.shtml

B-13

If you have questions regarding these comments, please contact me at (916) 464-4644 or Stephanie.Tadlock@waterboards.ca.gov.

Stephanie Tadlock

Stephanie Tadlock
Environmental Scientist

cc: State Clearinghouse unit, Governor's Office of Planning and Research, Sacramento

Response to Comment B-1: The commenter indicates that the Central Valley Regional Water Quality Control Board (Central Valley Water Board) reviewed the document and has provided comments. This comment is noted. No further response is required because this comment does not pertain to the adequacy of information and analysis presented in the environmental document.

Response to Comment B-2: The commenter discusses the regulatory setting surrounding Basin Plans and accompanying water quality objectives within the Central Valley Region. This comment is noted. No further response is required because the comment does not pertain to the adequacy of information and analysis presented in the environmental document.

Response to Comment B-3: The commenter discusses the regulatory setting surrounding wastewater discharges, and notes that the environmental document should evaluate potential impacts to both surface and groundwater quality. The Initial Study explains in the Hydrology and Water Quality Section (response to threshold question A) that the Project would implement BMPs, an SWPPP, and dewatering provisions (as applicable) to maintain water quality and comply with State Water Board's Regulation 68-16. These implementation strategies would ensure that water quality impacts associated with construction of the Project would be less than significant. The Central Valley Regional Water Quality Control Board states that the environmental document should analyze the potential for water degradation for both groundwater and surface water supplies. The Initial Study analyzes potential impacts to ground and surface water quality in the Hydrology section in compliance with the National Pollutant Discharge Elimination System (NPDES) permitting process.

Response to Comment B-4: The Central Valley Regional Water Quality Control Board states the parameters for which a project should seek to obtain coverages under the General Permit for Storm Water Discharges Associated with Construction Activities (Construction General Permit). In the Initial Study, under Section 3.6 Geology and Soils, threshold question B, it is discussed that the construction contractor would be required to comply with NPDES General Permit for Storm Water Discharges Associated with Construction and Land Disturbance Activities, 2009-0009-DWQ, as amended by 2010-0014-DWQ (General Construction Permit), during construction. Furthermore, potential short-term water quality impacts from construction related activities at the Project site would be minimized and reduced through implementation of BMPs and compliance with existing water quality regulatory requirements. These BMPs and compliance would be part of the conditions of approval for the Project to be approved.

Response to Comment B-5: The commenter summarizes the Municipal Separate Storm Sewer System Permit process. Comment is noted. To protect water quality and control sedimentation during and after Project implementation, the City will prepare and implement BMPs outlined in any authorizations or permits, issued under the authorities of the Clean Water Act. All refueling, maintenance, and staging of equipment and vehicles will occur at least 60 feet from riparian habitat or water bodies and not in a location from where a spill would drain directly toward aquatic habitat.

Response to Comment B-6: The commenter discusses the Industrial Storm Water General Permit process. This comment is directed towards parcels that will be used for industrial purposes. The proposed Project does not include industrial use components. No further response is required because this comment does not pertain to the adequacy of information and analysis presented in the environmental document.

Response to Comment B-7: The commenter discusses the requirement to obtain a permit pursuant to Section 404 of the Clean Water Act for all projects involving discharge or fill material in navigable waters or wetlands. The proposed Project is located in a fully urbanized location and would not involve discharge or fill material in navigable water or wetlands. The construction contractor would be required to comply with NPDES General Permit for Storm Water Discharge Associated with Construction and Land Disturbance Activities, 2009-0009-DWQ, as amended by 2010-0014-DWQ (General Construction Permit), during construction. With implementation of this permit as a condition of approval, necessary permitting will be pursued by the Project prior to construction as applicable.

Response to Comment B-8: The commenter discusses the requirements to obtain a Water Quality Certification pursuant to Section 401 of the Clean Water Act. The construction contractor would comply with NPDES General Permit for Storm Water Discharge Associated with Construction and Land Disturbance Activities, 2009-0009-DWQ, as amended by 2010-0014-DWQ (General Construction Permit), during construction. With implementation of this condition of approval and implementation of BMPs for water quality discussed in the Hydrology and Water Resources section of the IS/MND impacts to water quality would be less than significant and in compliance with the Water Quality Certification Section 401 and the Clean Water Act.

Response to Comment B-9: The commenter discusses Waste Discharge Requirements related to non-jurisdictional waters of the State, dredging activity, and septic tank and leach field regulation. The proposed Project would not be subject to these requirements as it is a water main replacement Project in an urbanized existing neighborhood. Additionally, BMPs discussed in the Hydrology and Water Quality section would be implemented to reduce waste discharge during Project construction. The proposed Project would not impact or be impacted by any septic tanks or leach field systems.

Response to Comment B-10: The commenter discusses the dewatering permit process. This comment is directed towards projects that discharge groundwater to land from excavation activities or from dewatering of underground utility vaults. Those projects would be eligible to apply for coverage under the Low Risk General Order or Low Risk Waiver through the Central Valley Water Board. With implementation of BMPs discussed in the Hydrology and Water Quality section of the IS/MND, the proposed Project would adhere to the requirements of the General Waste Discharge provisions. Additionally, the City would comply with all applicable provisions in the de minimus permit, including water sampling, analysis, and reporting of dewatering-related discharges as applicable and required.

Response to Comment B-11: The commenter discusses regulatory compliance for parcels that will be used for commercial irrigated agricultural use. The proposed Project is a water main replacement project and would not be used for commercial irrigated agricultural use. No further response is required because this comment does not pertain to the adequacy of information and analysis presented in the environmental document.

Response to Comment B-12: The commenter discusses the requirements for projects that include discharges of groundwater into waters of the U.S. during construction dewatering; the commenter indicates that these projects require coverage under a NPDES permit. With implementation of BMPs

associated with the NPDES permit as discussed in the Hydrology and Water Quality Resources section of the IS/MND, the City would prepare and implement temporary BMPs in compliance with provisions of the Caltrans Statewide NPDES Permit.

Response to Comment B-13: The commenter discusses the NPDES Permit Process. The City would be required to apply for an NPDES Permit and to follow the BMPs associated with the permit as a condition of approval. The City would prepare and implement temporary BMPs in compliance with provisions of the Caltrans Statewide NPDES Permit.

Letter C: State Clearinghouse Compliance Letter (6/1/18)

STATE OF CALIFORNIA

GOVERNOR'S OFFICE *of* PLANNING AND RESEARCH

EDMUND G. BROWN JR.
GOVERNOR

Letter C

KEN ALEX
DIRECTOR

June 1, 2018

Maricio Meza-Pedraza
City of West Sacramento
1110 W. Capitol Avenue, 2nd Floor
West Sacramento, CA 95691

Subject: Linden Acres Water Main Replacement Project
SCH#: 2018052001

Dear Maricio Meza-Pedraza:

The State Clearinghouse submitted the above named Mitigated Negative Declaration to selected state agencies for review. On the enclosed Document Details Report please note that the Clearinghouse has listed the state agencies that reviewed your document. The review period closed on May 31, 2018, and the comments from the responding agency (ies) is (are) enclosed. If this comment package is not in order, please notify the State Clearinghouse immediately. Please refer to the project's ten-digit State Clearinghouse number in future correspondence so that we may respond promptly.

Please note that Section 21104(c) of the California Public Resources Code states that:

C-1

> "A responsible or other public agency shall only make substantive comments regarding those activities involved in a project which are within an area of expertise of the agency or which are required to be carried out or approved by the agency. Those comments shall be supported by specific documentation."

These comments are forwarded for use in preparing your final environmental document. Should you need more information or clarification of the enclosed comments, we recommend that you contact the commenting agency directly.

This letter acknowledges that you have complied with the State Clearinghouse review requirements for draft environmental documents, pursuant to the California Environmental Quality Act. Please contact the State Clearinghouse at (916) 445-0613 if you have any questions regarding the environmental review process.

Sincerely,

Scott Morgan
Director, State Clearinghouse

Enclosures
cc: Resources Agency

Document Details Report
State Clearinghouse Data Base

SCH# 2018052001
Project Title Linden Acres Water Main Replacement Project
Lead Agency West Sacramento, City of

Type MND Mitigated Negative Declaration

Description The project is located in the city of West Sacramento and consist of ROW work and is bounded by Linden Rd to the north, Mojave Dr to the west, Trinity War and the proposed Liberty Specific Plan to the south, and the proposed Liberty SP to the east. The purpose of the project is to replace the existing backyard water mains with new mains in the city owned roadway within ROW and dedicated easements. The existing water mains will be abandoned in place at average depths ranging from 4 to 8 ft deep. The new water main will be placed in the existing roadway sections and will be accessible for future city maintenance. The replacement of water mains will include approx 11,000 lf of 16 and 18 in pipe with the addition of fire hydrants and service lines that will include new water meters. The service laterals consist of 111 pipes that are 0.75 inch in size and 94 pipes that are 1 inch in size. Most of the pipe excavations will be approx 4 ft deep, with a small section that will be as deep as 10 ft to connect to the existing system. The project will be implemented on the following roads: Linden Rd, Mojave Dr, Merced Way, Shasta Way, Carmel Court, Rubicon Way, Ironwood Way, Spruce St, Redwood Ave, Alder Way, Tamarack Rd, and Cedar St.

Lead Agency Contact

Name Maricio Meza-Pedraza
Agency City of West Sacramento
Phone (916) 617-4850 **Fax**
email
Address 1110 W. Capitol Avenue, 2nd Floor
City West Sacramento **State** CA **Zip** 95691

Project Location

County Yolo
City West Sacramento
Region
Lat / Long 38° 32' .581" N / 121° 31' .646" W
Cross Streets Linden Rd/Mojave Dr
Parcel No. city owned ROW
Township 8N **Range** 4E **Section** 57 **Base** MDBM

Proximity to:

Highways I-5
Airports Sacramento Executive
Railways SSR
Waterways Sacramento River
Schools River city HS
Land Use work will be occurring in city owned ROW roadways

Project Issues Aesthetic/Visual; Agricultural Land; Air Quality; Archaeologic-Historic; Biological Resources; Cumulative Effects; Drainage/Absorption; Flood Plain/Flooding; Forest Land/Fire Hazard; Geologic/Seismic; Landuse; Minerals; Noise; Population/Housing Balance; Public Services; Recreation/Parks; Schools/Universities; Sewer Capacity; Soil Erosion/Compaction/Grading; Solid Waste; Toxic/Hazardous; Traffic/Circulation; Vegetation; Water Quality; Water Supply; Wetland/Riparian

Reviewing Agencies Resources Agency; Central Valley Flood Protection Board; Department of Fish and Wildlife, Region 2; Department of Parks and Recreation; California Highway Patrol; Caltrans, District 3 N; Caltrans, Division of Aeronautics; State Water Resources Control Board, Division of Drinking Water; State Water Resources Control Board, Division of Drinking Water, District 9; Regional Water Quality Control

Note: Blanks in data fields result from insufficient information provided by lead agency.

Bd., Region 5 (Sacramento); Delta Protection Commission; Delta Stewardship Council; Native American Heritage Commission; State Lands Commission; Department of Toxic Substances Control

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Date Received 05/01/2018 ***Start of Review*** 05/02/2018 ***End of Review*** 05/31/2018

Response to Comment C-1: The commenter notes that the Initial Study was received by the State Clearinghouse and distributed to selected state agencies for review; any comments from those agencies received by the State Clearinghouse have been included with the commenter's letter. This comment is noted and no revisions to the environmental document are necessary.

6.0 MITIGATION MONITORING AND REPORTING PROGRAM

This Mitigation and Monitoring Reporting Program (MMRP) has been formulated based upon findings of the Initial Study/Mitigated Negative Declaration (IS/MND) prepared for the proposed Project. The purpose of this MMRP is to ensure the implementation of mitigation measures identified as part of the environmental review for the Project. The MMRP includes the following information:

- A list of mitigation measures
- The party responsible for implementing the mitigation measures
- The timing for implementation of the mitigation measure
- The agency/jurisdictional department responsible for monitoring the implementation
- The monitoring action and frequency

The City of West Sacramento must adopt this MMRP, or an equally effective program, if it approves the Project, with the mitigation measures that were adopted or made conditions of approval.

Monitoring Item Number	Initial Study Mitigation Measures	Mitigation Measure	Timing	Implementing Party	Monitoring Party	Frequency and Duration of Monitoring
1	MM AQ-1	The City of West Sacramento (City) or construction contractor shall implement the following measures at the Project site: ● Water all active construction sites at least twice daily. The frequency shall be based on the type of operation, soil, and wind exposure. ● Haul trucks shall maintain at least 2 feet of freeboard ● Cover all trucks hauling dirt, sand, or loose materials. ● Apply nontoxic binders (e.g., latex acrylic copolymer) to exposed areas after cut and fill operations and hydroseed the area as applicable. ● Apply chemical soil stabilizers on inactive construction areas (disturbed lands within construction projects that are unused for at least 4 consecutive days). ● Plant tree windbreaks on the windward perimeter of construction projects if adjacent to open land. ● Plant vegetative ground cover in disturbed areas as soon as possible. ● Cover inactive storage piles. ● Sweep streets if visible soil material is carried out from the construction site. ● Treat accesses to a distance of 100 feet from the paved road with a 6 to 12 inch layer of wood chips or mulch or with a 6-inch layer of gravel.	During Construction	City of West Sacramento or Construction Contractor	City of West Sacramento	During Construction
2	MM-BIO-1	The measures listed below shall be implemented to mitigate potential impacts to western burrowing owl: ● Preconstruction surveys for western burrowing owl shall be conducted by a qualified biologist in accordance with the California Department of Fish and Wildlife's (CDFW) 2012 Staff Report on Burrowing Owl Mitigation. ● If burrowing owls are identified during the preconstruction survey, passive exclusion shall be implemented per CDFW's 2012 Staff Report of Burrowing Owl Mitigation (including avoidance of occupied burrows during the breeding season [February 1 to August 31]) ● Following construction, all areas, temporarily impacted during Project construction shall be restored to pre-construction	Prior to and during construction	City of West Sacramento	Qualified Biologist	

<table>
<tr><th>Monitoring Item Number</th><th>Initial Study Mitigation Measures</th><th>Mitigation Measure</th><th>Timing</th><th>Implementing Party</th><th>Monitoring Party</th><th>Frequency and Duration of Monitoring</th></tr>
<tr><td></td><td></td><td>contours (if necessary) and revegetated with native species as specified in the table below:

Native Species Mix
<table>
<tr><th>Scientific Name</th><th>Common Name</th><th>Rate (lbs/acre)</th><th>Minimum Percent Germination</th></tr>
<tr><td>Artemisia douglasiana</td><td>Mugwort</td><td>2.0</td><td>50</td></tr>
<tr><td>Bromus carinatus carinatus</td><td>California brome</td><td>5.0</td><td>85</td></tr>
<tr><td>Elymus trachycaulus</td><td>Slender wheatgrass</td><td>2.0</td><td>60</td></tr>
<tr><td>Elymus X triticum</td><td>Regreen</td><td>10.0</td><td>80</td></tr>
<tr><td>Eschscholizia californica</td><td>California poppy</td><td>2.0</td><td>70</td></tr>
<tr><td>Hordeum brachyantherum</td><td>California barley</td><td>2.0</td><td>80</td></tr>
<tr><td>Lupinus bicolor</td><td>Bicolored lupine</td><td>4.0</td><td>80</td></tr>
</table></td><td></td><td></td><td></td><td></td></tr>
<tr><td>3</td><td>MM-CUL-1</td><td>Construction Monitoring. Prior to construction, the City shall retain a professional archaeologist <u>and invite a geographically-affiliated Native American Representative</u> to provide a pre-construction briefing to supervisory personnel of any excavation contractor to alert them to the possibility of exposing significant historic or prehistoric archaeological resources within the project area. The briefing shall discuss any resources that could be exposed, the need to stop excavation at the discovery site, and the procedures to follow regarding discovery protection and notification. The City will notify geographically-affiliated tribal groups 7 days prior to excavation, grading, and other earthmoving activities within 100 feet of the current site boundary of CA-YOL-18. During construction, a qualified archaeologist and a tribal representative from a geographically-</td><td>During Construction</td><td>City of West Sacramento</td><td>Qualified Archaeologist</td><td>During Construction</td></tr>
</table>

Monitoring Item Number	Initial Study Mitigation Measures	Mitigation Measure	Timing	Implementing Party	Monitoring Party	Frequency and Duration of Monitoring
		affiliated tribe shall be present to monitor Project excavation, grading, and other earthmoving activities within 100 feet of the current site boundary of CA-YOL-18. Monitoring shall continue until excavation, grading, and other earthmoving activities within 100 feet of the current site boundary have been completed. **Discovery of Unidentified Archaeological Resources.** If deposits of prehistoric or historical archaeological materials are encountered during Project construction activities, all work within 50 feet of the discovery shall be redirected and a qualified archaeologist should be contacted (if one is not already on site) to assess the situation and make recommendations regarding the treatment of the discovery, and to develop proper mitigation measures required for the discovery (California Code of Regulations [CCR] Title 14, § 15064.5 (f)). The City of West Sacramento shall also be notified. The archaeologist should prepare a report documenting the methods and results of the investigation, and provide recommendations for the treatment of the archaeological materials discovered. The report should be submitted to the City of West Sacramento and the Northwest Information Center. **Discovery of Human Remains.** During construction, consistent with the requirements outlined by CEQA Guidelines, Section 15064.5(e)(1), and in accordance with Section 7050.5 of the Health and Safety Code, Section 5097.98 of the PRC (Chapter 1492, Statutes of 1982, Senate Bill 297), as relevant, should be followed and no further disturbance shall occur until the Yolo County Coroner can evaluate them. If the human remains are of Native American origin, the coroner must notify the NAHC within 24 hours of identification. Pursuant to Section 5097.9 and 5097.993 of the PRC, the NAHC shall identify a “Native American Most Likely Descendent” to inspect the site and provide recommendations for the proper treatment of the remains and any associated grave goods.				

Monitoring Item Number	Initial Study Mitigation Measures	Mitigation Measure	Timing	Implementing Party	Monitoring Party	Frequency and Duration of Monitoring
4	MM-PALEO-1	If paleontological resources are encountered during project excavation and no monitor is present, all ground-disturbing activities within 50 feet of the find shall be redirected to other areas until a qualified paleontologist can be retained to evaluate the find and make recommendations for additional paleontological mitigation, which may include paleontological monitoring; collection of observed resources; preservation, stabilization, and identification of collected resources; curation of resources into a museum repository; and preparation of a final report documenting the monitoring methods and results to be submitted to the museum repository and the City.	During Construction	City of West Sacramento	Qualified Paleontologist	During Construction
5	MM-NOI-1	Prior to initiating construction, the Project proponent shall complete a noise reduction plan. The noise reduction plan shall identify the type and quantity of construction equipment to be operated, the expected noise levels of each piece of equipment, and the duration of operation at each area of construction. The noise reduction plan shall include measures to ensure construction of the Project will meet the standards of Chapter 17.32 of the City of West Sacramento Municipal Code (Performance Standards of Noise).	Prior to Construction	City of West Sacramento	City of West Sacramento	During Construction
6	MM-TRANS-1	A Traffic Management Plan/Emergency Services Plan shall be prepared by the Project proponent following the Yolo County Multi-Hazard Functional Plan recommendations. This plan shall be implemented during construction of the proposed Project to ensure that emergency access to and from the Project site is provided during construction activities and to ensure that emergency responders (i.e., fire, law enforcement, and paramedics/emergency medical technicians) are aware of potential detours and road closures to help in reducing emergency response times to the Project site.	Prior to Construction start	City of West Sacramento	City of West Sacramento	During Construction

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APPENDIX A

NOTICE OF COMPLETION

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Print Form

Appendix C

Notice of Completion & Environmental Document Transmittal

Mail to: State Clearinghouse, P.O. Box 3044, Sacramento, CA 95812-3044 (916) 445-0613
For Hand Delivery/Street Address: 1400 Tenth Street, Sacramento, CA 95814

SCH # 2018052001

Project Title: Linden Acres Water Main Replacement Project

Lead Agency: City of West Sacramento
Contact Person: Mauricio Meza-Pedraza
Mailing Address: 1110 West Capitol Avenue
Phone: 916 617 4850
City: West Sacramento
Zip: 95691
County: Yolo

Project Location: County: Yolo
City/Nearest Community: West Sacramento
Cross Streets: Linden Road/Mojave Drive
Zip Code: 95691
Longitude/Latitude (degrees, minutes and seconds): 38 ° 32 ′ 581 ″ N / 121 ° 31 ′ 646 ″ W
Total Acres: Boundary 109 acres
Assessor's Parcel No.: City owned Right-of-way (Road)
Section: 57
Twp.: 8N
Range: 4E
Base: Mt Diablo
Within 2 Miles: State Hwy #: Interstate 5
Waterways: Sacramento River
Airports: Sacramento Executive
Railways: SSR
Schools: River City H.S.

Document Type:

CEQA:
- ☐ NOP
- ☐ Early Cons
- ☐ Neg Dec
- ☒ Mit Neg Dec
- ☐ Draft EIR
- ☐ Supplement/Subsequent EIR (Prior SCH No.) _____
- Other: _____

NEPA:
- ☐ NOI
- ☐ EA
- ☐ Draft EIS
- ☐ FONSI

Other:
- ☐ Joint Document
- ☐ Final Document
- ☐ Other: _____

Local Action Type:

- ☐ General Plan Update
- ☐ General Plan Amendment
- ☐ General Plan Element
- ☐ Community Plan
- ☐ Specific Plan
- ☐ Master Plan
- ☐ Planned Unit Development
- ☐ Site Plan
- ☐ Rezone
- ☐ Prezone
- ☐ Use Permit
- ☐ Land Division (Subdivision, etc.)
- ☐ Annexation
- ☐ Redevelopment
- ☐ Coastal Permit
- ☒ Other: Utility

Development Type:

- ☐ Residential: Units _____ Acres _____
- ☐ Office: Sq.ft. _____ Acres _____ Employees _____
- ☐ Commercial: Sq.ft. _____ Acres _____ Employees _____
- ☐ Industrial: Sq.ft. _____ Acres _____ Employees _____
- ☐ Educational: _____
- ☐ Recreational: _____
- ☐ Water Facilities: Type _____ MGD _____
- ☐ Transportation: Type _____
- ☐ Mining: Mineral _____
- ☐ Power: Type _____ MW _____
- ☐ Waste Treatment: Type _____ MGD _____
- ☐ Hazardous Waste: Type _____
- ☒ Other: Utility Work - Water Main

Project Issues Discussed in Document:

- ☒ Aesthetic/Visual
- ☒ Agricultural Land
- ☒ Air Quality
- ☒ Archeological/Historical
- ☒ Biological Resources
- ☐ Coastal Zone
- ☒ Drainage/Absorption
- ☐ Economic/Jobs
- ☐ Fiscal
- ☒ Flood Plain/Flooding
- ☒ Forest Land/Fire Hazard
- ☒ Geologic/Seismic
- ☒ Minerals
- ☒ Noise
- ☒ Population/Housing Balance
- ☒ Public Services/Facilities
- ☒ Recreation/Parks
- ☒ Schools/Universities
- ☐ Septic Systems
- ☒ Sewer Capacity
- ☒ Soil Erosion/Compaction/Grading
- ☒ Solid Waste
- ☒ Toxic/Hazardous
- ☒ Traffic/Circulation
- ☒ Vegetation
- ☒ Water Quality
- ☒ Water Supply/Groundwater
- ☒ Wetland/Riparian
- ☐ Growth Inducement
- ☒ Land Use
- ☒ Cumulative Effects
- ☐ Other: _____

Present Land Use/Zoning/General Plan Designation:

Work will be occurring in City owned right-of-way roadways.

Project Description: *(please use a separate page if necessary)*

Please see separate pages that contain the Project Description.

Note: The State Clearinghouse will assign identification numbers for all new projects. If a SCH number already exists for a project (e.g. Notice of Preparation or previous draft document) please fill in.

Reviewing Agencies Checklist

Lead Agencies may recommend State Clearinghouse distribution by marking agencies below with and "X".
If you have already sent your document to the agency please denote that with an "S".

X Air Resources Board
___ Boating & Waterways, Department of
___ California Emergency Management Agency
___ California Highway Patrol
___ Caltrans District # ___
___ Caltrans Division of Aeronautics
___ Caltrans Planning
X Central Valley Flood Protection Board
___ Coachella Valley Mtns. Conservancy
___ Coastal Commission
___ Colorado River Board
___ Conservation, Department of
___ Corrections, Department of
___ Delta Protection Commission
___ Education, Department of
___ Energy Commission
X Fish & Game Region # 3
___ Food & Agriculture, Department of
___ Forestry and Fire Protection, Department of
___ General Services, Department of
___ Health Services, Department of
___ Housing & Community Development
___ Native American Heritage Commission

___ Office of Historic Preservation
___ Office of Public School Construction
___ Parks & Recreation, Department of
___ Pesticide Regulation, Department of
___ Public Utilities Commission
X Regional WQCB # 5
___ Resources Agency
___ Resources Recycling and Recovery, Department of
___ S.F. Bay Conservation & Development Comm.
___ San Gabriel & Lower L.A. Rivers & Mtns. Conservancy
___ San Joaquin River Conservancy
___ Santa Monica Mtns. Conservancy
___ State Lands Commission
___ SWRCB: Clean Water Grants
___ SWRCB: Water Quality
___ SWRCB: Water Rights
___ Tahoe Regional Planning Agency
___ Toxic Substances Control, Department of
___ Water Resources, Department of

___ Other: ___
___ Other: ___

Local Public Review Period (to be filled in by lead agency)

Starting Date May 2, 2018 Ending Date May 31, 2018

Lead Agency (Complete if applicable):

Consulting Firm: LSA
Address: 201 Creekside Ridge Court, Suite 250
City/State/Zip: Roseville, CA 95678
Contact: Chris Graham
Phone: 916 772 7450

Applicant: City of West Sacramento
Address: 1110 West Capitol Avenue
City/State/Zip: West Sacramento, CA 95691
Phone: 916-617-4850

Signature of Lead Agency Representative: Mauricio Meyer **Date:** 5/1/18

Authority cited: Section 21083, Public Resources Code. Reference: Section 21161, Public Resources Code.

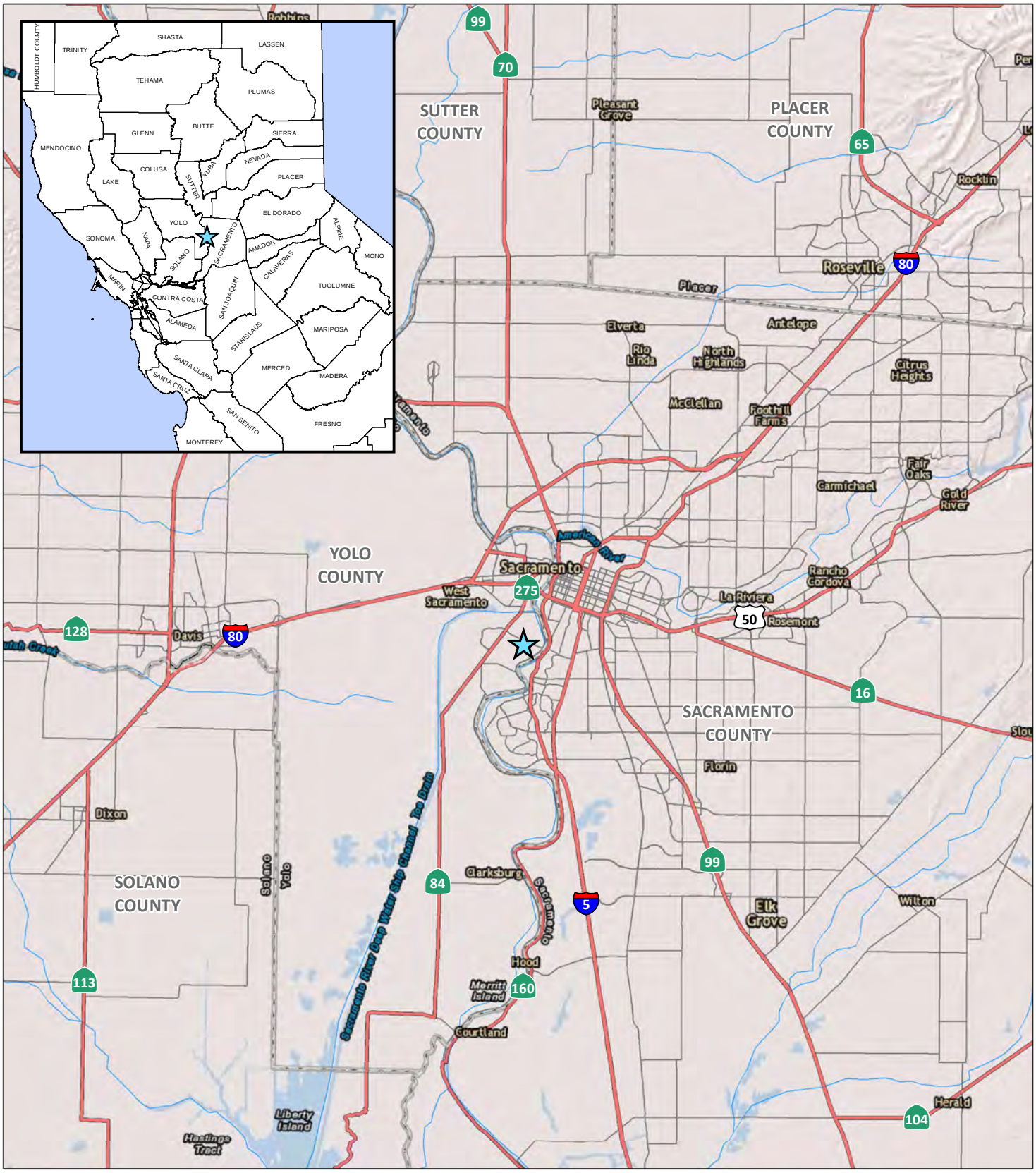
LINDEN ACRES WATER MAIN REPLACEMENT PROJECT DESCRIPTION FOR NOC

The Linden Acres Water Main Replacement Project (herein referred to as the proposed Project) is located in West Sacramento, within the Southport Planning Area. The Southport Planning Area lies in the southern half of West Sacramento and is bounded by the Deep Water Ship Channel to the north and west, the Sacramento River to the east, and the city limits to the south. The Project site consists of right-of-way (ROW) (roadway) work and is bounded by Linden Road to the north, Mojave Drive to the west, Trinity Way and the proposed Liberty Specific Plan to the south, and the proposed Liberty Specific Plan to the east. **Figure 2-1: Regional Location** and **Figure 2-2: Project Location** show the location of the Project site on a regional and local scale. **Figure 2-2** also shows the location of the proposed Project in comparison to the Liberty Specific Plan area.

The purpose of the Project is to replace the existing backyard water mains with new mains in the City of West Sacramento (City) owned roadway within ROW and dedicated easements. The existing water mains (16 inches, 12 inches, 8 inches, and 6 inches in diameter) will be abandoned in place at average depths ranging from 4 to 8 feet deep. The Project anticipates extending a water main to the south of Redwood Avenue, then east to reconnect at the east end of Tamarack Road to complete the water distribution system. It should be noted that this Project would extend into the boundaries of the privately owned Liberty Specific Plan to the east. The Liberty Specific Plan will eventually connect to the proposed Project at Tamarack Road and at the north water line coming east from Bastone Court in order to maintain the water distribution loop system. The water main associated with the proposed Project that extends into the Liberty Specific Plan area will be temporary and abandoned once development of the Liberty Specific Plan commences.

The new water main will be placed in the existing roadway sections and will be accessible for future City maintenance. The replacement of water mains will include approximately 11,000 linear feet of 16- and 8-inch pipe with the addition of fire hydrants and service lines (laterals) that will include new water meters. The service laterals consist of 111 pipes that are 0.75 inch in size and 94 pipes that are 1 inch in size. Most of the pipe excavations will be approximately 4 feet deep, with a small section that will be as deep as 10 feet to connect to the existing system. **Figure 2-3: Project Design** shows the design of the proposed Project. The Project will be implemented on the following roads: Linden Road, Mojave Drive, Merced Way, Shasta Way, Carmel Court, Rubicon Way, Ironwood Way, Spruce Street, Redwood Avenue, Alder Way, Tamarack Road, and Cedar Street.

Construction of the proposed Project is anticipated to commence in spring 2019 and will last 8 months (170 working days). During construction, affected roadways may be closed to through traffic; however, access by residents will still be permitted. The City of West Sacramento would direct the construction contractor to locate construction equipment at staging areas as far from residential units as possible. The construction equipment anticipated to be used includes: Hydrovac vacuum trucks, tractors, a backhoe, a trencher, a loader, a haul truck, an excavator, a compressor, a concrete saw, a concrete mixer truck, jack hammers, an asphalt planer, a trench paver, compactors, rollers, heavy-duty trucks, dump trucks, a street sweeper, a heavy equipment transport truck, a forklift, pickups, and chipping guns.



LSA

LEGEND

Project Area

FIGURE 2-1

Linden Acres Water Main Replacement Project
West Sacramento, Yolo County, California
LSA Project No. WSA1701

0 2.5 5
MILES

Regional Location

SOURCE: ESRI World Street Map (2016)

I:\WSA1701\GIS\Reports\Cultural Study\WSA1701_Figure1.mxd (6/6/2017)

LSA

LEGEND

Project Area (109.7 acres)

Liberty Specific Plan Area

N

0 500 1000
FEET

FIGURE 2-2

Linden Acres Water Main Replacement Project
West Sacramento, Yolo County, California
LSA Project No. WSA1701

Project Location

SOURCE: NAIP (2016); City of West Sacramento (2017)

I:\WSA1701\GIS\Reports\PD\Figure2.mxd (3/16/2018)

LSA

FIGURE 2-3

LEGEND

Project Area (109.7 acres)

Parcels

Trenching Location

Design

Existing Water Accessory

Right-of-Way

Existing Water Main

Water Fittings

Proposed Water Main

0 250 500

FEET

Linden Acres Water Main Replacement Project
West Sacramento, Yolo County, California
LSA Project No. WSA1701

Project Design

SOURCE: Esri World Imagery (2014); Design - City of West Sacramento (07/2017); Mapping - LSA (07/2017)

I:\WSA1701\GIS\Figure3_design.mxd (7/25/2017)

APPENDIX B

NOTICE OF DETERMINATION

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Print Form

Notice of Determination

Appendix D

To:

☒ Office of Planning and Research

U.S. Mail:	*Street Address:*
P.O. Box 3044	1400 Tenth St., Rm 113
Sacramento, CA 95812-3044	Sacramento, CA 95814

☒ County Clerk
County of: Yolo
Address: 625 Court Street #B01
Woodland, CA 95695

From:
Public Agency: City of West Sacramento
Address: 1110 West Capitol Avenue
West Sacramento, CA 95691
Contact: Mauricio Meza-Pedraza
Phone: 916-617-4645

Lead Agency (if different from above):
_____
Address: _____
_____
Contact: _____
Phone: _____

SUBJECT: Filing of Notice of Determination in compliance with Section 21108 or 21152 of the Public Resources Code.

State Clearinghouse Number (if submitted to State Clearinghouse): 2018052001

Project Title: Linden Acres Water Main Replacement Project

Project Applicant: City of West Sacramento

Project Location (include county): City of West Sacramento Yolo County

Project Description:
See attached Project Description.

This is to advise that the City of West Sacramento has approved the above
(☒ Lead Agency or ☐ Responsible Agency)

described project on July 13, 2018 and has made the following determinations regarding the above
(date)
described project.

1. The project [☐ will ☒ will not] have a significant effect on the environment.
2. ☐ An Environmental Impact Report was prepared for this project pursuant to the provisions of CEQA.
   ☒ A Negative Declaration was prepared for this project pursuant to the provisions of CEQA.
3. Mitigation measures [☒ were ☐ were not] made a condition of the approval of the project.
4. A mitigation reporting or monitoring plan [☒ was ☐ was not] adopted for this project.
5. A statement of Overriding Considerations [☐ was ☒ was not] adopted for this project.
6. Findings [☐ were ☒ were not] made pursuant to the provisions of CEQA.

This is to certify that the final EIR with comments and responses and record of project approval, or the negative Declaration, is available to the General Public at:
City of West Sacramento Public Works Department

Signature (Public Agency): _____ Title: _____

Date: _____ Date Received for filing at OPR: _____

Authority cited: Sections 21083, Public Resources Code.
Reference Section 21000-21174, Public Resources Code.

Revised 2011

APPENDIX C

STATE CLEARINGHOUSE COMPLIANCE LETTER

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STATE OF CALIFORNIA

GOVERNOR'S OFFICE *of* PLANNING AND RESEARCH

EDMUND G. BROWN JR.
GOVERNOR

KEN ALEX
DIRECTOR

June 1, 2018

Maricio Meza-Pedraza
City of West Sacramento
1110 W. Capitol Avenue, 2nd Floor
West Sacramento, CA 95691

Subject: Linden Acres Water Main Replacement Project
SCH#: 2018052001

Dear Maricio Meza-Pedraza:

The State Clearinghouse submitted the above named Mitigated Negative Declaration to selected state agencies for review. On the enclosed Document Details Report please note that the Clearinghouse has listed the state agencies that reviewed your document. The review period closed on May 31, 2018, and the comments from the responding agency (ies) is (are) enclosed. If this comment package is not in order, please notify the State Clearinghouse immediately. Please refer to the project's ten-digit State Clearinghouse number in future correspondence so that we may respond promptly.

Please note that Section 21104(c) of the California Public Resources Code states that:

> "A responsible or other public agency shall only make substantive comments regarding those activities involved in a project which are within an area of expertise of the agency or which are required to be carried out or approved by the agency. Those comments shall be supported by specific documentation."

These comments are forwarded for use in preparing your final environmental document. Should you need more information or clarification of the enclosed comments, we recommend that you contact the commenting agency directly.

This letter acknowledges that you have complied with the State Clearinghouse review requirements for draft environmental documents, pursuant to the California Environmental Quality Act. Please contact the State Clearinghouse at (916) 445-0613 if you have any questions regarding the environmental review process.

Sincerely,

Scott Morgan
Director, State Clearinghouse

Enclosures
cc: Resources Agency

1400 10th Street P.O. Box 3044 Sacramento, California 95812-3044
1-916-322-2318 FAX 1-916-558-3184 www.opr.ca.gov

Document Details Report
State Clearinghouse Data Base

SCH# 2018052001
Project Title Linden Acres Water Main Replacement Project
Lead Agency West Sacramento, City of

Type MND Mitigated Negative Declaration

Description The project is located in the city of West Sacramento and consist of ROW work and is bounded by Linden Rd to the north, Mojave Dr to the west, Trinity War and the proposed Liberty Specific Plan to the south, and the proposed Liberty SP to the east. The purpose of the project is to replace the existing backyard water mains with new mains in the city owned roadway within ROW and dedicated easements. The existing water mains will be abandoned in place at average depths ranging from 4 to 8 ft deep. The new water main will be placed in the existing roadway sections and will be accessible for future city maintenance. The replacement of water mains will include approx 11,000 lf of 16 and 18 in pipe with the addition of fire hydrants and service lines that will include new water meters. The service laterals consist of 111 pipes that are 0.75 inch in size and 94 pipes that are 1 inch in size. Most of the pipe excavations will be approx 4 ft deep, with a small section that will be as deep as 10 ft to connect to the existing system. The project will be implemented on the following roads: Linden Rd, Mojave Dr, Merced Way, Shasta Way, Carmel Court, Rubicon Way, Ironwood Way, Spruce St, Redwood Ave, Alder Way, Tamarack Rd, and Cedar St.

Lead Agency Contact

Name Maricio Meza-Pedraza
Agency City of West Sacramento
Phone (916) 617-4850 **Fax**
email
Address 1110 W. Capitol Avenue, 2nd Floor
City West Sacramento **State** CA **Zip** 95691

Project Location

County Yolo
City West Sacramento
Region
Lat / Long 38° 32' .581" N / 121° 31' .646" W
Cross Streets Linden Rd/Mojave Dr
Parcel No. city owned ROW
Township 8N **Range** 4E **Section** 57 **Base** MDBM

Proximity to:

Highways I-5
Airports Sacramento Executive
Railways SSR
Waterways Sacramento River
Schools River city HS
Land Use work will be occurring in city owned ROW roadways

Project Issues Aesthetic/Visual; Agricultural Land; Air Quality; Archaeologic-Historic; Biological Resources; Cumulative Effects; Drainage/Absorption; Flood Plain/Flooding; Forest Land/Fire Hazard; Geologic/Seismic; Landuse; Minerals; Noise; Population/Housing Balance; Public Services; Recreation/Parks; Schools/Universities; Sewer Capacity; Soil Erosion/Compaction/Grading; Solid Waste; Toxic/Hazardous; Traffic/Circulation; Vegetation; Water Quality; Water Supply; Wetland/Riparian

Reviewing Agencies Resources Agency; Central Valley Flood Protection Board; Department of Fish and Wildlife, Region 2; Department of Parks and Recreation; California Highway Patrol; Caltrans, District 3 N; Caltrans, Division of Aeronautics; State Water Resources Control Board, Division of Drinking Water; State Water Resources Control Board, Division of Drinking Water, District 9; Regional Water Quality Control

Note: Blanks in data fields result from insufficient information provided by lead agency.

Bd., Region 5 (Sacramento); Delta Protection Commission; Delta Stewardship Council; Native American Heritage Commission; State Lands Commission; Department of Toxic Substances Control

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Date Received 05/01/2018 ***Start of Review*** 05/02/2018 ***End of Review*** 05/31/2018

6.0 MITIGATION MONITORING AND REPORTING PROGRAM

This Mitigation and Monitoring Reporting Program (MMRP) has been formulated based upon findings of the Initial Study/Mitigated Negative Declaration (IS/MND) prepared for the proposed Project. The purpose of this MMRP is to ensure the implementation of mitigation measures identified as part of the environmental review for the Project. The MMRP includes the following information:

- A list of mitigation measures
- The party responsible for implementing the mitigation measures
- The timing for implementation of the mitigation measure
- The agency/jurisdictional department responsible for monitoring the implementation
- The monitoring action and frequency

The City of West Sacramento must adopt this MMRP, or an equally effective program, if it approves the Project, with the mitigation measures that were adopted or made conditions of approval.

Monitoring Item Number	Initial Study Mitigation Measures	Mitigation Measure	Timing	Implementing Party	Monitoring Party	Frequency and Duration of Monitoring
1	MM AQ-1	The City of West Sacramento (City) or construction contractor shall implement the following measures at the Project site: • Water all active construction sites at least twice daily. The frequency shall be based on the type of operation, soil, and wind exposure. • Haul trucks shall maintain at least 2 feet of freeboard • Cover all trucks hauling dirt, sand, or loose materials. • Apply nontoxic binders (e.g., latex acrylic copolymer) to exposed areas after cut and fill operations and hydroseed the area as applicable. • Apply chemical soil stabilizers on inactive construction areas (disturbed lands within construction projects that are unused for at least 4 consecutive days). • Plant tree windbreaks on the windward perimeter of construction projects if adjacent to open land. • Plant vegetative ground cover in disturbed areas as soon as possible. • Cover inactive storage piles. • Sweep streets if visible soil material is carried out from the construction site. • Treat accesses to a distance of 100 feet from the paved road with a 6 to 12 inch layer of wood chips or mulch or with a 6-inch layer of gravel.	During Construction	City of West Sacramento or Construction Contractor	City of West Sacramento	During Construction
2	MM-BIO-1	The measures listed below shall be implemented to mitigate potential impacts to western burrowing owl: • Preconstruction surveys for western burrowing owl shall be conducted by a qualified biologist in accordance with the California Department of Fish and Wildlife's (CDFW) 2012 Staff Report on Burrowing Owl Mitigation. • If burrowing owls are identified during the preconstruction survey, passive exclusion shall be implemented per CDFW's 2012 Staff Report of Burrowing Owl Mitigation (including avoidance of occupied burrows during the breeding season [February 1 to August 31]) • Following construction, all areas, temporarily impacted during Project construction shall be restored to pre-construction	Prior to and during construction	City of West Sacramento	Qualified Biologist	

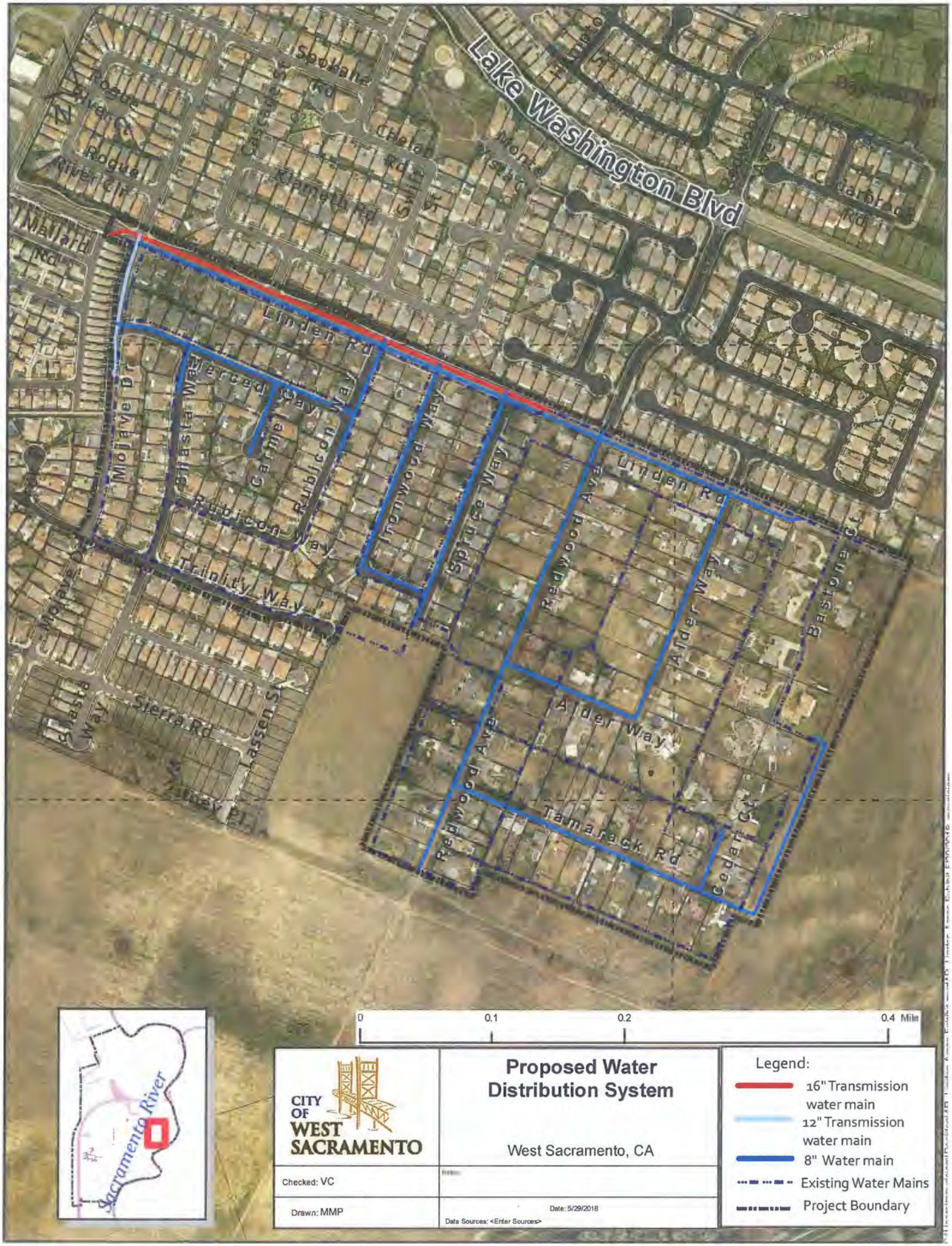
<table>
<tr><th>Monitoring Item Number</th><th>Initial Study Mitigation Measures</th><th>Mitigation Measure</th><th>Timing</th><th>Implementing Party</th><th>Monitoring Party</th><th>Frequency and Duration of Monitoring</th></tr>
<tr><td></td><td></td><td>contours (if necessary) and revegetated with native species as specified in the table below:

Native Species Mix
<table>
<tr><th>Scientific Name</th><th>Common Name</th><th>Rate (lbs/acre)</th><th>Minimum Percent Germination</th></tr>
<tr><td>Artemisia douglasiana</td><td>Mugwort</td><td>2.0</td><td>50</td></tr>
<tr><td>Bromus carinatus carinatus</td><td>California brome</td><td>5.0</td><td>85</td></tr>
<tr><td>Elymus trachycaulus</td><td>Slender wheatgrass</td><td>2.0</td><td>60</td></tr>
<tr><td>Elymus X triticum</td><td>Regreen</td><td>10.0</td><td>80</td></tr>
<tr><td>Eschscholizia californica</td><td>California poppy</td><td>2.0</td><td>70</td></tr>
<tr><td>Hordeum brachyantherum</td><td>California barley</td><td>2.0</td><td>80</td></tr>
<tr><td>Lupinus bicolor</td><td>Bicolored lupine</td><td>4.0</td><td>80</td></tr>
</table></td><td></td><td></td><td></td><td></td></tr>
<tr><td>3</td><td>MM-CUL-1</td><td>Construction Monitoring. Prior to construction, the City shall retain a professional archaeologist <u>and invite a geographically-affiliated Native American Representative</u> to provide a pre-construction briefing to supervisory personnel of any excavation contractor to alert them to the possibility of exposing significant historic or prehistoric archaeological resources within the project area. The briefing shall discuss any resources that could be exposed, the need to stop excavation at the discovery site, and the procedures to follow regarding discovery protection and notification. The City will notify geographically-affiliated tribal groups 7 days prior to excavation, grading, and other earthmoving activities within 100 feet of the current site boundary of CA-YOL-18. During construction, a qualified archaeologist and a tribal representative from a geographically-</td><td>During Construction</td><td>City of West Sacramento</td><td>Qualified Archaeologist</td><td>During Construction</td></tr>
</table>

Monitoring Item Number	Initial Study Mitigation Measures	Mitigation Measure	Timing	Implementing Party	Monitoring Party	Frequency and Duration of Monitoring
		affiliated tribe shall be present to monitor Project excavation, grading, and other earthmoving activities within 100 feet of the current site boundary of CA-YOL-18. Monitoring shall continue until excavation, grading, and other earthmoving activities within 100 feet of the current site boundary have been completed. **Discovery of Unidentified Archaeological Resources.** If deposits of prehistoric or historical archaeological materials are encountered during Project construction activities, all work within 50 feet of the discovery shall be redirected and a qualified archaeologist should be contacted (if one is not already on site) to assess the situation and make recommendations regarding the treatment of the discovery, and to develop proper mitigation measures required for the discovery (California Code of Regulations [CCR] Title 14, § 15064.5 (f)). The City of West Sacramento shall also be notified. The archaeologist should prepare a report documenting the methods and results of the investigation, and provide recommendations for the treatment of the archaeological materials discovered. The report should be submitted to the City of West Sacramento and the Northwest Information Center. **Discovery of Human Remains.** During construction, consistent with the requirements outlined by CEQA Guidelines, Section 15064.5(e)(1), and in accordance with Section 7050.5 of the Health and Safety Code, Section 5097.98 of the PRC (Chapter 1492, Statutes of 1982, Senate Bill 297), as relevant, should be followed and no further disturbance shall occur until the Yolo County Coroner can evaluate them. If the human remains are of Native American origin, the coroner must notify the NAHC within 24 hours of identification. Pursuant to Section 5097.9 and 5097.993 of the PRC, the NAHC shall identify a "Native American Most Likely Descendent" to inspect the site and provide recommendations for the proper treatment of the remains and any associated grave goods.				

Monitoring Item Number	Initial Study Mitigation Measures	Mitigation Measure	Timing	Implementing Party	Monitoring Party	Frequency and Duration of Monitoring
4	MM-PALEO-1	If paleontological resources are encountered during project excavation and no monitor is present, all ground-disturbing activities within 50 feet of the find shall be redirected to other areas until a qualified paleontologist can be retained to evaluate the find and make recommendations for additional paleontological mitigation, which may include paleontological monitoring; collection of observed resources; preservation, stabilization, and identification of collected resources; curation of resources into a museum repository; and preparation of a final report documenting the monitoring methods and results to be submitted to the museum repository and the City.	During Construction	City of West Sacramento	Qualified Paleontologist	During Construction
5	MM-NOI-1	Prior to initiating construction, the Project proponent shall complete a noise reduction plan. The noise reduction plan shall identify the type and quantity of construction equipment to be operated, the expected noise levels of each piece of equipment, and the duration of operation at each area of construction. The noise reduction plan shall include measures to ensure construction of the Project will meet the standards of Chapter 17.32 of the City of West Sacramento Municipal Code (Performance Standards of Noise).	Prior to Construction	City of West Sacramento	City of West Sacramento	During Construction
6	MM-TRANS-1	A Traffic Management Plan/Emergency Services Plan shall be prepared by the Project proponent following the Yolo County Multi-Hazard Functional Plan recommendations. This plan shall be implemented during construction of the proposed Project to ensure that emergency access to and from the Project site is provided during construction activities and to ensure that emergency responders (i.e., fire, law enforcement, and paramedics/emergency medical technicians) are aware of potential detours and road closures to help in reducing emergency response times to the Project site.	Prior to Construction start	City of West Sacramento	City of West Sacramento	During Construction

ATTACHMENT 4



ATTACHMENT 5

DATE: 5/29/2018	N	TITLE:	CITY OF WEST SACRAMENTO
DRAWN BY: D. REGALLER		**LINDEN ACRES WATER MAIN REPLACEMENT VICINITY MAP**	
CHECKED BY: M. PEDRAZA	SCALE: 1" = 2,000'		

\CD Engineering & Inspections\CIP\25001 Linden West Water Main Replacement & Meter Install Project\Sheet Drawings\sht 1 - cover.dwg 5/29/2018 10:28 AM Regaller, Dennis

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 11

SUBJECT:

CONSIDERATION OF APPROVAL OF RESOLUTION 18-54 APPROVING A CAPITAL IMPROVEMENT PROGRAM BUDGET AMENDMENT TO THE WASHINGTON DISTRICT SUSTAINABLE COMMUNITY INFRASTRUCTURE PROJECT FOR FISCAL YEAR 2018-2019

INITIATED OR REQUESTED BY:

[ ] Council [X] Staff
[ ] Other

REPORT COORDINATED OR PREPARED BY:

Edgar Medina, Senior Civil Engineer

Denix Anbiah, Director of Public Works Department

ATTACHMENT [X] Yes [ ] No [ ] Information [ ] Direction [X] Action

OBJECTIVE

The objective of this report is to facilitate City Council's consideration for approval of a Capital Improvement Program budget amendment that will increase the construction contingency for the Washington District Sustainable Community Infrastructure Project (WDSCIP).

RECOMMENDED ACTION

Staff respectfully recommended that the City Council:

1) Approve Resolution 18-54 approving an amendment to the Capital Improvement Program for an additional budget appropriation of $100,000 from the unrestricted fund balance in the General Community Investment Fund (Measure G) to the Washington District Sustainable Community Infrastructure Project Budget for Fiscal Year 2018-2019; and
2) Grant the City Manager or his designee the authority to issue Contract Change Orders up to $100,000 plus the previously approved contingency.

BACKGROUND

In 2015 the City was awarded a $6.7 million State cap-and-trade grant for affordable housing and infrastructure funding to jump-start new mixed-use development. A portion of the grant was slated for the development of 77 units of affordable housing at West Gateway Place (formerly Delta Lane development). The construction of this development was recently completed. Another portion of the grant, $4.1 million, was intended for infrastructure improvements including roadways, frontage, underground utilities, lighting and streetscape improvements in the Washington District and the City's Grand Gateway, a "hub bringing together our downtown Central Business District, the Bridge District, and the historic Washington District." A Project to design and construct these improvements was developed and is known as the Washington District Sustainable Community Infrastructure Project.

On May 18, 2016, Council approved a professional services contract with AECOM for the design and partial construction administration of the WDSCIP. Subsequently, on October 5, 2016 and October 4, 2017, Council approved Contract Amendments No. 1 & 2 respectively.

On September 20, 2017, Council awarded a construction contract to Teichert Construction for the WDSCIP. Construction began in November 2017 and is actively progressing towards substantially completion.

ANALYSIS

The Project Team has experienced a substantial amount of utility trenching along E, F, G and 5th Streets beyond what was originally contemplated in the approved construction plans. Currently, the approved plans include trench paving and some slurry seals as the final surface treatment on certain streets. However, after evaluating the extent of trenching, existing potholes, utility locations and ADA requirements, it is recommended that the limits of new paving be increased to address those concerns.

If this Budget Amendment is approved, E Street from 5th Street to 3rd Street will be prioritized for full paving. E Street has experienced a tremendous amount unforeseen underground utility work causing a patchwork of trenching in various directions. E Street also includes the largest aesthetic scope having incorporated planter boxes, sparkle concrete sidewalks, decorative granite poem pavers and decorative lighting.

The Project Team will also leverage the remaining contingency balance to maximize the paving limits on F, G and 5th Streets.

Environmental Considerations:
The project Final Environmental Impact Report was certified on February 15, 2017 when Council adopted Resolution 17-11.

Commission Recommendation:
N/A

Strategic Plan Integration:
The Project is consistent with its Mission of City Government that is Financially Sound and its Vision as a Vibrant City where you can Live, Work, Learn & Play as well as support a Strong, Diverse Local Economy by providing Quality Infrastructure.

Alternatives:
1. Staff respectfully recommends that the City Council:
    a. Approve Resolution 18-54 approving a Budget Amendment of $100,000 to increase the contingency amount of the WDSCIP project budget for Fiscal Year 2018-2019 (previously approved contingency $1,724,000.00); and
    b. Grant the City Manager or his designee the authority to issue additional Contract Change Orders up to $100,000.00
2. Council may elect not to proceed with the budget amendment. Staff does not recommend this option as the this would limit the ability to rehabilitate the impacted streets.
3. Council may choose to make adjustments or modifications to the project.

Coordination and Review
This report has been coordinated with the Finance Division of the Administrative Services Department, City Attorney, Building Division, and Public Works Department.

Budget/Cost Impact
The amount authorized for the Washington District Sustainable Community Infrastructure Project will require additional appropriations and amendments to the CIP. The approved amendment of the contract will require additional funding from the General Community Investment Fund, Measure G Capital (Fund 406) and/or the Parking Fund (Fund 219) towards the completion of this project. The following tables provide a breakdown of the project costs (Table 1), project appropriation request (Table 2) and project budget funding (Table 3).

Table 1 – Current Project Costs	
Design Cost	
Consultant Base Contract (AECOM)	$781,327.79
AECOM Contract Amendment No. 1	$174,392.52
Union Pacific Railroad Permit	$1,010.00
PG&E (Deposit FO + Application for services)	$26,000.00
Potholing (Marquez Pipelines)	$66,050.00
Right-of-Way Services (AR/WS)	$50,000.00
Staff Oversight & Project Management	$277,042.58
Sub Total Design Cost	***$1,375,822.89***
Construction Cost	
Teichert Construction Base Bid	$12,390,631.79
Alternate # 1 (7th St Parking Lot)	$345,335.00
Construction Contingency (10%)	$1,274,000.00
PG&E utility relocation cost (Amount subject to reimbursement by PG&E)	$800,000.00

Construction Management (Ghirardelli Associates)	$1,029,025.00
Cultural monitoring (AECOM Contract Amendment No. 2)	$60,000.00
Materials Testing /Environmental Mitigation (Blackburn Consulting)	$105,065.00
Staff Oversight & Project Management	$151,668.00
AECOM Contract Amendment No. 3	$203,834.00
Blackburn Consulting Contract Amendment No. 1	$29,138.00
West Capitol Parking Lot*	***$181,500.00****
Additional Street Paving	***$100,000.00***
Sub Total Construction Cost	***$16,670,196.79***
Total Project Cost	***$18,046,019.68***

Table 2 – Project Appropriation Request		
Budget Appropriation Request		
West Capitol Ave Parking Lot*		$181,500.00*
WDSCIP Additional Street Paving		$100,000.00
Total Appropriation Request		***$281,500.00***
Table 3 – Project Budget Funding		
Project Funding Sources	**Approved Budget Appropriation**	**Requested Budget Appropriation**
AHSC Grant	$4,100,000.00	N/A
Measure G – Appropriated	$2,500,000.00	N/A
Sewer Impact Fees	$1,500,000.00	N/A
ATP Grant	$87,000.00	N/A
PG&E/AT&T Reimbursement	$800,000.00	N/A
Sewer Capital Projects Funds (Sewer Bond Proceeds)	$4,150,000.00	
Measure G – Fund 406 - WDSCIP Additional Paving	$3,057,972.00	***$100,00.00***
Water Capital Projects Fund	$770,000.00	
Ziggurat Parking Fund	$500,000.00	
Storm Drain Fund	$300,000.00	
Measure G – Fund 406 – West Capitol Parking Lot*		***$50,000.00****
Measure G – Fund 106/406 – West Capitol Parking Lot*		***$131,500.00****
Total Fund Appropriations	***$17,764,972.00***	***$281,500.00***

**The West Capitol Ave Parking Lot item is covered under a separate council item on the same agenda.*

ATTACHMENT(S)

1) Resolution No 18-54: WDSCIP Budget Amendment

RESOLUTION 18-54

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF WEST SACRAMENTO APPROVING AMENDMENTS OF $100,000 TO THE WASHINGTON DISTRICT SUSTAINABLE COMMUNITY INFRASTRUCTURE PROJECT CAPITAL IMPROVEMENT PROGRAM BUDGET FOR FISCAL YEAR 2018-19

WHEREAS, the City Council adopted the biennial Capital Improvement Program (CIP) budget for Fiscal Years 2017-18 and 2018-19 on June 14, 2017; and

WHEREAS, the City Council adopted mid-term amendments to the CIP budget for Fiscal Year 2018/19 on May 23, 2018 via Resolution 18- 35; and

WHEREAS, based upon information received subsequent to the adoption of the budget, the City Manager has prepared and proposed an amendment to budgeted expenses for the 2018-19 fiscal year; and

WHEREAS, the available balance in the Measure G is projected at $3.3 million, taking into account appropriations already approved for the fiscal year, and is sufficient to cover the one-time cost of the proposed amendment; and

WHEREAS, the City Council has considered all information related to this matter, as presented at the public meetings of the City Council identified herein, including any supporting reports by City staff, and any other information provided during public meetings.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of West Sacramento as follows:

1. The City Council hereby approves amendments to the Capital Improvement Program (CIP) for the 2018-19 fiscal year as listed below.

SOURCES
Amount $100,000.00
Total Sources 406-0000-3100 Measure G Fund Balance

USES
Amount $100,000.00
Total Uses 406-9211-5530/406-15027-5800
Washington District Sustainable Community Infrastructure Project.

2. The City Council hereby finds that the facts set forth in the recitals to this Resolution are true and correct and establish the factual basis for the City Council's adoption of this Resolution.

3. This Resolution shall take effect immediately upon its adoption.

PASSED AND ADOPTED by the West Sacramento City Council this 22nd day of August 2018, by the following vote:

AYES:
NOES:
ABSENT:

Christopher L. Cabaldon, Mayor

ATTEST:

Kryss Rankin, City Clerk

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 12

SUBJECT:

CONSIDERATION OF APPROVAL OF CONTRACT AMENDMENT NO. 1 TO THE CONSTRUCTION CONTRACT TO TEICHERT CONSTRUCTION; AND CONSIDERATION OF APPROVAL OF RESOLUTION 18-56 APPROVING A BUDGET AMENDMENT TO THE WEST CAPITOL AVENUE PARKING LOT PROJECT CAPITAL IMPROVEMENT PRGROGRAM (CIP) BUDGET FOR FISCAL YEAR 2018-2019

INITIATED OR REQUESTED BY:

[ ] Council [X] Staff
[ ] Other

REPORT COORDINATED OR PREPARED BY:

Edgar Medina, Senior Civil Engineer

Denix Anbiah, Director of Public Works Department

ATTACHMENT [x] Yes [ ] No [ ] Information [ ] Direction [x] Action

OBJECTIVE

The objective of this report is to facilitate City Council's consideration for approval of Contract Amendment No. 1 to the construction contract with Teichert Construction for the Washington District Sustainable Community Infrastructure Project (WDSCIP).

RECOMMENDED ACTION

Staff respectfully recommends that the City Council:

1) Find that the West Capitol Avenue Parking lot scope is exempt from environmental review under CEQA pursuant to the Class 1 Exemption under California Code of Regulations, Title 14, Division 6, Chapter 3, Section 15301; and
2) Approve Contract Amendment No. 1 with Teichert Construction for construction services for the WDSCIP; and
3) Approve Resolution 18-56 approving an increase of $31,500 to the West Capitol Avenue Parking Lot Capital Improvement appropriation 14032 in the Parking Improvement Fund 219 for Fiscal Year 2018-2019.

BACKGROUND

On May 23, 2018, as part of the City's Council adoption of the 2018-2019 Capital Improvement Program (CIP), Council approved $150,000.00 of Measure G Funds 106/406 for the West Capitol Avenue Parking Lot Project (WO 14032). The total project cost including design and construction engineering is $181,500. Therefore, an additional $31,500 budget is required to cover the total expenses for the scope of work associated with this parking lot project.

This parking lot is owned and maintained by the City of West Sacramento, but serves the parking needs for Los Rios Community College, the City's Community Center, and County library. The parking lot has received comments from the community about flooding during the rainy season, deterioration of the parking lot surface, and missing parking stripes and lines for vehicular spaces. Currently, there are approximately ninety parking spaces, which reach capacity during the Los Rios school year and hours of operation. With planned improvements twenty additional spaces will be added, seasonal drainage and flooding at the site will be mitigated, and ADA accessibility will be available for the proposed project site.

The proposed work generally consists of 6" grinding and milling of existing asphaltic concrete (AC) pavement, hot mix asphalt paving, removing existing concrete sidewalk and constructing proposed driveway, constructing AC dike, furnishing and installing traffic signs, placement of pavement markings and striping, ADA improvements, installing concrete wheel stops and all related components necessary to complete the work as specified in the plans.

ANALYSIS

City Staff has been coordinating with the Los Rios Community College regarding the rehabilitation and improvements related to the West Capitol Avenue Parking Lot that is owned and maintained by the City. As a result, a CIP project was created and approved in the City's 2018-2019 Budget.

On September 20, 2017, Council awarded the construction contract to Teichert Construction for the WDSCIP. Construction began in November 2017 and is actively progressing towards substantially completion.

Recently, the Public Works Department, Economic Development Department, Finance Division of the Administrative Services Department and the City Attorney evaluated the project delivery options for the West Capitol Avenue Parking Lot project given the current bidding environment, budget, and school schedule. Through that effort, it was determined that the City utilizing Teichert's current construction contract for the Washington District Sustainable Community Infrastructure Project (WDSCIP) would be the best course of action to perform the West Capitol Avenue Parking Lot scope of work.

This determination was based on the following facts.

1) As part of the WDSCIP project, Teichert's current construction of a parking lot at 7th Street and Tower Bridge Gateway clearly demonstrates their expertise and capacity to perform the scope of work for the West Capitol Avenue Parking Lot project.
2) The West Capitol Avenue Parking Lot is in close proximity to the WDSCIP construction activities.
3) The bidding environment during this current peak construction season likely would lead to lack of interest in bidding and therefore higher construction costs to the City if the West Capitol Avenue Parking Lot was put out to bid at this time.
4) Commitments have been made by the City to the Los Rios Community College concerning timing of construction of the parking lot.
5) Utilizing an existing contract will reduce construction costs since there will not be any additional mobilization, general conditions, bonding, legal and other overhead costs associated with the processing of a new contract.
6) Staff's engineering estimate to perform the parking lot scope of work is $210,000.00 while the negotiated cost with Teichert Construction is $155,500.40.

If the Contract Amendment is approved, Staff will coordinate with the Los Rios Community College regarding the West Capitol Avenue Parking Lot work schedule and track the associated costs separately.

Environmental Considerations:

The appropriate level of environmental review for the West Capitol Avenue Parking Lot scope of work is a Categorical Exemption as identified under California Code of Regulations, Title 14, Division 6, Chapter 3, Section 15301 Class 1 (b) of the California Environmental Quality Act. This project is a rehabilitation and/or maintenance of a parking facility involving no expansion of use beyond the existing. A Notice of Exemption will be filed with Yolo County for this project.

Commission Recommendation:

N/A

Strategic Plan Integration:

The Project is consistent with its Mission of City Government that is Financially Sound and its Vision as a Vibrant City where you can Live, Work, Learn & Play as well as support a Strong, Diverse Local Economy by providing Quality Infrastructure.

Alternatives:

1. Staff recommends that the City Council approve the Recommended Action.
2. Council may elect not to proceed with the construction contract amendment. Staff does not recommend this option because it will delay the completion of paving the parking facility for the 2018-2019 Los Rios Community College school year.
3. Council may choose to make adjustments or modifications to the project.

Coordination and Review

This report has been coordinated among the City Attorney, Finance Division of the Administrative Services Department, Economic Development Department, Community Development Department, and Public Works Department.

Budget/Cost Impact
The Fiscal Year 2018/19 Fund 406 capital improvement project budget includes $150,000 programmed for transfer to the Parking Fund, 219 for the West Capitol Parking Lot, CIP 14032. The contribution from Fund 406 will be paid back from future parking permit fees. The project will require an increase of $31,500 to the appropriation from Fund 406 to Fund 219 for a total project cost of $181,500.

ATTACHMENT(S)

1) WDSCIP Contract Amendment No 1
2) Resolution 18-56
3) Parking Lot Layout

AMENDMENT NO. 1
to the
CONTRACT FOR SERVICES
between the
CITY OF WEST SACRAMENTO
And
TEICHERT & SON, INC. dba TEICHERT CONSTRUCTION
Dated September 20, 2017

This Amendment No. 1 to the Contract for Services between the **City of West Sacramento** and Teichert & Son, Inc. dba **Teichert Construction** dated September 20, 2017, is made and entered into this 22nd day of August 2018. Except as expressly amended herein, the September 20, 2017 Contract for Services is in full force and effect.

RECITALS

WHEREAS, on September 20, 2017, the City Council approved the construction contract with Teichert Construction for the Washington District Sustainable Community Infrastructure Project (WDSCIP); and

WHEREAS, the City Council adopted the biennial Capital Improvement Program (CIP) budget for Fiscal Years 2017-18 and 2018-19 on June 17, 2017; and

WHEREAS, the City Council adopted mid-term amendments to the Capital Improvement Program (CIP) budget for Fiscal Years 2018-19 on May 23, 2018 via Resolution 18-35; and

WHEREAS, the project team determined it is the best interest of the City to utilize the existing WDSCIP construction contract for the West Capitol Avenue Parking Lot project; and

WHEREAS, the City Council adopted Resolution 18-56 to amend the CIP Budget to fund the expenditures related to the West Capitol Avenue Parking Lot project.

WHEREAS, the City and Contractor desire to amend said contract; and

NOW, THEREFORE, IT IS MUTUALLY AGREED by parties hereto to amend said agreement as follows:

I. SCOPE OF SERVICES:
The scope of services as set forth in the Contract for Services dated September 20, 2017, shall be revised to include the additional construction services identified in the proposal dated August 6, 2018, attached hereto as Exhibit "A".

II. COMPENSATION:
The compensation as set forth in the Contract for Services dated September 20, 2017, shall be revised to include the additional construction services identified in the proposal dated August 6, 2018, attached hereto as Exhibit "A", but in no event shall total compensation for said additional construction services exceed One Hundred Fifty Five Thousand Five Hundred Dollars ($155,500.40), nor the total compensation under the original contract dated September 20, 2017, exceed Twelve Million Eight Hundred Ninety One Thousand Four Hundred Sixty Seven Dollars and Nineteen Cents ($12,891,467.19) without the City's prior written approval.

IN WITNESS WHEREOF the parties hereto have executed this Agreement as the date herein set forth.

CITY OF WEST SACRAMENTO

By:_____
Christopher L. Cabaldon, Mayor

TEICHERT & SON, INC. dba TEICHERT CONSTRUCTION

By:_____
Eric Stannard, Vice President

APPROVED AS TO FORM

By:_____
Jeffrey Mitchell, City Attorney

ATTEST:

By: _____
Kryss Rankin, City Clerk

Woodland Office
24207 County Road 100A
P.O. Box 1890
Davis, CA 95617-1890
(530) 406-4200 * FAX (530) 406-4299

TEICHERT CONSTRUCTION
Established 1887

August 6, 2018

Edgar Medina, PE
City of West Sacramento
1110 West Capitol Ave, 1st Floor
West Sacramento, CA 95691

Project: 15027

Subject: Parking Lot Reconstruct- 1305 West Capitol Ave.

Mr. Medina,

Pursuant to your request, attached is our pricing for the reconstruction of the approximate 53,000SF parking lot for the Los Rios Community College West Sacramento Center. Please feel free to call if you have any questions regarding this information.

Sincerely,

Cale Sherman
Project Manager
Teichert Construction

Parking Lot Paving				
Change Description	**Quantity**	**Unit**	**Unit Price**	**Total**
Demo/Grade Driveway	1.00	LS	$ 9,860.00	$ 9,860.00
Pour Concrete Driveway	1.00	LS	$ 6,500.00	$ 6,500.00
Grind Parking Lot	39,000.00	SF	$ 0.46	$ 17,940.00
Grade Parking Lot	40,000.00	SF	$ 0.48	$ 19,200.00
Pave Parking Lot	500.00	TNS	$ 132.00	$ 66,000.00
Install Dike	760.00	LF	$ 15.79	$ 12,000.40
Stripe/Signs	1.00	LS	$ 20,000.00	$ 20,000.00
Reinstall Existing Curb Stops	1.00	LS	$ 4,000.00	$ 4,000.00
				$ 155,500.40

**Proposal based on the milling of existing parking lot for base material and placing a 2" AC overlay. No new curb stops are included in this proposal.

FORCE ACCOUNT ANALYSIS

DISTRICT **Woodland** P.O. Box 1890, Davis, CA 95617-1890 Phone: (530) 406-4200 Fax (530) 406-4299

TASK Paving Parking Lot

OPERATION

LABOR

NIGHT WORK? (Blank for N, or Y)	CREW TIME	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE	SHIFT PAY	FRINGES	TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
	OE FOREMAN	10.00		1.0	44.67	67.01	-	30.78	754.50	-	754.50
	OE GROUP 2	10.00		1.0	43.14	64.71	-	30.78	739.20	-	739.20
	OE GROUP 2	10.00		1.0	43.14	64.71	-	30.78	739.20	-	739.20
	OE GROUP 2	10.00		1.0	43.14	64.71	-	30.78	739.20	-	739.20
	OE GROUP 2	10.00		1.0	43.14	64.71	-	30.78	739.20	-	739.20
	OE GROUP 2	10.00		1.0	43.14	64.71	-	30.78	739.20	-	739.20
	LAB GROUP 1	10.00		1.0	28.79	43.185	-	23.30	520.90	-	520.90
	LAB GROUP 1	10.00		1.0	28.79	43.185	-	23.30	520.90	-	520.90
	LAB GROUP 1	10.00		1.0	28.79	43.185	-	23.30	520.90	-	520.90
	LAB GROUP 1	10.00		1.0	28.79	43.185	-	23.30	520.90	-	520.90
	LAB GROUP 1	10.00		1.0	28.79	43.185	-	23.30	520.90	-	520.90
							-				
							-				
							-				
							-				
									7,055.00	-	
										Labor Total	7,055.00
										Labor Surcharge	776.05
										Enter Labor Markup either 30% or 35% 35%	2,740.87
										Total Labor	**10,571.92**

EQUIPMENT

EQUIPMENT	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE		TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
ACB - F250 & F350 Pickup	10.00		1.00	24.98	21.73		249.80	-	249.80
ACB - F250 & F350 Pickup	10.00		1.00	24.98	21.73		249.80	-	249.80
ACB - F250 & F350 Pickup	10.00		1.00	24.98	21.73		249.80	-	249.80
ACB - F250 & F350 Pickup	10.00		1.00	24.98	21.73		249.80	-	249.80
LBC - Loader, Scraper - JD 210LJ, Case 570LXT	10.00		1.00	33.73	29.35		337.30	-	337.30
RVD - Roller, Vibratory Double Drum	10.00		1.00	81.78	67.06		817.80	-	817.80
RVD - Roller, Vibratory Double Drum	10.00		1.00	81.78	67.06		817.80	-	817.80
LCB - Pickup Machine, Lincoln 660H	10.00		1.00	83.75	77.89		837.50	-	837.50
FBD - Asphalt Pavers, Cedarapids	10.00		1.00	181.75	154.49		1,817.50	-	1,817.50
								Equipment Total	5,627.10
								Enter Equipment Markup either 10% or 15% 20%	1,125.42
								Total Equipment	**6,752.52**

MATERIALS

MATERIALS	AMOUNT	UNIT PRICE	GRAND TOTAL
Buy AC	530.00	55.00	29,150.00
Truck AC	50.00	105.00	5,250.00
Mobilization	1.00	2,500.00	2,500.00
			-
			-
			-
			-
			-
		Materials Total	36,900.00
		Enter Materials Markup 10% or 15% 20%	7,380.00
		Total Materials	**44,280.00**

SUBCONTRACTS

SUBCONTRACTS	AMOUNT	# OF DAYS	GRAND TOTAL
Oil Truck	3,000.00	1.00	3,000.00
Sweeper	1,200.00	1.00	1,200.00
			-
			-
			-
			4,200.00
		Enter Subcontractor Markup 10% 15%	630.00
		Total Subcontracts	**4,830.00**

GRAND TOTAL **66,434.44**

FORCE ACCOUNT ANALYSIS

DISTRICT **Woodland** P.O. Box 1890, Davis, CA 95617-1890 Phone: (530) 406-4200 Fax (530) 406-4299

TASK Demo/Grade for Driveway

OPERATION

LABOR

NIGHT WORK? (Blank for N, or Y)	CREW TIME	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE	SHIFT PAY	FRINGES	TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
	OE FOREMAN	8.00		1.0	44.67	67.01	-	30.78	603.60	-	603.60
	OE GROUP 2	8.00		1.0	43.14	64.71	-	30.78	591.36	-	591.36
	OE GROUP 2	8.00		1.0	43.14	64.71	-	30.78	591.36	-	591.36
	LAB GROUP 1	8.00		1.0	28.79	43.19	-	23.30	416.72	-	416.72
	LAB GROUP 1	8.00		1.0	28.79	43.185	-	23.30	416.72	-	416.72
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
									2,619.76	-	

Labor Total		2,619.76
Labor Surcharge		288.17
Enter Labor Markup either 30% or 35%	35%	1,017.78
Total Labor		**3,925.71**

EQUIPMENT

EQUIPMENT	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE		TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
ACB - F250 & F350 Pickup	8.00		1.00	24.98	21.73		199.84	-	199.84
ACB - F250 & F350 Pickup	8.00		1.00	24.98	21.73		199.84	-	199.84
LBC - Loader, Scraper - JD 210LJ, Case 570LXT	8.00		1.00	33.73	29.35		269.84	-	269.84
SGF - Excavator, Komatsu PC300 LC-6	8.00		1.00	134.24	108.73		1,073.92	-	1,073.92

Equipment Total		1,743.44
Enter Equipment Markup either 10% or 15%	20%	348.69
Total Equipment		**2,092.13**

MATERIALS

MATERIALS	AMOUNT	UNIT PRICE	GRAND TOTAL
10 Wheeler	12.00	105.00	1,260.00
Sawcut	4.00	205.00	820.00
AB	20.00	25.00	500.00
			-
			-
			-
			-
			-

Materials Total		2,580.00
Enter Materials Markup 10% or 15%	20%	516.00
Total Materials		**3,096.00**

SUBCONTRACTS

SUBCONTRACTS	AMOUNT	# OF DAYS	GRAND TOTAL
			-
			-
			-
			-
			-
			-

Enter Subcontractor Markup 10%	15%	-
Total Subcontracts		**-**
GRAND TOTAL		**9,113.84**

FORCE ACCOUNT ANALYSIS

DISTRICT **Woodland** P.O. Box 1890, Davis, CA 95617-1890 Phone: (530) 406-4200 Fax (530) 406-4299

TASK Grind Parking Lot

OPERATION

LABOR

NIGHT WORK? (Blank for N, or Y)	CREW TIME	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE	SHIFT PAY	FRINGES	TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
	OE FOREMAN	8.00		1.0	44.67	67.01	-	30.78	603.60	-	603.60
	OE GROUP 2	8.00		1.0	43.14	64.71	-	30.78	591.36	-	591.36
	OE GROUP 2	8.00		1.0	43.14	64.71	-	30.78	591.36	-	591.36
	LAB GROUP 1	8.00		1.0	28.79	43.19	-	23.30	416.72	-	416.72
	LAB GROUP 1	8.00		1.0	28.79	43.185	-	23.30	416.72	-	416.72
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
									2,619.76	-	
										Labor Total	2,619.76
										Labor Surcharge	288.17
										Enter Labor Markup either 30% or 35% 35%	1,017.78
										Total Labor	**3,925.71**

EQUIPMENT

EQUIPMENT	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE		TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
ACB - F250 & F350 Pickup	8.00		1.00	24.98	21.73		199.84	-	199.84
ACB - F250 & F350 Pickup	8.00		1.00	24.98	21.73		199.84	-	199.84
LBC - Loader, Scraper - JD 210LJ, Case 570LXT	8.00		1.00	33.73	29.35		269.84	-	269.84
								Equipment Total	669.52
								Enter Equipment Markup either 10% or 15% 20%	133.90
								Total Equipment	**803.42**

MATERIALS

MATERIALS	AMOUNT	UNIT PRICE	GRAND TOTAL
Grinder	2.00	4,900.00	9,800.00
			-
			-
			-
			-
			-
			-
			-
		Materials Total	9,800.00
		Enter Materials Markup 10% or 15% 20%	1,960.00
		Total Materials	**11,760.00**

SUBCONTRACTS

SUBCONTRACTS	AMOUNT	# OF DAYS	GRAND TOTAL
Sweeper	1,200.00	1.00	1,200.00
			-
			-
			-
			-
			1,200.00
		Enter Subcontractor Markup 10% 15%	180.00
		Total Subcontracts	**1,380.00**

GRAND TOTAL **17,869.13**

FORCE ACCOUNT ANALYSIS

DISTRICT **Woodland** P.O. Box 1890, Davis, CA 95617-1890 Phone: (530) 406-4200 Fax (530) 406-4299

TASK Grade Parking Lot

OPERATION

LABOR

NIGHT WORK? (Blank for N, or Y)	CREW TIME	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE	SHIFT PAY	FRINGES	TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
	OE FOREMAN	8.00		1.5	44.67	67.01	-	30.78	905.40	-	905.40
	OE GROUP 2	8.00		1.5	43.14	64.71	-	30.78	887.04	-	887.04
	OE GROUP 2	8.00		1.5	43.14	64.71	-	30.78	887.04	-	887.04
	OE GROUP 2	8.00		1.5	43.14	64.71	-	30.78	887.04	-	887.04
	OE GROUP 2	8.00		1.5	43.14	64.71	-	30.78	887.04	-	887.04
	TMS GROUP 1	8.00		1.5	30.72	46.08	-	27.55	699.24	-	699.24
	LAB GROUP 1	8.00		1.5	28.79	43.185	-	23.30	625.08	-	625.08
	LAB GROUP 1	8.00		1.5	28.79	43.185	-	23.30	625.08	-	625.08
							-				
							-				
							-				
							-				
							-				
							-				
							-				
									6,402.96	-	
										Labor Total	6,402.96
										Labor Surcharge	704.33
										Enter Labor Markup either 30% or 35% 35%	2,487.55
										Total Labor	**9,594.84**

EQUIPMENT

EQUIPMENT	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE		TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
ACB - F250 & F350 Pickup	8.00		1.50	24.98	21.73		299.76	-	299.76
ACB - F250 & F350 Pickup	8.00		1.50	24.98	21.73		299.76	-	299.76
MHG - Motor Grader, Cat 14G, 14H	8.00		1.50	111.66	97.14		1,339.92	-	1,339.92
TRL - Truck, Water	8.00		1.50	59.69	51.93		716.28	-	716.28
RVN - Roller, Vibratory Pad Drum, I - R SD122PD	8.00		1.50	46.31	37.97		555.72	-	555.72
LBC - Loader, Scraper - JD 210LJ, Case 570LXT	8.00		1.50	33.73	29.35		404.76	-	404.76
LBF - Loader, Backhoe - JD 410J, CAT 430F	4.00		1.50	53.46	46.51		320.76	-	320.76
								Equipment Total	3,936.96
								Enter Equipment Markup either 10% or 15% 20%	787.39
								Total Equipment	**4,724.35**

MATERIALS

MATERIALS	AMOUNT	UNIT PRICE	GRAND TOTAL
Buy & Truck AB	60.00	19.95	1,197.00
Truck Dirt	24.00	95.00	2,280.00
Buy Water	1.00	50.00	50.00
			-
			-
			-
			-
			-
		Materials Total	3,527.00
		Enter Materials Markup 10% or 15% 20%	705.40
		Total Materials	**4,232.40**

SUBCONTRACTS

SUBCONTRACTS	AMOUNT	# OF DAYS	GRAND TOTAL
			-
			-
			-
			-
			-
			-
		Enter Subcontractor Markup 10% 15%	-
		Total Subcontracts	**-**
		GRAND TOTAL	**18,551.59**

FORCE ACCOUNT ANALYSIS

DISTRICT **Woodland** P.O. Box 1890, Davis, CA 95617-1890 Phone: (530) 406-4200 Fax (530) 406-4299

TASK Pour concrete

OPERATION

LABOR

NIGHT WORK? (Blank for N, or Y)	CREW TIME	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE	SHIFT PAY	FRINGES	TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
	CM FOREMAN	8.00		1.0	39.08	58.62	-	25.52	516.80	-	516.80
	CM GROUP 1	8.00		1.0	34.24	51.36	-	25.52	478.08	-	478.08
	CM GROUP 1	8.00		1.0	34.24	51.36	-	25.52	478.08	-	478.08
	CM GROUP 1	8.00		1.0	34.24	51.36	-	25.52	478.08	-	478.08
	CM GROUP 1	8.00		1.0	34.24	51.36	-	25.52	478.08	-	478.08
	CM GROUP 1	8.00		1.0	34.24	51.36	-	25.52	478.08	-	478.08
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
									2,907.20	-	

Labor Total		2,907.20
Labor Surcharge		319.79
Enter Labor Markup either 30% or 35%	35%	1,129.45
Total Labor		**4,356.44**

EQUIPMENT

EQUIPMENT	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE		TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
ACB - F250 & F350 Pickup	8.00		1.00	24.98	21.73		199.84	-	199.84
ACB - F250 & F350 Pickup	8.00		1.00	24.98	21.73		199.84	-	199.84
ACG - F550 Crew Truck	8.00		1.00	32.48	28.26		259.84	-	259.84

Equipment Total		659.52
Enter Equipment Markup either 10% or 15%	20%	131.90
Total Equipment		**791.42**

MATERIALS

MATERIALS	AMOUNT	UNIT PRICE	GRAND TOTAL
Concrete	8.00	120.00	960.00
Misc. Expenses/Form Matl's	1.00	250.00	250.00
			-
			-
			-
			-
			-
			-

Materials Total		1,210.00
Enter Materials Markup 10% or 15%	20%	242.00
Total Materials		**1,452.00**

SUBCONTRACTS

SUBCONTRACTS	AMOUNT	# OF DAYS	GRAND TOTAL
			-
			-
			-
			-
			-
			-

Enter Subcontractor Markup 10%	15%	-
Total Subcontracts		**-**
GRAND TOTAL		**6,599.86**

FORCE ACCOUNT ANALYSIS

DISTRICT **Woodland** P.O. Box 1890, Davis, CA 95617-1890 Phone: (530) 406-4200 Fax (530) 406-4299

TASK Install Dike

OPERATION

LABOR

NIGHT WORK? (Blank for N, or Y)	CREW TIME	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE	SHIFT PAY	FRINGES	TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
	OE FOREMAN	8.00		1.0	44.67	67.01	-	30.78	603.60	-	603.60
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
									603.60	-	

Labor Total		603.60
Labor Surcharge		66.40
Enter Labor Markup either 30% or 35%	35%	234.50
Total Labor		**904.49**

EQUIPMENT

EQUIPMENT	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE		TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
ACB - F250 & F350 Pickup	8.00		1.00	24.98	21.73		199.84	-	199.84

Equipment Total		199.84
Enter Equipment Markup either 10% or 15%	20%	39.97
Total Equipment		**239.81**

MATERIALS

MATERIALS	AMOUNT	UNIT PRICE	GRAND TOTAL
Asphalt	18.00	60.00	1,080.00
Trucking	8.00	95.00	760.00
			-
			-
			-
			-
			-
			-

Materials Total		1,840.00
Enter Materials Markup 10% or 15%	20%	368.00
Total Materials		**2,208.00**

SUBCONTRACTS

SUBCONTRACTS	AMOUNT	# OF DAYS	GRAND TOTAL
Dike Company	6,000.00	1.00	6,000.00
Sweeper	1,200.00	1.00	1,200.00
			-
			-
			-
			7,200.00

Enter Subcontractor Markup 10%	20%	1,440.00
Total Subcontracts		**8,640.00**
GRAND TOTAL		**11,992.30**

FORCE ACCOUNT ANALYSIS

DISTRICT **Woodland** P.O. Box 1890, Davis, CA 95617-1890 Phone: (530) 406-4200 Fax (530) 406-4299

TASK Reset Existing Curb Stops

OPERATION

LABOR

NIGHT WORK? (Blank for N, or Y)	CREW TIME	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE	SHIFT PAY	FRINGES	TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
	OE FOREMAN	4.00		1.0	44.67	67.01	-	30.78	301.80	-	301.80
	OE GROUP 2	8.00		1.0	43.14	64.71	-	30.78	591.36	-	591.36
	LAB GROUP 1	8.00		1.0	28.79	43.19	-	23.30	416.72	-	416.72
	LAB GROUP 1	8.00		1.0	28.79	43.19	-	23.30	416.72	-	416.72
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
									1,726.60	-	

Labor Total		1,726.60
Labor Surcharge		189.93
Enter Labor Markup either 30% or 35%	35%	670.78
Total Labor		**2,587.31**

EQUIPMENT

EQUIPMENT	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE		TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
ACB - F250 & F350 Pickup	4.00		1.00	24.98	21.73		99.92	-	99.92
ACB - F250 & F350 Pickup	8.00		1.00	24.98	21.73		199.84	-	199.84
LBC - Loader, Scraper - JD 210LJ, Case 570LXT	8.00		1.00	33.73	29.35		269.84	-	269.84

Equipment Total		569.60
Enter Equipment Markup either 10% or 15%	20%	113.92
Total Equipment		**683.52**

MATERIALS

MATERIALS	AMOUNT	UNIT PRICE	GRAND TOTAL
Misc. Materials/rebar	1.00	250.00	250.00
			-
			-
			-
			-
			-
			-
			-

Materials Total		250.00
Enter Materials Markup 10% or 15%	20%	50.00
Total Materials		**300.00**

SUBCONTRACTS

SUBCONTRACTS	AMOUNT	# OF DAYS	GRAND TOTAL
			-
			-
			-
			-
			-
			-

Enter Subcontractor Markup 10%	10%	-
Total Subcontracts		**-**

GRAND TOTAL	**3,570.83**

FORCE ACCOUNT ANALYSIS

DISTRICT Stockton P.O. Box 1118, Stockton, CA 95201-1118 Phone: (209) 983-2300 Fax (209) 983-2375

TASK

OPERATION

LABOR

NIGHT WORK? (Blank for N, or Y)	CREW TIME	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE	SHIFT PAY	FRINGES	TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
							-				
									-	-	

Labor Total -

Labor Surcharge -

Enter Labor Markup either 30% or 35% 30% -

Total Labor -

EQUIPMENT

EQUIPMENT	ST HOURS	OT HOURS	# OF DAYS	BASE RATE	OT RATE		TOTAL ST AMOUNT	TOTAL OT AMOUNT	GRAND TOTAL

Equipment Total -

Enter Equipment Markup either 10% or 15% 10% -

Total Equipment -

MATERIALS

	MATERIALS	AMOUNT	UNIT PRICE		GRAND TOTAL
					-
					-
					-
					-
					-
					-
					-
					-
					-

Materials Total -

Enter Materials Markup 10% or 15% -

Total Materials -

SUBCONTRACTS

	SUBCONTRACTS	AMOUNT	# OF DAYS		GRAND TOTAL
					-
					-
					-
					-
					-
					-

Enter Subcontractor Markup 10% 10% -

Total Subcontracts -

GRAND TOTAL -

Chrisp Company

Woodland Office
1805 East Beamer Rd.
Woodland CA 95776
Phone: (530) 406-2220 Fax: (530) 406-2221

Contractors License No. 374600
DIR Registration No. 1000000306
A General Engineering
C-13 Fence, Wire, wood
C-32 Highway Improvement

Union Contractor / No minority Status

Quotation

TO:

TEICHERT (WOODLAND)
40056 COUNTY ROAD 29

WOODLAND CA 95616
Phone: (530) 406-4200 Fax: (530) 406-4299

Contact: Cale Sherman
email: csherman@teichert.com

Current Date: 7/6/2018
BidDate: 7/6/2018
JobName: West Capital Parking Lot Improvements
Location: West Sacramento Bid#: 41000 0
Contract#: Job#
Working Days:
Liquidated Damages

ITEM NO.	DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	TOTAL
01	INSTALL PAINTED PARKING LOT STRIPING SHOWN ON SHEET 6 OF 6	1	LS	13900.000	$13,900.00
02	6-FT CONCRETE WHEEL STOP	121	EA	125.000	$15,125.00
03	INSTALL R99C SIGN	5	EA	425.000	$2,125.00
04	INSTALL R7-8B SIGN	2	EA	350.000	$700.00
	Subtotal				$31,850.00

TOTAL $31,850.00

Written Authorization is Required Prior to Proceeding With Work

ACCEPTED BY: _____ **P.O. Number** _____

TITLE: _____ **Job Number** _____

Bond Rate 1.2 Addendums

This quote includes all stipulations found on page 3

QUOTE PREPARED BY Joe Fini
(916) 549-8368
jfini@chrispco.com
CHRISP COMPANY

Chrisp Company
Woodland Office
1805 East Beamer Rd.
Woodland CA 95776
Phone: (530) 406-2220 Fax: (530) 406-2221

Quotation

Contractors License No. 374600
DIR Registration No. 1000000306
A General Engineering
C-13 Fence, Wire, wood
C-32 Highway Improvement

Union Contractor / No minority Status

COMMENTS, EXCLUSIONS, AND STIPULATIONS

The following stipulations are hereby incorporated into and made part of the quotation from Chrisp co. dated July 6, 2018 for the following project

West Capital Parking Lot Improvements

- NO RETENTION TO BE TAKEN ON CONTRACTS LESS THAN $5,000.00
- 1 Move- In(s) included. Additional Move- Ins $3,500.00
- Chrisp Company reserves the right to withdraw proposal if written acceptance is not received within 30 days of bid date
- EXCLUDE: Installation or removal of temporary delineation
- Clean, clear and unobstructed access required for Chrisp Co. work
- Final PAY QTYS presented by owner MUST be agreed by Chrisp Co.
- Changeable Message board if required to be provided by General Contractor for Chrisp Co. Use at no Charge to Chrisp Co.
- EXCLUDE: Traffic Control Plan
- EXCLUDE: Fire Lane Striping and Signage
- EXCLUDE: Concrete Coring for Signs
- EXCLUDE: Removal of Existing Striping & Markings
- EXCLUDE: Seal Coat
- EXCLUDE: Removal of Concrete Curing Material prior to PAINT / THERMOPLASTIC / PAVEMENT MARKER application.
- Parking Area is to be Clear of Debris and Parked Vehicles prior to Chrisp Company Arrival.
- EXCLUDE: Furnishing and Installation of Mast Arm Signs
- EXCLUDE: Red-Lined As-Built Drawings
- If Chrisp Co. is required to DIG BY HAND do to issues U.S.A., etc.an additional $20.00 per hole will be charged
- After receiving contract 10 working days written notice required prior to scheduling of work
- EXCLUDE: Special Insurance Requirements,(i.e. Railroad Insurance,_____)
- EXCLUDE: ALL Signs on Traffic Signal Plans
- EXCLUDE: ALL Solar Powered LED Signs to be Done By Others
- EXCLUDE: Construction Area Signs
- This quotation excludes Primary Liability Insurance greater than $2,000,000.
- This quotation excludes all work associated with the project SWPP and WPCP.
- This quotation excludes installing permanent striping while other operations are in progress on jobsite.
- This quotation Excludes furnishing or placing ALL signage for advance posting of ramp closures / detours for our operations.
- EXCLUDE: Encroachment Permit (If required to be provided at No Cost to Chrisp Company)
- EXCLUDE: ALL Safety Training, Badging, Drug Testing and Orientation (To be paid at time and material Cal-Trans rates)
- Access / and or Accessibility by 1 Ton Vehicle required for Chrisp Company work

Chrisp Company
Woodland Office
1805 East Beamer Rd.
Woodland CA 95776
Phone: (530) 406-2220 Fax: (530) 406-2221

Contractors License No. 374600
DIR Registration No. 1000000306
A General Engineering
C-13 Fence, Wire, wood
C-32 Highway Improvement

Union Contractor / No minority Status

Quotation

- **EXCLUDE: Advanced posting of "No Parking" signs**
- **ALL Bid items shall NOT be adjusted or removed without written / or verbal permission.**
- **Retainage percentage on monthly progress payments to be no greater than the percentage retained by the Owner on its payment to the Contractor. If Owner reduces the retainage percentages on payments to the Contractor, the Contractor shall likewise reduce the retainage percentage on payment to Chrisp Company. Full retention to be released no later than thirty, (30) Days after completion of Chrisp Companys work. Excessive or delinquent retainage will be subject to the highest interest rate allowed by law.**
- **Chrisp Company shall be reimbursed the full amount for bonds upon submission to General Contractor. Retention shall not be held from bond reimbursement.**
- **THIS QUOTATION IS BASED ON A STANDARD MONDAY THROUGH FRIDAY WORK WEEK. ADDITIONAL COSTS WILL OCCUR FOR WORK PERFORMED ON WEEKENDS**
- **CHRISP COMPANY WILL USE EXISTING EMPLOYEE POOL TO PERFORM ALL WORK ON THIS PROJECT. IF THERE ARE ANY HIRING GOALS FOR THIS PROJECT THIS WILL BE EXCLUDED**
- **EXCLUDE: ALL HARD WIRED ELECTRICAL SIGNS**

ATTACHMENT 2

RESOLUTION 18-56

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF WEST SACRAMENTO APPROVING AMENDMENTS OF $31,500 TO THE WEST CAPITOL AVENUE PARKING LOT PROJECT CAPITOL IMPROVEMENT PROGRAM BUDGET FOR FISCAL YEAR 2018-19

WHEREAS, the City Council adopted the biennial Capital Improvement Program (CIP) budget for Fiscal Years 2017-18 and 2018-19 on June 14, 2017; and

WHEREAS, the City Council adopted mid-term amendments to the Capital Improvement Program (CIP) budget for Fiscal Year 2018/19 on May 23, 2018 via Resolution 18-35; and

WHEREAS, based upon information received subsequent to the adoption of the budget, the City Manager has prepared and proposed an amendment to budgeted expenses for the 2018/19 fiscal year; and

WHEREAS, the City approved the West Capitol Avenue Parking Lot project in the 2018-2019 Capital Improvement Program; and

WHEREAS, the available balance in the Parking Improvement Fund, Fund 219, is projected at $115,000 taking into account appropriations already approved for the fiscal year, and is sufficient to cover the one- time cost of the proposed amendment; and

WHEREAS, the City Council has considered all information related to this matter, as presented at the public meetings of the City Council identified herein, including any supporting reports by City staff, and any other information provided during public meetings.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of West Sacramento as follows:

1. The City Council hereby approves amendments to the Capital Improvement Program (CIP) for the 2018-19 fiscal year as listed below.

SOURCES

Amount	$31,500
Total Sources	219-0000-3110 #14032 Parking Improvement Fund- Fund Balance

USES

Amount	$31,500
Total Uses	219-93252-5530/219-14032-5800

2. The City Council hereby finds that the facts set forth in the recitals to this Resolution are true and correct and establish the factual basis for the City Council's adoption of this Resolution.

3. This Resolution shall take effect immediately upon its adoption.

PASSED AND ADOPTED by the West Sacramento City Council this 22nd day of August 2018, by the following vote:

AYES:
NOES:
ABSENT:

Christopher L. Cabaldon, Mayor

ATTEST:

Kryss Rankin, City Clerk

ATTACHMENT 3

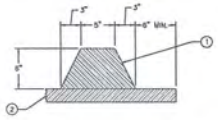
PARKING SPACES	
REGULAR	102
COMPACT	14
ADA	5

SYMBOL LEGEND		
DESCRIPTION	SYMBOL EXIST	SYMBOL PROP
TRAFFIC SIGN		
UTILITY STRUCTURE		
LANDSCAPE AREA		
CALTRANS TRAFFIC STRIPE DETAIL	SM	
GRIND AND PAVE AC PAVEMENT TO GRADE SHOWN ON GRADING PLAN		
SLURRY SEAL EX. CONCRETE TO GRADE SHOWN ON GRADING PLAN		
PROPOSED AC PAVEMENT MIN 3" AC MIN 5.5" AB COMPACTED SUBGRADE		

ESTIMATED QUANTITIES THIS SHEET

LOCATION	QUANTITY	UNIT
4" WHITE STRIPE	2950	LF
4" BLUE STRIPE	680	LF
PAVEMENT MARKING	177	SF
INSTALL WHEEL STOPS	20	EA
DRIVEWAY	215	SF
AC DIKE	756	LF
TRUNCATED DOMES	15	SF
9' TELESPAR POLE	5	EA
INSTALL R99C SIGN	5	EA
INSTALL R7-8B SING	2	EA

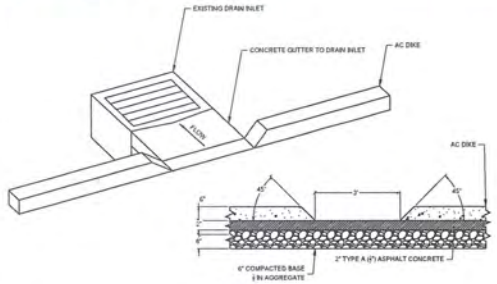
NOTES:
1) ALL TRAFFIC STRIPES SHALL BE 4" WIDTH
2) USE CALTRANS STANDARD PLANS A90A FOR ALL ADA ACCESSIBILITY STANDARDS



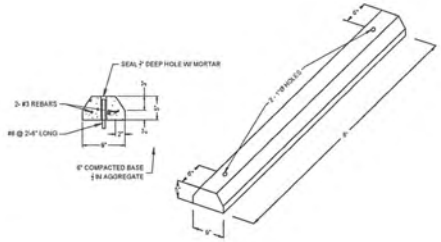
1. ASPHALT CONCRETE DIKE SHALL BE INSTALLED IN CONFORMANCE WITH SECTION 39-7 OF THE STATE STANDARD SPECIFICATIONS.
2. ASPHALT CONCRETE PAVEMENT.

NOTE:
A. ASPHALTIC EMULSION SHALL BE USED BETWEEN DIKE AND AC PAVEMENT.

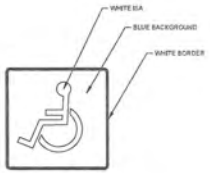
DETAIL A
AC DIKE
N.T.S.



DETAIL B
VERTICAL CURB CUT
N.T.S.



DETAIL C
WHEEL STOP
N.T.S.



DETAIL D
ADA PARKING MARKING
N.T.S.

NO.	REVISION	BY	DATE

DESIGNED: EM / RT
DRAWN: RT
CHECKED: EM

RECORD DRAWING DATE: 00/00/00
SCALE: HORZ: 1" = 20' / VERT: N/A
PROJECT NO. 14032
CAD FILE: west capitol parking lot.dwg

REGISTERED PROFESSIONAL ENGINEER EDGAR MEDINA No. 86093 Civil STATE OF CALIFORNIA
DATE:

CITY OF WEST SACRAMENTO

PUBLIC WORKS DEPARTMENT
ENGINEERING & TRANSPORTATION DIVISION
1110 WEST CAPITOL AVENUE
WEST SACRAMENTO, CA 95691

HORIZONTAL LAYOUT

WEST CAPITOL PARKING LOT IMPROVEMENTS

6 OF 6

DWG. NO. 2018-03-F

CITY OF WEST SACRAMENTO | AGENDA REPORT

MEETING DATE: August 22, 2018 | **ITEM #** 13

SUBJECT:

CONSIDERATION OF APPROVAL OF A CONTRACT FOR PROFESSIONAL SERVICES WITH SIMPSON GUMPERTZ & HEGER INC. TO DESIGN FIRE STATION 45 FACILITY REPAIRS

INITIATED OR REQUESTED BY:

[ ] **Council** [X] **Staff**

[ ] **Other**

REPORT COORDINATED OR PREPARED BY:
Jeramy Chapdelaine, Senior Project Manager

Denix Anbiah, Director of Public Works

ATTACHMENT [X] **Yes** [ ] **No** [ ] **Information** [ ] **Direction** [X] **Action**

OBJECTIVE

The objective of this report is to obtain Council approval of a professional service contract with Simpson Gumpertz & Heger Inc. (SGH) for leak detection and construction design document services for the Station 45 Facility Repairs project.

RECOMMENDED ACTION

Staff respectfully recommends that the City Council:

1. Find that the Station 45 Facility Repairs project is exempt from CEQA analysis under California Code of Regulations, Title 14, Chapter 3, Article 19, Sections 15301 (d) Existing Facilities and 15309 Inspections and that these determinations represent the independent judgment of the City; and
2. Award the Contract for Professional Services to Simpson Gumpertz & Heger Inc. in the amount of $111,000;
3. Grant the City Manager or his designee authority to issue monthly progress payments to the contractor; and
4. Authorize the City Manager or his designee to make contract amendments up to 10% ($11,100) of the value of the contract.
5. Authorize the City Manager to enter into a contract with Simpson Gumpertz & Heger Inc. substantially in the form presented, together with such changes as may be approved by the City Manager and City Attorney.

BACKGROUND

Facilities maintenance has addressed numerous seasonal leaks over the past few years at the Fire Station 45. It became apparent there was a much bigger issue than routine maintenance when mold developed in some areas over last winter. Funds to make the necessary repairs were requested and approved in the current budget.

Staff immediately started working with contractors to assess and conduct remediation of mold contamination at Fire Station 45. Following completion of mold remediation, staff has worked to contract a firm who can provide both leak identification, engineering design plan services and guidance to construction contractors in the application of repairs. Staff has been challenged in contracting service acquisition due to a higher level of engineering complexity and risk, need for a timely solution before the rainy season, and the current construction climate.

Staff has also coordinated all remediation with the City's insurance group YCPARMIA and insurance will cover all remediation costs incurred. Some costs of the proposed contract and future repairs items may also be covered by insurance.

ANALYSIS

Fire Station 45 is in immediate need of specialized engineering services to be applied in a timely manner so that construction services may be obtained to conduct repairs to existing leaks and to reduce future risk of damage to the facility posed by impending seasonal rains on existing building conditions. Staff recommends sole source procurement due to the complex nature of the required services and an extremely low number of firms that even offer the services. In addition, if a traditional RFP process is utilized, all the repair work cannot be completed before the rainy season starts.

Once all leak detection and construction design document services have been completed staff intends to advertise the complete repairs as a traditional low-bid procurement bid package. Due to the higher than

anticipated design costs, additional funds for the repairs will need to be obtained, but an accurate estimate will not be available until after the design phase.

Environmental Considerations
This project qualifies for categorical exemption to CEQA Guidelines § 15301(d) Existing Facilities and 15309 Inspections.

Commission Recommendation
N/A

Strategic Plan Integration
This project is consistent with the mission of West Sacramento City Government to provide quality municipal services, infrastructure and facilities.

Alternatives
Staff respectfully recommends that the City Council review and approve the recommendations described in this report. The Council's primary alternatives are summarized below.

1. Reject the recommended actions or direct staff to alter the project. Changes to the proposed scope of services may increase costs and delay construction;
2. Defer or cancel the project. Delays at this stage may lead to increased cost for construction, due to escalating labor, material and fuel prices. Delays may also lead to additional damage to the facility.

Coordination and Review
This report has been reviewed by the Public Works Department, Fire Department and the Finance Department.

Budget/Cost Impact
The City Council approved the Station 45 Facility Repairs for Fiscal Year 2018/19 for a total of $250,0000 from the General Facilities Fee.

ATTACHMENT
Contract for Professional Services

CONTRACT FOR SERVICES

THIS CONTRACT is made on June 20, 2018, by and between the CITY OF WEST SACRAMENTO ("City"), and Simpson Gumpertz & Heger Inc. (SGH) ("Consultant").

WITNESSETH:

WHEREAS, the City desires Simpson Gumpertz & Heger Inc. provide water testing and forensic investigation of water intrusion at walls and roof;

WHEREAS, the Consultant has presented a proposal for such services to the City, dated June 15, 2018, (attached hereto as **Exhibit "A"**) and is duly licensed, qualified and experienced to perform those services;

NOW, THEREFORE, the parties hereto mutually agree as follows:

1. SCOPE OF SERVICES:

A. Consultant shall do all work, attend all meetings, produce all reports and carry out all activities necessary to completion of the services described in **Exhibit "A"**. This Contract and its exhibits shall be known as the "Contract Documents." Terms set forth in any Contract Document shall be deemed to be incorporated in all Contract Documents as if set forth in full therein. In the event of conflict between terms contained in these Contract Documents, the more specific term shall control. If any portion of the Contract Documents shall be in conflict with any other portion, provisions contained in the Contract shall govern over conflicting provisions contained in the exhibits to the Contract.

B. Consultant enters into this Contract as an independent contractor and not as an employee of the City. The Consultant shall have no power or authority by this Contract to bind the City in any respect. Nothing in this Contract shall be construed to be inconsistent with this relationship or status. All employees, agents, contractors or subcontractors hired or retained by the Consultant are employees, agents, contractors or subcontractors of the Consultant and not of the City. The City shall not be obligated in any way to pay any wage claims or other claims made against Consultant by any such employees, agents, contractors or subcontractors, or any other person resulting from performance of this Contract.

C. The Consultant agrees it has satisfied itself by its own investigation and research regarding the conditions affecting the work to be done and labor and materials needed, and that its decision to execute this Contract is based on such independent investigation and research.

2. TERM OF CONTRACT

A. The services of Consultant are to commence upon execution of this Contract by the City, and shall be undertaken and completed in accordance with the Schedule of Performance attached hereto and incorporated herein by this reference as **Exhibit "B."**

B. Consultant's failure to complete work in accordance with the Schedule of Performance may result in delayed compensation as described in Section 3.

C. The City Manager or his or her designee may, by written instrument signed by the Parties, extend the duration of this Contract for a period equal to the original term of this Contract in the manner provided in Section 5, provided that the extension does not require the payment of compensation in excess of the maximum compensation set forth in Section 3, Compensation.

3. COMPENSATION:

A. The Consultant shall be paid monthly for the actual fees for all time and materials required and expended, but in no event shall total compensation exceed One Hundred Eleven Thousand Dollars ($111,000.00), without City's prior written approval. Consultant's fees shall be as specified in the Schedule of Fees, which is attached hereto an incorporated herein as **Exhibit "A"**.

B. Said amount shall be paid upon submittal of a monthly billing showing completion of the tasks that month. Consultant shall furnish City with invoices for all expenses as well as for all materials authorized by this Contract. The invoices shall be submitted with the monthly billings. If Consultant's performance is not in conformity with the Schedule of Performance, payments may be delayed or denied, unless the Consultant's failure to perform in conformity with the Schedule of Performance is a documented result of the City's failure to conform with the Schedule of Performance, or if the Schedule of Performance is extended pursuant to Section 5.

C. If the work is halted at the request of the City, compensation shall be based upon the proportion that the work performed bears to the total work required by this Contract, subject to Section 4.

4. TERMINATION:

A. This Contract may be terminated by either party, provided that the other party is given not less than 30 calendar days' written notice (delivered by certified mail, return receipt requested) of intent to terminate.

B. The City may temporarily suspend this Contract, at no additional cost to City, provided that the Consultant is given written notice (delivered by certified mail, return receipt requested) of temporary suspension. If City gives such notice of temporary suspension, Consultant shall immediately suspend its activities under this Contract.

C. Notwithstanding any provisions of this Contract, Consultant shall not be relieved of liability to the City for damages sustained by the City by virtue of any breach of this Contract by Consultant, and the City may withhold any payments due to Consultant until such time as the exact amount of damages, if any, due the City from Consultant is determined.

D. In the event of termination, the Consultant shall be compensated as provided for in this Contract, except as provided in Section 4C. Upon termination, the City shall be entitled to all work, including but not limited to, appraisals, inventories, studies, analyses, drawings and data estimates performed to that date in accordance with Section 7 hereof.

5. AMENDMENTS, CHANGES OR MODIFICATIONS:

Amendments, changes or modifications in the terms of this Contract may be made at any time by mutual written agreement between the parties hereto and shall be signed by the persons authorized to bind the parties hereto.

6. EXTENSIONS OF TIME:

Consultant may, for good cause, request extensions of time to perform the services required hereunder. Such extensions shall be authorized in advance by the City in writing and shall be incorporated in written amendments to this Contract or the attached Work Program in the manner provided in Section 5.

7. PROPERTY OF CITY:

A. Upon full payment for services rendered it is mutually agreed that all materials prepared by the Consultant under this Contract shall become the property of the City, and the Consultant shall have no property right therein whatsoever. Immediately upon termination, the City shall be entitled to, and the Consultant shall deliver to the City, all data, drawings, specifications, reports, estimates, summaries and other such materials as may have been prepared or accumulated to date by the Consultant in performing this Contract which is not Consultant's privileged information, as defined by law, or Consultant's personnel information, along with all other property belonging exclusively to the City which is in the Consultant's possession.

B. Additionally, it is agreed that the parties intend this to be a contract for services and each considers the products and results of the services to be rendered by Consultant hereunder (the "Work") to be a work made for hire. Consultant acknowledges and agrees that the Work (and all rights therein, including, without limitation, copyright) belongs to and shall be the sole and exclusive property of the City. The reuse of any of the materials or documents described in Paragraph 7.A by the City on any other project without the written permission of the Consultant, shall be at the City's sole risk and the City agrees to defend, indemnify and hold harmless the Consultant from all claims, damages and expenses, including attorney's fees, arising out of such unauthorized reuse by the City or by others acting through the City. The Consultant is not responsible and liability is waived by the City as against the Consultant for use by the City or any other person of any data, reports, plans or drawings not signed by the Consultant.

8. COMPLIANCE WITH ALL LAWS:

A. Consultant shall comply with all applicable laws, ordinances, and codes of federal, State and local governments, and shall commit no trespass on any public or private property in performing any of the work authorized by this Contract.

B. Consultant warrants to the City that it is licensed by all applicable governmental bodies to perform this Contract and will remain so licensed throughout the progress of the Work, and that it has, and will have, throughout the progress of the Work, the necessary experience, skill and financial resources to enable it to perform this Contract.

FOR WORK SUBJECT TO PREVAILING WAGES

C. The work contemplated under this Contract is a public work for the purposes of Labor Code section 1720, and is subject to the payment of prevailing wages. Accordingly, Consultant shall comply with the provisions of Exhibit **"C"**.

9. WARRANTIES AND RESPONSIBILITIES - CONSULTANT:

A. Consultant agrees and represents that it is qualified to properly provide the services set forth in **Exhibit "A"** in a manner which is consistent with the generally accepted standards of Consultant's profession.

B. Consultant agrees and represents that the work performed under this Contract shall be in accordance with applicable federal, State and local law in accordance with Section 17A hereof and 14 hereof.

C. Consultant shall designate a project manager who at all times shall represent the Consultant before the City on all matters relating to this Contract. The project manager shall continue in such capacity unless and until he or she is removed at the request of the City, is no longer employed by Consultant, or is replaced with the written approval of the City, which approval shall not be unreasonably withheld.

D. Consultant shall provide corrective services without charge to the City for services which fail to meet the above professional and legal standards and which are reported to Consultant in writing within sixty (60) days of discovery. Should Consultant fail or refuse to perform promptly its obligations, the City may render or undertake performance thereof and the Consultant shall be liable for any expenses thereby incurred.

10. SUBCONTRACTING:

None of the services covered by this Contract shall be subcontracted without the prior written consent of the City, which will not be unreasonably withheld. Consultant shall be as fully responsible to the City for the negligent acts and omissions of its contractors and subcontractors, and of persons either directly or indirectly employed by them, as it is for the negligent acts and omissions of persons directly employed by Consultant.

11. ASSIGNABILITY:

Consultant shall not assign or transfer any interest in this Contract whether by assignment or novation, without the prior written consent of the City which will not be unreasonably withheld. However, claims for money due or to become due Consultant from the City under this Contract may be assigned to a financial institution, or to a trustee in bankruptcy, without such approval. Notice of any assignment or transfer whether voluntary or involuntary shall be furnished promptly to the City.

12. INTEREST IN CONTRACT:

Consultant covenants that neither it, nor any of its employees, agents, contractors, subcontractors has any interest, nor shall they acquire any interest, direct or indirect, in the subject of the Contract, nor any other interest which would conflict in any manner or degree with the performance of its services hereunder. Consultant shall make all disclosures required by the City's conflict of interest code in accordance with the category designated by the City, unless the

City Manager determines in writing that Consultant's duties are more limited in scope than is warranted by the category designated by the City code and that a narrower disclosure category should apply. Consultant also agrees to make disclosure in compliance with the City conflict of interest code if, at any time after the execution of this Contract, City determines and notifies Consultant in writing that Consultant's duties under this Contract warrant greater disclosure by Consultant than was originally contemplated. Consultant shall make disclosures in the time, place and manner set forth in the conflict of interest code and as directed by the City.

13. MATERIALS CONFIDENTIAL:

All of the materials prepared or assembled by Consultant pursuant to performance of this Contract are confidential and Consultant agrees that they shall not be made available to any individual or organization without the prior written approval of the City, except by court order.

14. LIABILITY OF CONSULTANT-NEGLIGENCE:

Consultant shall be responsible for performing the work under this Contract in a manner which is consistent with the generally-accepted standards of the Consultant's profession and shall be liable for its own negligence and the negligent acts of its employees, agents, contractors and subcontractors. The City shall have no right of control over the manner in which the work is to be done but only as to its outcome, and shall not be charged with the responsibility of preventing risk to Consultant or its employees, agents, contractors or subcontractors.

15. INDEMNITY AND LITIGATION COSTS:

Consultant shall protect, hold free and harmless, defend and indemnify the Agency, its consultants, and each of their officers, employees and agents, from any and all liability, penalties, costs, losses, damages, expenses, causes of action, claims or judgments, including attorney's fees and expenses of litigation arising out of, pertaining to, or relating to the negligence, recklessness, or willful misconduct of the Consultant, its employees or anyone else employed by the Consultant in the performance of professional design services under this agreement, to the extent of the Consultant's proportionate percentage of fault.

To the extent permitted by Civil Code section 2782.8, for all other claims unrelated to the provision of professional design services, the Consultant shall protect, hold free and harmless, defend and indemnify the Agency, its consultants, and each of their officers, employees and agents, from any and all liability, penalties, costs, losses, damages, expenses, causes of action, claims or judgments, including attorney's fees and expenses of litigation, which arise out of or are in any way connected with the Consultant's, or its subcontractors' or suppliers', performance under this agreement or failure to comply with any of the obligations contained in the agreement. This indemnity shall imply no reciprocal right of the Consultant in any action on the agreement pursuant to California Civil Code section 1717 or section 1717.5. To the fullest extent legally permissible, this indemnity, defense and hold harmless agreement by the Consultant shall apply to any and all acts or omissions unrelated to the provision of professional design services, whether active or passive, on the part of the Consultant or its agents, employees, representatives, or subcontractor's agents, employees and representatives, resulting in claim or liability, irrespective of whether or not any acts or omissions of the parties to be indemnified hereunder may also have been a contributing factor to the liability, except such loss or damage which was caused by the active negligence, the sole negligence, or the willful misconduct of the Agency.

16. CONSULTANT TO PROVIDE INSURANCE:

A. Consultant shall not commence any work before obtaining, and shall maintain in force at all times during the duration and performance of this Contract, the policies of insurance specified in this Section. Such insurance must have the approval of the City as to limit, form, and amount, and shall be placed with insurers with a current A.M. Best's rating of no less than A VII (an NR rating is acceptable for Worker's Compensation insurance written with the State Compensation Insurance Fund of California).

B. Prior to execution of this Contract and prior to commencement of any work, the Consultant shall furnish the City with certificates of insurance and copies of endorsements providing evidence of coverage for all policies required by the Contract. The Consultant and its contractors and subcontractors shall, at their expense, maintain in effect at all times during the performance of work under the Contract not less than the following coverage and limits of insurance, which shall be maintained with insurers and under forms of policy satisfactory to the City. The maintenance by Consultant and its contractors and subcontractors of the following coverage and limits of insurance is a material element of this Contract. The failure of Consultant or of any of its contractors or subcontractors to maintain or renew coverage or to provide evidence of renewal may be treated by the City as a material breach of this Contract. Approval of the insurance by the City shall not relieve or decrease any liability of Consultant.

1. Worker's Compensation and Employer's Liability Insurance

a. Worker's Compensation - Insurance to protect the Consultant, its contractors and subcontractors from all claims under Worker's Compensation and Employer's Liability Acts, including Longshoremen's and Harbor Worker's Act ("Acts"), if applicable. Such coverage shall be maintained, in type and amount, in strict compliance with all applicable state and Federal statutes and regulations. The Consultant shall execute a certificate in compliance with Labor Code Section 1861, on the form provided in the Contract Documents.

b. Consultant shall provide a Waiver of Subrogation endorsement in favor of the City, its officers, officials, employees, agents and volunteers for losses arising from work performed by the Consultant.

2. Commercial General Liability Insurance

a. The insurance shall be provided on form CG0001, or it's equivalent, and shall include coverage for claims for bodily injury or property damage arising out of premises/operations, products/completed operations, contractual liability, and subconsultant's work and personal and advertising injury resulting from actions, failures to act, or operations of the insured, or by its employees or agents, or by anyone directly or indirectly employed by the insured. The amount of insurance coverage shall not be less than **$1,000,000.00** per occurrence and **$2,000,000** general and products/completed operations aggregates.

b. The commercial general liability insurance shall also include the following:

i. Endorsement equivalent to CG 2010 1185 naming the City, its officers, officials, employees, agents, and volunteers as additional insureds. The endorsement shall contain no special limitations on the scope of protection afforded to the City, its officers, officials, employees or volunteers.

ii. Endorsement stating insurance provided to the City shall be primary as respects the City, its officers, officials, employees and any insurance or self insurance maintained by the City, its officers, officials, employees or volunteers shall be in excess of the Consultant's insurance and shall not contribute with it, to the payment or satisfaction of any defense expenses, loss, or judgment.

iii. Provision or endorsement stating that the Consultant's insurance shall apply separately to each insured against whom claim is made or suit is brought, except with respect to the limits of the insurer's liability.

3. Commercial Automobile Insurance

a. The insurance shall include, but shall not be limited to, coverage for claims for bodily injury or property damage for owned, non-owned, and hired automobiles resulting from actions, failures to act, or operations of the insured, or by its employees or agents, or by anyone directly or indirectly employed by the insured. The amount of insurance coverage shall not be less than **$1,000,000.00** per accident.

b. The commercial automobile insurance shall include the same endorsements required for the commercial general liability policy (see Section 16.B.2.b).

4. Professional Liability. The Consultant and its contractors and subcontractors shall secure and maintain in full force, during the term of this Contract and for five years thereafter, professional liability insurance policies appropriate to the respective professions and the work to be performed as specified in this Contract. The limits of such professional liability insurance coverage shall not be less than **$1,000,000** per claim and aggregate.

C. In addition to any other remedy the City may have, if Consultant fails to maintain the insurance coverage as required in this Section, the City may obtain such insurance coverage as is not being maintained, in form and amount substantially the same as is required herein, and the City may deduct the cost of such insurance from any amounts due or which may become due Consultant under this Contract.

D. No policy required by this Contract shall cancelled unless Consultant has provided thirty (30) days prior written notice by mail, to the City.

E. Any deductibles or self-insured retentions in excess of $25,000 must be declared to, and approved by, the City.

F. The requirement as to types, limits, and the City's approval of insurance coverage to be maintained by Consultant are not intended to, and shall not in any manner, limit or qualify the liabilities and obligations assumed by Consultant under the Contract.

17. MISCELLANEOUS PROVISIONS:

A. Subject to standard of care in section 14 hereof, Consultant shall keep itself fully informed of, shall observe and comply with, and shall cause any and all persons, firms or corporations employed by it or under its control to observe and comply with, applicable federal, state, county and municipal laws, ordinances, regulations, orders and decrees which in any manner affect those engaged or employed on the work described by this Contract or the materials used or which in any way affect the conduct of the work.

B. Consultant shall not engage in unlawful employment discrimination. Such unlawful employment discrimination includes, but is not limited to, employment discrimination based upon a person's race, religious creed, color, national origin, ancestry, physical handicap, medical condition, marital status, gender, citizenship, or sexual orientation.

C. Consultant shall maintain and make available for inspection by the City and its auditors accurate records of all of its costs, disbursements and receipts with respect to any work under this Contract. Such inspections may be made during regular office hours at any time until six (6) months after the final payments under this Contract are made to the Consultant.

D. This Contract constitutes the entire agreement between the parties relative to the services specified herein and no modification hereof shall be effective unless and until such modification is evidenced by a writing signed by both parties to this Contract. There are no understandings, agreements, conditions, representations, warranties or promises, with respect to this Contract, except those contained in or referred to in the writing.

E. All notices that are required to be given by one party to the other under this Contract shall be in writing and shall be deemed to have been given if delivered personally or enclosed in a properly addressed envelope and deposited in a United States Post Office for delivery by registered or certified mail addressed to the parties at the following addresses:

City: City of West Sacramento
c/o Jeramy Chapdelaine
1110 West Capitol Ave
West Sacramento, CA 95691

Consultant: Simpson Gumpertz & Heger Inc.
c/o Kenneth A. Klein
100 Pine Street, Suite 1600
San Francisco, CA 94111

F. This Contract shall be interpreted and governed by the laws of the State of California..

G. Any action arising out of this Contract shall be brought and maintained in Yolo County California, regardless of where else venue may lie.

H. In any action brought by either party to enforce the terms of this Contract, each party shall be bear responsibility for its attorney's fees and all costs regardless of whether one party is determined to be the prevailing party.

[Signatures on following page]

Date	Simpson Gumpertz & Heger Inc.
Date	Christopher L. Cabaldon, Mayor City of West Sacramento

Approved as to form:

Jeffrey Mitchell, City Attorney

Attest:

Kryss Rankin, City Clerk

EXHIBIT A
Proposal

14 August 2018
(Revised 14 August 2018)

Mr. James Strand
Project Manager
City of West Sacramento
1110 West Capitol Avenue, 2nd Floor
West Sacramento, CA 95691

Re: Building Envelope Investigation and Repair Design, Fire Station No. 45, West Sacramento, CA

Dear Mr. Strand:

We would be pleased to perform an investigation into the water intrusion at the above-named project and prepare design documents for the necessary remediation. Our scope of work is based on the as-built drawings provided on 9 August 2018 and subsequent email correspondence with you. We intend to retain FD Thomas to provide contractor support during our investigation, as well as cost estimating and constructability consulting during the development of the design documents.

BACKGROUND

Fire Station No. 45 (FS45) is a two-story, wood-framed structure with exposed structural clay brick and reinforced CMU walls, as well as cement plaster cladding and aluminum-framed storefront and windows. There are low-slope modified bituminous roofs at various levels, as well as a roof deck at Level 2. The project also includes a corrugated metal mechanical screen. We understand that the current water intrusion occurs primarily at the exterior walls. We will assist the City of West Sacramento in identifying and addressing water intrusion at each of the following building envelope components:

- Reinforced CMU.
- Structural Clay Brick.
- Cement Plaster Cladding.
- Aluminum-Framed Windows and Storefront.
- Decks.
- Low-Slope Roofs.

SCOPE OF WORK

The scope of work included in this proposal includes the investigation phase and the design phase. Note that the design phase indicates a range of fees that will be further narrowed after the completion of the investigation phase.

Investigation Phase

The Investigation Phase will include the following items:

- Interview the building manager or other knowledgeable parties via phone and/or email to understand the extent of past and current water intrusion.

- Prepare a field investigation plan indicating the locations and extent of proposed testing and investigation for City review and approval.

- Perform AAMA 511 water testing and destructive openings to identify the source(s) of water intrusion and to verify the roof, wall, and deck assemblies. We have assumed up to two wall openings (cement plaster only), three roof openings, and one deck opening. Our subconsultant will repair destructive openings to a watertight condition. We will also perform a full survey of the exterior walls and roofs to determine the general condition of the building envelope systems. We have included five full days on site for two engineers.

- Prepare a scope document briefly describing our findings and discussing options and pricing for remediation of each building envelope component.

- Meet with City personnel and our subconsultant one time via online conferencing to discuss the remediation options, performance expectations, and budget constraints.

Note that we have reached out to FD Thomas to provide investigation support, and included their pricing below. Following the completed investigation and a decision by the City regarding the remediation approach, we will confirm our design phase fees and prepare a proposal/addendum for construction administration services. The fee associated with the design and construction phases will depend on the selected remediation approach and extent.

Design Phase

The Design Phase will include the following items, to be further clarified after the investigation phase:

- Prepare complete drawings for the repair/replacement of each building envelope component that is contributing to leakage. We will submit the drawings to the City for review at 100% Design Development and 50% Construction Documents. We will also submit the drawings to the authority having jurisdiction for permitting. Our fee assumes up to one revision based on authority having jurisdiction comments.

- Prepare specifications related to the building envelope components.

- Meet with the City as required to discuss the design progress. We anticipate up to four meetings.

- Participate in a bid walk and review contractor bids on behalf of the City.

Note that we have reached out to FD Thomas to provide design phase constructability and cost estimating support, and included their pricing below.

Construction Administration (formal scope and fee to be finalized after investigation phase)

The Construction Administration Phase will include the following items, to be further clarified after the investigation phase:

- Attend preconstruction and coordination meetings with the contractor and owner.
- Review and respond to submittals pertaining to the building envelope repair/ replacement work.
- Review and respond to requests for information (RFIs) from the contractor pertaining to the building envelope repair/replacement work.
- Visit the site at intervals appropriate to the stage of construction, or weekly, to observe the construction progress and verify compliance with the design documents. We will prepare field reports with photographs for each site visit.

PROPOSED STAFFING

For this project, Kenneth A. Klein, P.E., will serve as Principal-in-Charge, with Craig W. Allender, P.E., as Project Manager. We will provide our services for this project from our San Francisco office.

COMPENSATION

We recommend that you establish the following budget for our fees for the investigation and design services described above on this project.

Included Scope		Fee
SGH Investigation		$24,500
Contractor Support		$20,000
SGH Design Phase		$25,000 – $55,000
Cost Estimating		$5,000
	Subtotal	$74,500 – $104,500
SGH Expenses		$6,500
	TOTAL	**$81,000 – $111,000**

This is not an upset limit, but we will notify you before exceeding the budgeted amount. We anticipate that our fees for the construction administration phases will be within the following range, including expenses:

Future Scope	Fee Range
Construction Administration	$40,000 – $70,000

Our fees are computed on an hourly basis as shown on the attached Fee Schedule and Payment Terms. Travel time is charged at full rates from 8:00 a.m. to 6:00 p.m. and at half rates during other periods. SGH will bill direct expenses at actual cost plus 10%. Direct expenses are out-of-pocket expenses and include, but are not limited to, subconsultants, travel, outside services, copying, and charges for the use of SGH field and laboratory equipment and specialized computer software.

This proposal is valid for sixty days. Our proposed agreement consists of this proposal and the enclosed Contract Provisions and Fee Schedule and Payment Terms. If acceptable, please sign and return one copy of this letter.

If you have any questions or comments, please do not hesitate to contact us.

Sincerely,
SIMPSON GUMPERTZ & HEGER INC.

Kenneth A. Klein, P.E.
Senior Principal
CA License No. 44803

Craig W. Allender, P.E.
Senior Project Manager
CA License No. 77190

Accepted: CITY OF WEST SACRAMENTO

Signature: _____

Printed Name: _____

Title: _____

Date: _____

mlr (20180814/SF18-0001003r-KAK) 0101C18 CP-2-CA

Encls.

SIMPSON GUMPERTZ & HEGER INC.
FEE SCHEDULE AND PAYMENT TERMS

Personnel Category	Hourly Billing Rate
Senior Principal	$330
Principal	$310
Associate Principal	$290
Staff Consultant	$245 – $290
Senior Project Manager	$250 – $290
Senior Project Supervisor	$215 – $255
Senior Staff II	$200 – $225
Senior Staff I	$160 – $190
Staff IV	$220 – $230
Staff III	$190
Staff II	$145 – $155
Staff I	$135
Technical Aide	$80
Senior Laboratory Technician	$125 – $155
Laboratory Technician	$115
Senior Graphics Specialist	$230
Graphics Specialist II	$165
Graphics Specialist	$120
Senior Project Drafter	$160
Senior Drafter	$135
Drafter	$110
Non-Technical	$95 – $110

Rates are in US dollars. Individuals performing services are billed at the applicable rate(s) stated above. Where ranges of hourly rates are shown for a single title they reflect the varying rates of the particular individuals with that title. SGH will provide specific hourly rates for individuals assigned to the project if requested. An annual rate adjustment, based upon salary increases, will apply on 1 January each year.

Invoices will be submitted showing labor (hours worked) by labor category and total expenses.

FORM 0101C18 01 January 2018

SIMPSON GUMPERTZ & HEGER INC. (SGH) CONTRACT PROVISIONS

CLIENT: CITY OF WEST SACRAMENTO

1. **CONTRACT** – These Contract Provisions and the accompanying Proposal and Fee Schedule constitute the entire Agreement of the parties, and supersede all prior negotiations, agreements, and understandings with respect to the subject matter of this Agreement. These Contract Provisions shall take precedence over any inconsistency or contradictory provisions contained in any proposal, contract, purchase order, requisition, notice to proceed, or like document. The parties may only amend this Agreement by a written document duly executed by both parties.

2. **RIGHT OF ENTRY** – When entry to property is required by the work, the Client agrees to obtain legal right-of-entry on the property.

3. **DOCUMENTS** – All reports, notes, drawings, specifications, data, calculations, and other documents prepared by SGH are instruments of SGH's service that shall remain SGH's property. The Client agrees not to use SGH-generated documents for marketing purposes or for projects other than the project for which the documents were prepared by SGH without SGH's prior written permission.

   Any reuse or disbursement to third parties without such express written permission or project-specific adaptation by SGH will be at the Client's sole risk and without liability to SGH or its subsidiaries, independent professional associates, subconsultants, and subcontractors. Accordingly, the Client shall, to the fullest extent permitted by law, defend, indemnify, and hold harmless SGH from and against any and all costs, expenses, fees, losses, claims, demands, liabilities, suits, actions, and damages whatsoever arising out of or resulting from such unauthorized reuse or disbursement. Any release or project-specific adaptation by SGH will entitle SGH to further compensation at rates to be agreed upon by the Client and SGH.

4. **DISPOSAL OF SAMPLES** – SGH will discard samples upon completion of the work covered under this Agreement, unless the Client instructs otherwise in writing.

5. **HAZARDOUS MATERIALS** – The scope of SGH's services for this Agreement does not include any responsibility for detection, remediation, accidental release, or services relating to waste, oil, asbestos, lead, or other hazardous materials, as defined by Federal, State, and local laws or regulations.

6. **CONSTRUCTION SERVICES** – When construction-phase services are included in the Agreement, SGH will provide personnel to evaluate whether construction is in general accordance with the construction contract, but not to perform detailed observations or inspections of the work.

   SGH is not a guarantor or insurer of the contractor's work; the contractor is solely responsible for the accuracy and adequacy of construction and for all other activities performed by the contractor, including the means and methods of construction; supervision of personnel and construction; control of machinery; false work, scaffolding, and other temporary construction aids; safety in, on, and about the job site; and compliance with OSHA and all other applicable regulations. SGH's evaluation of the contractor's performance will not include review or observation of the adequacy of the contractor's safety measures or of safety conditions on the project site nor of Contractor's means or methods of construction.

7. **STANDARD OF CARE** – SGH and its subsidiaries, independent professional associates, subconsultants, and subcontractors will exercise that degree of care and skill ordinarily practiced under similar circumstances by engineers and architects providing similar services. The Client agrees that services provided by SGH will be rendered without any warranty, express or implied.

   SGH shall exercise usual and customary professional care in its efforts to comply with codes, regulations, laws rules, ordinances, and such other requirements in effect as of the date of execution of this Agreement.

   The Client agrees that SGH has been engaged to provide technical professional services only, and that SGH does not owe a fiduciary responsibility to the Client.

8. **OPINION OF PROBABLE COSTS** – When required as part of our work, SGH will furnish opinions of probable cost but does not guarantee the accuracy of such estimates. Opinions of probable cost, financial evaluations, feasibility studies, economic analyses of alternate solutions, and utilitarian considerations of operations and maintenance costs prepared by SGH hereunder will be made on the basis of SGH's experience and qualifications and will represent SGH's judgment as an experienced and qualified design professional. SGH does not have control over the cost of labor, material, equipment, or services furnished by others or over market conditions or contractors' methods of determining prices or performing the work.

9. **SUSPENSION OF WORK** – The Client may, at any time, by written notice, suspend further work by SGH. The Client shall remain liable for, and shall promptly pay SGH for all services rendered to the date of suspension of services plus suspension charges. Suspension charges shall include the cost of assembling documents, personnel and equipment rescheduling or reassignment, and commitments made to others on the Client's behalf. If after ninety (90) days the Client resumes SGH's work on the Project, SGH and the Client shall renegotiate SGH's fee.

   If payment of invoices by the Client is not maintained current, SGH may, upon written notice to the Client, suspend further work until payments are brought current. The Client agrees to indemnify and hold SGH harmless from any claim or liability resulting from such suspension.

10. **TERMINATION** – The Client or SGH may terminate this Agreement for cause, except only the Client may terminate for convenience. The party initiating termination shall so notify the other party. The Client shall compensate SGH for services performed prior to termination and for prior authorized commitments made by SGH on the Client's behalf.

11. **CHANGES OR DELAYS** – Unless the accompanying Proposal provides otherwise, the proposed fees constitute SGH's estimate to perform the services required to complete the Project. Required services often are not fully definable in the initial planning; accordingly, developments may dictate a change in the scope of services to be performed. Where this occurs, changes in the Agreement shall be negotiated and an equitable adjustment shall be made. In addition, costs and schedule commitments shall be subject to renegotiation for unreasonable delays caused by the Client's failure to provide specified facilities, direction, or information.

12. **FORCE MAJEURE** – SGH will not be liable to the Client for delays in performing its Services or for direct or indirect costs resulting from such delays that may result from labor strikes, riots, war, acts of governmental authorities, extraordinary weather conditions or other natural catastrophes, or any other cause beyond the reasonable control or contemplation of either party.

13. **LIABILITY** – SGH will furnish appropriate insurance certificates for general and professional liability upon request. The Client agrees that SGH's total liability to the Client and the total liability to the Client of SGH's principals, officers, agents, and employees, for any and all injuries, claims, losses, expenses, or damages whatsoever, including attorney's fees, arising out of or in any way related to the Project or this Contract from any cause or causes, including, but not limited to, SGH's negligence, errors, omissions, strict liability, breach of contract, or breach of warranty shall not exceed SGH's total fee under this Agreement or $50,000, whichever is greater. In no event shall SGH be liable to Client for any indirect, incidental, special or consequential damages whatsoever (including but not limited to lost profits or interruption of business) arising out of or related to the services provided under the Agreement.

14. **CONFLICTS OF INTEREST** – This assignment may presently or in the future involve parties with potentially adverse interests to those of SGH's existing or future clients ("Affected Parties" or "Affected Party"). Prior to SGH's acceptance of this assignment, SGH will make reasonable attempts to identify any Affected Parties based on information SGH has in its possession from the Client and any Affected Parties and SGH's search of its project and proposal databases. To the extent that SGH identifies a relationship with an Affected Party, SGH will inform the Client as to the identity of such parties. Client agrees to allow SGH to release to any Affected Parties the fact of SGH's engagement by the Client and any other information required to evaluate any potential conflict.

    SGH's ability to inform the Client of a relationship with an Affected Party is limited by the thoroughness and accuracy of the information provided to SGH by the Client and any Affected Parties, and by SGH's limitations in reasonably and diligently discovering all relationships with Affected Parties. Regardless of SGH's relationship with an Affected Party, and, provided such relationship with an Affected Party does not arise from SGH's willful disregard of a relationship with the Affected Party, SGH shall be entitled to payment for all services rendered to the date of discovery or notice, whichever occurs first, of a relationship between SGH and an Affected Party. SGH does not guarantee that a relationship between the Client and an Affected Party, which may be perceived by the Client as a conflict, will not arise during the course of an assignment or thereafter. SGH disclaims responsibility for such occurrences and to the fullest extent permitted by law, the Client agrees to waive any claim against SGH arising out of any such actual or potential conflict-related occurrences. Subsequent to the date of this Agreement, SGH will not be in a position to guaranty that it can advise the Client of any future Affected Parties or perceived or actual conflict circumstances that may arise, but will endeavor to notify Client of such situations.

15. **INDEMNIFICATION** – SGH shall, subject to the limitation of liability contained in Section 13, indemnify (but not defend) the Client for any loss or damage caused solely by the professional negligence of SGH in performance of the services under this Agreement.

16. **MISCELLANEOUS**

    Governing Law: The laws of the state in which the Project is located shall govern the validity and interpretation of this Agreement.

    Invalid Terms: If any of these Contract Provisions shall be finally determined to be invalid or unenforceable in whole or in part, the remaining provisions hereof shall remain in full force and effect and be binding upon the parties. The parties agree to reform the contract between them to replace any such invalid or unenforceable provision with a valid and enforceable provision that comes as close as possible to the intention of the stricken provision.

    SGH Reliance: Unless otherwise specifically indicated in writing, SGH shall be entitled to rely, without liability, on the accuracy and completeness of information provided by the Client, the Client's consultants and contractors, and information from public records, without the need for independent verification.

    Copyright Infringement Indemnification: To the fullest extent permitted by law, the Client agrees to defend, indemnify, and hold harmless SGH from any and all claims, damages, suits, causes of action, liabilities or costs, including reasonable attorneys' fees and costs of defense, arising out of or in any way connected with SGH's use of documents or designs prepared by the Client's consultants, that may be asserted against or incurred by SGH.

    Certifications: SGH shall not be required to sign any documents, no matter by whom requested, that would result in SGH's having to certify, guaranty, or warrant the existence of conditions that SGH cannot ascertain.

    Payment: Invoices will be submitted periodically, and are due and payable upon receipt. Unpaid balances shall be subject to an additional charge at the rate of 1-1/2% per month from the date of invoice if the unpaid balance is not paid within thirty (30) days. The Client shall reimburse SGH for all attorney's fees and costs related to collection of overdue payments.

    Litigation: All costs and labor associated with compliance with any subpoena or other official request for documents, for testimony in a court of law (other than in connection with expert witness services), or for any other purpose relating to work performed by SGH, in connection with work performed for the Client, shall be paid by the Client as a direct expense (actual cost plus 10%).

    Taxes: Client shall, in addition to the other amounts payable under this Agreement, pay, on a timely basis, all sales, use, value added or other taxes, federal, state or otherwise, however designated (hereinafter "Taxes"), which are levied or imposed by reason of the transactions contemplated by this Agreement or any of the Services, except for taxes on SGH's net income. Client shall promptly pay SGH for any Taxes actually paid by SGH on behalf of Client, or which are required to be collected or paid by SGH. SGH may bill Client separately for such Taxes.

(010617-2-CA)

EXHIBIT B

Schedule of Performance

Task 1: Onsite Testing and Assessment	2-4 weeks from receiving a fully executed agreement.
Task 2: Report Findings	Within 2-4 weeks of completing Task 1.
Task 3: Project Closeout	Within 6-8 weeks of completing Task 2.

EXHIBIT C

CERTIFICATE OF COMPLIANCE WITH LABOR CODE § 3700
[Labor Code § 1861]

I am aware of the provisions of Section 3700 of the Labor Code which require every employer to be insured against liability for workers' compensation or to undertake self-insurance in accordance with the provisions of that code, and I will comply with such provisions before commencing the performance of the work of this contract.

CONSULTANTS

By: _____
Simpson Gumpertz & Heger Inc.

EXHIBIT D

LABOR COMPLIANCE

1. PREVAILING WAGE

A. The Work contemplated herein constitutes a public work within the meaning of Labor Code sections 1720 and 1771. It shall be mandatory upon the Consultant and upon any Subcontractor, to pay not less than the said specified prevailing rates of wages to all workers employed by them under the Contract in accordance with Labor Code section 1774. The Director of the Department of Industrial Relations ("DIR") of the State of California has determined the general prevailing rate of wages of per diem wages in the locality in which the work is to be performed for each craft or type of worker needed to execute the Contract. The Consultant acknowledges that it has examined the prevailing rate of per diem wages as established by the DIR. Copies of the current schedules for prevailing wages are on file at City Hall, and the contents of those schedules are incorporated herein as if set forth in full. The Consultant shall post a copy of the applicable prevailing wage determinations at each job site, along with any other work place posters required by law.

B. The City will not recognize any claims for additional compensation because of the payment of prevailing wages. The possibility of wage increases is one of the elements to be considered by the Consultant in determining its proposal, and will not under any circumstances be considered as the basis of a claim against the City.

C. By executing this Contract Consultant warrants that it has registered with the Department of Industrial Relations in accordance with Labor Code section 1725.5.

2. PREVAILING WAGE RECORDS

A. The Consultant and each subcontractor shall keep an accurate payroll record which shows the name, address, social security number, correct work classification (in accordance with the wage decision), both straight and overtime worked each day and week, and hourly rate of pay, gross wages earned, deductions made and net wages paid to each journeyman, apprentice, worker or other employee paid by the Consultant /subcontractor in connection with the Work. These payroll records shall be certified and shall be made available at Consultant's principal office. These records shall be maintained during the course of the Work. The Consultant and all subcontractors shall make the certified payroll records available for inspection by City representatives upon request and shall permit such representatives to interview employees during the work hours on the job site.

B. The City shall notify the Consultant in writing of any discrepancies or violations that are discovered during such inspections. Written notification pursuant to this Section shall include the actions that will be necessary to resolve the discrepancies and/or violations. The Consultant shall be held entirely responsible for the prompt resolution of all non-compliances with the prevailing wage laws, including those pertaining to all subcontractors and any lower-tier subcontractors. The Consultant shall forfeit as penalty to the City the amount specified by law for each calendar day or portion thereof for each worker (whether employed by the Consultant or any subcontractor) paid less than the stipulated prevailing rates for any work done under the Contract in violation of the provisions of the Labor Code and in particular, Section 1775.

C. To the extent applicable, Consultant and subcontractors shall maintain and furnish to the Department of Industrial Relations ("DIR"), a certified copy of each weekly payroll (but no less often than monthly), with a statement of compliance signed under penalty of perjury. Such certified payroll reports shall be transmitted electronically to the DIR.

D. The City will not recognize any claims for additional compensation because of the payment of the prevailing wages. The possibility of wage increases is one of the elements to be considered by the Consultant in entering into the Contract, and will not under any circumstances, other than delays caused by the City, or the City's agents, be considered as the basis of a claim against the City.

3. Labor Discrimination

Attention is directed to Section 1735 of the Labor Code, which reads as follows:

> "No discrimination shall be made in the employment of persons upon public works because of their race, color, national origin or ancestry, physical handicap, mental condition, marital status, or sex of such person, except as provided in Section 12940 of the Government Code, and every General Contractor for public works violating this section is subject to all the penalties imposed for a violation of this chapter."

4 Eight-Hour Day Limitation

(a) In accordance with the provisions of the Labor Code, and in particular, Sections 1810 to 1815 thereof, inclusive, incorporated herein by reference, eight hours labor shall constitute a day's work, and no worker in the employ of Consultant, or any Subcontractor, doing or contracting to do any part of the work contemplated by the Contract, shall be required or permitted to work more than eight (8) hours in any one calendar day and forty (40) hours in any one calendar week in violation of those provisions; provided that subject to Labor Code Section 1815, a worker may perform work in excess of either eight (8) hours per day or forty (40) hours during any one week upon compensation for all hours worked in excess of eight (8) hours per day or forty (40) hours during any one week at not less than one and one-half times the basic rate of pay.

(b) The Consultant and each Subcontractor shall also keep an accurate record showing the names and actual hours worked of all workers employed by them in connection with the Contract. This record shall be open at all reasonable hours to the inspection of the City. It is hereby further agreed that, except as provided in (a) above, the Consultant shall forfeit as a penalty to the City the sum of twenty-five dollars ($25) for each worker employed in the performance of the Contract by the Consultant or by any of its Subcontractors for each calendar day during which such worker is required or permitted to labor more than eight (8) hours in and one calendar day and forty (40) hours in any one calendar week in violation of Sections 1810 through 1815.

5. Compliance with State Requirements for Employment of Apprentices

(a) The Consultant's attention is directed to Section 1777.5 of the Labor Code. Provisions of said Section pertaining to employment of registered apprentices are hereby incorporated by reference into this Contract. As applicable, the Consultant or any Subcontractor employed by the Consultant in the performance of this Contract shall take such actions as necessary to comply with the provisions of Section 1777.5.

CITY OF WEST SACRAMENTO | AGENDA REPORT

MEETING DATE: August 22, 2018 | **ITEM #** 14

SUBJECT:

CONSIDERATION OF UPDATE ON THE PILOT ON-DEMAND RIDESHARE SERVICE

INITIATED OR REQUESTED BY:

[X] Council [ ] Staff

[ ] Other

REPORT COORDINATED OR PREPARED BY:

Sarah Strand, Associate Transportation Planner

Denix Anbiah, Director of Public Works

ATTACHMENT [X] Yes [ ] No [X] Information [ ] Direction [ ] Action

OBJECTIVE

The purpose of this report is to provide a regular update on the status of the City's Pilot On-Demand Rideshare service, in accordance with the City Council's request for monthly project reports.

RECOMMENDED ACTION

Staff respectfully recommends that the City Council receive the provided informational update on the Pilot On-Demand Rideshare service.

BACKGROUND

The Pilot On-Demand Rideshare service is an innovative public transportation model being tested by the City as an early action item of the broader Mobility Action Plan (MAP). The Pilot will operate citywide for one (1) full year using a fleet of dedicated, co-branded Mercedes Benz Metris vans to provide on-demand, share rides for a flat fare. Ongoing performance monitoring and data collection will be leveraged to inform broader transportation planning considerations, including opportunities to optimize fixed route bus service and increase mobility for underserved communities. Throughout the life of the Pilot, service parameters such as fares, hours operation, or service areas may be modified to better meet demands.

On January 17, 2018, the City Council elected to award a contract to Nomad Transit LLC, a wholly-owned subsidiary of Via Transportation Inc., to assist with the planning, marketing, launch, operation, maintenance and performance evaluation of the service. At that time, City Council directed staff to return monthly with project updates, including proposed service modifications, data on service performance and ridership, as well as higher-level policy considerations to ensure ample opportunity for Councilmembers to provide input on the project. At the end of the Pilot term, a final performance evaluation report will be compiled by the UC Berkeley Transportation Sustainability Research Center and submitted for Council's consideration. City Council may elect to continue or terminate service, contingent on performance and cost to sustain the program.

This report is submitted for City Council consideration and provides updated performance metrics on ridership and quality of service, as well as project related activities such as marketing and outreach.

ANALYSIS

As of August 12, 2018, the City's Pilot On-Demand Rideshare program has completed thirteen (13) weeks of service. The service has been operating in "full launch" mode with extended hours and alternative pricing options (ViaPass) available for eight (8) weeks. A total of 1,898 people have created a Via West Sacramento account since the Pilot began.

Overview of Ridership Data, Trends & Customer Feedback

Ridership has continued to grow consistently week after week, with a total of 1,077 rides provided in week 13, and 7,252 rides completed to-date (as of August 12, 2018). As demand has risen, average ETAs and trip times have experienced a slight growth, up to an average 8-minute wait time, and 8.5-minute trip time. Customer satisfaction has maintained strong customer satisfaction ratings, at 4.92 out of 5 on average.

Roughly 125 Via users have applied for and received a Senior or Disabled Rider discount. Seventy-three percent of discounted recipients received the Senior discount and 13% received a Disabled Rider discount, while 14% were eligible for both. When polled at time of issuing the discount, roughly 60% of Seniors and Disable Riders reported that they will primarily book rides using the Smart Phone App, and 40% plan to book their rides using the dial-up service on a traditional telephone. A total of 43 Wheelchair Accessible Vehicle (WAV) Via trips have been requested and completed to date.

The most popular pick-up and drop-off locations continue to focus on commercial and retail shopping centers, with Walmart and the Riverpoint Plaza, Southport Town Center, and the plaza at W Capitol Ave and Harbor Blvd topping the list. The Civic Center and surrounding shops also seem to attract strong ridership, with a top pick-up and drop-off location at W Capitol Ave and Jefferson Blvd. Anecdotally, some users are continuing to take Via to the intersection nearest to the Tower Bridge, then using JUMP bikes or walking into the Downtown core.

Generally, feedback has continued to be positive, as many Seniors and disabled people in the community have expressed a renewed sense of independence, mobility, and ability to be more spontaneous with their travel plans within the City. A growing number of community members have stated that their teenagers are hugely benefiting from the program, as are parents with limited capacity to transport family members to school, work, or recreation. With the start of the school year, staff will be initiating engagement with the Washington Unified School District (WUSD) and River City High School to identify opportunities to raise awareness about the rideshare program among parents with children over the age of 13.

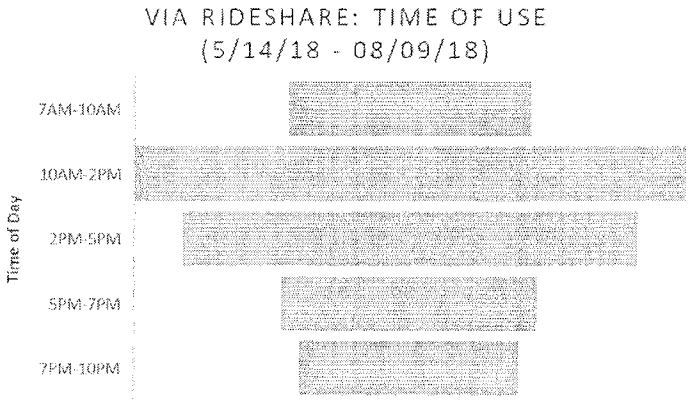
Via riders have continued to provide feedback on the service, as well as suggest locations for expansion. Of note, strong community interest indicates significant demand for connections to the new Kaiser Permanente location in Downtown Sacramento, and to a slightly lesser extent, the Sacramento Valley Amtrak Station. Additionally, staff has received limited inquiries from community members requesting that the minimum age to ride alone be dropped to allow children to use the service accompanied by teen siblings, as well as to provide car seats for infants and small children. Currently, youth ages 13 and older may ride unaccompanied by an adult with parent/guardian consent, and users must bring their own car seats. Staff is currently exploring the implications of such policy changes to the age-restriction and will return to City Council to submit a recommendation, as appropriate, at a later date. Provision of car seats would be at-cost to the City and would likely require the purchase of multiple types of car seats to accommodate different aged children, as well as use a limited stowing capacity in each van. Staff currently does not recommend making this change but will further explore options to better accommodate families and return to City Council with any relevant recommendations.

Shared Rides & Bookings

As previously reported, staff requested that Via provide regular updates on metrics showing the percentage of shared rides, which is defined as any ride with more than one (1) passenger, and shared bookings, which is defined as any trip that accommodates multiple bookings (i.e., two strangers separately book a ride but share a portion of their trip). To date, an average of 32% of rides are shared (2 or more passengers) and an average of 7% of all bookings to-date have been shared. In the first 4-8 weeks of service, shared bookings occurred far less frequently (1-5%); however, the portion of shared bookings has grown each week in step with rising demand for service, with 10-15% of all bookings being shared over the past 4-6 weeks of operations. See **Attachment 1** for a breakdown of shared rides and bookings by week.

Time of Use

Data on time of use was provided to give the City more insights on when Via rides are being taken throughout the day. Interestingly, current usage patterns indicate that most rides are taken between the hours of 10AM to 2PM, and 2PM to 5PM, which is inverse from the peak AM/PM peaks observed in Via's other more commuter-centric markets. This early pattern may also be a result of the additional outreach efforts the City has made to engage with the older adult community, many of whom are retired and do most of their shopping and recreation during these times. Since expanding hours to 10pm, close to 900 additional rides were made on the platform between 7 pm-10 pm.



Marketing, Outreach & Events
Staff is continuing to plan and implement marketing, outreach and promotions to raise awareness about the Via Rideshare option and encourage community members to try it out and provide feedback.

Additionally, the City continues to be contacted by other jurisdictions and public agencies to learn more about West Sacramento's innovative transportation model. Below is a summary of relevant activities that have been completed or are planned in the near future:

- July 9th: An information project report was submitted to the Transportation, Mobility and Infrastructure Commission.

- July 19th: Staff met with the Yolo County HHSA to provide information on the Via service and to discuss the West Sacramento Social Services Office's interests in leveraging the service for client transportation needs.

- July 23rd: The Summer STEPS Interns received a presentation on the Via rideshare service teaching them how to create an account and use the App. On July 27th, the interns rode Via to and from their tour of Fire Station 45. Staff hope some of them will serve as ambassador's among their peers for this program, especially as school returns to session this month.

- July 24th: Staff met with DaVita Dialysis Clinic upon a request for more information about the Via program. The office was provided with posters and brochures to distribute to local clientele.

  Staff from Mitsubishi's Isuzu Business Division traveled from Tokyo, Japan to West Sacramento as part of a national tour of Via projects. City staff provided an overview and Q&A on the West Sacramento On-Demand service and discussed the City's current approach to performance monitoring and data sharing with the Mitsubishi team. Mitsubishi representatives stated an interest in lobbying the Japanese government to allow for new mobility options like rideshare to operate, particularly as the country is increasingly implementing restrictions on driving among Seniors in light of a dramatic spike in fatal collisions involving elderly drivers suffering from vision impairment, dementia and other age-related ailments. Japan currently has the oldest population in the world with 1/7 people over the age of 75 compared to 1/16 in the United States. Of special interest was how a service like Via might improve quality of life for Seniors in rural Japan. In addition, Japan's bus systems are currently comprised by a multitude of private fixed route operators that are heavily subsidized, whereas a service model more like the Via West Sacramento model could results in efficiencies and cost savings.

- July 26th: Staff attended the Food Truck Mania event to conduct outreach and pass out Via flyers.

- August 2nd: Another "Learn-to-Ride" event was held from 11 am – 1 pm at the Community Center offering community members an opportunity to get help creating a Via account, ask questions, provide feedback, and/or apply for their Senior/Disabled Rider discount.

- August 4th: Staff will attend the Holy Cross Festival to distribute Via Rideshare brochures and flyers, and to be available for questions from community members. This event is historically well attended.

- August 7th: Staff brought a Via van and joined the Yolo County Housing National Night Out location on Lighthouse Drive to distribute flyers and help sign community members up for Via accounts.

- August 14th: Staff will host members of the McClellan Park TMA that contacted the City for a presentation and demo of the Via rideshare program as they are similarly exploring transportation options to move people from transit centers into and around the park.

- August 18th: The West Sacramento Via Rideshare program has been named presenting sponsor for the four (4) shop local events in the Chamber of Commerce led "Third Saturday Swarm" events, the first of which will be held at Southport Town Center. Attendees will have the chance to win a full month of free Via rides. Via will send an e-blast to existing users to promote the event.

- September 5th: Transportation staff will assist the City Manager's Office in hosting an on-boarding event for participants recruited for the AARP micro-mobility incentives study.

- September 7th: Staff from Washoe County Regional Transportation Commission (RTC) will be visiting from Nevada to receive a demonstration and to learn more about the City's on-demand rideshare program in partnership with Via.

- September 15th/16th: A promotion offering $1 rides to/from the Mediterranean Food & Music Festival with promo code "MEDFEST" will be publicized in an effort to spread the word about the service, as well as help alleviate parking demand at this popular local event.

Challenges/Barriers
Staff has received feedback from some Via users that have reported problems reaching a Via customer service representative attempting to book rides over the phone. Staff has identified this as a top priority issue for the Via technical team to investigate. Since the first complaints of this issue emerged, the Via team has been closely tracking performance of the call center and monitoring calls to identify the root of the problem. Although the problem has not been reported at high frequencies, it is a critical service for users that do not own a smart phone to be able to book their return trip. Staff will continue to closely monitor this issue to ensure that it is fully resolved in a timely manner.

Demand climbed faster than anticipated on Thursday August 9 and Friday August 10, resulting in slightly longer than normal wait times and demand errors for some customers. Staff speculates that an increased attendance at River Cats games combined with an overall growth in ridership resulted in the unprecedented ridership of more than 200 rides on each day. Via will be adding additional Drivers to the platform in response to the growth in demand to maintain high quality of service.

Additionally, staff has requested that the Via team re-visit opportunities to ensure that all Driver partners participating on the Via platform are abiding by all traffic laws and regulations and demonstrate a high level of professionalism and safe driving conduct while utilizing the Via vans. Currently, Via closely monitors Driver activity through GPS enabled tracking built into the Mercedes Benz Metris fleet in addition to the Driver's cell phone.

Proposed/Implemented Service Modifications
Staff is currently coordinating with the Via team on lowering the standard fare from $3.50. Staff intends to develop a lower fare recommendation that will balance an anticipated increase in ridership with a baseline revenue recovery that could help maintain the provision of service at a sustainable cost to the City. Since implementing the $10 weekly pass option, over 1500 rides have been taken using the ViaPass.

Other
AARP Challenge Grant Coordination
Staff has been and will continue to work closely with the City Manager's office on the AARP grant to study the effectiveness of varying incentives and educational approaches to encourage older adults in the community to utilize new micro-mobility options, such as Via rideshare and JUMP bikes. The Via team is assisting by providing unique and trackable promo codes to provide to study participants and will provide usage data to the City for analysis at the end of the AARP pilot term. Transportation staff will provide support at one or more events to help on-board AARP study participants, create Via accounts, and provide outreach and education on how to use the City's on-demand rideshare service.

YCTD Coordination
Ongoing communication and support has continued between the City and the Yolo County Transit District. YCTD has added a promotion of Via to their webpage, and the City will be coordinating on a Utility Billing mailer to remind residents of all of their mobility options offered by Yolobus in the Fall. To date, YCTD has reported that there have been no negative incidents or conflicts between the new Via service and existing fixed route operations. Prior to launching the Via service, YCTD indicated that a portion of a Low Carbon Transit Operations Program (LCTOP) grant the district received would be used to supply round-trip fixed route vouchers for distribution to Via riders wishing to transfer to the Downtown Shuttle. Staff is currently awaiting further direction from YCTD, pending the finalization of their LCTOP grant agreement. Staff will provide updates to the City Council as soon as additional information on timing is provided by YCTD staff.

Environmental Considerations
On January 17, 2018, the City Council approved a Categorical Exemption for this project under Class 6, Guidelines Section 15306 (Information Collection) of CEQA since the Pilot will focus on data collection, research and evaluation activities which do not result in a serious or major disturbance to an environmental resource and will inform the City's consideration of approving and funding the service for additional years. A Notice of Exemption was submitted to the County Clerk's Office. No additional environmental review is required for this informational update.

Commission Recommendation
Staff has been and will continue to provide regular project status updates on the Via Rideshare program to the Transportation, Mobility, and Infrastructure Commission for the term of the Pilot program. An informational item was presented on the Consent Agenda on July 9, 2018. The next update to the TMI Commission will be presented on September 10, 2018.

Strategic Plan Integration
This project advances the 2018 Strategic Plan Management Agenda item, "Mobility Action Plan."

Alternatives
1) Accept this informational item about the Pilot On-Demand Rideshare service.
2) Elect not to accept this informational item at this time.

Coordination and Review
This report was prepared by the Traffic and Transportation Division of the Public Works Department in coordination with the Via Transportation, Inc. project team. Marketing and outreach strategies have been developed in coordination with the Communications and Media Division of the City Manager's Office.

Budget/Cost Impact
This project has an approved budget of $749,000 funded by a $149,999 SACOG TDM Innovation Grant and a Council allocation of $599,001 from using local Transportation Development Act (TDA) funds [a combination of State Transit Assistance (STA) and Local Transportation Funds (LTF)]. No additional budget impact is associated with this informational update.

ATTACHMENT(S)
Attachment 1 – Via Ridership Report (May 14 – August 11, 2018)

ATTACHMENT 1

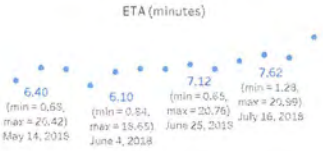
CITY OF WEST SACRAMENTO

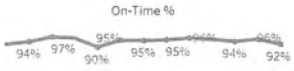
Last 13 Week(s) (May 14, 2018 - Aug 12, 2018)

7,252 completed rides since launch

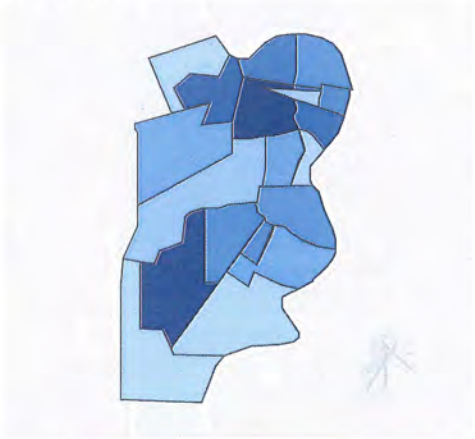
Look back weeks
13

RIDES	DRIVERS	QUALITY OF SERVICE
7,252 completed rides	1.66 utilization	7.64 minute ETAs 95% pickups on-time
1,514 completed Viapass rides	24 active drivers	82% requested rides completed
8.57 minutes per ride	4,370 driver hours	4.92 average ride rating
2.98 miles per ride		



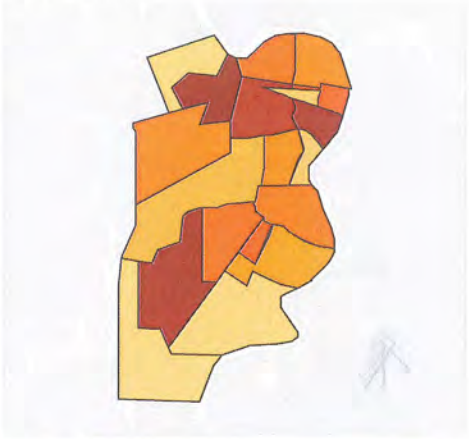


Pickup Requests



Requests 3 1,129

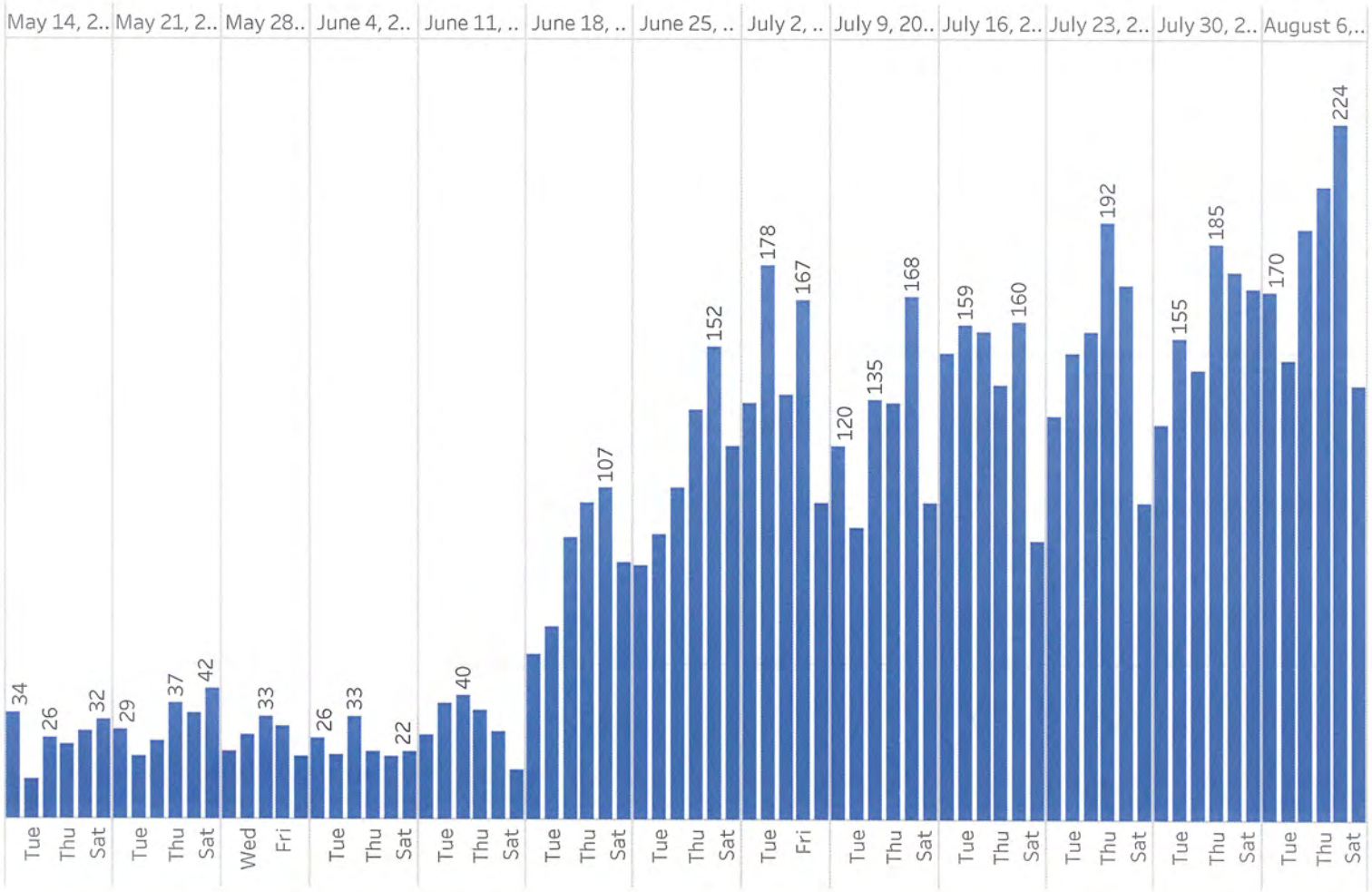
Dropoff Requests



Requests 3 1,085

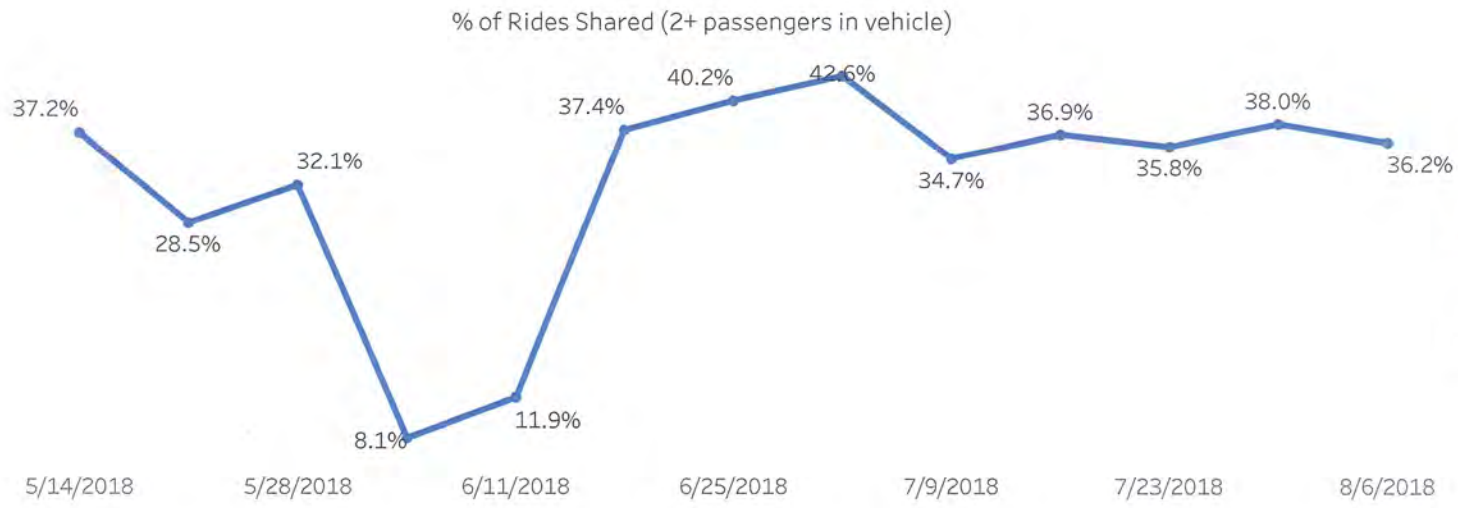
- a driver is **active** if he/she gave at least one ride in the specified time period
- a pickup is **on-time** if the actual pickup time was less than 5 min. earlier or later than the ETA
- **Rides** metrics consider all completed rides
- **Quality of service** metrics consider all sessionized requests

Rides/Day



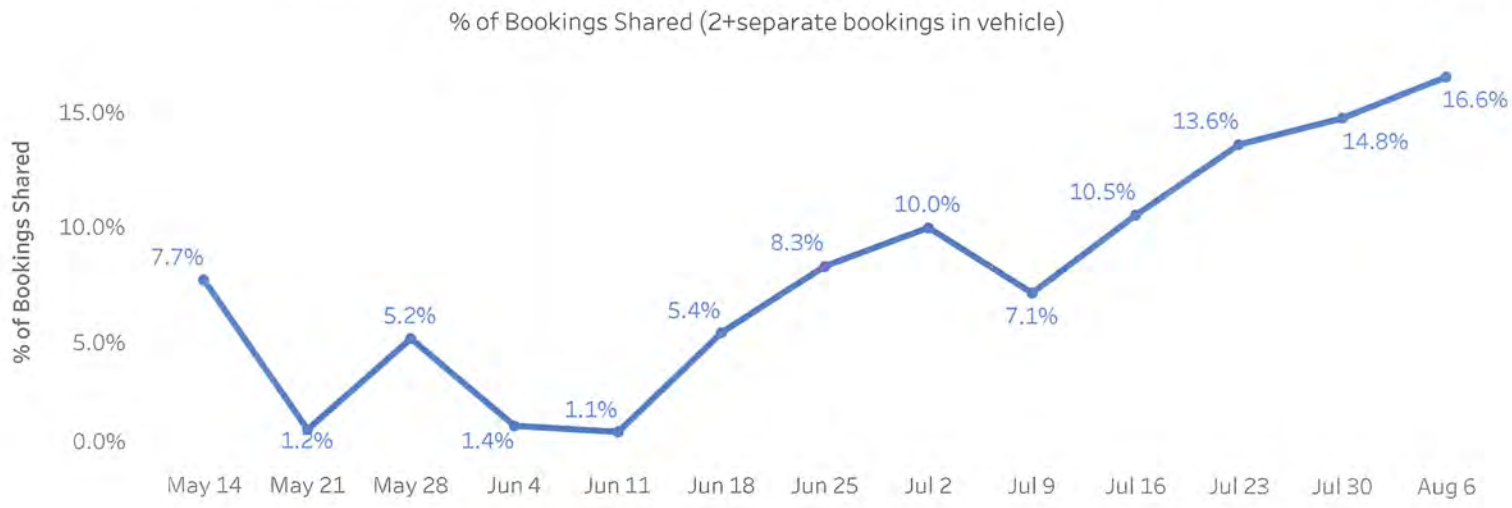
CITY OF WEST SACRAMENTO

Look back weeks
13



CITY OF WEST SACRAMENTO

Look back weeks
13



CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 15

SUBJECT:

CONSIDERATION OF RESPONSE TO YOLO COUNTY GRAND JURY REPORT ON YOLO COUNTY CITY PENSION AND RETIREMENT MEDICAL COSTS

INITIATED OR REQUESTED BY:

[ ] **Council** [X] **Staff**

[ ] **Other**

REPORT COORDINATED OR PREPARED BY:
Roberta Raper, Director of Administrative Services

Aaron Laurel, City Manager

ATTACHMENT [X] **Yes** [ ] **No** [ ] **Information** [ ] **Direction** [X] **Action**

OBJECTIVE
The objective of this report is to seek City Council authorization on a response to the Yolo County Grand Jury report titled "The Looming Crisis of Yolo County City Pension & Retirement Medical Costs."

RECOMMENDED ACTION
Staff respectfully recommends that the City Council review and authorize the attached response to the Yolo County Grand Jury report titled "The Looming Crisis of Yolo County City Pension & Retirement Medical Costs."

BACKGROUND
California cities are experiencing fiscal burdens due to increasing expenses and liabilities related to retiree pensions and medical cost, including Yolo County's four cities (Davis, West Sacramento, Winters and Woodland). The 2017-2018 Yolo County Grand Jury investigated potential impacts on these cities with primary focus on unfunded retiree cost liabilities; annual retiree expense management; best practices collaboration; and transparency to city residents.

On June 13, city staff received the Grand Jury report titled "The Looming Crisis of Yolo County City Pension & Retirement Medical Costs." (Attachment 1.) According to Penal Code Section 933 and 933.05, the City Council is required to respond in writing and in electronic form to certain findings and recommendations of the Grand Jury report within 90 days.

ANALYSIS
Staff has reviewed the Grand Jury report, and the findings and recommendations for which responses are required by the City Council. The report makes the following findings and recommendations:

FINDINGS
F1. For many Yolo County residents, poor transparency and difficulties in accessing information make it hard to understand the consequences of mushrooming retirement benefit expenses and liabilities. This jeopardizes the citizens' ability to hold elected officials responsible for providing adequate funding to all high-priority services.

F2. Several studies reveal that the retirement benefit system has been compromised by "golden handshakes" (e.g. special pension benefit deals or enhancements) and failure to consider the cost of lifetime benefits and likely investment earning levels. This happens every time a public agency negotiates a contract with its employees. Future fiscal solutions will depend, in part, on the public's willingness to hold state and local politicians accountable for their fiduciary responsibility as required by law and ethics (see Bibliography item 10).

F3. Many city councils seem to have found it politically unpalatable or fiscally difficult to find adequate funding resources to make high enough payments to reduce unfunded pension and other post-employment benefit liabilities beyond the required payments. Additionally, when revenue generation is increased (e.g., from bonds, parcel taxes, sales, or utility taxes or fees), the money collected that may be restricted for a specific purpose, makes available other unrestricted general funds to fund retirement cost increases. This is not always clearly communicated to the public.

F4. Beyond CalPERS requirements, the four cities approach the transparency, analysis, management and containment of growing retiree costs in different ways. There is an opportunity for increased collaboration among the cities. For example, Davis has developed a financial forecasting tool that projects revenues and expenses many years into the future. Some cities show retirement costs' share of the "General" or "All" Funds.

RECOMMENDATIONS

R1. By February 1, 2019, City Councils and staff should conduct public education campaigns to increase transparency and awareness of the alarming burdensome impact on city service priorities that is being created by retirement pension and medical insurance costs. Examples of public education could be in the form of education forums, explanatory inserts in utility statements, multi-media articles and/or candid conversation at governmental meetings.

R2. By February 1, 2019, City Councils and staff should create a simple statistical template and/or graph that shows three-year past (actual) and projected (look back, look forward) pension costs and liabilities and their impact (% of total) on the city budget General and All Fund base. This is necessary to assure transparency to the public (for an example developed by the Grand Jury, see the Appendix).

R3. By July 1, 2019, Yolo County City Councils should investigate and consider alternatives to the existing CalPERS managed pension systems in order to achieve a more sustainable and less burdensome financial impact on city budgets. An alternative hybrid-defined pension option is included in the proposed Public Employees' Pension Reform Action of 2018 (Senate Bill B-32). Any alternative plans considered by city governments should be transparent to the public.

R4. By September 1, 2018, collaboration among cities in Yolo County should be increased so that best practices in analysis and cost containment of pensions and other retiree benefits can be shared. The best practices and innovative ideas should be transparent to the public.

The Grand Jury specifically requests the West Sacramento City Council respond to Findings F1, F2, F3 and F4 and Recommendations R1, R2, R3 and R4. Staff has prepared a draft response for City Council review and approval. (Attachment 2.) A final version of the response will need to be authorized by the City Council and submitted to the Presiding Judge by September 30, 2018.

Environmental Considerations
N/A

Commission Recommendation
N/A

Strategic Plan Integration
N/A

Alternatives
The City Council's response to the Grand Jury report on the Yolo County City Pension & Retirement Medical Costs is required by law. As an alternative to staff's recommendation, however, the Council could choose to make revisions to the proposed response to the report.

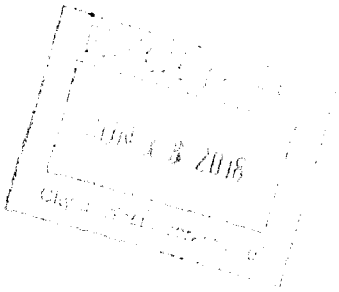
Coordination and Review
This report was prepared by City Manager's Office with input from the Administrative Services Department.

Budget/Cost Impact
None.

ATTACHMENTS

1. Yolo County Grand Jury Report titled "The Looming Crisis of Yolo County City Pension & Retirement Medical Costs."
2. Draft City Council Response to the Yolo County Grand Jury Report

Attachment 1



June 12, 2018

West Sacramento City Council
1110 West Capitol Ave., Third Floor
West Sacramento, CA 95691

RE: The Looming Crisis of Yolo County City Pension & Retirement Medical Costs

The 2017-2018 Yolo County Grand Jury herewith provides the referenced report for your review. The findings in this document report the conclusions reached by the Grand Jury. Although all the findings are based upon evidence, they are the product of the Grand Jury's independent judgment; some findings are the opinion of the Grand Jury rather than indisputable statements of fact.

In accordance with Penal Code Section §933.05(f), a copy of this report is being provided to affected agencies two working days prior to its public release and after being approved by the Advising Judge of the Superior Court. Please note that Section 933.05(f) specifies that no officer, agency, department or governing body of a public agency shall disclose any contents of the report prior to its public release.

This report will be filed with the County Clerk and released to the public on June 30, 2018.

Sincerely,

Judy Wohlfrom
Foreperson

Enclosure

California Penal Code Section §933

California Penal Code, Title 4

§933. (a) Each Grand Jury shall submit to the Presiding Judge of the superior court a final report of its findings and recommendations that pertain to county government matters during the fiscal or calendar year. Final reports on any appropriate subject may be submitted to the Presiding Judge of the superior court at any time during the term of service of a Grand Jury. A final report may be submitted for comment to responsible officers, agencies, or departments, including the county board of supervisors, when applicable, upon finding of the Presiding Judge that the report is in compliance with this title. For 45 days after the end of the term, the Foreperson and his or her designees shall, upon reasonable notice, be available to clarify the recommendations of the report. (b) One copy of each final report, together with the responses thereto, found to be in compliance with this title shall be placed on file with the clerk of the court and remain on file in the office of the clerk. The clerk shall immediately forward a true copy of the report and the responses to the State Archivist who shall retain that report and all responses in perpetuity. (c) No later than 90 days after the Grand Jury submits a final report on the operations of any public agency subject to its reviewing authority, the governing body of the public agency shall comment to the Presiding Judge of the superior court on the findings and recommendations pertaining to matters under the control of the governing body, and every elected county officer or agency head for which the Grand Jury has responsibility pursuant to Section §914.1 shall comment within 60 days to the Presiding Judge of the superior court, with an information copy sent to the board of supervisors, on the findings and recommendations pertaining to matters under the control of that county officer or agency head and any agency or agencies which that officer or agency head supervises or controls. In any city and county, the mayor shall also comment on the findings and recommendations. All of these comments and reports shall forthwith be submitted to the Presiding Judge of the superior court who impaneled the Grand Jury. A copy of all responses to Grand Jury reports shall be placed on file with the clerk of the public agency and the office of the county clerk, or the mayor when applicable, and shall remain on file in those offices. One copy shall be placed on file with the applicable Grand Jury final report by, and in the control of the currently impaneled Grand Jury, where it shall be maintained for a minimum of five years. (d) As used in this section "agency' includes a department.

§933.05 (a) For purposes of subdivision (b) of Section §933, as to each Grand Jury finding, the responding person or entity shall indicate one of the following: (1) The respondent agrees with the finding. (2) The respondent disagrees wholly or partially with the finding, in which case the response shall specify the portion of the finding that is disputed and shall include an explanation of the reasons therefor. (b) For purposes of subdivision (b) of Section §933, as to each grand jury recommendation, the responding person or entity shall report one of the following actions: (1) The recommendation has been implemented, with a summary regarding the implemented action. (2) The recommendation has not yet been implemented, but will be implemented in the future, with a timeframe for implementation. (3) The recommendation requires further analysis, with an explanation and the scope and parameters of an analysis or study, and a timeframe for the matter to be prepared for discussion by the officer or head of the agency or department being investigated or reviewed, including the governing body of the public agency when applicable. This timeframe shall not exceed six months from the date of publication of the grand jury report. (4) The recommendation will not be implemented because it is not warranted or is not reasonable, with an explanation therefor. (c) However, if a finding or recommendation of the grand jury addresses budgetary or personnel matters of a County agency or department headed by an elected officer, both the agency or department head and the Board of Supervisors shall respond

if requested by the grand jury, but the response of the Board of Supervisors shall address only those budgetary or personnel matters over which it has some decision making authority. The response of the elected agency or department head shall address all aspects of the findings or recommendations affecting his or her agency or department. (d) A grand jury may request a subject person or entity to come before the grand jury for the purpose of reading and discussing the findings of the grand jury report that relates to that person or entity in order to verify the accuracy of the findings prior to their release. (e) During an investigation, the grand jury shall meet with the subject of that investigation regarding that investigation, unless the court, either on its own determination or upon request of the foreperson of the grand jury, determines that such a meeting would be detrimental. (f) A grand jury shall provide to the affected agency a copy of the portion of the grand jury report relating to that person or entity two working days prior to its public release and after the approval of the Presiding Judge. No officer, agency, department, or governing body of a public agency shall disclose any contents of the report prior to the public release of the final report.

RESPONSE REQUIRED

June 30, 2018

West Sacramento City Council
1110 West Capitol Ave., Third Floor
West Sacramento, CA 95691

RE: The Looming Crisis of Yolo County City Pension & Retirement Medical Costs

The 2017-2018 Yolo County Grand Jury herewith provides the referenced report for review and comments by the governmental entity, elected official or officer of a public agency. The findings in this document report the conclusions reached by the Grand Jury. Although all the findings are based upon evidence, they are the product of the Grand Jury's independent judgment; some findings are the opinion of the Grand Jury rather than indisputable statements of fact.

In accordance with Penal Code Section §933.05(f), a copy of this report is being provided to affected agencies two working days prior to its public release and after being approved by the Advising Judge of the Superior Court. Please note that Section 933.05(f) specifies that no officer, agency, department or governing body of a public agency shall disclose any contents of the report prior to its public release. This report will be filed with the County Clerk and released to the public on June 30, 2018.

According to Penal Code Section §933 and §933.05, you are required to respond in writing and in electronic form to the findings and recommendations of the Grand Jury report. By law, an officer of a public agency must submit a response to the Yolo County Superior Court within 90 days. The written responses should be mailed to The Honorable David W. Reed, Superior Court of California, Yolo County, 1000 Main Street, Woodland, CA 95695. The Grand Jury address is PO Box 2142, Woodland CA 95776. The electronic version can be e-mailed as an attachment to the Grand Jury (*grand-jury@sbcglobal.net*).

Sincerely,

Judy Wohlfrom
Foreperson

Enclosures:
- Grand Jury Report section
- Response procedure form
- California Penal Code §966 and §933.05

The Looming Crisis of Yolo County City Pension and Retirement Medical Costs

SUMMARY

California cities are experiencing an alarming fiscal burden due to increasing expenses and liabilities related to retiree pensions and health insurance. Yolo County's four cities (Davis, West Sacramento, Winters, and Woodland) are no exception to this retiree cost crisis. However, information about this looming fiscal crisis is not commonly known to many city residents, nor easily discovered. The 2017-2018 Yolo County Grand Jury (Grand Jury) investigated potential impacts on cities with primary focus on:

- Unfunded retiree cost liabilities
- Annual retiree expense management
- Best practices collaboration
- Transparency to city residents

The Grand Jury found that current and future retiree benefits are putting extreme pressure on other city service priorities (road maintenance and improvements, public works, parks and recreation, public safety, etc.) and revenue sources. The retirement benefit costs (pensions and health insurance) are consuming increasing portions of local city budgets.

The California Public Employee Retirement System (CalPERS), which manages all city plans within Yolo County, is in the midst of a planned multi-year escalation in employer contribution rates. This is due to changes CalPERS has made in calculating payments in order to build assets to pay for future pension payments. CalPERS has been gradually ramping up its requirements for "unfunded accrued liability" payments (see Glossary) statewide, which total $8.9 billion more than anticipated out of governmental entity coffers in only three years (FY2017-18 through FY2019-20).

Yolo County's four cities are contributing varying portions of their fair share of these "catch-up" costs (see Glossary) to ensure their retirement programs can cover future liabilities (payments to retirees). Some cities in the County are projecting that their "catch-up" payments will double for all pension funds over the next six years. Retiree medical insurance payments by cities add to this financial challenge. All of these increases are large relative to available budgets and are growing faster than projected current revenue sources.

When looking at total ("normal" and "catch-up") pension costs over the next seven years (Fiscal Year 2017-18 through Fiscal Year 2024-25), CalPERS anticipates staggering increases for Yolo County cities:

▪ Davis	$8.7 million	87% increase
▪ West Sacramento	$6.9 million	90% increase
▪ Winters	$0.4 million	67% increase
▪ Woodland	$6.3 million	78% increase

The Grand Jury recommends that Yolo County city councils become more transparent to taxpayers concerning growth of retirement costs and the negative impact of this growth on city priorities and fiscal health. Cities should consider creating a simple statistical template, such as that created by the Grand Jury for city managers (see Appendix), showing historic and projected budget impact of retirement costs. The Grand Jury also recommends that cities consider more sustainable alternatives to the existing retiree benefit programs managed by CalPERS. Finally,

collaboration should increase among cities, where allowed by law, to share best practices for managing these cost increases.

BACKGROUND

Public pensions and retiree medical insurance have always been important benefits for city and county government employees. However, when considering pension and medical benefit burdens on California cities and counties, two perennial questions are: (1) How much are the costs going to increase? (2) What degree of stress are those increases going to place on other services expected to be provided by city governments? CalPERS, the nation's biggest pension system, and individual cities have completed studies (see Bibliography items 1, 2, 6) that address these questions. Yet this growing fiscal crisis is unknown to, or misunderstood by many taxpayers in Yolo County cities.

The Grand Jury chose to investigate several aspects of the growing budgetary crisis caused by pensions and retiree medical insurance costs in each of Yolo County's cities. The investigation focused on the impact of current levels of payments for these benefits on other city service priorities (such as public safety, parks and recreation, public works, street maintenance and improvements), the unfunded liabilities for future retiree payments, and the lack of transparency about these issues with citizens of each city.

APPROACH

During the investigation, the Grand Jury interviewed the City Managers in Yolo County and obtained information from some of the city finance departments. In addition to the interviews and follow-up conversations with the City Managers, the Grand Jury reviewed numerous documents and sources:

- Yolo County Cities Finance Department-provided pension and Other Post Employment Benefit statistics (OPEB)
- Yolo County Cities Comprehensive Annual Financial Reports
- Yolo County Cities Annual Fiscal Adopted Budgets
- CalPERS Annual Valuation Reports for Police, Fire, and Miscellaneous Employee Programs
- California Public Employees' Pension Reform Act of 2013 (PEPRA)
- California Public Employees' Pension Reform Act of 2018 proposal – Senate Bill 32
- CalPensions State Bulletins (CalPERS, CalSTRS and other government pensions)
- "How Much More Will Cities and Counties Pay CalPERS?" California Policy Center Study
- City of Monrovia pension case study related to balancing increasing CalPERS payments with payments for other city services.
- City of Vallejo, California pension case study related to bankruptcy
- Information and news concerning city and general retiree pension and medical insurance costs from multiple sources cited in the bibliography

- California legal codes and definitions cited in the bibliography and glossary

DISCUSSION

The Grand Jury identified and investigated several areas of concern in Yolo County's four cities related to the growing pension and retiree medical insurance payments and liabilities. Although this report only focuses on the four cities, the same factors and concerns exist in Yolo County itself and other government entities within this county. The alarming increase in the rate of projected expenses and unfunded liabilities is neither easily found nor understood by many Yolo County city residents, which means that city governments can do more to be transparent.

Lack of Transparency

City officials are accountable for being transparent and forthcoming with the public about local government finances.

- Council members have a fiduciary role that includes financial oversight, sound policies and awareness of the fiscal and service impacts of the decisions they make, according to training curriculum provided to them by the League of California Cities (see Bibliography item 13).

- In California, the people's right to know what their government is doing has been enshrined as a fundamental right in the state Constitution. "The people have the right of access to information concerning the conduct of the people's business, and, therefore the meetings of public bodies and the writings of public officials and agencies shall be open to public scrutiny" (CA Constitution article 1, section 3, subd. (b)).

- "Without a duty of accountability [by government officials], the public's ability to monitor the behavior of public fiduciaries would be severely limited. From the duty of accountability flow the duty of transparency and the concepts of disclosure, open meetings, and accessibility of public records" (see Bibliography item 10).

However, communication with the public about the growing burden on city finances of retiree costs has not been very transparent.

- The most common sources of publicly available pension and retiree medical insurance statistical information can be found only in city Comprehensive Annual Financial Reports, city Annual Fiscal Adopted Budgets, and CalPERS Annual Valuation Reports. Once found, the information can be difficult for city residents to understand, especially with respect to its effects on other city service priorities.

- The City of Davis provided a recent example of a missed opportunity to educate its taxpayers in a spring 2018 utility bill insert, "Expenditures – Where does the money go?" This summary did not mention anything about pensions or retiree benefits (see Bibliography item 12).

Pension and Retiree Annual Costs – "Normal" and "Catch-up" Costs

- The *employer* contribution to the pension programs is a combination of "normal cost" (see Glossary), calculated as a percentage of employee payroll, and "catch-up" (see Glossary) dollar payments required to cover "unfunded accrued liabilities" (see Glossary). These liabilities have been increasing at an alarming rate when considered in relation to CalPERS investment returns, which have not been meeting projections,

according to CalPERS Annual Valuation Reports and city financial documents. The "normal" employer cost as a percentage of payroll has been more stable and predictable. The employer pension contribution includes "catch-up" costs that fluctuate based on CalPERS investment returns and the amount of unfunded pension accrued liabilities. This is a subject of growing concern for most cities.

- The *employer* contribution shown in CalPERS required annual payment calculations, seen in its Annual Valuation statements, does not explain that the "normal cost" will decline over time as new employees are hired into pension plans under the Public Employee Pension Reform Act (PEPRA) pension contribution cost sharing criteria.

- The *employee* pension contribution varies among Yolo County cities and is typically determined through collective bargaining with various employee unions that represent police, fire, and other miscellaneous employee groups. This contribution remains constant from year-to-year as evidenced in CalPERS Annual Valuation Reports and PEPRA limitations (see Bibliography item 3). The percentage of payroll contributed by employees in Yolo County cities range from 6.9% to 9.0%.

- According to CalPERS projections and Grand Jury interviews, the four Yolo County cities' payments to CalPERS for pensions rise sharply from current levels in the next seven years, ranging from approximately 67% for Winters to a 90% for West Sacramento. In dollars, these increases are projected to range from $0.4 million for Winters to $8.7 million for Davis. The following Figure 1 and 2 information provides additional four-city facts.

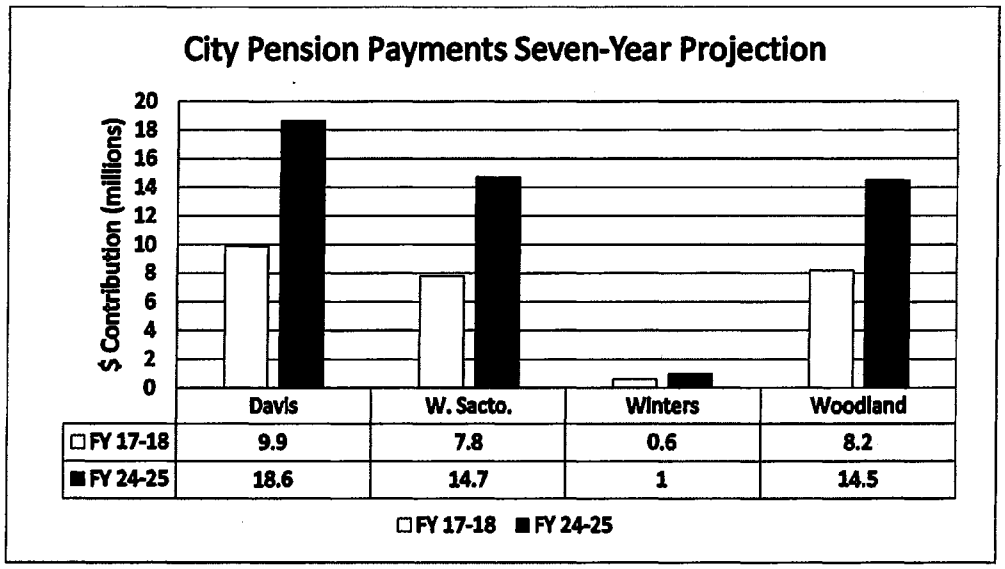


Figure 1 – City Pension Liability Currently and in Seven Years

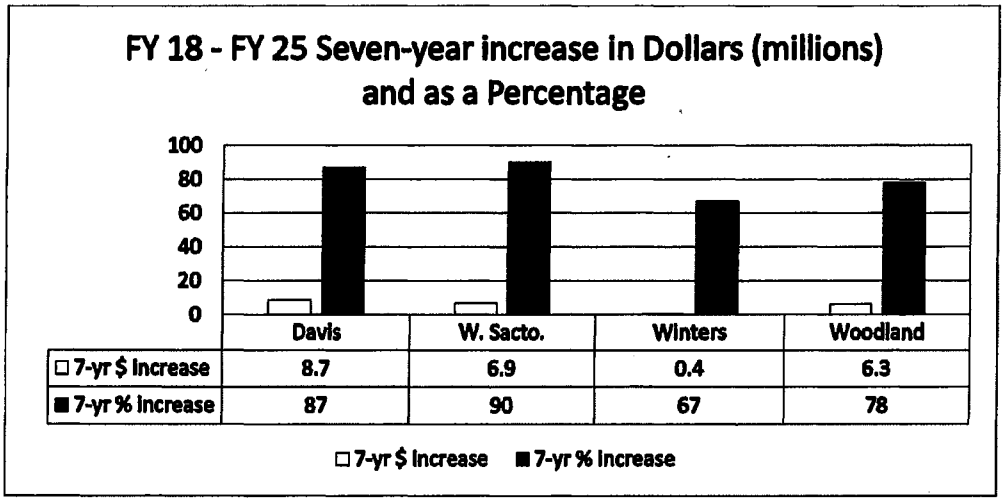


Figure 2 – Pension increase over the next seven years shown in dollars and percent.

- According to a February 13, 2018, Sacramento Bee editorial (see Bibliography item 14), the League of California Cities has determined that statewide pension payments are about 11% of General Fund budgets, on average. These are expected to become about 16% of General Fund budgets in the next seven years. Showing retirement expenses as a percentage of the General Fund is an effective way of educating the public about the importance of retirement costs competing with other city service priorities.

- According to information obtained from city financial reports and forecast statistics, Davis is contributing about 19% of the city's general fund budget to pensions and retiree health benefits, a share that will rise to approximately 26% by 2025. West Sacramento can expect its pension and retiree benefits to increase from 16% of its general fund budget this year to approximately 17% by 2025. Winters will see that share jump from 12% to 16% and in Woodland, it will climb from 14% to 18%.

- City Adopted Budgets and Annual Financial Reports, and accompanying notes, show that City Councils have found it very difficult to absorb the rising retirement payments to CalPERS without compromising other city services. The result, at times, has been unpopular new taxes and fees, and voters are often not told that more of their money is needed for retirement costs (see Bibliography item 11).

Pension Unfunded Liabilities

- According to the Annual Financial Reports dated June 1, 2017, Unfunded Accrued Liabilities for the four Yolo County cities are alarming relative to city balance sheets. Davis, for example, has $110.1 million of unfunded obligations. On the low end, the liability for Winters is $4.4 million. These liability levels have grown significantly in the last two years as shown in Figure 3 below.

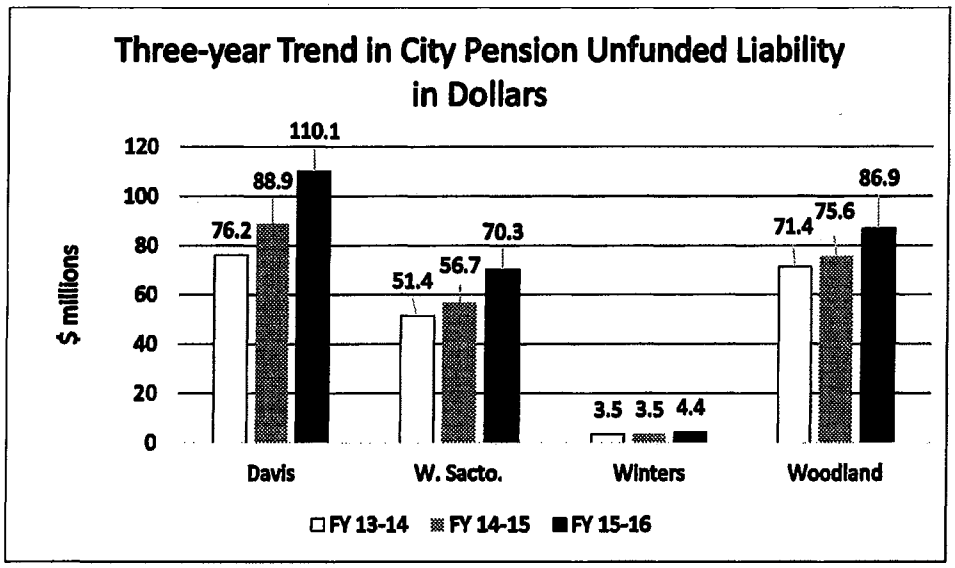


Figure 3 – City Unfunded Liability Dollars over Three Years

- CalPERS is significantly escalating city-required "catch-up" payments due to changes it has made in rate smoothing calculations, amortization of unfunded liability dollars, accelerated retirements of baby boomers, and new mortality estimates for beneficiaries. These "catch-up" payments are scheduled to extend through at least FY 2022-23. These ramped up calculations will double some cities' total Unfunded Accrued Liability payments for all pension funds over the next six years. These "catch-up" payments are based on projections, so if investment returns are better or worse than predicted, the level of payments will vary.

- When a "normal" contribution is insufficient, and the pension plan becomes underfunded, the level of underfunding is compounded every year because there isn't enough money in the fund earning interest and providing investment returns. According to CalPERS actuarial tables, the longer that "catch-up" payments are deferred into the future (amortized over longer time periods), the worse the underfunding becomes, depending on the investment rate of return.

- According to the California Policy Center, "Virtually every pension reform over the past decade or so has exempted the majority of active public employees from helping to pay down the unfunded liability" (see Bibliography item 6) unless cost sharing is successfully negotiated into labor contracts such as done by the city of Woodland. Instead, most increased employee payments apply only to the "normal" employee contribution. Public employee unions, quite understandably, negotiate for the lowest possible employee contributions to pension funds. The "normal cost" has historically been calculated by CalPERS based on financially optimistic projections. Grand Jury interviews, past CalPERS actuarial reports, and city annual financial reports reveal that only minimal catch-up payments were made each year in exchange for bigger catch-up payments in the future.

- At present, the "future" is being dealt with by elected and appointed governmental officials who foresee pension and retiree benefit costs rising dramatically faster than

revenues, according to City Annual Financial Reports and Annual Fiscal Adopted Budgets.

- Most pension plans remain underfunded even after a longer than usual bull stock market. When stocks and real estate have been running up in value for eight years, pension plans should not be underfunded. According to CalPERS Annual Valuation Reports' statistics, CalPERS and the public employee unions that dominate CalPERS have done a disservice to taxpayers, public agencies, and ultimately to the individual participants who are counting on CalPERS to know what they are doing with respect to investment strategies.

- The Public Employees' Pension Reform Act (PEPRA) of 2013 and the current proposed 2018 reform working its way through the state Legislature includes strategies that address special "Golden Handshake" benefits negotiated in the past. These "Golden Handshake" benefits are partially responsible for compromising the sustainability of the CalPERS pension programs used by the four Yolo County cities (see Bibliography items 3, 4 and 5).

- Based on the most recent three years shown in annual financial reports (FY13-14 through FY15-16), the "funded status" (see Glossary) of the four cities' pension plans are showing signs of declining by 7-8% annually. "Funded status" reflects having sufficient current assets to pay future pension payments. The recent declines are a function of past contributions and less-than-projected fund investment returns. For example, the "funded status" of Davis's three pension plans has dropped from an average of 72% to 64%, meaning that the city currently has only enough assets to pay two thirds of its future pension payments. CalPERS statewide liability funding in 2016 stood at 68%. Refer to the following Figure 4 for the four-city facts.

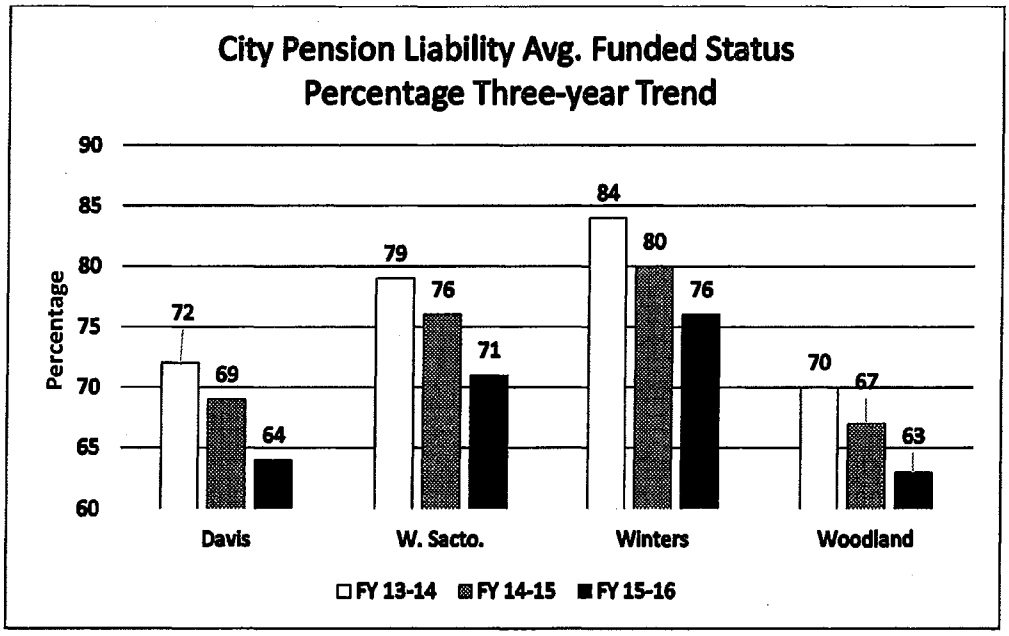


Figure 4 – Percentage of City Pensions future liabilities that have been "funded" or invested.

- To create a more financially sustainable retirement system, the University of California and some California cities have offered a hybrid pension option, such as a 401(k) combined with a much smaller employer-paid pension (see Bibliography item 7). This

type of system is also included in the proposed Public Employees' Pension Reform Act of 2018 (Senate Bill B-32[see Bibliography item 5]).

Retiree Medical Insurance Unfunded Liabilities

- Retiree medical insurance subsidies fall into an expense category that cities call Other Post-Employment Benefits (OPEB [see Glossary]).

- Yolo County cities' future obligations for retiree medical insurance are even more alarming than their pension liabilities. Currently, according to the most recent Annual Financial Reports, the four cities do not have sufficient current assets to pay future medical insurance liabilities. West Sacramento has the highest funded status at 48%, meaning the city has enough assets to pay half its future liability. Winters has the lowest, at 0%. Figure 5 below provides four-city information.

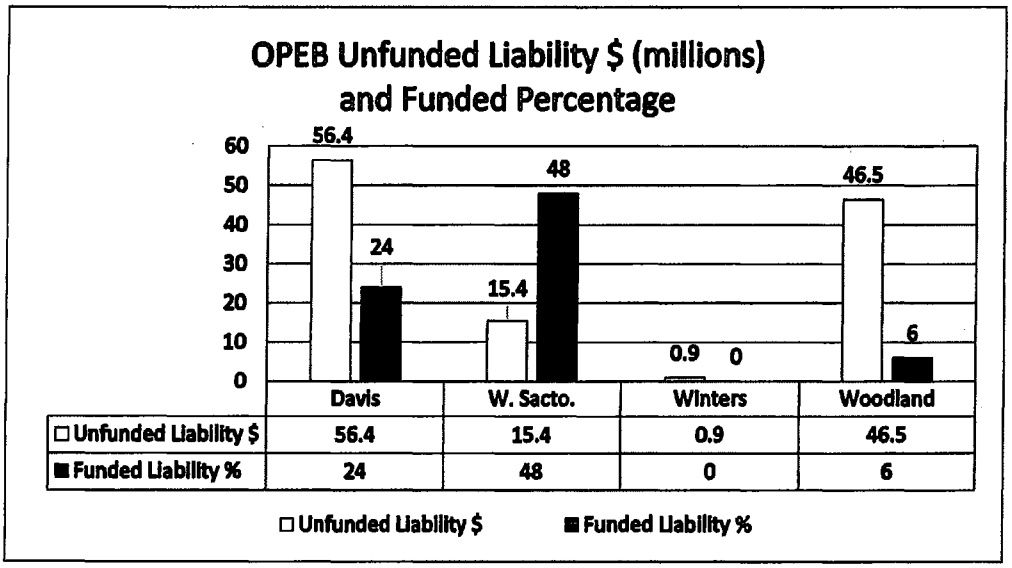


Figure 5 – Other Post-Employment Benefits Unfunded Dollar Liability and "Funded" %.

- With respect to retiree medical insurance, most California cities were "pay as you go" until recently. In other words, cities budgeted and paid for each year's required costs with little planning for the future. According to the FY2017-18 Adopted Budget, beginning in FY2013-14, the City of Woodland began funding OPEB contributions over and above historical pay-as-you-go levels. Each of Yolo County's cities is approaching this issue with different strategies, according to their respective budget documents.

City Council Impact

- City Councils have a fiduciary and fiscal responsibility regarding pension and retirement systems that is guided by the California Government Code. Under CA Government Code section 45342, "Any pension or retirement system adopted shall be on a sound actuarial basis and provide for contributions by both the city and the employee members of the system which shall be based on percentages of payroll to be changed only by adjustments on account of experience under the system." Additionally, "Contributions shall be in the amounts which will accumulate at retirement a fund sufficient to carry out the promise to pay benefits to the individual on account of his service as a member of the system, without further contributions from any source" (CA Government Code section 45343).

- Historically, elected city councils have been pressured to agree to pension benefit enhancements based on overly optimistic, often inaccurate investment earnings projections. As a result, too many decision makers failed to realize that pension contributions would eventually become a significant burden on cities, counties and other governmental entities, and by extension, taxpayers. The "normal" contributions (see Bibliography item 6) by employee and employer have been considered sufficient for pension plans to remain fully funded and fiscally solvent.

- In future years, most of the current local officials in each city will be gone due to normal voluntary or election turnover. Meanwhile, finding adequate monies to keep city pension and medical insurance plans solvent remains a critical and ongoing requirement and challenge for elected officials and their supporting city staff. The most common method of finding new revenue sources for retirement costs is through proposed new city taxes and fees, such as sales tax increases or parcel taxes. However, rarely are these tax or fee initiatives labeled as strategies to pay for employee retirement costs.

- According to Grand Jury interviews and city financial documents, developing and utilizing financial forecasting tools, such as those mentioned by the city of Davis in its Adopted Budget 2017-18, significantly improves continuity of knowledge across successive city councils. This financial forecasting tool shows the evolution of the city general fund's share of total pension costs. Woodland also has a pension and OPEB forecasting analysis to educate its elected officials and staffs.

State vs. Local Decision Making

Making changes to city pension plans and Other Post-Employment Benefits is severely complicated by ever-changing state regulations. Regulations governing retirement benefit plans are spelled out in the California Public Employees' Pension Reform Act of 2013 and the proposed California Public Employees' Pension Reform Act 2018 (see Bibliography items 4 and 5). CalPERS also has its own actuarial valuation (see Glossary) and investment return criteria. In addition, local public employee labor contracts influence some benefit levels, employee contribution rates, and retirement ages.

FINDINGS

F1. For many Yolo County residents, poor transparency and difficulties in accessing information make it hard to understand the consequences of mushrooming retirement benefit expenses and liabilities. This jeopardizes the citizens' ability to hold elected officials responsible for providing adequate funding to all high-priority services.

F2. Several studies reveal that the retirement benefit system has been compromised by "golden handshakes" (e.g. special pension benefit deals or enhancements) and failure to consider the cost of lifetime benefits and likely investment earning levels. This happens every time a public agency negotiates a contract with its employees. Future fiscal solutions will depend, in part, on the public's willingness to hold state and local politicians accountable for their fiduciary responsibility as required by law and ethics (see Bibliography item 10).

F3. Many city councils seem to have found it politically unpalatable or fiscally difficult to find adequate funding resources to make high enough payments to reduce unfunded pension and other post-employment benefit liabilities beyond the required payments. Additionally,

when revenue generation is increased (e.g., from bonds, parcel taxes, sales, or utility taxes or fees), the money collected that may be restricted for a specific purpose, makes available other unrestricted general funds to fund retirement cost increases. This is not always clearly communicated to the public.

F4. Beyond CalPERS requirements, the four cities approach the transparency, analysis, management and containment of growing retiree costs in different ways. There is an opportunity for increased collaboration among the cities. For example, Davis has developed a financial forecasting tool that projects revenues and expenses many years into the future. Some cities show retirement costs' share of the "General" or "All" Funds.

RECOMMENDATIONS

R1. By February 1, 2019, City Councils and staff should conduct public education campaigns to increase transparency and awareness of the alarming burdensome impact on city service priorities that is being created by retirement pension and medical insurance costs. Examples of public education could be in the form of education forums, explanatory inserts in utility statements, multi-media articles and/or candid conversation at governmental meetings.

R2. By February 1, 2019, City Councils and staff should create a simple statistical template and/or graph that shows three-year past (actual) and projected (look back, look forward) pension costs and liabilities and their impact (% of total) on the city budget General and All Fund base. This is necessary to assure transparency to the public (for an example developed by the Grand Jury, see the Appendix).

R3. By July 1, 2019, Yolo County City Councils should investigate and consider alternatives to the existing CalPERS managed pension systems in order to achieve a more sustainable and less burdensome financial impact on city budgets. An alternative hybrid-defined pension option is included in the proposed Public Employees' Pension Reform Action of 2018 (Senate Bill B-32). Any alternative plans considered by city governments should be transparent to the public.

R4. By September 1, 2018, collaboration among cities in Yolo County should be increased so that best practices in analysis and cost containment of pensions and other retiree benefits can be shared. The best practices and innovative ideas should be transparent to the public.

REQUIRED RESPONSES

Pursuant to Penal Code section 933.05, the grand jury requests responses as follows:

From the following governing bodies:

- City Councils in Davis, West Sacramento, Winters, and Woodland – F1, F2, F3, F4; R1, R2, R3, R4.

INVITED RESPONSES

- City Managers in Davis, West Sacramento, Winters and Woodland – F1, F2, F3, F4; R1, R2, R3, R4.

The governing body indicated above should be aware that the comment or response of the governing body must be conducted subject to notice, agenda, and open meeting requirements of the Brown Act.

BIBLIOGRAPHY

1. "Public Agency Actuarial Valuation Reports https://www.calpers.ca.gov/page/employers/actuarial-services/employer-contributions/public-agency-actuarial-valuation-reports
2. California Policy Center How Fraudulently Low "Normal Contributions" Wreak Havoc on Civic Finances https://californiapolicycenter.org/fraudulently-low-normal-contributions-wreak-havoc-civil-finances/
3. Public Employees' Pension Reform Act (PEPRA) https://www.calpers.ca.gov/page/employers/policies-and-procedures/pension-reform-impacts
4. CA Public Employees' Pension Reform Act of 2013 (PEPRA) [California Legislative Information] and http://www.seiu1021.org/2013/01/28/ten-things-you-need-to-know-about-pepra/
5. Senate Bill 32 – CA Public Employees' Pension Reform Act of 2018 [California Legislative Information] and https://www.calpers.ca.gov/page/employers/policies-and-procedures/pension-reform-impacts
6. California Policy Center https://californiapolicycenter.org/much-will-cities-counties-pay-calpers/
7. CalPensions Article 401(k) option https://calpensions.com/2018/04/09/calpers-may-join-union-foes-of-401k-option/
8. https://calpensions.com/2011/08/08vallejo-exists-bankruptcy-paying-more-for-pensions/
9. http://www.cityofmonrovia.org/Home/ShowDocument?id=11586
10. "The values behind government ethics laws" https://www.scu.edu/ethics/focus-areas/government-ethics/resources/public-officials-as-fiduciaries/
11. Dan Walters "Commentary: Local tax hikes cleverly packaged" https://calmatters.org/articles/commentary/commentary-local-tax-hikes-cleverly-packaged/
12. "2017-18 City of Davis Budget: Expenditures – Where does the money go?" http://cityofdavis.org/city-hall/finance/city-budget
13. Michael Coleman, Financial Responsibilities and City Revenues for Mayors and Council Members https://www.cacities.org/Resources-Documents/Member-Engagement/Regional-Divisions/Los-Angeles/LAnewCouncilGovnWkshp2017p.aspx
14. Sacramento Bee editorial, February 13, 2018, page 33: https://www.cacities.org/Resources-Documents/Policy-Advocacy-Section/Hot-Issues/Retirement-System-Sustainability/League-Pension-Survey-(web)-FINAL.aspx

GLOSSARY

- **Accrued Liability** – The total dollars needed as of the valuation date to fund all benefits earned in the past for current members.
- **Actuarial Valuation** – The determination, as of a valuation date of the Normal Cost, Accrued liability, and related actuarial present values of a pension or other benefit plan. These valuations are performed annually or when an employer is contemplating a change to its plan provisions.
- **Catch-up Costs** – This is explained under Unfunded Accrued Liability (UAL).
- **Funded Status** – A measure of how well funded, or how "on track" a plan or risk pool is with respect to assets versus accrued liabilities. A percentage greater than 100 means the plan or risk pool has more assets than liabilities and a percentage less than 100 means liabilities are greater than assets.
- **Normal Costs** – The annual payment (cost) for the upcoming fiscal year to pay for future retirement benefits for current employees. The normal cost should be viewed as the long-term contribution rate and is the amount that it will cost to pay for future benefits.
- **Other Post-Employment Benefits (OPEB)** – Retiree benefits other than pensions, normally consisting on an employer's contribution to medical insurance during retirement.
- **Public Employee Pension Reform Act (PEPRA)** – California legislative reforms passed and implemented in 2003. There is a pending additional 2018 reform act currently moving through the legislature.
- **Unfunded Accrued Liability (UAL)** – When a plan or pool's value of assets is less than its Accrued Liability, the difference is the plan's or pool's Unfunded Accrued Liability (or unfunded liability). If the unfunded liability is positive, the plan or pool will have to make contributions exceeding the Normal Cost. This is commonly referred to as "catch-up" costs.

DISCLAIMER

Reports issued by the Grand Jury do not identify individuals interviewed. Penal Code section 929 requires that reports of the Grand Jury not contain the name of any person or facts leading to the identity of any person who provides information to the Grand Jury.

APPENDIX

City Pension and OPEB Cost Trend Information Template

(created by the Grand Jury as an example for all cities to use as a simple tool to provide transparency for citizens to understand retirement benefits' impact on city budgets)

2017-18 Grand Jury Budget Committee
City Pension and OPEB Analysis Information
5-year look back and 5-year look forward

		FY12-13	FY13-14	FY14-15	FY15-16	FY16-17	FY17-18	FY18-19	FY19-20	FY20-21	FY21-22
General Fund											
PERS Misc. Employee*	$										
PERS Safety*	$										
Total	$										
% of General Fund	%										
Other Post-Employ. Ben.**	$										
% of General Fund	%										
Total Pension and OPEB	$										
% of General Fund	%										
All Funds											
PERS Misc. Employee*	$										
PERS Safety*	$										
Total	$										
% of All Funds	%										
Other Post-Employ. Ben.**	$										
% of All Funds	%										
Total Pension and OPEB	$										
% of All Funds	%										
Funded Liability Based on CALPERS and OPEB Actuarial Calculations											
PERS	$										
	%										
OPEB***	$										
	%										

*Actual Payments for Employer Costs from CalPERS Annual Valuation Report (Includes UAL + Actual Normal Costs)
Employers Costs exclude:
1. Statutory Employee Costs if not paid by City
2. Negotiated Employees Share of Employers Costs

**Annual OPEB Costs based on Retiree Healthcare Plan Actuarial Obligations
***Based on Government Accounting Standard Board ruling 45 Actuarial Valuations and Projections
Other post-employment benefits (OPEB) are the benefits that an employee will begin to receive at the start of retirement (not including pension benefits paid to the retired employee).

RESPONSE PROCEDURE

RESPONSE TO GRAND JURY REPORT

The governance of responses to the Grand Jury Final Report is contained in Penal Code §933 and §933.05. Responses must be submitted within 60 or 90 days. Elected officials must respond within sixty (60) days, governing bodies (for example, the Board of Supervisors) must respond within ninety (90) days. Please submit all responses in writing and digital format to the Advising Judge and the Grand Jury Foreperson.

Report Title: The Looming Crisis of Yolo County City Pension & Retirement Medical Costs
Report Date: June 12, 2018

Response by: _____ Title: _____

FINDINGS

☐ I (we) agree with the findings numbered:

_____

☐ I (we) disagree wholly or partially with the findings numbered:

_____

RECOMMENDATIONS

☐ Recommendations numbered: _____
have been implemented (attach a summary describing the implemented actions).

☐ Recommendations numbered: _____
require further analysis (attach an explanation of the analysis or study, and the time frame for the matter to be prepared by the officer or director of the agency or department being investigated or reviewed; including the governing body where applicable. The time frame shall not exceed six (6) months from the date of the Grand Jury Report).

☐ Recommendations numbered: _____
will not be implemented because they are not warranted and/or are not reasonable (attach an explanation).

Date: _____ Signed: _____

Total number of pages attached _____

RESPONSE PROCEDURE

RESPONSE TO GRAND JURY REPORT

The governance of responses to the Grand Jury Final Report is contained in Penal Code §933 and §933.05. Responses must be submitted within 60 or 90 days. Elected officials must respond within sixty (60) days, governing bodies (for example, the Board of Supervisors) must respond within ninety (90) days. Please submit all responses in writing and digital format to the Advising Judge and the Grand Jury Foreperson.

Report Title: The Looming Crisis of Yolo County City Pension & Retirement Medical Costs
Report Date: June 12, 2018

Response by: Christopher Cabaldon Title: Mayor

FINDINGS

☑ I (we) agree with the findings numbered:
F4

☑ I (we) disagree wholly or partially with the findings numbered:
F1, F2, F3

RECOMMENDATIONS

☑ Recommendations numbered: R1, R2, R3, R4
have been implemented (attach a summary describing the implemented actions).

☐ Recommendations numbered: _____
require further analysis (attach an explanation of the analysis or study, and the time frame for the matter to be prepared by the officer or director of the agency or department being investigated or reviewed; including the governing body where applicable. The time frame shall not exceed six (6) months from the date of the Grand Jury Report).

☐ Recommendations numbered: _____
will not be implemented because they are not warranted and/or are not reasonable (attach an explanation).

Date: 8/23/2018 Signed: _____

Total number of pages attached 4

CITY OF WEST SACRAMENTO

August 23, 2018

The Honorable David W. Reed
Superior Court of California, Yolo County
1000 Main Street
Woodland, CA 95695

Subject: **City of West Sacramento Response to Yolo County Grand Jury 2017-2018 Final Report, The Looming Crisis of Yolo County City Pension & Retirement Medical Costs**

Dear Judge Reed,

The City of West Sacramento has received the Grand Jury report titled "The Looming Crisis of Yolo County City Pension and Retirement Medical Costs" transmitted to the City Council by the letter dated June 12, 2018. We thank the Grand Jury for its efforts and appreciate the opportunity to provide a response.

The report makes four findings (F1, F2, F3, and F4) and four recommendations (R1, R2, R3, and R4). Pursuant to California Penal Code Section 933.05, the Grand Jury requested the City Council response to Findings F1, F2, F3 and F4, and Recommendations R1, R2, R3, and R4. As requested, the City Council has completed the response form provided by the Grand Jury (see attached) and provides additional support for such responses below.

GRAND JURY FINDINGS

F1: *For many Yolo County residents, poor transparency and difficulties accessing information made it hard to understand the consequences of mushrooming retirement benefit expenses and liabilities. This jeopardizes citizens' ability to hold elected officials responsible for providing adequate funding to all high-priority services.*

We partially agree with this finding. Pension information is shared through CalPERS actuarial reports which are published on CalPERS' website and include projections for 5 years into the future, as well as historical data. Pension and OPEB information, including annual contributions, unfunded liability, market value of assets, etc. is available in several places, including the City's Comprehensive Annual Financial Report (CAFR). Additionally, in the last two years, we have shared information with the public regarding the growing pension and OPEB costs and unfunded liabilities in the following ways, among others:

- In May 2017, a workshop was held to discuss the budget and the possibility of the City adopting a Section 115 Trust for pension cost stabilization to mitigate the impact of the rising pension costs on future budgets.

- In June 2017, the City Council adopted a Resolution adopting the Section 115 Trust for Pension and OPEB funding.
- Revenue and expenditure forecasts were provided and discussed with Council at the May 2018 budget workshop and adoption, including significant focus on pension and OPEB costs and the impact we expect rising costs and liabilities to have on future budgets.
- The City of West Sacramento utilizes Open Gov, a data transparency tool which provides detailed budget information in various formats for citizens, accessible directly from the City's website. This tool drills down to the object level, showing annual pension and OPEB payments for each fund, department, and the City as a whole.

F2: *Several studies reveal that the retirement benefit system has been compromised by "golden handshakes" (e.g. special pension benefit deals or enhancements) and failure to consider the cost of lifetime benefits and likely investment earning levels. This happens every time a public agency negotiates a contract with its employees. Future fiscal solutions will depend, in part, on the public's willingness to hold state and local politicians accountable for their fiduciary responsibility as required by law and ethics.*

We partially agree with this finding. While such studies may have revealed that the retirement system has been compromised by "golden handshakes", these golden handshakes and "failure to consider the cost of lifetime benefits and likely investment earnings" do not happen every time a public agency negotiates a contract with its employees. The City of West Sacramento not only considers the impacts of labor negotiations on the annual City budget, but also considers any impact to unfunded liability or future liability and costs related to pension, OPEB and other obligations necessitated by the MOU.

F3: *Many city councils seem to have found it politically unpalatable or fiscally difficult to find adequate funding resources to make high enough payments to reduce unfunded pension and other post-employment benefit liabilities beyond the required payments. Additionally, when revenue generation is increased (e.g. from bonds, parcel taxes, sales, or utility taxes or fees), the money collected that may be restricted for a specific purpose, makes available other unrestricted general funds to fund retirement cost increases. This is not always clearly communicated to the public.*

We partially agree with this finding. While it is true that it can be fiscally difficult to find adequate funding resources to make additional payments to CalPERS and/or the OPEB Trust to further fund the unfunded liability, particularly during times of economic downturn or revenue decline, it is not necessarily true that when other revenues are increased through the issuance of debt and/or taxes or fees, that the increase in restricted revenues creates capacity in the General Fund to fund more of the unfunded liabilities. Rather, when restricted funds are increased, those funds are legally restricted for a particular purpose. It is a rare occurrence that an increase in revenues for a specific purpose creates capacity in the General Fund, unless the General Fund was subsidizing the costs in those restricted funds to begin with.

F4: *Beyond CalPERS requirements, the four cities approach transparency, analysis, management and containment of growing retiree costs in different ways. There is an opportunity for increased collaboration among the cities. For example, Davis has developed a financial forecasting tool that projects revenues and expenses many years into the future. Some cities show retirement costs' share of the "General" or "All" Funds.*

We agree with this finding. Each City is different and is impacted by pension and OPEB liabilities in different ways. The cities already collaborate through monthly City Manager meetings and through regular correspondence between the agency's finance and management staff. In addition, the City collaborates with other agencies, both inside and outside of Yolo County, on best practices and strategies for funding pensions, OPEB and other large needs through their relationship with regional JPAs and professional associations regionally, as well as at the state and national levels.

GRAND JURY RECOMMENDATIONS

R1: *By February 1, 2019, City Councils and staff should conduct public education campaigns to increase transparency and awareness of the alarming burdensome impact on city service priorities that is being created by retirement pension and medical insurance costs. Examples of public education could be in the form of education forums, explanatory inserts in utility statements, multi-media articles and/or candid conversation at government meetings.*

The City of West Sacramento already has plans to increase education and transparency around budget projections in general, and more specifically, the impacts of rising pension and OPEB costs to our General Fund and All Funds budgets. The City Finance Division has implemented a pension forecasting tool and is in process of implementing the OPEB module of that forecasting tool in addition to developing a long-term financial forecast for the general fund to inform the upcoming budget process. The results of those forecasts will be shared with City Council at a public meeting, and available to the public through the City's website.

R2: *By February 1, 2019, City Councils and staff should create a simple statistical template and/or graph that shows three-year past (actual) and projected (look back, look forward) pension costs and liabilities and their impact (% of total) on the city budget General and All Fund base. This is necessary to assure transparency to the public.*

The City's Long-Term Financial Forecast looks at historical trends for a 5-10 year period and forecasts several years into the future, focusing on major revenue sources, major expense categories and wages, benefits, pension and OPEB costs in-depth. A summary of the forecast will be included in the City's adopted FYs 2019/20 and 2020/21 budget document which will be accessible on the City's website once adopted.

R3: *By July 1, 2019, Yolo County City Councils should investigate and consider alternatives to the existing CalPERS managed pension systems in order to achieve a more sustainable and less burdensome financial impact on city budgets. An alternative hybrid-defined pension option is included in the proposed Public Employees' Pension Reform Action of 2018 (Senate Bill B-32). Any alternative plans considered by city governments should be transparent to the public.*

The City has already begun to mitigate the rising annual costs and unfunded liabilities for pension and OPEB in the following ways:

- The City has successfully negotiated changes in retiree medical benefits for new employees in some bargaining groups, including transitioning from a defined benefit retiree medical plan to a defined contribution retiree medical plan funded by contributions during active employment rather than set benefits at retirement. This will serve to reduce the future costs and liability related to Other Post-Employment Benefit payments through natural attrition.
- City of West Sacramento employees contribute a set dollar amount toward the OPEB liability each month (cost-share).
- The City opened a CalPERS Employers' Retirement Benefits Trust (CERBT) to pre-fund OPEB costs and has been contributing the annual required contribution (ARC) since FY 2007/08. As of June 30, 2018, the assets in trust were $17.7 million and the liability was funded at over 56%. The City is on track to fully fund the OPEB liability before 2040.
- Some public safety employees (non-PEPRA) have been contributing additional amounts toward pension since 2006. Currently, some public safety employees contribute 13.325% (4.325% more than the required employee contribution of 9%) toward the unfunded pension liability.

R4: *By September 1, 2018, collaboration among cities in Yolo County should be increased so that best practices in analysis and cost containment of pensions and other retiree benefits can be shared. The best practices and innovative ideas should be transparent to the public.*

This recommendation has already been implemented. The City Managers from each agency meet on a monthly basis and share policies, status, suggestions and ideas for various issues, including the pension and OPEB liabilities (the agenda topic of a recent meeting), options and funding mechanisms. These regularly scheduled meetings as well as the ad-hoc coordination between Finance staff of the municipalities within the County provide beneficial information for addressing the intent of the recommendation by the Grand Jury with respect to collaboration between the Yolo County agencies.

Rising pension costs are a real and complicated issue plaguing local government agencies throughout the state, and it will take time and some creative strategies to address the ever-increasing liability. The City of West Sacramento agrees that transparency and outreach are crucial, and we look forward to continuing and intensifying our engagement efforts with the broader community and collaborating with other leaders throughout Yolo County.

Sincerely,

Christopher Cabaldon
Mayor

Enclosure

Cc: Yolo County Grand Jury Foreperson (via email only)

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 16

SUBJECT:

CONSIDERATION OF APPROVAL OF A LETTER OF PUBLIC CONVENIENCE AND NECESSITY FOR THE GROCERY OUTLET INC.

INITIATED OR REQUESTED BY:

[ ] Council [ ] Staff

[X] Other

REPORT COORDINATED OR PREPARED BY:

Kryss Rankin, City Clerk

Amanda Berlin, Assistant City Manager

ATTACHMENT [X] Yes [ ] No [ ] Information [ ] Direction [X] Action

OBJECTIVE

The objective of this report is to provide the Council with information to provide a determination on the issuance of a new Alcoholic Beverage Control (ABC) Type 21 off-sale liquor license for the Grocery Outlet located at 845 Harbor Boulevard.

RECOMMENDED ACTION

Staff respectfully recommends that Council approve the letter of public convenience and necessity (PCN) for the issuance of a new Alcoholic Beverage Control Type 21 off-sale liquor license for the Grocery Outlet located at 845 Harbor Boulevard.

BACKGROUND

Business & Professions Code Section 23958.4 provides for a definition of "undue concentration," thereby limiting the number of liquor licenses allowed within a census tract based on population. Under this law, certain areas of West Sacramento are deemed "over concentrated" and the State office of Alcoholic Beverage Control (ABC) will not be issued a new license unless the City determines that the issuance of the license would serve a public convenience or necessity.

ANALYSIS

Grocery Outlet is located in Census Tract 102.03, which allows for four (4) Off-Sale ABC licenses but currently has 15 existing ABC licenses. ABC, therefore, requires that a letter of PCN be granted by the City Council in the event a licensee within the Census Tract wishes to replace an existing license with a new license. Grocery Outlet currently possesses a Type 20 (off-sale beer and wine only) license, which would be surrendered if the new Type 21 (off-sale general alcohol) license is granted.

The Police Department has reviewed the application and recommends that Grocery Outlet management participate in the Police Department's TEAM (Together Everyone Achieves More) program that serves to educate businesses that possess and utilize ABC licenses for the sale of alcohol. If approved, this recommendation is included in the PCN letter (attached).

Although the area is over-concentrated, staff recommends approving the PCN based on the following determinations:

- The proposed licensing does not appear to be hazardous to the health, safety or welfare of the community;
- The use would enhance the economic viability of the area;
- The proposed license is compatible with existing uses in the area;
- The background of the proposed licensee and the history or extent of problems on any premises is acceptable; and

- An analysis of the crime statistics in Census Tract 102.03 does show an increased level of alcohol related calls for service, however, no considerable issues are associated with this establishment.

Based on common practice within the city and as a precaution against potential future issues, the Police Department does propose the following conditions be placed on the ABC license:

- There shall be no single container/unit beer sales including 32 and 40-ounce containers;
- Beer and/or malt beverages shall be sold in original factory packages of six-pack or greater, except malt-based coolers;
- Wine coolers, whether made with wine or malt products, shall not be sold in quantities of less than a factory four-pack;
- The licensee shall insure the exterior of the retail facility is free from litter and trash;
- The exterior shall be properly lighted to allow a clear view of the entire building during the hours of darkness;
- The licensee shall insure there is no consumption of alcoholic beverages on the premises;
- The licensee shall post the property against loitering on the licensed premises;
- The licensee shall participate in the Police Department's "Together Everyone Achieves More" program.

Upon approval of this PCN, a letter requesting these restrictions will be sent to ABC along with the PCN letter.

Environmental Considerations
Not applicable.

Commission Recommendation
Not applicable.

Strategic Plan Integration
Not applicable.

Alternatives
The City Council may choose not to approve the PCN letter. The effect of this action would leave the Grocery Outlet with an existing beer and wine only license and prevent the issuance of the general alcohol license.

Coordination and Review
This report was coordinated with the Community Development and Police Departments.

Budget/Cost Impact
Not applicable.

ATTACHMENT(S)

1. Application for Public Convenience/Necessity
2. Public Convenience/Necessity Letter

CITY OF WEST SACRAMENTO

Public Convenience/Necessity Application Form

Application to the West Sacramento City Council for a Letter of Public Convenience/Necessity (PCN) relating to the sale of alcohol within the city limits of the City of West Sacramento. Please refer to the PCN instructions for completion and filing.

Name of Business: Grocery Outlet
Type of Business: Grocery store
Address 845 Harbor Blvd
City, State, Zip West Sacramento CA 95691
Telephone: 916-372-6575
Assessor's Parcel No.: 067-120-063-000
Hours of Operation: 8am-9pm 7 days/week
Census Tract No.: 102.03

Name of Applicant should match the applicant named on the ABC License

APPLICANT

Name: Grocery Outlet Inc
Mailing Address: 5650 Hollis St
Emeryville CA 94608
Contact Phone: 510-704-2859
Contact Email: pburke@cfgo.com

Complete this section if Application is being submitted by someone other than the Applicant:

AGENT

Name: Katy Schardt
Mailing Address: Compass Commercial Group
3005 Douglas Blvd, Suite 200, Roseville CA 95661
Contact Phone: 916-660-9623
Contact Email: katy@katyschardt.com

Relationship to Applicant: Agent ☑ Other ☐ Please Specify: _____

Please provide a written statement demonstrating, by substantial evidence, that the public convenience or necessity would be served by the issuance of a license from ABC. You may attach pages as needed.

1. Is the Business operating now?
   Yes ☑ No ☐

   If yes, how long? Nov 2002

   If no, what is the anticipated date of opening?
   N/A

2. Is opening of the business contingent upon issuance of a liquor license? N/A Yes ☐ No ☐

3. Has an application been submitted to ABC? Yes ☑ No ☐

4. ABC License #: 587067 ABC Representative: Janette Flores

5. Type of ABC license being requested: 21

   Date of posting: 4/25/18

6. Reason for PCN: Over Concentration ☑ * High Crime ☐ * Current license (type 20) will be surrendered upon issuance of the type 21.

7. Is a Conditional Use Permit required? Yes ☐ No ☑ Don't Know ☐

8. Has a Conditional Use Permit been applied for? N/A Yes ☐ No ☐

   If yes, City Permit # (example: P##-####): N/A

9. Has a Conditional Use Permit been approved by the City? N/A Yes ☐ No ☐
   If yes, date approved*: No CUP needed for off-sale alcohol license at a bona fide grocery store.

* Attach copy of final Planning Commission Action Form and conditions of approval

This application will not be accepted until all of the information and accompanying documents are complete and submitted along with appropriate fees.

Signature of Applicant: [signature] Date: 6-12-2018

Signature of Agent (if applicable): [signature] Date: 6/14/18

Return completed application and required documents to:

City of West Sacramento
City Manager's Office
1110 West Capitol Avenue, 3rd Floor
West Sacramento, CA 95691

Office Use Only
15 PKH
JUN 18 2018
City of West Sacramento

GROCERYOUTLET

Letter of Justification

Grocery Outlet, 845 Harbor Blvd, West Sacramento, CA

Request for a Finding of Public Convenience or Necessity at an existing Grocery Outlet store

Background

Grocery Outlet has been selling beer and wine at its grocery store on Harbor Blvd since it opened in 2002. In the recent ABC priority lottery, Grocery Outlet was successful in obtaining a type 21 general license that allows the sale of beer, wine and spirits. ABC requires a finding of Public Convenience or Necessity from the local jurisdiction in order to issue the license.

Grocery Outlet offers fantastic bargains and deals on a variety of products including grocery, frozen foods, deli items, vitamins, health and beauty items, housewares, gift items and much, much more. Brand names for less is the name of the game and smart shoppers get incredible savings over regular store prices every day when they shop Grocery Outlet. Grocery Outlet gets products by searching the nation's major manufacturers for amazing one-time buys resulting from excess production, packaging/design changes, and special promotions. Grocery Outlet's assortment of product changes frequently because of the nature of the one-time buys.

The beer and wine sales at the West Sacramento Grocery Outlet are approximately 5% of total sales. Customers desire the convenience of being able to purchase beer and wine together with the typical grocery items. This convenience is imperative to Grocery Outlet's ability to serve and attract customers. Being able to purchase spirits, in addition to beer and wine, will make this store even more convenient and attractive to its customers. Moreover, Grocery Outlet believes that offering a full assortment of products typically found in competing grocery stores is beneficial to the neighborhood and city.

Grocery Outlet owns over 100 stores in California with off-sale alcohol licenses. Operators and their employees at all of the locations are careful to make sure the store is always clean, neat and free from debris. Grocery Outlet has had no significant issues raised by local law enforcement agencies with respect to alcoholic products or public disturbances. Grocery Outlet has a history of service to the California community and will continue to operate in a conscientious and thoughtful way with sensitivity to nearby residents and businesses. Grocery Outlet is fully aware of the unique responsibilities of operating a store associated with alcoholic beverage sales.

The store opens at 8:00AM and closes at 9:00PM seven days a week. Grocery Outlet is staffed with approximately 35 total employees and 12 employees on-site at any given time during store operations. The register clerks are trained in proper alcoholic beverage sales rules, including use of Point-of-Sale required identification age verification. All of the clerks have read and signed the Alcoholic Beverage Control's Clerk's Affidavit. These affidavits are kept on file by the store operator. The store has a 16 camera security system which has a view of all parts of the store, especially the alcoholic beverage display area and the check out counters. The video system has a 30-day retrieval function which can track previous days' activity.

Grocery Outlet has applied for the type 21 general license with the California Department of Alcoholic Beverage Control. The poster notifying the public and customers of the new license type was posted at the store on 4/25/18. No comments were sent to the Department of Alcoholic Beverage Control during the 30-day posting period. The issuance of the PCN finding is justified based on the following:

Justification Statements

1. The use is essential or desirable to the public convenience and public welfare. The proposed availability of alcohol with the sale of staple products such as fruits, vegetables, milk, juice, eggs, bread, cereal, coffee, soup and pasta as well as consumer products such as toothpaste, detergent and paper towels allows customers to have readily available products in one location where they would otherwise be obliged to travel to other locations to further purchase their items; thus the issuance of an off-sale type 21 license serves the public convenience.

2. Granting the permit will not be materially detrimental to the public welfare and to other property in vicinity. The granting of the permit would not be detrimental to the public health, safety or welfare because the store is designed as a safe family-oriented grocery store. The cashiers are located near the entrance of the store; and security cameras are located throughout the store. Furthermore, the store is primarily for the sales of staple groceries and consumer products. The sale of alcoholic beverages for off-site consumption is incidental and in conjunction with the sale of such items. Additionally, the convenience of a wide variety of groceries and consumer products in one location promotes the general welfare.

3. The use conforms to good zoning practices and development standards. The store is located in a commercial zone on a major thoroughfare and is consistent with good community development practices and standards.

Grocery Outlet appreciates the city's consideration of this ABC license upgrade at its West Sacramento store.

CITY OF WEST SACRAMENTO

August 23, 2018

Maryanne Gilchrist
ABC Licensing Supervisor
Alcoholic Beverage Control
2400 Del Paso Rd. Suite 155
Sacramento, CA 95834

SUBJECT: Application for Public Convenience & Necessity Finding – Grocery Outlet, Inc.

Dear Ms. McCarthy:

The City of West Sacramento City Council, during its regular meeting of August 22, 2018, approved, with conditions, the application for the Determination of Public Convenience and Necessity for a Department of Alcoholic Beverage Control Type 21 Off-Sale liquor license for Grocery Outlet, Inc. located at 845 Harbor Blvd., West Sacramento, CA 95691. The following license conditions were imposed on the applicant:

- There shall be no single container/unit beer sales including 32 and 40-ounce containers;
- Beer and/or malt beverages shall be sold in original factory packages of six-pack or greater, except malt-based coolers;
- Wine coolers, whether made with wine or malt products, shall not be sold in quantities of less than a factory four-pack;
- The licensee shall ensure the exterior of the retail facility is free from litter and trash;
- The exterior shall be properly lighted to allow a clear view of the entire building during the hours of darkness;
- The licensee shall insure there is no consumption of alcoholic beverages on the premises;
- The licensee shall post the property against loitering on the licensed premises;
- The licensee shall participate in the Police Department's "Together Everyone Achieves More" program.

Please contact me at kryssr@cityofwestsacramento.org or 916-617-4500 if you have any questions. Thank you.

Sincerely,

Kryss Rankin, City Clerk

Enclosure: City Council 8/22/18 Staff Report

Cc: Thomas McDonald, Chief of Police

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 17

SUBJECT:

CONSIDERATION OF UPDATED 2018/2019 BENEFITS SUMMARY FOR CITY MANAGER AND JOB CLASSIFICATION DESCRIPTION UPDATES FOR ASSOCIATE CIVIL ENGINEER, SENIOR CIVIL ENGINEER, CHIEF TREATMENT PLANT MECHANIC AND TREATMENT PLANT MECHANIC I/II

INITIATED OR REQUESTED BY:

[ ] **Council** [X] **Staff**

[ ] **Other**

REPORT COORDINATED OR PREPARED BY:
Liane Lee, Human Resources Manager

Amanda Berlin, Assistant City Manager

ATTACHMENT [X] **Yes** [ ] **No** [ ] **Information** [ ] **Direction** [X] **Action**

OBJECTIVE
The purpose of this report is to seek City Council consideration and approval of the document updating the City Manager Benefit Summary and updates to existing job classification descriptions.

RECOMMENDED ACTION
Staff respectfully recommends that the City Council:

1. Approve the updated Benefits Summary for City Manager;
2. Approve the modified job classification description for Associate Civil Engineer;
3. Approve the modified job classification description for Senior Civil Engineer;
4. Approve the modified job classification description for Chief Treatment Plant Mechanic; and
5. Approve the modified job classification description for Treatment Plant Mechanic I/II.

BACKGROUND
The City compiles Benefits Summaries which contain information regarding benefits offered to the various City employee groups. When the Benefits Summaries are updated, they are presented to Council for consideration and approval.

Pursuant to City Personnel Rules, Section 3.4, each position in the City is required to be assigned to an appropriate classification. For each classification, a written description of the classification is required to be prepared by the City Manager and approved by the City Council. The description must include the official title of the classification and the minimum qualifications required for admission in the classification. While the City Manager is allowed to periodically perform maintenance updates to classification descriptions, when changes in duties and responsibilities or qualifications are required, the City Council must approve such updates.

ANALYSIS
At the June 27, 2018 Council meeting, Council approved an updated Benefits Summary for the City Manager classification which was inclusive of both the then current and the incumbent City Manager employment contract provisions while both the current City Manager and incumbent City Manager inhabited the City Manager position during a period of time in July 2018. Now that former City Manager Martin Tuttle has separated from employment, staff is presenting an updated City Manager Benefits Summary for consideration that reflects the salary and benefits contract provisions for the current City Manager Aaron Laurel.

Staff has been actively recruiting to fill a vacant Associate/Senior Civil Engineer position within the Public Works Department since March 30, 2018. To facilitate the ability to recruit for the most qualified applicants outside of the state of California, the language regarding required licenses is being updated and expanded for Associate Civil Engineer and Senior Civil Engineer to allow for possession of registration as a Professional Civil Engineer outside of California with the ability to obtain California registration as a Professional Civil Engineer within one (1) year of appointment.

The job classification descriptions for Treatment Plant Mechanic II and Chief Treatment Plant Mechanic are being updated to include a requirement to possess and maintain a Class A driver license within one (1) year of appointment. Job duties within these classifications occasionally require the incumbent to operate a truck and trailer with a gross vehicle weight exceeding 26,000 pounds for which a Class A driver license is required. The license would be optional for existing employees, as there is current capacity to meet the job requirements, but would be mandatory for new hires or promotions into these classifications.

Environmental Considerations
Not applicable

Commission Recommendation
Not applicable

Strategic Plan Integration
These recommendations support the City's Mission of a City government that is financially sound, has a superior workforce and is recognized as a regional leader.

Alternatives
The City Council may choose to not approve the updates to the City Manager Benefits Summary or the revised job classifications.

Coordination and Review
This Report has been coordinated with the City Mangers Office. Representatives of Stationary Engineers Local 39, Public Works staff and Human Resources staff have met and conferred and agree to the changes in the represented job descriptions.

Budget/Cost Impact
There is no additional budget impact.

ATTACHMENT(S)
1. City Manager Benefits Summary
2. Job classification description for the Associate Civil Engineer
3. Job classification description for the Senior Civil Engineer
4. Job classification description for the Chief Treatment Plant Mechanic
5. Job classification description for the Treatment Plant Mechanic I/II

City of West Sacramento
Benefits Summary

EXHIBIT A

CITY MANAGER
EFFECTIVE 07/01/2018

Annual Physical:	Employee shall submit to an annual physical at City's expense. Cost not to exceed $500.
Car Allowance:	City shall pay employee $500 per month as a car allowance.
Cellular Phone:	City provided phone or stipend ($75 month).
Holidays:	Ten (10) set in Personnel Rules and 3.5 floating holidays. Floating holidays are credited each July.
Life Insurance:	City pays premium for $300,000 term life insurance policy.
Long-Term Disability:	City pays premium for Standard LTD program offered to management employees.
Management Leave:	Eighty (80) hours per fiscal year. No accumulation. Eighty (80) hours is credited at the beginning of each fiscal year.
Medical, Dental, & Vision Insurance:	City shall pay 100% of the premiums for coverage for employee and family.
Medical after Retirement:	City shall pay 100% of the premiums for medical and dental coverage for employee and family. Vision is offered for up to 18 months (COBRA) and is paid by the retiree.
Money Purchase Plan (401a):	City contributes 4.5% of base salary, mandatory employee contribution of 5.5%. Contributions to be in equal, proportional amounts each pay period.
Retirement:	CalPERS 2.5% @ 55 plan, includes highest thirty-six months for final compensation determination, 1959 Survivor's Benefit (3rd Level) and Unused Sick Leave Credit. Employee pays entire 8% employee contribution.
Salary:	$200,000 annually.
Sick Leave:	Accrual rate is eight (8) hours a month.
Vacation:	Accrual per year based on years of employment with the City: 1-5 yrs = 10 days; 6-10 yrs = 15 days; 11-19 yrs = 20 days; 20+ yrs - 25 days. Maximum accumulation = 456 hours per year. Total payoff at separation.

See Personnel Rules for further information on Employer/Employee relations, personnel practices, and terms and conditions of employment.

CITY OF WEST SACRAMENTO

ASSOCIATE CIVIL ENGINEER

DEFINITION: ~~Under the direction of the City Engineer or his/her designee,~~ T~~t~~he Associate Civil Engineer assists in the planning, design and management of public infrastructure and facilities including transportation facilities and sewer, storm drainage and water system facilities.

~~Upon the recommendation of the Department Head and approval by the City Manager, an Assistant Engineer may advance to an Associate Civil Engineer after demonstrating the abilities and competencies necessary to perform the full range of Associate Civil Engineer duties, meeting the minimum qualifications and obtaining the required registration of a Professional Civil Engineer in the State of California.~~

~~Upon the recommendation of the Department head and approval by the City Manager, an Associate Civil Engineer may advance to Senior Civil Engineer following attainment of the minimum qualifications for the position and after he/she has demonstrated the abilities and competencies necessary to perform the full range of Senior Civil Engineer~~

SUPERVISION RECEIVED AND EXERCISED: Receives general supervision from the Director of Public Works, Director of Community Development or their designee. May exercise technical and functional direction over assigned staff.

CLASS CHARACTERISTICS: This is the journey level class within the professional engineering series. This class is distinguished from the Assistant Engineer by assignment of the full range of professional engineering duties.

This class is distinguished from the Senior Engineer in that the latter is an advance journey level class responsible for complex and difficult engineering projects and programs and exercises direct supervision over professional engineering staff.

Upon the recommendation of the Department Head and approval by the City Manager, an Assistant Engineer may advance to an Associate Civil Engineer after demonstrating the abilities and competencies necessary to perform the full range of Associate Civil Engineer duties, meeting the minimum qualifications and obtaining the required registration of a Professional Civil Engineer in the State of California.

Upon the recommendation of the Department head and approval by the City Manager, an Associate Civil Engineer may advance to Senior Civil Engineer following attainment of the minimum qualifications for the position and after he/she has demonstrated the abilities and competencies necessary to perform the full range of Senior Civil Engineer

EXAMPLES OF ESSENTIAL FUNCTIONS: (Illustrative Only): *Management reserves the right to add, modify, change or rescind the work assignments of different positions and to make reasonable accommodations so that qualified employees can perform the essential functions of the job.*

- ~~Depending upon assigned areas of responsibility, assists~~A in the preparation of various capital improvement programs.~~;~~
- ~~p~~Performs project administration and management.~~;~~
- R~~r~~eviews and prepares condition of approval for land use entitlement applications.~~;~~
- P~~p~~erforms plan checking and inspection.~~;~~
- P~~p~~erforms field reviews and formulates engineering recommendations.~~;~~
- P~~p~~repares construction plans, specifications and estimates for various types of construction projects including streets, storm drainage, sewer and water systems.~~;~~
- I~~i~~nspects structures and construction.~~;~~
- ~~a~~Assists in the management, operation and maintenance of utilities systems.~~;~~
- P~~p~~repares comprehensive reports.~~;~~
- C~~c~~oordinates project activities with other city departments and public agencies.~~;~~
- P~~p~~repares written staff reports for departmental use and/or presentation to the City Council.~~;~~
- ~~and~~ M~~m~~ay supervise sub-professional and non-professional personnel.
- Meets and confers with governmental officials and the public on matters related to assigned areas of responsibility.~~;~~
- A~~a~~ssists with Request For Proposals and consultant design contracts.~~;~~
- A~~a~~ssists with consultant services proposals.~~;~~
- P~~p~~articipates in complex engineering analysis and presentation of studies and recommendations in public forums.~~;~~
- ~~p~~Performs related duties and responsibil~~=~~ities as required.

QUALIFICATIONS:

Knowledge of: Civil engineering principles, practices, techniques and materials dealing with the design, inspection, and construction of streets, storm drainage facilities, water and wastewater facilities and related structures; applicable laws and regulatory codes relevant to public works operations; engineering principles, materials and techniques used in the planning, design, operation and maintenance of public works facilities; principles of supervision and management; and safe work practices.

Skill in: Planning, directing, and coordinating~~e~~ the work of subordinates and the planning, design, operation and maintenance of water and wastewater systems; effectively managing~~e~~ and supervising~~e~~; making complex engineering computations; checking design and preparing~~e~~ or supervising~~e~~ the preparation of engineering plans and specifications; communicating~~e~~ clearly and concisely both orally and in writing; planning, coordinating~~e~~, and prioritizing~~e~~ assigned work; performing assigned work with ~~a~~ minimum of supervision; establishing and maintaining effective work relationships with the public, representatives of other public agencies, subordinates, and supervisors; ~~ability to~~ maintaining physical condition appropriate to the per-formance of

assigned duties and responsibil~~-~~ities which may include sitting for extended periods of time, operating assigned office equipment or field trips to review existing or planned facilities; ~~ability to~~ maintaining effective audio-visual discrimina~~-~~tion and perception needed for making observa~~-~~tions, com~~-~~municating with others, reading and writing, and operating assigned office equipment; ~~ability to~~ maintaining mental capacity which al~~-~~lows for effective interaction and communication with others.

EDUCATION AND EXPERIENCE: *Any combination of training and experience which would provide the required knowledge and skill. A typical way to obtain the required knowledge and skill would be:*

Education: A Bachelor's degree in Civil Engineering from an accredited university or college.

Experience: At least four (4) years of increasingly responsible professional civil engineering experience. Public works/municipal engineering experience is highly desirable.

LICENSES AND CERTIFICATES: Possession of a valid certificate of registration as a professional civil engineer. Possession of a valid certificate of registration as a professional civil engineer in the State of California within one (1) year of appointment. If assigned duties require driving, possession and maintenance of a valid ~~Class "3" or "C"~~ California driver's license.

WORKING CONDITIONS: Work in an office, field and construction site environments; travel from site to site; walking, standing, or sitting for periods of time; exposure to dust and noise; some climbing, stooping, light lifting and inspecting in confined areas; travel to attend a variety of meetings. May be required to attend off-hour meetings.

PHYSICAL DEMANDS: Mobility to work in a standard office or construction environment, use standard office equipment and attend off-site meetings. On an intermittent basis, sit at a desk for long periods of time; intermittently walk, stand, bend, squat, twist, and reach while performing office duties; mobility to occasionally traverse uneven terrain, periodically standing, stooping, bending, climbing and kneeling to perform inspections; on occasion may be required to work in exposure to weather. Manual dexterity to use standard office equipment and supplies and small tools and to manipulate both single sheets of paper and large document holders (manuals, binders, etc.); vision to read handwritten and printed material and a computer screen; hearing and speech to communicate in person and by telephone; ability to lift and carry items weighing up to 25 pounds such as files, manuals, or small tools and equipment for distances up to 50 feet.

ENVIRONMENTAL ELEMENTS: Employees work in an office environment with moderate noise levels, controlled temperature conditions and no direct exposure to hazardous physical substances. Employees may interact with upset staff and/or public and private representatives in interpreting and enforcing department guidelines, policies and procedures.

Maintenance Update: 09/03/02
Title Change:
Job Category: Professional
Bargaining Unit: Specialists & Professionals
At-Will: ☐ Yes ☑ No

Form 700: ☑ Yes ☐ No
FLSA Status: ☑ Non-Exempt ☐ Exempt

CITY OF WEST SACRAMENTO

SENIOR CIVIL ENGINEER

DEFINITION: Under the direction of the Director of Public Works, Director of Community Development or his/her designee, the Senior Civil Engineer performs a wide variety of administrative, managerial, professional and technical tasks.

SUPERVISION RECEIVED AND EXERCISED: Receives general supervision from the Director of Public Works, Director of Community Development or his/her designee. Exercises direct supervision over professional level staff, as well as clerical, technical and/or temporary staff.

CLASS CHARACTERISTICS: This is the advanced journey level classification in the series and is responsible for assisting with the management of an engineering section. Incumbents are expected to possess the skills and abilities necessary to perform all duties and responsibilities of the classification with little supervision. The incumbent has responsibility for supervision of assigned subordinate staff.

Upon the recommendation of the Department Head and approval by the City Manager, an Associate Civil Engineer may advance to Senior Civil Engineer following attainment of the minimum qualifications for the position and after he/she has demonstrated the abilities and competencies necessary to perform the full range of Senior Civil Engineer.

EXAMPLES OF ESSENTIAL FUNCTIONS: (Illustrative Only): *Management reserves the right to add, modify, change or rescind the work assignments of different positions and to make reasonable accommodations so that qualified employees can perform the essential functions of the job.*

Capital Projects Division:

- Assists in the design and management of public works, capital improvements, street, utility and storm drain maintenance.
- Performs engineering planning, plan checking and inspection activities.
- Prepares specifications and estimates for various types of construction projects, including streets, storm drains, sewer and water systems.
- Inspects structures and construction.
- Assists in the management, operation, and maintenance of utilities systems.
- Serves as the Resident Engineer in charge of large, more complex capital improvement construction projects.
- Coordinates activities with other departments and divisions as appropriate.
- Conducts special studies relative to assigned area(s) of responsibility and prepares appropriate analyses and reports.
- Supervises, trains, evaluates and disciplines assigned professional, subprofessional, technical and administrative staff.
- Serves as staff to a variety of city commissions, boards and committees as assigned.
- Represents the department to outside agencies and organizations.

- May stamp and sign civil design plans.
- Assists with Requests for Proposals and consultant design contracts.
- May evaluate consultant services proposals.

Development Services Division:

- Assists in the review and update of city design standards for public works construction.
- Performs civil engineering review of land use entitlement applications including parcel and subdivision maps, coordinating such maps with the City Surveyor.
- Serves as the City Engineer for purposes set forth in the California Subdivision Map Act and assists in the development of appropriate mitigation measures and conditions of approval.
- Coordinates infrastructure needs and requirements, consistent with city standards and infrastructure planning documents, among various development projects and with appropriate city staff.
- Performs engineering planning, plan checking, and inspection activities.
- Oversees the calculation and collection of adopted development impact fees.
- Prepares specifications and estimates for various types of construction projects including streets, storm drains, sewer and water systems.
- Inspects structures and construction.
- Coordinates activities with other departments and divisions as appropriate.
- Conducts special studies relative to assigned area(s) of responsibility and prepare appropriate analyses and reports.
- Supervises, trains, evaluates and disciplines assigned professional, subprofessional, technical and administrative staff.
- Serves as staff to a variety of city commissions, boards and committees as assigned.
- Represents the department to outside agencies and organizations.
- Assists with Requests for Proposals and consultant design contracts.
- May evaluate consultant services proposals.

QUALIFICATIONS:

Knowledge of: Civil engineering principles and practices as applied to municipal public works, including planning and development, design and construction, and operation and maintenance; applicable laws and regulatory codes relevant to public works operations; methods, materials and techniques used in the construction of public works projects; principles of supervision and management; principles and practices of work safety; the latest Caltrans Standard Plans and Specifications reference documentation for construction projects.

Skill in: Planning, directing, reviewing and evaluating the work of assigned staff, including professional and technical staff; performing difficult technical research and analyzing complex engineering problems; making complex engineering computations; checking design and supervising the preparation of engineering plans and specifications; communicating clearly and concisely both orally and in writing; developing and implementing goals, objectives, policies, procedures, work standards and internal controls; establishing and maintaining effective work relationships with those contacted in the course of work; coordinating activities with other city

departments and public agencies; and preparing written staff reports for department use and/or presentation to the City Council.

EDUCATION AND EXPERIENCE: *Any combination of training and experience which would provide the required knowledge and skill. A typical way to obtain the required knowledge and skill would be:*

Education: A Bachelor's Degree or equivalent in Civil Engineering from an accredited college or university.

Experience: At least six (6) years of increasingly responsible professional civil engineering experience. Experience must include four (4) years in the public works/municipal engineering field.

LICENSES AND CERTIFICATES: If required to drive, possession and maintenance of a valid California driver's license. Possession and maintenance of a valid certificate of registration as a Professional Civil Engineer ~~in the State of California~~ is required. Possession and maintenance of a valid certificate of registration as a Professional Civil Engineer in the State of California within one (1) year of appointment.

WORKING CONDITIONS: May be required to attend off-hour meetings.

PHYSICAL DEMANDS: Mobility to work in a standard office or construction environment, use standard office equipment and attend off-site meetings. On an intermittent basis, sit at a desk for long periods of time; intermittently walk, stand, bend, squat, twist, and reach while performing office duties; mobility to occasionally traverse uneven terrain, periodically standing, stooping, bending, climbing and kneeling to perform inspections; on occasion may be required to work in exposure to weather. Manual dexterity to use standard office equipment and supplies and small tools and to manipulate both single sheets of paper and large document holders (manuals, binders, etc.); vision to read handwritten and printed material and a computer screen; hearing and speech to communicate in person and by telephone; ability to lift and carry items weighing up to 25 pounds such as files, manuals, or small tools and equipment for distances up to 50 feet.

ENVIRONMENTAL ELEMENTS: Employees generally work in an office environment with moderate noise levels, controlled temperature conditions and no direct exposure to hazardous physical substances. Employees may also work in a construction or field environment including inclement weather conditions and construction and traffic hazards. Employees may interact with upset staff and/or public and private representatives in interpreting and enforcing department guidelines, policies and procedures.

Adopted: 10/01/1997
Revised: 08/21/2002; 02/20/2009; 074/30/2007; 02/13/2017; 8/22/18
Maintenance Update:
Title Change:
Job Category: Administrators/Managers

Bargaining Unit: Management
At-Will: ☐ Yes ☑ No
Form 700: ☑ Yes ☐ No
FLSA Status: ☐ Non-Exempt ☑ Exempt

CITY OF WEST SACRAMENTO

CHIEF TREATMENT PLANT MECHANIC

DEFINITION: The Chief Treatment Plant Mechanic performs skilled installation, maintenance and repair of a wide variety of mechanical and electrical equipment found in a water treatment plant and remote water distribution, sewer collection and storm water related facilities. The Chief Treatment Plant Mechanic is the senior level in this series, providing lead supervision and training to subordinate personnel.

SUPERVISION RECEIVED AND EXERCISED: Receives general direction from the Equipment and Facilities Superintendent. Exercises technical and functional direction and training over assigned staff.

CLASS CHARACTERISTICS: This is the advanced journey level, lead class in the Treatment Plant Mechanic series that performs the most complex duties related to the City's mechanical and electrical equipment in a water treatment plant and remote water distribution, sewer collection and storm water related facilities. Positions at this level are distinguished from those in the lower classification of Treatment Plant Mechanic I/II by the independence with which incumbents perform their duties of handling the most difficult and complex work and performing lead work. This class is distinguished from the Equipment and Facilities Superintendent in that the latter is the full supervisory level class responsible for organizing, assigning, supervising, and reviewing the work of assigned staff involved in maintenance of mechanical and electrical equipment.

EXAMPLES OF ESSENTIAL FUNCTIONS: (Illustrative Only): *Management reserves the right to add, modify, change or rescind the work assignments of different positions and to make reasonable accommodations so that qualified employees can perform the essential functions of the job.*

- Assists the Equipment and Facilities Superintendent to implement goals, objectives, policies, and priorities.
- Assigns and performs a variety of preventative maintenance on equipment and facilities including lubricating, cleaning, and painting.
- Assigns and performs corrective maintenance on plant equipment and process systems such as troubleshooting and repairing of complex mechanical, hydraulic, and basic electrical parts replacement, equipment replacement, pump testing, removal and replacement of electric motors, cleaning of tanks and basins.
- Maintains equipment and structures at remote sites, including pumps, wet wells, lift stations and grounds.
- Maintains accurate maintenance and repair records.
- Procures parts and maintains inventory of parts and supplies.
- Evaluates the cost-effectiveness of repair versus replacement.
- Implements the department's preventative maintenance program.
- Conducts safety inspections on plant equipment and monitors safety programs.

- Assigns and performs mechanical and identifies and performs basic electrical work involving disassembly and replacement of worn or broken parts, removal of equipment for shop repair, reinstallation and realignment.
- Assists the Equipment and Facilities Superintendent to implement goals, objectives, policies, and priorities.
- Assigns and performs the most complex and highly skilled operational duties such as budget administration, parts, materials and equipment procurement, and preparing operational procedures.
- Maintains detailed records.
- Prepares reports.
- Prepares time and material cost estimates for work requested,
- Responds to inquiries and complaints.
- Evaluates operational effectiveness.
- Coordinates, assigns, and reviews the work of subordinate personnel.
- Provides technical training and lead supervision to subordinate personnel.
- Evaluates the maintenance and preventative maintenance needs of City treatment plant, pump stations, lift stations, and water wells.

QUALIFICATIONS:

Knowledge of: Maintenance of pumps, motors, piping and control systems found in water treatment plants and related facilities; mechanical, electrical, and hydraulic principles; operation of design, construction of water treatment plants, pumping station and equipment; and safe work practices.

Skill in: Organizing, implementing and directing mechanical and electrical maintenance; performing required maintenance on water treatment plant equipment and facilities; maintaining accurate records; identifying and interpreting technical information; following oral and written directions; working effectively with co-workers and supervisors; maintaining physical condition appropriate to the performance of assigned duties and responsibilities which may include standing for extended periods of time and operating assigned equipment; maintaining effective audio-visual discrimination and perception needed for making observations, reading and writing, and operating assigned equipment; communicating effectively, both orally and in writing.

EDUCATION AND EXPERIENCE: *Any combination of training and experience which would provide the required knowledge and skill. A typical way to obtain the required knowledge and skill would be:*

Education: High school diploma or equivalent. Coursework in Mechanical Electrical Technologist is desirable.

Experience: Five (5) years experience comparable to a Treatment Plant Mechanic II with at least three (3) years participation on on-call rotation work.

LICENSES AND CERTIFICATES: Possession of a valid Class "A" California driver's license within one (1) year of appointment and maintenance of license pursuant to the Department of Transportation regulations~~Possession of a valid California driver license~~; possession of a Grade T1 Water Treatment Plant Operator's Certificate or CWEA Grade I Mechanical Technologist certificate; possession of Grade D2 Water Distribution Certificate highly desirable.

WORKING CONDITIONS: Ability to wear and use SCBA units as regulated by CalOSHA. May be required to participate in an on-call rotation work in confined spaces; may be required to work unusual and prolonged work schedules; work multiple shifts during emergencies; and respond to off-hour emergencies.

PHYSICAL DEMANDS: Ability and mobility to work in a water treatment plant and field environment; walk, stand, and sit for prolonged periods of time; frequently stoop, bend, kneel, crouch, crawl, climb, reach, twist, grasp, and make repetitive hand movements in the performance of daily duties; lift, carry, push, and/or pull moderate to heavy amounts of weight, up to fifty (50) pounds, and heavier weights with the use of proper equipment.

ENVIRONMENTAL ELEMENTS: Work in a water treatment plant and field environment and in inclement weather conditions; exposure to water, dust, grease, hazardous materials, noise, chemicals, gas fumes, unpleasant odors, and other environmental substances; work in and/or around water; travel to and from various job sites; exposure to hazardous traffic conditions.

Adopted: 10/18/2017
Revised: 07/20/2018
Maintenance Update:
Title Change:
Job Category: Service and Maintenance

Bargaining Unit: Stationary Engineers, Local 39
At-Will: ☐ Yes ☑ No
Form 700: ☐ Yes ☑ No
FLSA Status: ☑ Non-Exempt ☐ Exempt

CITY OF WEST SACRAMENTO

TREATMENT PLANT MECHANIC I/II

DEFINITION: The Treatment Plant Mechanic I/II performs skilled installation, maintenance and repair of a wide variety of mechanical and electrical equipment found in a water treatment plant and remote water distribution, sewer collection and stormwater related facilities.

SUPERVISION RECEIVED AND EXERCISED: The Treatment Plant Mechanic I receives immediate direction from the Public Works Maintenance Superintendent or his/her designee. The Treatment Plant Mechanic II receives general direction from the Public Works Maintenance Superintendent or his/her designee. No direct supervision of staff is exercised by either level.

CLASS CHARACTERISTICS: The Treatment Plant Mechanic I is the entry level class in this series. Incumbents are considered to be in training status under immediate direction. The Treatment Plant Mechanic II is considered the journey level in this series. Incumbents work independently, under general direction. Advancement to the Treatment Plant Mechanic II level may occur after satisfactorily performing the duties of a Treatment Plant Mechanic I for two (2) years and upon recommendation of the Department Head. An employee may be advanced to the II level in less than two (2) years if the employee's performance is noted as exceptional by the Department Head and the employee is recommended for early advancement by the Department Head.

EXAMPLES OF ESSENTIAL FUNCTIONS: (Illustrative Only): *Management reserves the right to add, modify, change or rescind the work assignments of different positions and to make reasonable accommodations so that qualified employees can perform the essential functions of the job.*

- Performs a variety of preventative maintenance on equipment and facilities including lubricating, cleaning, and painting.
- Performs corrective maintenance on plant equipment and process systems such as troubleshoot and repair of complex mechanical, hydraulic and basic electrical parts replacement, equipment replacement, pump testing, removal and replacement of electric motors, cleaning of tanks and basins.
- Maintains equipment and structures at remote sites, including pumps, wet wells, lift stations and grounds.
- Maintains accurate maintenance and repair records.
- Procures parts and maintains inventory of parts and supplies.
- Evaluates the cost-effectiveness of repair versus replacement.
- Implements the department's preventative maintenance program.
- Conducts safety inspections on plant equipment and monitors safety programs.
- Performs mechanical and electrical work involving disassembly and replacement of worn or broken parts, removal of equipment for shop repair, reinstallation, and realignment.

QUALIFICATIONS:

Knowledge of: Maintenance of pumps, motors, piping and control systems found in water treatment plants and related facilities; mechanical, electrical, and hydraulic principles; operation of water treatment plant equipment; and safe work practices.

Skill in: Perform required maintenance on water treatment plant equipment and facilities; maintain accurate records; communicate effectively; follow oral and written directions; and to work effectively with co-workers and supervisors; maintain physical condition appropriate to the performance of assigned duties and responsibilities which may include standing for extended periods of time and operating assigned equipment; maintain effective audio-visual discrimination and perception needed for making observations, communicating with others, reading and writing, and operating assigned equipment; maintain mental capacity which allows for effective interaction and communication with others.

EDUCATION AND EXPERIENCE: *Any combination of training and experience which would provide the required knowledge and skill. A typical way to obtain the required knowledge and skill would be:*

Education: High school diploma or equivalent.

Experience: *Treatment Plant Mechanic I:* Minimum of three (3) years experience in the maintenance of pumps, motors, piping, hydraulic or pneumatic systems, or other mechanical/process equipment. *Treatment Plant Mechanic II:* Two (2) years experience comparable to a Treatment Plant Mechanic I.

LICENSES AND CERTIFICATES: *Treatment Plant Mechanic I:* Possession of a valid California ~~Class "3" or "C"~~ driver's license. *Treatment Plant Mechanic II:* Possession of a valid Class "A" California driver's license within one (1) year of appointment. Both levels must maintain the required driver's license pursuant to the Department of Transportation regulations. ~~;~~ P~~p~~ossession of a Grade TI Water Treatment Operator's certificate or CWEA Grade I Mechanical Technologist certificate is highly desirable, but not required. A Grade D2 Water Distribution Certificate is also highly desirable, but not required.

WORKING CONDITIONS: Ability to wear and use SCBA units as regulated by Cal/OSHA. May be required to participate in an on-call rotation work in confined spaces; may be required to work unusual and prolonged work schedules; work multiple shifts during emergencies; and respond to off-hours emergencies.

PHYSICAL DEMANDS: Ability and mobility to work in a water treatment plant and field environment; walk, stand and sit for prolonged periods of time; frequently stoop, bend, kneel, crouch, crawl, climb, reach, twist, grasp and make repetitive hand movements in the performance of daily duties; lift, carry, push, and/or pull moderate to heavy amounts of weight up to 50 pounds and heavier weights with the use of proper equipment.

ENVIRONMENTAL ELEMENTS: Work in a water treatment plant and field environment and in inclement weather conditions; exposures to water, dust, grease, hazardous materials, noise,

chemicals, gas fumes, unpleasant odors, and other environmental substances; work in and/or around water; travel to and from various job sites; exposure to hazardous traffic conditions.

Adopted: 07/20/1988
Revised: 04/19/1989; 09/20/1989; 08/18/1993; 06/12/1996; 02/01/2000; 06/11/2003; 05/25/2011; 7/20/2011; 7/20/2018
Maintenance Update: 05/13/2014
Title Change:
Job Category: Service & Maintenance

Bargaining Unit: Stationary Engineers, Local 39
At-Will: ☐ Yes ☑ No
Form 700: ☐ Yes ☑ No
FLSA Status: ☑ Non-Exempt ☐ Exempt

REGULAR MEETING OF THE
CITY OF WEST SACRAMENTO CITY COUNCIL,
REDEVELOPMENT SUCCESSOR AGENCY, AND
WEST SACRAMENTO FINANCING AUTHORITY
July 18, 2018
Minutes

DRAFT

The closed session was called to order at 6:30 PM. There were no members of the public present. Councilmembers Orozco and Sandeen were absent. All other members were present. The session adjourned at 7:00 PM.

The regular meeting was called to order at 7:05 PM in the Council Chambers, 1110 West Capitol Avenue, West Sacramento, California. All members were present. Mayor Cabaldon presided.

The Pledge of Allegiance was led by Aaron Laurel, City Manager.

Entry No. 1
Heard General Administration Functions as follows:

Heard presentations by the public on matters not on the agenda.

Councilmember Johannessen reported that the Yolo-Solano Air Quality Management District Board reviewed the bus and truck regulation update; reviewed a proposed emissions rule regarding graphic arts printing operations in the district of which there are 12 such businesses; and a season review of particulate matter 2.5 which is a very small particle that could pose potential harmful health effects, confirming that the number of exceedance days was mostly due to heat and the fires.

Councilmember Johannessen also reported that the Yolo County Housing Authority reviewed the smoke-free housing policy; authorized the organization to apply for the Housing and Urban Development rental assistance demonstration program.

Councilmember Sandeen reported that in the absence of a quorum, the Water Resources Association of Yolo County Board heard a legislative briefing; heard that the Northern California Water Association supports increased state funding for Sacramento Valley Flood Protection Infrastructure which would benefit the City of West Sacramento.

Councilmember Sandeen continued to report that the Yolo Subbasin Groundwater Agency Board took up several administrative items such as approving budget adjustments and adopting a policy on cash reserves; and continued work on their primary purpose of developing the Groundwater Sustainability Plan for Yolo County.

In addition, Councilmember Sandeen reported that the West Sacramento Area Flood Control Agency renewed the general liability insurance policy; approved midterm budget adjustments; and heard updates on the Southport Project which was toured by Councilmember Sandeen in June.

Councilmember Sandeen concluded by reporting that the Sacramento-Yolo Port District Commission approved a Memorandum of Understanding with Armbrust Aviation Group for potential development of a fuel storage facility at the North Terminal of the Port.

Councilmember Orozco reported that along with City staff and members of the community, she attended the Memorial Park Open House event which was held on the evening of July 10, where she was very impressed with community involvement and with City staff on their organization and presentations highlighting the planned improvements for the park; discussed how questionnaires were used to solicit community feedback; and expressed her appreciation to Parks and Recreation Director Cindy Tuttle.

Councilmember Orozco also announced that the Yolo County Children's Alliance is conducting a school supply and backpack drive for our community through August 2; and announced that she will be coordinating an extended period of donations and will post further information on her City Council website.

Mayor Cabaldon acknowledged Deputy City Manager Jon Robinson and City staff in Economic Development & Housing, Public Works Transportation, Parks & Recreation, and many other City departments for their coordinated efforts in recent major announcements for the City. One being the AARP announcement that the City of West Sacramento, one of 129 participating cities nationwide, ranked as number five in the state to win the Community Challenge which is $10,000 in resources to assist with the work that we are already doing to assure that folks of all ages in the community have access to mobility; the other for the newest businesses opening locations in the City of West Sacramento: LaMesa RV Center, the largest recreational vehicle showroom in America; and Mikuni Color which began as a firm that made color ink for printing processes and has evolved to make conductive material inside of electric vehicle batteries and for home use.

Mayor Cabaldon continued to report that he attended the Mayors Entrepreneurship Summit which was held in Kansas City, where he had opportunity to promote and help other cities to learn from what we are doing in the entrepreneurship space; found that there is a lot of interest in the West Sacramento Home Run program and the relationship between entrepreneurship and internship; and made connections with potential funders to broaden additional resources for the West Sacramento Home Run which led to the publication of an article featuring the program as one of the nation's leading innovations in the entrepreneurship space.

Mayor Cabaldon concluded by reporting on his attendance of the Food and Agriculture Innovation Council branch of the Greater Sacramento Economic Council, which focused on growing existing companies, startups, and attracting companies from elsewhere into the food and agriculture innovation space; found that of the six regional counties, at least half of the active members are from Yolo County; the Council is co-chaired by Michael Teel, Chief Executive Officer of Raley's, and by the Chief Executive Officer of Rabobank; the Council is working with the Bayer Collaborator Space and Venture Capital; planning a study mission to the Research Triangle and peer regions in North Carolina and St. Louis; and is working with U.C. Davis making this a very useful forum to make the necessary collaborations necessary to transfer technology and patents to startups making them real companies that produce wealth, income, and jobs in the community. Mayor Cabaldon also acknowledged that Diane Richards, Economic Development & Housing Department Program Manager is a member of the Greater Sacramento Economic Council.

Entry No. 2
Minute Order 18-57: Appointed Councilmember Sandeen as delegate to the League of California Cities Annual Conference.

MOTION: Ledesma. SECOND: Orozco. AYES: Johannessen, Sandeen, Cabaldon.

Entry No. 3
Minute Order 18-58: Acted on the Consent Agenda as follows:

Adopted **Resolution 18-14** authorizing the collection of a special tax and to place these assessments on the secured tax roll to be collected in the same manner as the ordinary *ad valorem* property taxes. Furthermore, the actions recognize that the collection of taxes will be used to pay outstanding bond debt and other purposes previously authorized by the City.

Adopted **Resolution 18-15** authorizing the collection of special tax to pay for certain Community Facility District services on the secured tax roll in the same manner as the ordinary *ad valorem* property taxes.

Found that the accepting of multiple Irrevocable Offers of Dedication of two public roads, and miscellaneous public utility and drainage easements is consistent with the Southport Framework Plan Environmental Impact Report and no further environmental review under the California Environmental Quality Act (CEQA) is required pursuant to CEQA Guidelines, Section 15183; and adopted **Resolution 18-50** accepting subject to improvement, the Irrevocable Offers of Dedication of two public roads, and miscellaneous public utility and drainage easements, which are intended to serve the Capitol Estates project.

Found that the project is exempt from further environmental review pursuant to a Class 1 (Existing Facilities) Categorical Exemption under the California Environmental Quality Act (CEQA) and found that the Exemption reflects the independent judgment of the City as lead agency under CEQA; waived the second reading and read by title and number only, and adopted **Ordinance 18-11** approving the first amendment to the River Wellness Development Agreement; and directed staff to file a Notice of Exemption pursuant to the CEQA Guidelines.

Found that the West Gateway Place II development is consistent with the Bridge District Specific Plan Supplemental Environmental Impact Report (BDSP SEIR), and that no further California Environmental Quality Act (CEQA) review is required pursuant to Public Resources Code, Section 21166 and CEQA Guidelines Section 15162; designated the City Manager to enter into an option agreement with JHC Acquisitions LLC (Jamboree) in substantially the form as presented; and designated the City Manager to take any other actions necessary to effectuate the transaction described in this staff report, including but not limited to modifying or amending the option agreement, subject to the approval of the City Attorney.

Found that the Community Facility District Sidewalk Maintenance Contract Amendment No. 1 is exempt from environmental review pursuant to CEQA Guidelines Section 15301; approved Contract Amendment No.1 to the existing professional services agreement with Precision Concrete Cutting for additional services in the amount of $459,520.60 for a total contract amount not to exceed $509,520.60; approved an extension for the duration of the contract through June 30, 2021; authorized the Mayor to execute the contract on behalf of the City of West Sacramento; granted the City Manager or his designee the authority to issue contract change orders up to 10 percent of the total contract price for all areas in an amount not to exceed $41,774.60; and approved a Capital Improvement Project (CIP) budget appropriation in the amount of $20,945.31 for CFD F (Pheasant Hollow) services.

Approved a contract for professional services with Domenichelli and Associates in the amount of $454,578; and authorized the City Manager or his designee to make contract amendments up to ten percent ($45,458) of the value of the contract.

Authorized the City Manager to approve Amendment No. 2 to the "Agreement Between the City and Washington Unified School District (WUSD) To Partner in the Use of Facilities" (Joint Use Agreement), in substantially the form presented, together with such minor modifications and revisions as may be approved by the City Manager and City Attorney and agreed to by WUSD, in order to comply with the grant requirements for the Sycamore Trail Phase II & III Project.

Approved a two-year contract for professional services for Staff Augmentation Services with Willdan Engineering in an amount not to exceed $500,000; authorized the City Manager or his designee, to make contract amendments to extend the duration of this contract with three (3) one (1) year extensions, at the City's discretion, in an amount not to exceed $250,000 per year; and authorized the City Manager or his designee to make contract amendments up to ten percent of the value of the contract.

Approved the proposed 2018 Bicycle, Pedestrian, and Trails Master Plan.

Authorized the City Manager to execute the Interagency and Cost Reimbursement Agreement to facilitate Downtown Riverfront Streetcar project advancement as recommended by the Federal Transit Administration ("FTA"). This action does not increase the City's prior funding commitment to the project.

Directed staff to monitor the process of the establishment of a joint governance mechanism for participating Yolo County Animal Services jurisdictions to collaboratively explore the development and operation of a new animal shelter, and, in the interim, identify and analyze options for animal services while continuing to utilize animal services provided by the Sheriff's Office.

Authorized the appropriation of $27,179.39 of Measure E funds from the identified funding framework for the College Promise program of the West Sacramento Home Run for expenditure on reimbursement of eligible expenses for the first-year administration of the West Sacramento College Promise by the Los Rios Community College District (LRCCD), by and through its college, Sacramento City College (SCC), and; approved appropriation of $27,179.39 of unencumbered Measure E funds to fund 110-9017.

Authorized the City Manager to execute a reimbursement agreement between the City of West Sacramento and the West Sacramento Foundation for the administration of the career ready internship and digital badge programs of the Home Run and; approved appropriation of $257,000 of unencumbered Measure E funds to fund 110-9017.

Approved the terms of the successor agreement and adopted the Memorandum of Understanding between the City of West Sacramento and the West Sacramento Police Manager's effective July 1, 2017 through December 31, 2020; directed staff to incorporate these terms into the salary ranges for each PMA-represented job classification (the Classification Plan) and return with an updated Classification Plan for approval at a subsequent City Council Meeting; and, adopted **Resolution 18-49** approving amendments to the approved biennial budget for Fiscal Year 2018/19 for $40,614 in ongoing cost and $11,800 in one-time cost and direct staff to return to Council for additional budget appropriations upon successful passage of a sales tax measure in November 2018.

Approved the minutes of the June 27 regular City Council meeting and July 9, 2018 adjourned regular meeting.

MOTION: Ledesma. SECOND: Sandeen. AYES: Johannessen, Orozco, Cabaldon.

Entry No. 4
Minute Order 18-59: Opened a public hearing to receive public comment regarding delinquent water, sewer, and refuse charges. Hearing from no one, closed the public hearing; and adopted **Resolution 18-46** certifying inclusion of delinquent charges and penalties for water, sewer and refuse services on the secured tax roll in the same manner as the general taxes and directing that a lien also be placed on the subject properties.

MOTION: Johannessen. SECOND: Ledesma. AYES: Orozco, Sandeen, Cabaldon.

Entry No. 5
Minute Order 18-60: Opened a public hearing to take testimony on any objections to past due Code Enforcement charges. Hearing from no one, closed the public hearing; and adopted **Resolution 18-41**, as amended, certifying the list of past due Code Enforcement charges, directing that a lien be placed on the subject properties and requesting inclusion of these charges on the secured tax roll in order that they may be collected in the same manner as general taxes.

MOTION: Sandeen. SECOND: Orozco. AYES: Johannessen, Ledesma, Cabaldon.

Entry No. 6
Minute Order 18-61: Opened a public hearing to receive public comment on the charges for mandatory weed abatement services initiated by the City. Hearing from no one, closed the public hearing; and adopted **Resolution 18-17** certifying the list of weed abatement charges and direct the Yolo County Auditor to add the expenses to the individual tax rolls.

MOTION: Ledesma. SECOND: Orozco. AYES: Johannessen, Sandeen, Cabaldon.

Entry No. 7
Heard a workshop presentation on the Parks and Open Space Master Plan Update, including review of the level of service analysis completed for existing and future facilities; and provided feedback to staff related to the planning effort.

Entry No. 8
Heard staff's presentation, conducted a discussion, and then provided direction on policy definitions, program features and key performance indicators for inclusion in a final West Sacramento Home Run Policy framework.

Entry No. 9
Heard staff's presentation, then conducted a discussion and provided direction on key smart city concepts that it wishes to see included in a draft smart city strategy.

Entry No. 10
Heard General Administration Function, Part II.

Assistant City Manager Berlin announced that the candidates for City Council filing period has opened and will be closing at 5:00 PM on August 10; the Youth Fire Academy Graduation will be held in the Galleria on July 25 beginning at 5:30 PM; Sumer Night Lights #2 will be held at Memorial Park on July 28 beginning at 6:00 PM; the State of the County Address will be held on August 2; the Washington District Public Art Event will be held at the 5th & C Street Garage on August 4 from 11:00 – 4:00 PM; Summer Night Lights #3 will be held at Riverbank Elementary School on August 11 beginning at 6:00 PM; and the next City Council meeting is scheduled for August 22.

City Manager Laurel reported that due to the short period since the last City Council meeting, staff did not provide a monthly update on the VIA on-demand rideshare program. However, Public Works Director Anbiah and Assistant Transportation Planner Strand reported that we are on the cusp of averaging about 1,000 riders per week with the most popular destinations continuing to be the major shopping centers; and that approximately ninety riders have applied to receive the senior/disabled rider discount. There will be a regular update, including data on shared rides, presented to the City Council on August 22.

City Manager Laurel also recognized the Summer STEPS interns who were in attendance of this City Council meeting; and announced that Katy Jacobson is the newly appointed Director of Economic Development & Housing.

Councilmember Johannessen suggested promoting the opportunity to use VIA on-demand rideshare for getting residents to and from their polling places on Election Day.

PUBLIC REPORT OF CLOSED SESSION
Conference with Legal Counsel – Significant Exposure to Litigation - GC §54956.9(b): 1
No action taken.

Conference with Labor Negotiator - GC §54957.6
Agency Negotiator: Laura Izon Powell
Employee Organization: Police Managers' Association (PMA)
No action taken.

This meeting adjourned at 9:36 PM.

_____
Kryss Rankin, City Clerk

Minutes approved as presented by a majority vote of the City Council on August 22, 2018.

_____
Kryss Rankin, City Clerk

CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 19

SUBJECT:

CONSIDERATION AND APPROVAL OF CONTRACT WITH DOWNTOWN STREETS, INC. AND ADOPTION OF RESOLUTION 18-53 APPROVING APPROPRIATION FROM MEASURE E FUND

INITIATED OR REQUESTED BY:

[ ] Council [X] Staff

[ ] Other

REPORT COORDINATED OR PREPARED BY:

Amber Whitaker, Community Investment Specialist

Katy Jacobson, Economic Development & Housing Director

ATTACHMENT [X] Yes [ ] No [ ] Information [ ] Direction [X] Action

OBJECTIVE

The purpose of this report is to provide information to the City Council to consider and approve a contract with Downtown Streets, Inc. that would implement a new experience and case management program for individuals experiencing, or at risk of experiencing, homelessness.

RECOMMENDED ACTION

Staff respectfully recommends that the Council:

1. Authorize the City Manager to execute a one-year contract with Downtown Streets, Inc. in the amount of $200,000; and
2. Adopt Resolution 18-53 approving an appropriation of $200,000 from the Measure E Fund, Fund 110, for the program.

BACKGROUND

In early 2017, the City was contacted by a Bay Area-based non-profit organization known as Downtown Streets Team (DST) about bringing their unique work experience program for homeless and at-risk individuals to West Sacramento. The goal of DST's program model is to reduce homelessness by restoring dignity and preparing participants for employment. Under the program, homeless participants become part of a "team" that carries out community beautification and maintenance activities in exchange for receiving case management (counseling and housing navigation services), employment assistance services, and financial rewards for basic needs in the form of rent stipends or gift cards. Program activities are conducted during daytime hours and may include tasks such as garbage pickup, graffiti removal, and even assisting with the cleanup of homeless encampments.

DST is designed to operate within downtown districts to decrease the incidence of panhandling and other visible effects of homelessness while simultaneously improving those areas and providing unsheltered individuals a path to housing and employment stability. DST cites that since 2009, its various programs have resulted in finding permanent housing for over 800 of its participants and lasting employment for over 700 individuals.

In April of 2018, staff conducted a Council workshop on the proposed DST program. Council authorized staff to negotiate an agreement for a one-year pilot program with DST and return to Council at an upcoming meeting for consideration of the agreement.

ANALYSIS

Upon direction from Council, staff met with DST staff on numerous occasions to negotiate a contract with DST for a West Sacramento pilot program. The contract terms are summarized below.

- Term and Cost: The proposed contract with DST is for a one-year pilot term at a total cost of $200,000. Staff recommends that a one-year pilot term is sufficient time to evaluate the program's effectiveness while allowing staff to implement a longer-term funding strategy, if the Council determines that the program should continue. For at least the initial year, staff recommends allocating Measure E Homeless Services funding to cover $200,000 of the annual cost. Additional costs for the program will either be leveraged or privately raised by DST. Most of the expenses associated with the program are related to DST's staff costs and the basic needs stipends provided to program participants (see Attachment 3 for copy of budget). This model is consistent with other DST programs.

DST programs are typically funded through a combination of public and private sources, including City funds, donations, and contributions from local business communities through assessment districts such as Property and Business Improvement Districts (PBIDs). If the Council decides to move forward with the contract, staff will explore long-term funding strategies during this pilot year.

- Team Participation: DST proposes to operate one team exclusively in West Sacramento consisting of between 12-15 homeless individuals at one time, and up to 50 homeless individuals over the initial term of the program. A full-time project manager employed by DST will coordinate the team and program volunteers.

- Referrals from Homeless Coordinator: The contract provides that DST will accept referrals to the program from the City's Homeless Coordinator and screen all potential participants to confirm that they are truly West Sacramento-based homeless residents. This provision is designed to address concerns that incentive-based programs like DST could attract homeless individuals from outside the city. The referral and certification system also provides for an effective assessment of the program's impact on the outcomes of the individuals who are already under the Homeless Coordinator's case management oversight.

- Cleanup Tasks and Focus Areas: Cleanup activities will be focused along the West Capitol Avenue corridor and the riverfront, with the additional possibility of the team supplementing the City's encampment cleanup efforts. Initial boundaries will include, the West Capitol Avenue corridor, bound by the Tower Bridge on the east and Joey Lopes Park on the west, and the riverfront, bound by the Broderick Boat Ramp on the north and the Mill Street Pier on the south (see Attachment 4 for focus area map). Team members will pick up litter and other debris in the focus areas, and potentially conduct higher-skilled cleanup activities such as graffiti abatement. Cleanup operations will be managed by DST program staff under the coordination and oversight of City staff and the Homeless Coordinator, working with the City's existing homeless strike team when appropriate. DST has agreed to participate in camp cleanups within more remote areas of the City when necessary.

- Case Management Activities: DST participants will receive ongoing work readiness training, case management, and employment services from program staff and volunteers in coordination with other local service providers including Yolo County and non-profit organizations. DST staff hosts weekly "success meetings" with team participants and those interested in joining the program. The meetings serve as regular check-ins on the successes and challenges of participants and opportunities to access social services. These weekly meetings are to be held in a conference room at the County Services building at 500 Jefferson Boulevard; and, the City would aim to coordinate the timing of those meetings with the regular drop-in hours of the Homeless Coordinator.

- Data Collection: DST will collect and provide data on program participants to the City to evaluate how effective of the program is in achieving positive outcomes. The project will utilize DST's custom data impact system to measure human service results. This new system utilizes Clarity software, the industry leader in Homeless Management Information Systems (HMIS). Furthermore, user data will be collected annually through an anonymous and optional feedback survey. The survey will also capture qualitative data such as improved health, increased dignity and more positive interactions with the criminal justice system. In addition, qualitative data will be collected from businesses within the focus area, prior to the launch of the program, and then again six months into the program. This information will be collected through a survey that will capture data pertaining to the impact of homelessness on local businesses and impacts of the DST program after six months of operation in that area.

  DST will work with City staff to integrate data collected by DST with data collected by Outreach Grid to support on-going communications with the community and City Council on the outcomes of the program's efforts. Specifically, DST will utilize Outreach Grid's ability to track and report on current encampment locations, provide data regarding the cleanup activities associated with the encampments, and communicate with City staff and PD regarding areas cleaned or needing to be cleaned. In addition, throughout the contract term, DST will collaborate with the City and Outreach Grid staff to expand the utility of the software to help further its intended objectives. This collaboration will include, but not be limited to, monthly check-in calls to provide staff with feedback regarding the software's performance as it relates to DST's work in West Sacramento.

DST will be required to provide a monthly report to the City on the outcomes of the program that will include the following information:

a) Number of Team Members who have moved into housing, employment, or gained shelter
b) Number of volunteer hours performed by Team Members
c) Number of active Team Members
d) Number of people on the waitlist
e) Amount of debris removed from each project area
f) Number of needles removed from each project area
g) Snapshot of monthly highlights such as new partner relationships with service providers, and any planned initiatives/events

Staff also plans to use the ZenCity platform as an additional tool to evaluate the effectiveness of the Downtown Streets program. While other metrics such as volume of garbage removed and number of homeless persons housed will directly measure the impact of the program, they don't measure whether those changes are perceptible in the community. By comparing the percentages of positive and negative online discourse related to homelessness before and after the first year of the pilot, the ZenCity analysis should help illuminate the extent to which the Downtown Streets' work has impacted public perceptions of homelessness in West Sacramento.

- Goals and Outcomes: In addition to the results of the cleanup activities, DST's goals for the program include securing at least 10 jobs of at least 90 days in duration for program participants and placing at least 8 homeless individuals into permanent housing. In evaluating the DST program and from the City's experience, staff believes these goals are realistic.

The program is innovative in that it offers the community and team participants an opportunity to directly mitigate the impacts of homelessness while promoting positive long-term behavioral changes for the participants. DST has the potential to fill a gap in the City's overall effort to manage homelessness by providing employment readiness training and an outlet for productive daytime activity for its homeless participants, while adding another resource to the existing but insufficient supply of County and non-profit offerings for case management services. It is also a highly visible activity in the community, which is important to achieving buy-in from property owners, businesses, and residents who have a stake in the program outcomes.

Environmental Considerations
This action is not a project that is subject to CEQA because it is not an activity that may cause either a direct or reasonably foreseeable indirect physical change in the environment. (Pub. Resources Code, § 21065; CEQA Guidelines §§ 15060(c), 15378(a).)

Commission Recommendation
The contract was not presented to any Commissions as the recommended action does not include any new policy implication or direction.

Strategic Plan Integration
This item is in support of the Council's 2018 Strategic Plan Top Priority Agenda item, "Homelessness and Community Impacts (Measure E)".

Alternatives
As an alternative to staff's recommendation, the Council could defer approval and request that staff return to the Council with additional information or choose not to enter into this contract at this time. Staff does not recommend this alternative since, delaying the start of the program would postpone providing the program's services to individuals who have already been referred to the program.

Coordination and Review
This contract was coordinated and reviewed by the City Attorney's Office, Economic Development and Housing Department, Finance Department, and the City's Homeless Coordinator.

Budget/Cost Impact
This contract will be funded by an appropriation of $200,000 from unencumbered fund balance in Fund 110-9014-5530/110-52073-5800 (Measure E). As discussed earlier in this report, it is recommended that the program costs be covered initially by Measure E (see Attachment 5 for Measure E Analysis) and private funds

raised by Downtown Streets, and then by a combination of private funds, PBID funds (if the district is successfully formed), and reduced Measure E contributions.

ATTACHMENT(S)

1. DST Contract
2. Resolution 18-53
3. DST Program Budget
4. Focus Area Map
5. Measure E Analysis

Attachment 1

CONTRACT FOR SERVICES

THIS CONTRACT is made on July 18, 2018, by and between the CITY OF WEST SACRAMENTO ("City"), and Downtown Streets, Inc., a California 501(c)3 organization ("DST").

WITNESSETH:

WHEREAS, the City desires the services of DST to supplement existing cleaning initiatives undertaken by the City;

WHEREAS, DST has presented a proposal for such services to the City, (attached hereto as Exhibit A) and is duly licensed, qualified and experienced to perform those services;

NOW, THEREFORE, the parties hereto mutually agree as follows:

1. SCOPE OF SERVICES:

A. DST shall do all work, attend all meetings, produce all reports and carry out all activities necessary to completion of the services described in the Scope of Work, attached hereto and incorporated herein by this reference as Exhibit B. This Contract and its exhibits shall be known as the "Contract Documents." Terms set forth in any Contract Document shall be deemed to be incorporated in all Contract Documents as if set forth in full therein. In the event of conflict between terms contained in these Contract Documents, the more specific term shall control. If any portion of the Contract Documents shall be in conflict with any other portion, provisions contained in the Contract shall govern over conflicting provisions contained in the exhibits to the Contract.

B. DST enters into this Contract as an independent contractor and not as an employee of the City. DST shall have no power or authority by this Contract to bind the City in any respect. Nothing in this Contract shall be construed to be inconsistent with this relationship or status. All employees, agents, contractors or subcontractors hired or retained by DST are employees, agents, contractors or subcontractors of DST and not of the City. The City shall not be obligated in any way to pay any wage claims or other claims made against DST by any such employees, agents, contractors or subcontractors, or any other person resulting from performance of this Contract.

C. DST agrees it has satisfied itself by its own investigation and research regarding the conditions affecting the work to be done and labor and materials needed, and that its decision to execute this Contract is based on such independent investigation and research.

D. In order to assist DST with its services, the City will issue DST a parking permit in order for DST to park one vehicle in the City Hall parking lot. In addition, the City agrees that if DST needs temporary storage or office space during the term of this Contract, the City will provide such space to DST at nominal cost, pursuant to the terms of the City's standard license agreement approved by the City Manager, in a location approved by the City Manager.

2. TERM OF CONTRACT

A. The services of DST are to commence upon execution of this Contract by the City, and shall be undertaken and completed in accordance with the Schedule of Performance, attached hereto and incorporated herein by this reference as Exhibit C.

B. DST's failure to complete work in accordance with the Schedule of Performance may result in delayed compensation as described in Section 3.

3. COMPENSATION:

A. DST shall be paid monthly for all time and materials required and expended, but in no event shall total compensation exceed Two Hundred Thousand Dollars ($200,000), without City's prior written approval. DST's fees and costs shall be as specified in the budget, (see page 3 of Exhibit A).

B. Said amount shall be paid upon submittal of a monthly billing showing completion of the tasks that month. DST shall furnish City with invoices for all expenses as well as for all materials authorized by this Contract. The invoices shall be submitted with the monthly billings. If DST's performance is not in conformity with the Schedule of Performance, payments may be delayed or denied, unless the DST's failure to perform in conformity with the Schedule of Performance is a documented result of the City's failure to conform with the Schedule of Performance, or if the Schedule of Performance is extended pursuant to Section 5.

C. If the work is halted at the request of the City, compensation shall be based upon the proportion that the work performed bears to the total work required by this Contract, subject to Section 4.

4. TERMINATION:

A. This Contract may be terminated by either party, provided that the other party is given not less than 30 calendar days' written notice (delivered by certified mail, return receipt requested) of intent to terminate.

B. The City may temporarily suspend this Contract, at no additional cost to City, provided that the DST is given written notice (delivered by certified mail, return receipt requested) of temporary suspension. If City gives such notice of temporary suspension, DST shall immediately suspend its activities under this Contract.

C. Notwithstanding any provisions of this Contract, DST shall not be relieved of liability to the City for damages sustained by the City by virtue of any breach of this Contract by DST, and the City may withhold any payments due to DST until such time as the exact amount of damages, if any, due the City from DST is determined.

D. In the event of termination, DST shall be compensated as provided for in this Contract, except as provided in Section 4C. Upon termination, the City shall be entitled to all work, including but not limited to, reports, inventories, studies, analyses, drawings and data estimates performed to that date in accordance with Section 7 hereof.

5. AMENDMENTS, CHANGES OR MODIFICATIONS:

Amendments, changes or modifications in the terms of this Contract may be made at any time by mutual written agreement between the parties hereto and shall be signed by the persons authorized to bind the parties hereto.

6. EXTENSIONS OF TIME:

DST may, for good cause, request extensions of time to perform the services required hereunder. Such extensions shall be authorized in advance by the City in writing and shall be incorporated in written amendments to this Contract or the attached Work Program in the manner provided in Section 5.

7. PROPERTY OF CITY:

A. It is mutually agreed that all materials prepared by the DST under this Contract shall become the property of the City, and the DST shall have no property right therein whatsoever. Immediately upon termination, the City shall be entitled to, and the DST shall deliver to the City, all data, drawings, specifications, reports, estimates, summaries and other such materials as may have been prepared or accumulated to date by the DST in performing this Contract which is not DST's privileged information, as defined by law, or DST's personnel information, along with all other property belonging exclusively to the City which is in DST's possession.

B. Additionally, it is agreed that the parties intend this to be a contract for services and each considers the products and results of the services to be rendered by DST hereunder (the "Work") to be a work made for hire. DST acknowledges and agrees that the Work (and all rights therein, including, without limitation, copyright) belongs to and shall be the sole and exclusive property of the City.

8. COMPLIANCE WITH ALL LAWS:

A. DST shall comply with all applicable laws, ordinances, and codes of federal, State and local governments, and shall commit no trespass on any public or private property in performing any of the work authorized by this Contract. It shall be City's responsibility to obtain all rights of way and easements to enable DST to perform its services hereunder. DST shall assist City in providing the same.

B. DST warrants to the City that it is licensed by all applicable governmental bodies to perform this Contract and will remain so licensed throughout the progress of the Work, and that it has, and will have, throughout the progress of the Work, the necessary experience, skill and financial resources to enable it to perform this Contract.

9. WARRANTIES AND RESPONSIBILITIES - DST:

A. DST agrees and represents that it is qualified to properly provide the services set forth in Exhibit B in a manner which is consistent with the generally accepted standards of DST's profession.

B. DST agrees and represents that the work performed under this Contract shall be in accordance with applicable federal, State and local law in accordance with Section 17A hereof.

C. DST shall designate a project manager who at all times shall represent the DST before the City on all matters relating to this Contract. The project manager shall continue in such capacity unless and until he or she is removed at the request of the City, is no longer employed by DST, or is replaced with the written approval of the City, which approval shall not be unreasonably withheld.

D. DST shall provide corrective services without charge to the City for services which fail to meet the above professional and legal standards and which are reported to DST in writing within sixty (60) days of discovery. Should DST fail or refuse to perform promptly its obligations, the City may render or undertake performance thereof and the DST shall be liable for any expenses thereby incurred.

10. SUBCONTRACTING:

None of the services covered by this Contract shall be subcontracted without the prior written consent of the City, which will not be unreasonably withheld. DST shall be as fully responsible to the City for the negligent acts and omissions of its contractors and subcontractors, and of persons either directly or indirectly employed by them, as it is for the negligent acts and omissions of persons directly employed by DST.

11. ASSIGNABILITY:

DST shall not assign or transfer any interest in this Contract whether by assignment or novation, without the prior written consent of the City which will not be unreasonably withheld. However, claims for money due or to become due DST from the City under this Contract may be assigned to a financial institution, or to a trustee in bankruptcy, without such approval. Notice of any assignment or transfer whether voluntary or involuntary shall be furnished promptly to the City.

12. INTEREST IN CONTRACT:

DST covenants that neither it, nor any of its employees, agents, contractors, subcontractors has any interest, nor shall they acquire any interest, direct or indirect, in the subject of the Contract, nor any other interest which would conflict in any manner or degree with the performance of its services hereunder. DST shall make all disclosures required by the City's conflict of interest code in accordance with the category designated by the City, unless the City Manager determines in writing that DST's duties are more limited in scope than is warranted by the category designated by the City code and that a narrower disclosure category should apply. DST also agrees to make disclosure in compliance with the City conflict of interest code if, at any time after the execution of this Contract, City determines and notifies DST in writing that DST's duties under this Contract warrant greater disclosure by DST than was originally contemplated. DST shall make disclosures in the time, place and manner set forth in the conflict of interest code and as directed by the City.

13. MATERIALS CONFIDENTIAL:

All of the materials prepared or assembled by DST pursuant to performance of this Contract are confidential and DST agrees that they shall not be made available to any individual or organization without the prior written approval of the City, except by court order.

14. LIABILITY OF DST-NEGLIGENCE:

DST shall be responsible for performing the work under this Contract in a manner which is consistent with the generally-accepted standards of the DST's profession and shall be liable for its own negligence and the negligent acts of its employees, agents, contractors and subcontractors. The City shall have no right of control over the manner in which the work is to be done but only as to its outcome, and shall not be charged with the responsibility of preventing risk to DST or its employees, agents, contractors or subcontractors.

15. INDEMNITY AND LITIGATION COSTS:

DST shall indemnify, defend, and hold harmless the City, its officers, officials, agents, and employees against all claims, damages, demands, liability, costs, losses and expenses, including without limitation court costs and reasonable attorneys' fees, arising from DST's negligent acts or negligent failure to act, errors, omissions or willful misconduct incident to the performance of this Contract except such loss or damage caused by the active negligence, sole negligence, or willful misconduct of the City. The provisions of this paragraph shall survive termination or suspension of this Contract.

16. CONSULTANT TO PROVIDE INSURANCE:

A. DST shall not commence any work before obtaining, and shall maintain in force at all times during the duration and performance of this Contract, the policies of insurance specified in this Section. Such insurance must have the approval of the City as to limit, form, and amount, and shall be placed with insurers with a current A.M. Best's rating of no less than A VII (an NR rating is acceptable for Worker's Compensation insurance written with the State Compensation Insurance Fund of California).

B. Prior to execution of this Contract and prior to commencement of any work, DST shall furnish the City with certificates of insurance and copies of endorsements providing evidence of coverage for all policies required by the Contract. DST and its contractors and subcontractors shall, at their expense, maintain in effect at all times during the performance of work under the Contract not less than the following coverage and limits of insurance, which shall be maintained with insurers and under forms of policy satisfactory to the City. The maintenance by DST and its contractors and subcontractors of the following coverage and limits of insurance is a material element of this Contract. The failure of DST or of any of its contractors or subcontractors to maintain or renew coverage or to provide evidence of renewal may be treated by the City as a material breach of this Contract. Approval of the insurance by the City shall not relieve or decrease any liability of DST.

1. Worker's Compensation and Employer's Liability Insurance

a. Worker's Compensation - Insurance to protect DST, its contractors and subcontractors from all claims under Worker's Compensation and Employer's Liability Acts, including Longshoremen's and Harbor Worker's Act ("Acts"), if applicable. Such coverage shall be maintained, in type and amount, in strict compliance with all applicable state and Federal statutes and regulations. The DST shall execute a certificate in compliance with Labor Code Section 1861, on the form provided in the Contract Documents.

b. DST shall provide a Waiver of Subrogation endorsement in favor of the City, its officers, officials, employees, agents and volunteers for losses arising from work performed by the DST.

2. Commercial General Liability Insurance

a. The insurance shall be provided on form CG0001, or its equivalent, and shall include coverage for claims for bodily injury or property damage arising out of premises/operations, products/completed operations, contractual liability, and subconsultant's work and personal and advertising injury resulting from actions, failures to act, or operations of the insured, or by its employees or agents, or by anyone directly or indirectly employed by the insured. The amount of insurance coverage shall not be less than $1,000,000.00 per occurrence and $2,000,000 general and products/completed operations aggregates.

b. The commercial general liability insurance shall also include the following:

i. Endorsement equivalent to CG 2010 1185 naming the City, its officers, officials, employees, agents, and volunteers as additional insureds. The endorsement shall contain no special limitations on the scope of protection afforded to the City, its officers, officials, employees or volunteers.

ii. Endorsement stating insurance provided to the City shall be primary as respects the City, its officers, officials, employees and any insurance or self insurance maintained by the City, its officers, officials, employees or volunteers shall be in excess of the DST's insurance and shall not contribute with it, to the payment or satisfaction of any defense expenses, loss, or judgment.

iii. Provision or endorsement stating that the DST's insurance shall apply separately to each insured against whom claim is made or suit is brought, except with respect to the limits of the insurer's liability.

3. Commercial Automobile Insurance

a. The insurance shall include, but shall not be limited to, coverage for claims for bodily injury or property damage for owned, non-owned, and hired automobiles resulting from actions, failures to act, or operations of the insured, or by its employees or agents, or by anyone directly or indirectly employed by the insured. The amount of insurance coverage shall not be less than $1,000,000.00 per accident.

b. The commercial automobile insurance shall include the same endorsements required for the commercial general liability policy (see Section 16.B.2.b).

4. Professional Liability. The DST and its contractors and subcontractors shall secure and maintain in full force, during the term of this Contract and for five years thereafter, professional liability insurance policies appropriate to the respective professions and the work to be performed as specified in this Contract. The limits of such professional liability insurance coverage shall not be less than $1,000,000 per claim.

C. In addition to any other remedy the City may have, if DST fails to maintain the insurance coverage as required in this Section, the City may obtain such insurance coverage as is not being maintained, in form and amount substantially the same as is required herein, and the City may deduct the cost of such insurance from any amounts due or which may become due DST under this Contract.

D. No policy required by this Contract shall be suspended, cancelled, terminated by either party, or reduced in coverage or in limits unless DST has provided thirty (30) days prior written notice by certified mail, return receipt requested, to the City.

E. Any deductibles or self-insured retentions in excess of $10,000 must be declared to, and approved by, the City.

F. The requirement as to types, limits, and the City's approval of insurance coverage to be maintained by DST are not intended to, and shall not in any manner, limit or qualify the liabilities and obligations assumed by DST under the Contract.

17. MISCELLANEOUS PROVISIONS:

A. DST shall keep itself fully informed of, shall observe and comply with, and shall cause any and all persons, firms or corporations employed by it or under its control to observe and comply with, applicable federal, state, county and municipal laws, ordinances, regulations, orders and decrees which in any manner affect those engaged or employed on the work described by this Contract or the materials used or which in any way affect the conduct of the work.

B. DST shall not engage in unlawful employment discrimination. Such unlawful employment discrimination includes, but is not limited to, employment discrimination based upon a person's race, religious creed, color, national origin, ancestry, physical handicap, medical condition, marital status, gender, citizenship, or sexual orientation.

C. DST shall maintain and make available for inspection by the City and its auditors accurate records of all of its costs, disbursements and receipts with respect to any work under this Contract. Such inspections may be made during regular office hours at any time until six (6) months after the final payments under this Contract are made to the DST.

D. This Contract constitutes the entire agreement between the parties relative to the services specified herein and no modification hereof shall be effective unless and until such modification is evidenced by a writing signed by both parties to this Contract. There are no understandings, agreements, conditions, representations, warranties or promises, with respect to this Contract, except those contained in or referred to in the writing.

E. All notices that are required to be given by one party to the other under this Contract shall be in writing and shall be deemed to have been given if delivered personally or

enclosed in a properly addressed envelope and deposited in a United States Post Office for delivery by registered or certified mail addressed to the parties at the following addresses:

City:

City of West Sacramento
1110 West Capitol Avenue
West Sacramento, CA 95691

DST:

Downtown Streets, Inc.
1671 The Alameda, Suite 306
San Jose, CA 95126

F. This Contract shall be interpreted and governed by the laws of the State of California.

G. Any action arising out of this Contract shall be brought and maintained in Yolo County California, regardless of where else venue may lie.

H. In any action brought by either party to enforce the terms of this Contract, each party shall be bear responsibility for its attorney's fees and all costs regardless of whether one party is determined to be the prevailing party.

[Signatures on following page]

CITY OF WEST SACRAMENTO

By: _____
Aaron Laurel, City Manager

ATTEST:

By: _____
Kryss Rankin, City Clerk

APPROVED AS TO FORM:

By: _____
Jeffrey Mitchell, City Attorney

Downtown Streets, Inc.

By: _____
Title:

EXHIBIT A

PROPOSAL

West Sacramento Team

Downtown Streets Team (DST) proposes bringing its best practice work experience model to West Sacramento in 2018 to provide a pathway to homes/jobs for Team Members, beautify the community, engage business owners and publicly address homelessness with the greater community.

Who: DST is a volunteer work experience model in which unhoused Team Members beautify their community in exchange for case management, employment services and a basic needs stipend. Through their volunteer work, Team Members rebuild effective work habits, take on leadership opportunities, further their education and ready themselves to reenter the workforce and housing through the support of a positive community. It's a win for the community, a win for the environment, and a win for the increased self-sufficiency of the Team Members. DST has placed over 700 individuals into employment (lasting at least 90 days) and almost 800 into permanent housing. One person at a time, DST is ending homelessness through the dignity of work.

Program: We envision a standing Team of 15 volunteers (serving at least 60 individuals in our heavy touch manner) to supplement existing cleaning initiatives undertaken by the City of West Sacramento. Team Members will pick-up litter, needles and encampment debris in the project area while outreaching to the current residents. The Team will provide work readiness training, Case Management and Employment Services to all Team Members. In addition, DST will host a weekly success meeting to serve as a consistent check-in point for existing Team Members, an entry point for people interested in joining the Team and offer a venue to provide housing resources and

job leads. As with every other Downtown Streets Team, the West Sacramento Team will work with existing housing and shelter providers, other service providers (medical, legal, etc.), employers and local government agencies to remove barriers to self-sufficiency for every Team Member. Lastly, the Team will work to increase the cooperation of encamped residents with project partners such as Public Works. The Team will also hand out bags to encampment dwellers, pick up used needles and monitor/service trash bins in conjunction with the City.

Community Benefit: DST has proven to provide:

- Cleaner/safer streets, communities, encampments and natural environments
- A cost-effective solution to homelessness with savings to local government
- An individualized case management model and gateway program for the "service resistant"
- Immediate results
- An intervention with positive behavioral changes
- A highly visible model which provides good PR for the unhoused community and project partners

Impact and Methodology: The project will utilize DST's custom data impact system to measure human service results. This new system utilizes Clarity software, the industry leader in Homeless Management Information Systems (HMIS), which can integrate into existing HMIS data systems in West Sacramento. Furthermore, user data will be collected twice annually through an anonymous and optional feedback survey. The survey also captures qualitative data such as improved health, increased dignity and more positive interactions with the criminal justice system.

The West Sacramento Team will record and report on the amount of debris and needles removed from the project area, the results from the outside debris evaluations, the results of the user surveys, the number of Team Members housed and those employed 90 days or longer. We are happy to report on other outcomes desired by the City of West Sacramento per mutual agreement.

The project will secure 10 jobs of at least 90 days for participants. Furthermore, 8 will be placed into permanent housing.

West Sacramento Team Budget		
Line Item	**Annual Cost**	
Basic Needs Stipends	$	67,350.40
Supplies	$	6,000.00
Debris Removal		Leveraged
Vehicle Expenses (all in)	$	18,000.00
Marketing	$	1,200.00
Meeting Space and Occupancy	$	13,200.00
Clarity License(s)	$	600.00
Mileage	$	2,400.00
Computers and phones	$	2,100.00
0.6 Staff (PM, CM or ES)	$	48,000.00
Associated Staffing Costs	$	11,040.00
Professional Development	$	750.00
Management, Legal, Accounting and G&A	$	30,181.58
Total Cost of Program:	**$**	**200,821.98**
Total Amount to be Locally Raised:	**$**	**821.98**
Total Amount Requested from City:	**$**	**200,000.00**

Sustainability: DST is able to launch your Team within 30 days of finalizing a contract.

This proposal is meant to act as an initial representation of our shared intent. All matters of project area, schedule, days volunteered, number of Team Members, etc. are adjustable to meet the needs of the City of West Sacramento. We welcome any opportunity to discuss this project in greater detail.

Thank you for the opportunity!

Rachel Davidson
Project Manager • Sacramento
Phone: (408) 691-9943
Rachel@StreetsTeam.org

Addendum: DST staff will accept referrals from City of West Sacramento's Homeless Coordinator and work in collaboration on outreach efforts. Furthermore, Downtown Streets Team will be screening all new participants during intake to ensure their ties with the City of West Sacramento. City staff will approve of the assessment tool. Should a Team Member prove ineligible through the self-declaration, DST staff will not submit their basic needs voucher for reimbursement.

EXHIBIT B

SCOPE OF WORK

SCOPE OF SERVICES
West Sacramento DOWNTOWN STREETS TEAM

Downtown Streets Team (DST) is a non-profit organization that works to end homelessness through work readiness, case management and employment services. People experiencing homelessness volunteer to be a part of a DST work crew, picking up litter, needles and debris from encampments and outreaching to the community. Through this volunteer work, team members not only give back to the community but also rebuild effective work habits, take on leadership opportunities, further their education, and ready themselves for permanent employment.

Goals

The goals of the West Sacramento Downtown Street Team are to:

- Work with City of West Sacramento Homeless Services Coordinator to identify Street Team members to work in the community
- Clean up West Capital Avenue business corridor, River Walk Park/Trail, and identified encampments (see Attachment 1 for focus areas)
- Provide case management, employment services and on-going support to the Team Members
- Provide on-going communication on community work projects through community forums and to City staff/Council

Capacity and Timing

The West Sacramento DST will include 12-15 volunteer Team Members at any point in time, and at least 50 unique individuals over the course of the contract. DST will launch this program by August 23, 2018.

Program Description and Services

Starting in September 2018, DST will stand up a Street Team of 12-15 volunteers (serving at least 50 individuals in our heavy touch manner in the pilot period) to supplement existing cleaning initiatives undertaken by the City of West Sacramento. Team Members will pick-up litter, needles and encampment debris in the project area while outreaching to the current homeless residents. The Team will provide work readiness training, Case Management and Employment Services to all Team Members and outreach services to those living in and around the shelter.

In addition, DST will host a weekly success meeting to serve as a consistent check-in point for existing Team Members, an entry point for people interested in joining the Team and offer a

venue to provide housing resources and job leads to Team Members. The West Sacramento Team will work with existing housing and shelter providers, other service providers (medical, legal, etc.), employers and local government agencies to remove barriers to self-sufficiency for every Team Member. Lastly, the Team will work to increase the cooperation of encamped residents with project partners such as City Police, the Parks Department, and the City's Public Works Department. The Team will also hand out trash bags to encampment dwellers, pick up used needles and monitor/service trash bins in conjunction with the City. This will create a safer and cleaner environment in terms of debris, human waste, and transmittable diseases (e.g. Hep A).

DST operates in four-hour shifts. Cleanup crews will work up to five days a week in the morning. Cleanup crews can be added by the City's discretion in the afternoon hours or on weekends. Downtown Streets Team operates 50 weeks out of the year. Team Members do not volunteer in inclement weather because of the health risk inherent due to lack of housing. Team Members receive their stipend for meeting with supportive service staff, attending interviews, trainings and other employment readiness activities. They also do not volunteer during observed holidays or during periods of inclement weather. A list of these holidays can be provided to funding agencies and partners.

Identification of Projects and Community Engagement

The DST Team(s) will concentrate efforts around highly blighted neighborhoods surrounding the West Capital Avenue business corridor, River Walk Park/Trail, and encampments designated by the City's Homeless Services Coordinator. Cleanup efforts will be directed by City staff and focused on public areas impacted by unhoused residents.

Program Referral Process

DST gains most of its new Team Members through its peer to peer outreach model; this means that while Team Members are on shift they will interact with others, provide information about DST and how to access the program. This has been found to be an incredibly successful form of outreach. In addition, our Case Manager will be present at Open Office Hours with West Sacramento's Homeless Coordinator and other local service providers on Thursdays, and we will work to form partner relationships with other agencies in the area, so they can lead clients to our program, as well.

The first step for someone is to attend our weekly Team Success meetings, which will be held on Thursdays immediately following, and in the same room, where the West Sacramento Homeless Coordinator holds his weekly open office hours. The DST Case Manager will be in attendance during the West Sacramento Homeless Coordinator's open office hours, and the West Sacramento Homeless Coordinator will be in attendance at every Team Success meeting. Having

the West Sacramento Homeless Coordinator at the weekly Team Success meetings will ensure that folks attending the meetings can also begin engagement with the Homeless Coordinator, if they haven't already, as well as to gain the West Sacramento Homeless Coordinator's valuable knowledge regarding the criteria set in our Intake form. We know that homelessness can strike rapidly for many people living paycheck to paycheck, and these people often have little to no prior engagement with law enforcement or other service providers.

All interested individuals wanting to participate in the program will be required to fill out an intake form (Attachment 2) that will demonstrate their connection with West Sacramento. Applicants must be affiliated to West Sacramento in one of the following ways in order to be eligible to participate in the program:

- Currently receiving housing/shelter/outreach/food services in West Sacramento
- Currently "couch surfing" or staying at a friend/family member's house in West Sacramento AND have been for at least one month
- Last permanent address was in West Sacramento
- Staying in an encampment in West Sacramento AND have been for at least one month
- Been unhoused, living in West Sacramento for more than one year

Prior to an applicant's acceptance into the program, the City's Homeless Coordinator will review the intake form to confirm, to the best of his/her ability, the reported connection to West Sacramento. This confirmation process will consist of, but not be limited to, a mandatory interview with the applicant to evaluate the details of the reported connection to West Sacramento. At the end of the interview, the Homeless Coordinator will be required to sign-off on the intake form, verifying that he/she has conducted the interview and has confirmed the applicant's connection to West Sacramento.

Data Collection and Outcomes

DST will utilize Clarity, a custom data impact system, to measure human service results and will join the City in using the City's Outreach Grid system to geo-locate camps and coordinate with City staff. User data will also be collected annually (January-February) through an anonymous and optional feedback survey that will capture qualitative data such as improved health, increased dignity and more positive interactions with the criminal justice system.

In addition, DST will survey businesses from within the focus area prior to program launch and again six months into the program to collect qualitative data pertaining to the impacts of homelessness and the effects of DST's efforts.

DST will provide a monthly report to the City on the outcomes of the Downtown Streets Team that will include the following information:

a) Number of Team Members who have moved into housing, employment, or gained shelter
b) Number of volunteer hours performed by Team Members
c) Number of active Team Members
d) Number of people on the waitlist
e) Amount of debris removed from each project area
f) Number of needles removed from each project area
g) Snapshot of monthly highlights such as new partner relationships with service providers, and any planned initiatives/events

Data collected by DST and data collected by Outreach Grid will be integrated to support on-going communication with the community and City Council on the outcomes of the program's efforts. Specifically, DST will utilize Outreach Grid's ability to track and report on current encampment locations, provide data regarding the cleanup activities associated with the encampments, and communicate with City staff and PD regarding areas cleaned or needing to be cleaned. In addition, throughout the contract term, DST will collaborate with City and Outreach Grid staff to expand the utility of the software to help further its intended objectives. This collaboration will include, but not be limited to, monthly check-in calls to provide staff with feedback regarding the software's performance as it relates to DST's work in West Sacramento.

The West Sacramento Team will work with the City and the community to establish other metrics as appropriate.

Monitoring and Evaluation

The City may at any time, evaluate this Program. Adequate notice shall be given to DST of such action, and DST shall be given opportunities to participate and respond in the evaluation process.

Attachment 1

Focus Area Map

Attachment 2

Intake Form

EXHIBIT C

SCHEDULE OF PERFORMANCE

DST's services shall commence on July 18, 2018 and end on July 18, 2019 and shall be carried out as prescribed within DST's Scope of Work.

Date	**Task**
July 18, 2018	Contract is effective
August 23, 2018- August 24, 2018	Local business survey is distributed and results collected
August 2, 2018	DST Case Manager (CM) begins attending open office hours (this will be a weekly ongoing activity)
August 30, 2018	Informational Meeting
September 6, 2018	First Official Team Success Meeting-Welcome first West Sac Team Members
September 10, 2018	Clean-up crew begins work
September 10, 2018- July 18, 2019	Clean-up crew work in effect

EXHIBIT D

CERTIFICATE OF COMPLIANCE WITH LABOR CODE § 3700
[Labor Code § 1861]

I am aware of the provisions of Section 3700 of the Labor Code which require every employer to be insured against liability for workers' compensation or to undertake self-insurance in accordance with the provisions of that code, and I will comply with such provisions before commencing the performance of the work of this contract.

Downtown Streets, Inc.

By: _____
Title:

Attachment 2

RESOLUTION 18-53

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF WEST SACRAMENTO APPROVING AN APPROPRIATION FROM MEASURE E FUND, FUND 110, TO FUND A WORK EXPERIENCE AND CASE MANAGEMENT PROGRAM FOR INDIVIDUALS EXPERIENCING OR AT RISK OF EXPERIENCING HOMELESSNESS

WHEREAS, the City Council adopted the Measure E policy document on September 20, 2017, which outlines the intended uses of the Measure E funding, such as reducing homelessness and its community impacts; and

WHEREAS, the City has identified a programmatic activity that aligns with the intended uses of the Measure E fund to reduce homelessness and its community impacts, and

WHEREAS, the City will enter into a one-year contract with Downtown Streets, Inc. to provide such activities at a one-time cost of $200,000; and

WHEREAS, the available balance in the Measure E Fund is projected at $2,400,000, taking into account appropriations already approved for the fiscal year, and is sufficient to cover the one-time cost of the proposed contract; and

WHEREAS, the City Council has considered all information related to this matter, as presented at the public meetings of the City Council identified herein, including any supporting reports by City staff, and any other information provided during public meetings.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of West Sacramento as follows:

1. The City Council hereby approves an appropriation from the current fund balance in the Measure E Fund as listed below.

SOURCES

$200,000.00 Measure E: Fund Balance (110-0000-3110)

USES

$200,000.00 Measure E: Homeless Services (110-9014-5530/110-52073-5800)

2. The City Council hereby finds that the facts set forth in the recitals to this Resolution are true and correct and establish the factual basis for the City Council's adoption of this Resolution.

3. This Resolution shall take effect immediately upon its adoption.

PASSED AND ADOPTED by the West Sacramento City Council this 22nd day of August 2018, by the following vote:

AYES:
NOES:
ABSENT:

Christopher L. Cabaldon, Mayor

ATTEST:

Kryss Rankin, City Clerk

Program Budget

West Sacramento Team Budget		
Line Item	**Annual Cost**	
Basic Needs Stipends	$	67,350.40
Supplies	$	6,000.00
Debris Removal		Leveraged
Vehicle Expenses (all in)	$	18,000.00
Marketing	$	1,200.00
Meeting Space and Occupancy	$	13,200.00
Clarity License(s)	$	600.00
Mileage	$	2,400.00
Computers and phones	$	2,100.00
0.6 Staff (PM, CM or ES)	$	48,000.00
Associated Staffing Costs	$	11,040.00
Professional Development	$	750.00
Management, Legal, Accounting and G&A	$	30,181.58
Total Cost of Program:	**$**	**200,821.98**
Total Amount Locally Raised by DST*:	**$**	**821.98**
Total Amount Requested from City:	**$**	**200,000.00**

*Downtown Streets, Inc. will be responsible for for the additional costs in excess of the $200,000 funded by the City.

Focus Area Map

Measure E Analysis

Project Description

This requested expenditure is to fund a contract with Downtown Streets, Inc. (DST) that would provide a work experience and case management program for individuals experiencing or at risk of experiencing homelessness. Under the program, homeless participants become part of a "team" that carries out community beautification activities in exchange for receiving case management (counseling and housing navigation services), employment assistance services, and financial rewards for basic needs in the form of rent stipends or gift cards. The program would serve at least 50 individuals in this manner to supplement existing cleaning initiatives undertaken by the City of West Sacramento.

Policy Context

"Homelessness and Community Impacts" is a Top Priority Policy Agenda item from the Council's 2018 Strategic Plan. Additionally, voter-approved Measure E specifically listed "reducing community impacts of homelessness" as one of its programmatic uses.

Supplementary Funds

The proposed expenditure is for a pilot program that would be fully funded by the City. This effort will, however, create fiscal as well as other benefits by supplementing the City's existing public space cleaning initiatives and by providing the City's Homeless Coordinator with an additional resource to help West Sacramento residents escape homelessness. While difficult to quantify, the project is also expected to create a soft cost savings to the Police Department by reducing the amount of time spent on homeless-related enforcement activities. Additionally, part of the scope for this project is for Downtown Streets to collaborate with staff to explore long-term funding options that would reduce or eliminate the need for Measure E funding should the City wish to proceed with the effort beyond the initial year. Options to be explored include the potential creation of a Property-Based Improvement District (PBID) that would allow businesses to help fund this effort.

Staff Costs

The proposed Measure E expenditure will not be used to cover any City staff costs. The funds will be used to pay costs associated with the program, with the majority of the costs being related to DST's staff costs and the basic needs stipends provided to program participants.

Desired Outcome

The outcomes from this proposed Measure E expenditure are to reduce the incidence of homelessness its community impacts. The program will provide case management services to all of its team members, which will consist of 12-15 members at any point in time, and at least 50 unique individuals over the course of the contract. In addition, the program will seek to secure 10 jobs of at least 90 days for participants and place eight into permanent housing; and clean up West Capital Ave. business corridor, River Walk Park/Trail, and identified encampments.

Theory of Change

The proposal's outcomes are premised on two key assumptions: (1) DST's program model will directly promote positive behavioral changes for participants by providing employment readiness training and an outlet for productive daytime activity for its homeless participants; (2) an increase in positive behavioral changes will further encourage participants to rebuild effective work habits, take on leadership opportunities, further their education, and ready themselves to reenter the workforce and housing through the support of a positive community.

There are two key risks to this vision: (1) DST fails to secure enough willing participants for the program, and (2) participants remain unresponsive to the increase in positive behavior changes.

To mitigate the first risk, the contract is structured such that participants have multiple entry points to the program. These entry points include: DST's peer-to-peer outreach model, which means while Team Members are on shift they will interact with others, provide information about DST and how to access the program; referrals from the West Sacramento Homeless Coordinator; and referrals from partner relationships with other agencies in the City. The following factors mitigate the second risk: (1) homeless persons who refuse services and camp illegally or engage in other illegal behaviors will be subject to enforcement actions from the Police Department; and (2) if in the City's sole discretion Downtown Streets' efforts are not producing the intended result, the contract may be terminated with 30 days notice.

Project Budget

Total cost for the contract with Downtown Streets, Inc. is $200,000 for a one-year pilot term that would provide sufficient time to evaluate the program's effectiveness while allowing staff to implement a longer-term funding strategy should the Council want the program to continue. For a breakdown of costs, please see the program budget below.

West Sacramento Team Budget		
Line Item	**Annual Cost**	
Basic Needs Stipends	$	67,350.40
Supplies	$	6,000.00
Debris Removal		Leveraged
Vehicle Expenses (all in)	$	18,000.00
Marketing	$	1,200.00
Meeting Space and Occupancy	$	13,200.00
Clarity License(s)	$	600.00
Mileage	$	2,400.00
Computers and phones	$	2,100.00
0.6 Staff (PM, CM or ES)	$	48,000.00
Associated Staffing Costs	$	11,040.00
Professional Development	$	750.00
Management, Legal, Accounting and G&A	$	30,181.58
Total Cost of Program:	**$**	**200,821.98**
Amount to be Locally Raised by DST:	**$**	**821.98**
Total Amount Requested from City:	**$**	**200,000.00**

Performance Metrics

DST will utilize Clarity, a custom data impact system, to measure human service results and will join the City in using the City's Outreach Grid system to geo-locate camps and coordinate with City staff. User data will also be collected annually (January-February) through an anonymous and optional feedback survey that will capture qualitative data such as improved health, increased dignity and more positive interactions with the criminal justice system.

Staff also plans to work with DST to survey businesses within the focus area prior to the launch of the program, and then again six months into the program. This pre-test/post-test model will attempt to capture business owners' perceptions on the impact of homelessness on local businesses and impacts of the DST program after six months of operation in that area. To augment this analysis, staff will work with ZenCity to analyze social media sentiment related to

homelessness before, and during DST's contract term with the goal of assessing whether DST's work is having a detectable effect on public perceptions of homelessness and the safety of the City's public spaces.

Finally, DST will provide a monthly report to the City on the outcomes of the Downtown Streets Team that will include the following information:

- Number of Team Members who have moved into housing, employment, or gained shelter
- Number of volunteer hours performed by Team Members
- Number of active Team Members
- Number of people on the waitlist
- Amount of debris removed from each project area
- Number of needles removed from each project area
- Snapshot of monthly highlights such as new partner relationships with service providers, and any planned initiatives/events

Collectively, staff believes the analyses summarized above will provide the Council with ample information to review the proposed program's performance and assess whether to extend this contract beyond the pilot period.

CITY COUNCIL **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 20

SUBJECT:

PRESENTATION AND DISCUSSION ON THE 2018 PAVEMENT MANAGEMENT SYSTEM UPDATE

INITIATED OR REQUESTED BY:

[ ] **Commission** [x] **Staff**

[ ] **Other**

REPORT COORDINATED OR PREPARED BY:

Vin Cay, Supervising Civil Engineer
Jesse Khatkar, Assistant Engineer

Denix Anbiah, Director
Public Works Department

ATTACHMENT [X] **Yes** [ ] **No** [X] **Information** [ ] **Direction** [ ] **Action**

OBJECTIVE

The purpose of this report is to present findings on 2018 Pavement Management System Update and obtain Council direction and policy guidance given the limited available funding relative to the projected costs to maintain City roads.

RECOMMENDED ACTION

Staff respectfully recommends that the City Council receive the presentation on the 2018 Pavement Management System Update and provide comments and policy direction to prepare a multi-year pavement rehabilitation program

BACKGROUND

West Sacramento first established a Pavement Management System (PMS) in 1991 to help identify, track, and prioritize roadway maintenance, rehabilitation and pavement preservation treatments. Additional updates to the PMS were completed in 1999, 2005 and 2013. Over the years, the scope of street pavement rehabilitation projects has expanded as mandated by regulation to include improvements related to ADA compliance and complete streets emphasizing better bicycle and pedestrian facilities. Further, an up-to-date PMS is now required to be eligible for federal and state grants for road repair and preservation.

The PMS update contains information necessary to understand the City's current and long term needs relative to maintenance, rehabilitation and preservation. It details both the short term and long-term costs associated with implementing the PMS program. On October 4, 2017, Council approved a $50,000 allocation of Measure E to go with the $50,000 SB1 funding for this project.

Implementation of the PMS has guided the City in its prioritization of much needed road maintenance, rehabilitation and preservation and has highlighted the need for additional funding to properly maintain the City's roadways. In the last PMS update in 2013, the average Pavement Condition Index (PCI) of the City streets was generally better than the remainder of Yolo County. More recently, the City has been receiving numerous pothole complaints and experiencing pavement failures on a variety of its streets.

As in most localities, the City has three basic needs relative to pavement management: maintenance, rehabilitation, and preservation.

> Maintenance: There are two types of maintenance: "reactive" maintenance and "planned" maintenance. Examples of reactive maintenance include the filling of a pothole with "cold-mix" asphalt, reinstalling a downed traffic sign, removal of graffiti, street sweeping, traffic signal maintenance, and unplugging of storm drain grates. This type of maintenance is typically performed by maintenance crews and funded by gas taxes and Measure K. As the City has grown, this type of maintenance has increased while the funding has generally decreased. Gas taxes have been decreasing over the past ten years as a result of decreased driving, more fuel-efficient cars, and the introduction of hybrid, electric and fuel-cell vehicles. In addition, the allocation of Measure K funds has not kept up with the estimated funding range called for in City Resolution 2-58 according to the June 27, 2018, Measure K Report to the County Council. Examples of planned maintenance include work performed by contractors for items such as deep grinding and patching with "hot-mix" asphalt repairs of failing pavement in select locations and major striping along

heavily travelled corridors separate from other capital projects. Planned maintenance work is generally intended to be funded through the $250,000 received annually from Waste Management as mitigation for the damage its trucks cause to City streets. This work is typically done through the public bidding of smaller construction contracts similar to the 2018 Pavement and Striping Repair Project.

- Preservation: The National Center for Pavement Preservation defines pavement preservation as "a cost-effective set of practices that extend pavement life and improve safety and motorist satisfaction while saving public tax dollars." Essentially, a pavement preservation program seeks to create a system of implementing relatively small-scale projects on roads such as slurry seals, chip seals, and thin overlays on a specific schedule prior to structural degradation to avoid the necessity of full blown road reconstruction. Successful pavement preservation can result in long term savings by reducing higher cost reconstruction needs, reduced traffic congestion with smoother ride-ability, and safer roads when combined with regular refreshing of pavement delineation. Pavement preservation should be accepted as good City philosophy, an important priority, and worthy long-term commitment.

- Rehabilitation: Prior to Measure E and SB1, there basically were no funds available for major rehabilitation and preservation of roadways. Measure K (Capital) funding was used in the past for this purpose. However, that revenue stream ended. Rehabilitation of roads is generally defined as doing whatever is necessary to bring the road up to current standards for pavement and items such as construction of curb, gutter and sidewalk. It can include the complete removal of pavement, base and subgrade if necessary in certain areas of pavement failure or it can be as simple as a pavement overlay in areas where the pavement hasn't failed. An example of rehabilitation would be the upcoming West Capitol Avenue Road Rehabilitation and Safety Enhancement Project.

Between Measure E and SB1, the City is currently on track to receive $1.7 million annually for both pavement rehabilitation and preservation. Although these funds will be used to leverage a variety of grant funding sources, the total funding is low compared to the projected need.

The PMS Update project is intended to include a two-step engagement. After receiving Council direction tonight, staff will return in the fall to present a program of potential projects for funding consideration.

ANALYSIS

The City is responsible for the repair and maintenance of 437 lane miles of paved streets. The overall replacement cost can be compared to an investment portfolio valued at approximately $276.5 million. This can be viewed as the value of the City's pavement network, and is the amount needed to fund the full reconstruction of the network. This amount does not include related infrastructure assets, such as sidewalks, signals, markings, signs, etc.

The road network is maintained by six full time employees. One full-time position is dedicated to traffic signal operations and maintenance, two positions are dedicated to street sweeping, the remaining three positions are responsible for re-installation of downed traffic signs, removal of graffiti, filling potholes with temporary "cold-mix" asphalt-- and in the past have been responsible for crack sealing and repairing potholes with "hot-mix" asphalt. With the current level of staff, the road maintenance crew is not adequately staffed to perform these tasks which requires a minimum crew of five members.

The condition of the roadway network was assessed by data collection vehicle equipped with real time automated 3D digital imaging and laser scanners to collect various pavement distresses. These distresses include the amount of cracking, rutting, base failures, road surface roughness, etc. The information was then checked by a pavement expert within the vehicle during data collection.

Staff recently received the results of the results of the pavement condition survey. The overall network Pavement Condition Index (PCI) is 60 (Attachment 1). This is a decline from a PCI of 74 in 2013 and 84 in 2005. In general terms the new PCI rating, which is an average of all the roads in the network, can be characterized as FAIR and is summarized below:

		Current Distribution	2013 Distribution	Percent Change
PCI: 100	NEW	-	-	-
PCI: 70 or above	GOOD	36.5%	72.7%	-36.2%
PCI: 69 to 50	FAIR	33.0%	15.0%	+18.0%
PCI: 49 to 25	POOR	25.6%	9.1%	+16.5%
PCI: 25 to 0	FAILED	4.9%	3.2%	+1.7%

From the above inventory distribution, 30.5% of the streets are assessed as in POOR or FAILED condition. These streets are located predominantly in the northern and older areas of the City but also include major arterials (e.g., Southport Parkway) and collector (e.g., Gateway Drive) roads throughout the City. The industrial parts of West Sacramento serve as a logistics hub and even the newer yet to be built-out Southport arterials are failing due to heavy vehicle traffic.

The updated maintenance need (also known as backlog) over the next 10 years is estimated at $121.3 million. This is approximately 44% of the replacement value of pavement within the City and does not include related infrastructure assets and soft costs which can add an additional 25 -30% to the backlog. For reference, the 2013 PMS update estimated a backlog to bring the pavement network in the City up to a "Good" level was $69 million.

Like most other jurisdictions, it is unrealistic for a City to fully fund the maintenance backlog to improve all roads. The identified funding includes Measure E($500,000/year), Waste Management Truck Mitigation($250,000/year) and SB 1 ($1.2 million/year) estimated at $20 million plus grants over the same 10-year period. There is, however, a voter referendum to repeal SB1 at this year's election in November which could reduce the funding level to $7.5 million plus grants. At this level of funding, the pavement network is projected to deteriorate further to an overall PCI of 43 in ten (10) years. Below is a summary of various funding levels and the projected pavement condition.

Annual Funding Level	10 Year Projected PCI
$12.2M	80
$8.4M	70
$6.0M	60
$2.0M	48
$0.75M	43

With the backlog far exceeding the available funding, staff is seeking policy guidance on how to prioritize future projects. Below are some potential options the Council may consider:

- Cost-effectiveness. This approach is based on maximizing the roadway surface area to improve and generally involves pavement preservation methods such as slurry sealing. However, this can only be utilized effectively on roads with PCI rating close to the GOOD condition. This also means that the worst roads will not be the first to be repaired.

- Economic Drivers. The economic vitality of the City is dependent on bringing in new and keeping existing businesses. These businesses rely on the roadway network to distribute, receive, and provide goods and services. Attachment 2 shows relative locations of top economic drivers throughout the City and the condition of the roadways surrounding them. The roadways that serve these businesses tend to be expensive to rehabilitate once they have deteriorated.

- Geographic Equity. The northern part of the City generally has roads that are older than the newer roads constructed with the subdivisions in Southport.

- Grant Eligibility. Only arterials and collector classifications and above are eligible for grant applications. Local neighborhood roads are expected to be maintained by local jurisdictions or the affiliated development. For example, the road rehabilitation in the Memorial Park/State Streets neighborhood is expected to rely on Measure E, SB1 or other local funds.

- Heavy Vehicle Traffic. Truck traffic places far more wear on certain roads and adjoining land use and should also be considered as these roads will fail more frequently leading to complaints.

- Traffic Volume. Parkways, arterials and collector street classifications serve more of the general public.

- Funding Sources. The revenue sources for road maintenance including from the gasoline tax, SB1, Measure E and Waste Management are insufficient. Other revenue sources including general revenue

such as Enhanced Infrastructure Financing Districts (EIFDs) may be considered to augment road maintenance. Senate Bill 1145 has been passed by the state legislature pending the Governor's signature. The bill would amend existing law to allow the use of EIFD revenues for maintenance purposes including road repair. Private investment via Property and Business Improvement Districts (PBIDs) may be an option with support from the business community. Existing Community Facility Districts (CFDs) created with the development of residential subdivisions do not contribute any funding to road maintenance. Future Development Agreements could be negotiated to include road funding.

With Council policy guidance, the project team will return with a strategy including a program of limited projects for consideration.

Environmental Considerations
N/A

Commission Recommendation
On July 10, staff presented the findings of the PMS Update and requested feedback and suggestions for policy considerations on how to address road maintenance given the very limited funding. The Commission criteria was mentioned for prioritization:

- Multi-modal travel, not just vehicular travel, re-stripe arterials and collectors for bikes, get people out of cars.
- Apply new pavement technology
- Road Safety
- Prioritize based on traffic volume to serve the most
- Explore PBIDs by partnering with private businesses for funding
- Seek funding support from general revenues as roadways affect almost every citizen
- Maintain and preserve newer roadways such as Village Parkway with cost effective treatments before more expensive repairs are needed.
- Consider impacts of bad roads to commerce and tax revenue which fund current and future City services

Strategic Plan Integration
The PMS Update project is consistent with the West Sacramento's *Mission of City Government that is Financially Sound* and its *Vision as a Vibrant City where you can Live, Work, Learn & Play* as well as support a *Strong, Diverse Local Economy* by providing *Quality Infrastructure*. A sustainable road funding strategy is needed to provide a well-functioning transportation infrastructure to keep the City's diversity of economy and varied tax base which directly impact economic development and business retention.

Alternatives
N/A

Coordination and Review
This report was prepared and coordinated with the Public Works Department and City Manager's Office.

Budget/Cost Impact
This report is an update for comment and direction at this time and actual budget impact is yet to be determined.

ATTACHMENT(S)

1. Draft Report - Pavement Management System Update
2. Economic Drivers – Top Employers, Sales and Property Tax Generators

Pavement Management Program Update
Draft Report

NCE Project No. 657.04.55
August 2018

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Section Description Inventory Report – Sorted by PCI (Descending)

Appendix B:
Maintenance and Rehabilitation Decision Tree

Appendix C:

Budget Needs – Projected PCI/Cost Summary Report
Budget Needs – Preventive Maintenance Treatment/Cost Summary Report
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Scenarios 1 – 4:
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Background

The City of West Sacramento selected Nichols Consulting Engineers, Chtd. (NCE) to update their StreetSaver Pavement Management Program (PMP) in 2018. Broadly, a *"… pavement management [program] system is designed to provide objective information and useful data for analysis so that … managers can make more consistent, cost-effective, and defensible decisions related to the preservation of a pavement network.[1]"* In other words, a PMP is designed to assist counties with answering questions such as:

- What does the City's pavement network consist of? How many miles of streets quality for federal or state funds?
- What is the existing condition of the pavement network? Is this an acceptable level for the City? If not, what is an acceptable level? How much will it cost to bring it up to an acceptable level?
- How will the condition of the pavement network respond over time under existing funding levels?
- What maintenance and rehabilitation (M&R) strategies exist to improve current pavement conditions?
- What maintenance activities or treatments have occurred in the past on any given street?
- What impact would either additional funding (e.g., SB1 funds) or decreased funding have on the condition of the overall pavement network?
- What is the backlog of needed M&R? What are the future M&R needs? Are there different needs for different classes of streets (i.e., arterials vs. residentials)?
- Under different funding levels, what is the most cost-effective way to implement a multi-year capital improvement program? Maintenance work program?

The City has utilized the StreetSaver PMP for more than five , and this report is based on the 2018 results.

[1] AASHTO "Guidelines for Pavement Management Systems". American Association of State Highway and Transportation Officials, Washington DC, July 1990.

Purpose

The purpose of this report is to assist decision makers at the City of West Sacramento in utilizing the results of the StreetSaver PMP. Specifically, this report assesses the adequacy of projected revenues to meet the following:

1) Implementing a multi-year street rehabilitation and maintenance program,
2) Developing a preventive maintenance program, and
3) Selecting streets with the most cost effective repairs.

This report examines the overall condition of the street network and highlights options for improving the current network's pavement condition index (PCI). The pavement condition index, or PCI, is a measurement of the pavement condition and ranges from 0 to 100. A newly constructed street will have a PCI of 100, while a failed street will have a PCI of 25 or less. Options for improving the overall network PCI are developed by conducting "what if" analyses. Varying the budget amounts available for pavement maintenance and repair shows how different funding strategies would affect the City's streets over the next 10 years.

Project Approach

Streets were ranked using the Metropolitan Transportation Commission (MTC) distress protocols[2]. In this update, all paved streets were inspected (approximately 185.4 centerline miles).

The inspections were completed in May 2018 and all distress data were entered into the StreetSaver database. Note that the condition inspections did not address non-pavement issues such as traffic, safety and road hazards, geometric issues, street shoulders, sidewalks, curb and gutters, drainage issues, or immediate maintenance needs.

Upon completion of the data collection activities, NCE reviewed M&R strategies with City staff. This included the selection of appropriate treatments such as surface seals or overlays, and the determination of unit costs. The unit costs are based on recent bid tabs from the surrounding agencies and include all related construction costs and engineering and design costs. Once appropriate M&R treatments were defined, the decision tree was updated.

NCE next performed a budget needs analysis using an analysis period of 10 years with an annual inflation rate of 3 percent. This identified M&R requirements for each street section and determined the total M&R requirements. Finally, four budgetary scenarios were analyzed for the pavement network.

[2] PCI Distress Identification Manuals (AC 4th Edition, PCC 3rd Edition). Metropolitan Transportation Commission, San Francisco, CA, March 2016.

Street Network Description

The City is responsible for the repair and maintenance of approximately 185.4 centerline miles of paved streets, or 1140 pavement sections. Table 1 below summarizes the pavement network by functional class.

Table 1: Network Summary Statistics for City Streets

Functional Class	Sections	Centerline Miles	Lane Miles	Percent Paved of the Entire Network (by Pavement Area)
Arterials	203	46.7	141.9	32.4
Collectors	139	28.5	61.7	16.4
Residentials/Locals	798	110.2	219.8	51.2
Gravel*	7	1.0	4.2	--
South River Road*	11	4.4	8.8	--
	1158	**190.8**	**436.4**	**100**

*Note: Gravel streets and portions of South River Road are not included in the analysis.

The network replacement cost of the maintained sections is approximately $276.5 million. This can be viewed as the value of the City's pavement network, and is the amount needed to fund the full reconstruction of the network. It does not include related infrastructure assets, such as sidewalks, signals, markings, signs, etc.

All network sections, their corresponding 2018 PCI, and other attribute data are provided in Appendix A. For convenience, there are two listings – one sorted by street name and the other sorted by descending PCI.

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Pavement Current Condition

Pavement condition is primarily affected by the climate, traffic loads and volumes, construction materials and age. The symptoms manifested by the pavement as it ages or fails include:

Asphalt concrete:

- Alligator (Fatigue) Cracking
- Block Cracking
- Distortions
- Longitudinal/Transverse Cracking
- Patching and Utility Cut
- Rutting and Depressions
- Raveling
- Weathering

Portland cement concrete:

- Corner Break
- Divided Slab
- Faulting
- Linear Cracking
- Patching or Utility Cut
- Scaling, Map Cracking and Crazing
- Spalling

The average 2018 PCI of the City's street network is 60 (in 2012, it was 74). Note that these values are projected and area-weighted calculations from StreetSaver. The average remaining service life (RSL) is estimated to be approximately 14 years for the streets (this is the time required for pavement to reach a "Very Poor/Failed" condition if no maintenance occurs).

In 2012, when the PMP was last implemented, the analyses showed that approximately $4.3 million per year was required to maintain the PCI at 74. However, due to lack of funding, the PCI has since deteriorated to 60.

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Figure 1 illustrates the definitions of the five pavement condition categories. Streets in "Good" condition include streets with both non-load-related (weathering or raveling) and load-related (e.g., alligator cracking) distresses. Since these distresses are markedly different, the treatments used to address these conditions are also different, as are the costs of these treatments. Generally, streets with load-related distress are more expensive to repair. The two categories of distress are identified by II (non-load-related) and III (load-related). StreetSaver® will assign the appropriate treatments and costs to streets identified within each category. Note that the "Maintenance and Rehabilitation Decision Tree" in Appendix B assigns different condition category labels than those in Figure 1 but the PCI ranges are equivalent.

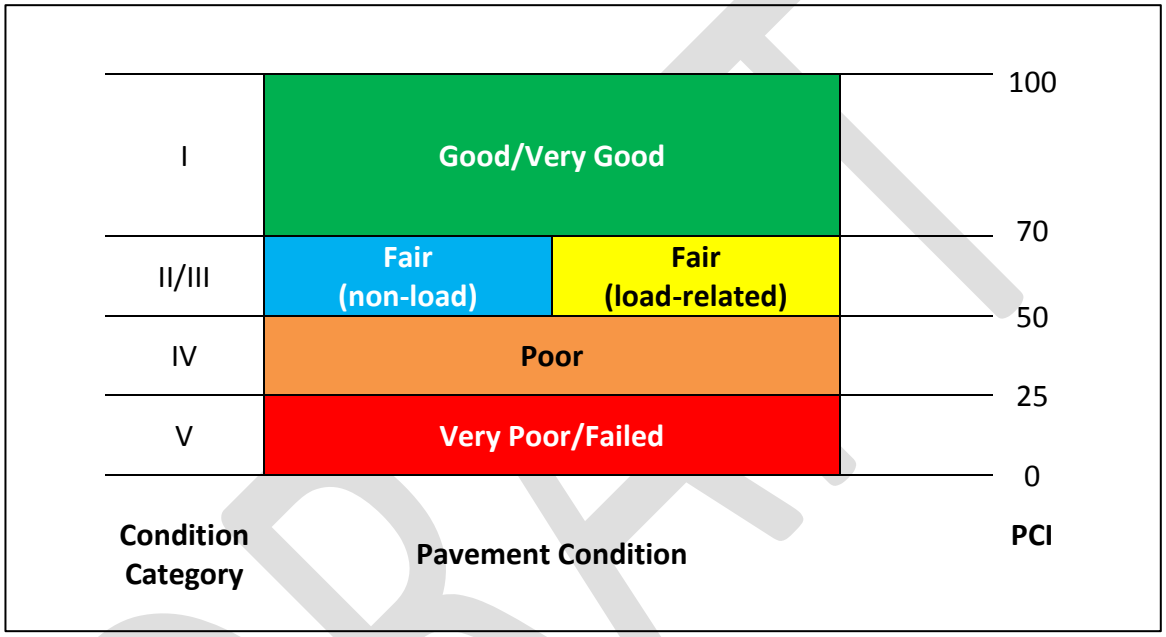

Figure 1: Pavement Condition Categories

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The photos in Figure 2 illustrate streets with a range of PCIs.

Figure 2: Examples of Streets with Different PCIs

Table 2, Figure 3 and Figure 4 detail the network by PCI ranges or condition category. As can be seen, only 37.1 percent (72.7 percent in 2012) of the City's streets are in the "Good/Very Good"" condition and nearly one third of the City's streets are in the "Poor" or "Very Poor/Failed" categories. Therefore, a high percentage of the network will require significant funding for repair. Overall, residentials are in better condition than both arterials and collectors.

Table 2: 2018 Pavement Condition Breakdowns by Area (Entire Network)

Condition Category	PCI Range	Arterials (%)	Collectors (%)	Residentials (%)	Entire Network (%)
Good/Very Good (I)	70-100	8.9	2.5	25.7	37.1
Fair (II/III)	50-69	11.9	6.7	14.1	32.7
Poor (IV)	25-49	9.4	6.9	9.3	25.6
Very Poor/Failed (V)	<25	2.2	0.3	2.1	4.6
Total		**32.4**	**16.4**	**51.2**	**100**

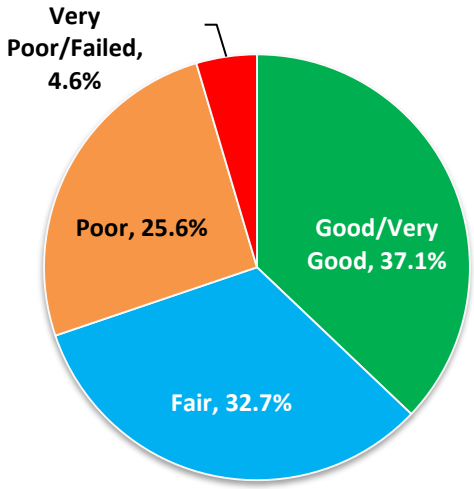

Figure 3: Pavement Condition Summary by Condition Categories (Entire Network by Area, 2018)

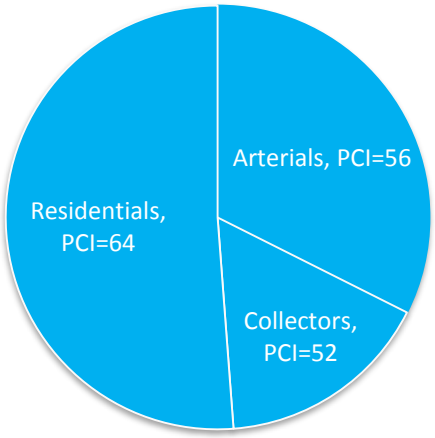

Figure 4: Pavement Condition Summary by Functional Classification (Entire Network by Area, 2018)

International Roughness Index (IRI)

The International Roughness Index (IRI) is one of the most common roughness indices used in evaluating pavement profile and ride quality. The IRI values are determined by the accumulated response from a quarter-car model as shown in Figure 5.

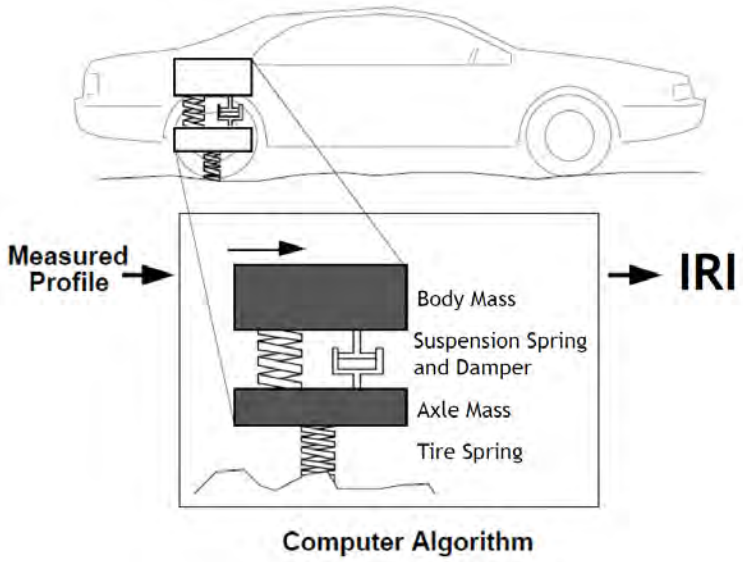

Figure 5: Quarter-Car Model for Defining IRI[3]

The FHWA has defined IRI categories of "Good", "Fair" and "Poor" as shown in Table 3. Using this classification, the majority of the streets in the City fall into the "Poor" category.

Table 3: International Roughness Thresholds in MAP-21

Condition Category	Good	Fair	Poor
IRI (inch/mile)	<95	95-170	>170

*MAP-21: Moving Ahead for Progress in the 21st Century

However, it should be noted that profile measurement devices were developed for highway applications, and have difficulty collecting accurate data at slow speeds, which can cause problems in urban areas with traffic lights or stop signs. This is especially true for streets in residential areas with cul-de-sacs. Therefore, the results should be used with caution. Note too that there are 85 sections with no IRI data available due to the above limitations. Appendix E contains an IRI map of the street network.

[3]Quarter-Car Model, The Little Book of Profiling, Sayer and Karamihas, 1998.

Maintenance and Rehabilitation Strategies

Historically, the City has utilized slurry and cape seals and Hot Mix Asphalt (HMA) overlays as M&R strategies. Seals are usually utilized as a preventive maintenance technique when the pavements are in "Good/Very Good" condition or above. As the pavement condition deteriorates, cape seals and HMA overlays are applied. Digouts are typically used as preparation prior to overlays and surface seals as necessary. These pavement treatments are formalized in the M&R decision tree shown in Appendix B.

Figure 6 below demonstrates that pavement maintenance follows the old colloquial saying of "pay me now, or pay me more later". History has shown that it costs much less to maintain streets in "Good/Very Good" condition than to repair streets that have failed. For example, applying a slurry seal to an arterial street may cost $6.00 per square yard (SY); if allowed to deteriorate, costs may rise to $31.00/SY to overlay and $77.00/SY for surface reconstruction or Full Depth Reclamation (FDR). In other words, delays in repairs can result in construction costs increasing as much as 12 times. Appendix B shows the detailed decision tree and the unit costs associated with each type of treatment.

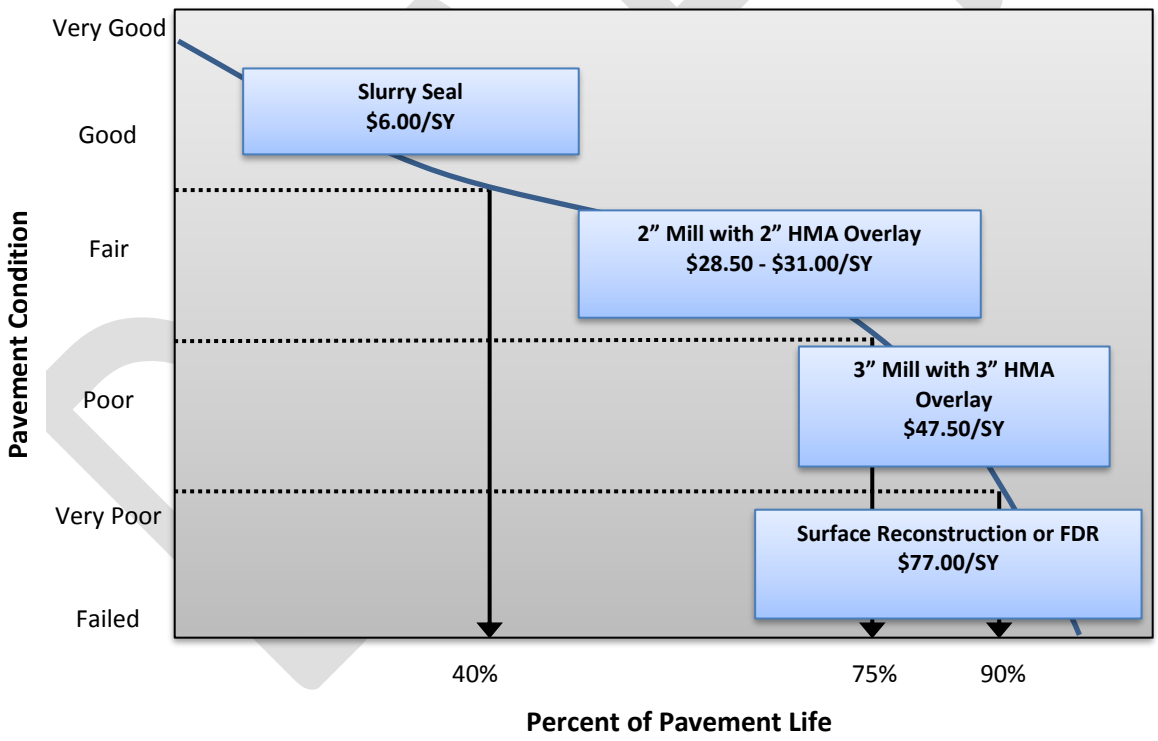

Figure 6: Costs of Maintaining Pavements over Time

Budget Needs

Based on the principle that it costs less to maintain streets in good condition than those in bad condition, the PMP strives to develop a maintenance strategy that will improve the overall condition of the network to an optimal PCI and then sustain it at that level. By not addressing the maintenance needs, the quality of the street network will inevitably decline. In order to correct these deficiencies, a cost-effective funding and maintenance strategy must be implemented.

The first step in developing a cost-effective M&R strategy is to determine the maintenance "needs" of the pavement network. Using the StreetSaver budget needs module with an inflation rate of 3 percent, the maintenance needs over the next 10 years were estimated at approximately $121.3 million for the entire network. If the City follows the strategy recommended by the program, the average network PCI will be in the 80s. If, however, no maintenance is applied over the next 10 years, already distressed streets will continue to deteriorate, and the network PCI will drop to 36 by 2027 (Table 4).

Table 4: Summary Results from Needs Analysis

Year	2018	2019	2020	2021	2022	2023	2024	2025	2026	2027	Total
PCI Treated	91	85	83	82	84	82	81	82	85	82	
PCI Untreated	60	58	55	53	50	47	44	41	39	36	
Needs ($Millions)	68.6	3.7	3.2	0.8	6.8	7.7	5	9.4	10.7	5.4	121.3

The results of the budget needs analysis represent the ideal funding strategy recommended by StreetSaver. Of the $121.3 million in maintenance needs shown, approximately $25.2 million (21 percent) is earmarked for preventive maintenance, while the majority, $95.8 million (79 percent), is allocated for more costly rehabilitation and reconstruction.

It should be noted that the prediction models in StreetSaver may result in a more conservative performance because the impacts of newer and more cost-effective technologies may not be included. For example, if improved materials are utilized (e.g., asphalt-binders with rubber or polymers), the actual performance of these treatments may be under-stated by the models. However, if the City continues to assess the pavement conditions regularly, the prediction accuracy of future conditions will continue to improve.

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Budget Scenarios

Once maintenance needs are assessed, the next step in developing a cost-effective M&R strategy is to conduct several "what-if" analyses using StreetSaver's budget scenario module. The program projects the effects of the different scenarios on PCI, deferred maintenance (unfunded backlog), and average RSL of the network. By examining the effects on these indicators, the advantages and disadvantages of different funding levels and maintenance strategies become clear.

Scenario 1: Current Funding Levels with SB1 ($2 Million/Year)– Based on the City's annual funding of $2 million per year for the next 10 years, the overall PCI will decrease to 48. The deferred maintenance is predicted to increase to $133.3 million by 2027.

Scenario 2: Maintain Current PCI ($6 Million/Year)– This scenario aims to ensure that the overall pavement network PCI does not drop below 60 over the next 10 years, an annual budget of $6 million is required. The deferred maintenance would increase from $68.6 million to $91.3 million by 2027.

Scenario 3: Increase PCI to 70 ($8.4 Million/Year) – This scenario seeks to improve the network PCI to 70 in 10 years by considering arterials and collectors at the highest priority. An annual budget of $8.4 million is required; the deferred maintenance would decrease to $62.4 million by 2027.

Scenario 4: Increase PCI to 70 ($8.4 Million/Year) – This scenario seeks to improve the network PCI to 70 in 10 years by considering residentials at the highest priority. An annual budget of $8.4 million is required; the deferred maintenance would increase to $72.7 million by 2027.

Note: The term "deferred maintenance" consists of pavement maintenance that is needed, but cannot be performed due to lack of funding. The deferred maintenance for the City is currently $68.6 million. Shrinking budgets have forced many cities and counties to defer much needed pavement maintenance. By deferring maintenance, the frequency of citizens' complaints about the condition of the network increases and the cost to repair these streets rises as well. More detailed results of the budget needs and scenarios are included in Appendix C.

Appendix E contains maps that illustrate the results of each scenario. The maps show the pavement network, highlighting the color-coded condition category of each pavement section in 2027 for Scenarios 1, 2, 3 and 4. A map illustrating the current condition is also provided for comparison.

Scenario 1: City Current Funding with SB1($2 Million/Year)

This scenario determines the impacts of the City's existing funding of $2.0 million per year for the next 10 years (70% of the budget allocated to arterials and collectors). The results indicate that the network PCI would drop to 48 by 2027 and approximately 45.9 percent of the network would be in the "Poor" and "Very Poor" conditions. The deferred maintenance would increase to $133.3 million by 2027, and the projected remaining service life (RSL) would be 12 years (see Table 5 and Figure 7). Appendix D provides a list of candidate sections selected for treatment in this scenario.

Table 5: Summary Results for Scenario 1

Year	2018	2019	2020	2021	2022	2023	2024	2025	2026	2027	Total
Budget ($Millions)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	20.0
Deferred Maintenance ($ Millions)	66.6	72.2	78.8	85.7	94.2	104.1	110.9	117.7	124.4	133.3	
Network PCI	64	61	60	58	57	55	53	51	50	48	
Remaining Service Life (Years)	16	15	15	14	14	13	13	13	12	12	

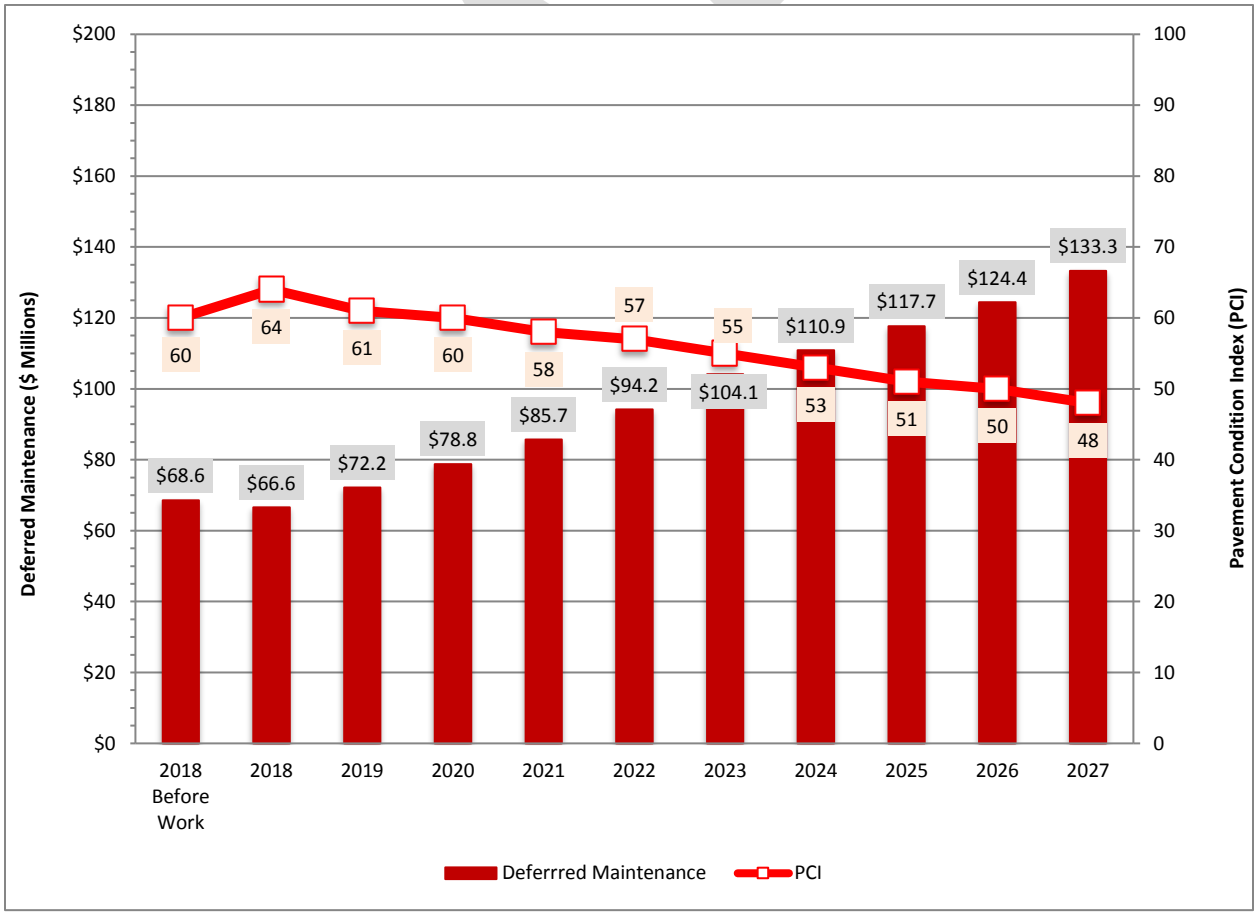

Figure 7: PCI vs Deferred Maintenance for Scenario 1

Scenario 2: Maintain Current PCI ($6 Million/Year)

This scenario indicates that approximately $6 million per year is required to maintain the overall PCI at no less than 60 over 10 years. By 2027, 72.8 percent of the network would be in in the "Good/Very Good" or "Fair" condition categories. The deferred maintenance would grow to $91.3 million by 2027, and the RSL of the overall network would be 19 years (see Table 6 and Figure 8).

Table 6: Summary Results for Scenario 2

Year	2018	2019	2020	2021	2022	2023	2024	2025	2026	2027	Total
Budget ($Millions)	1.0	2.0	4.0	6.0	7.0	7.5	8.5	8.5	8.5	7.0	60.0
Deferred Maint. ($ Millions)	67.6	73.2	77.9	81.0	83.4	87.4	88.3	88.7	87.9	91.3	--
Network PCI	63	61	60	60	60	60	60	60	60	60	--
Remaining Service Life (Years)	15	15	15	16	16	17	17	18	18	19	--

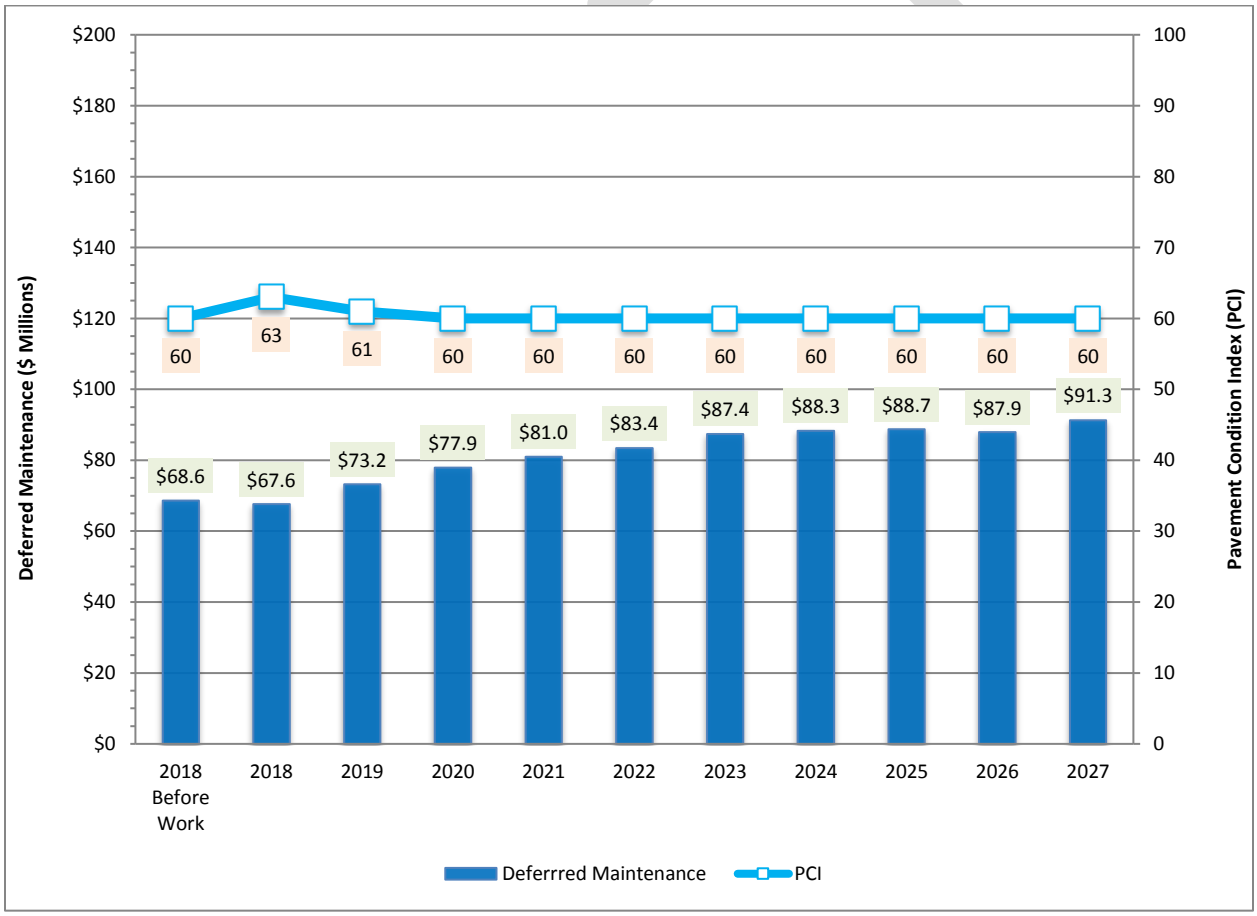

Figure 8: PCI vs Deferred Maintenance for Scenario 2

Scenario 3: Increase PCI to 70 (Arterial/Collector)

This scenario seeks to improve the network PCI to 70 over the next 10 years, and requires $8.4 million per year. In this budget scenario, arterials and collectors are prioritized over residentials. Approximately 76 percent of the pavement network would be in the "Good/Very Good" condition with 15.2 percent in the "Very Poor/Failed" category. The deferred maintenance would drop to $62.4 million from $68.6 million by 2027 and the RSL would be 22 years. Table 7 below summarizes the results, and Figure 9 illustrates the predicted change in PCI and deferred maintenance over the next 10 years.

Table 7: Summary Results for Scenario 3

Year	2018	2019	2020	2021	2022	2023	2024	2025	2026	2027	Total
Budget ($Millions)	8.4	8.4	8.4	8.4	8.4	8.4	8.4	8.4	8.4	8.4	84.0
Deferred Maint. ($ Millions)	60.2	59.2	59.0	59.3	62.5	65.3	64.7	64.5	63.1	62.4	--
Network PCI	67	66	66	66	66	66	67	68	69	70	--
Arterials PCI	61	65	65	64	65	65	67	66	63	61	
Collector PCI	58	57	54	59	57	58	57	59	56	54	
Residential PCI	67	71	70	70	70	70	69	70	74	78	
Remaining Service Life (Years)	17	18	19	19	19	20	20	21	22	22	--

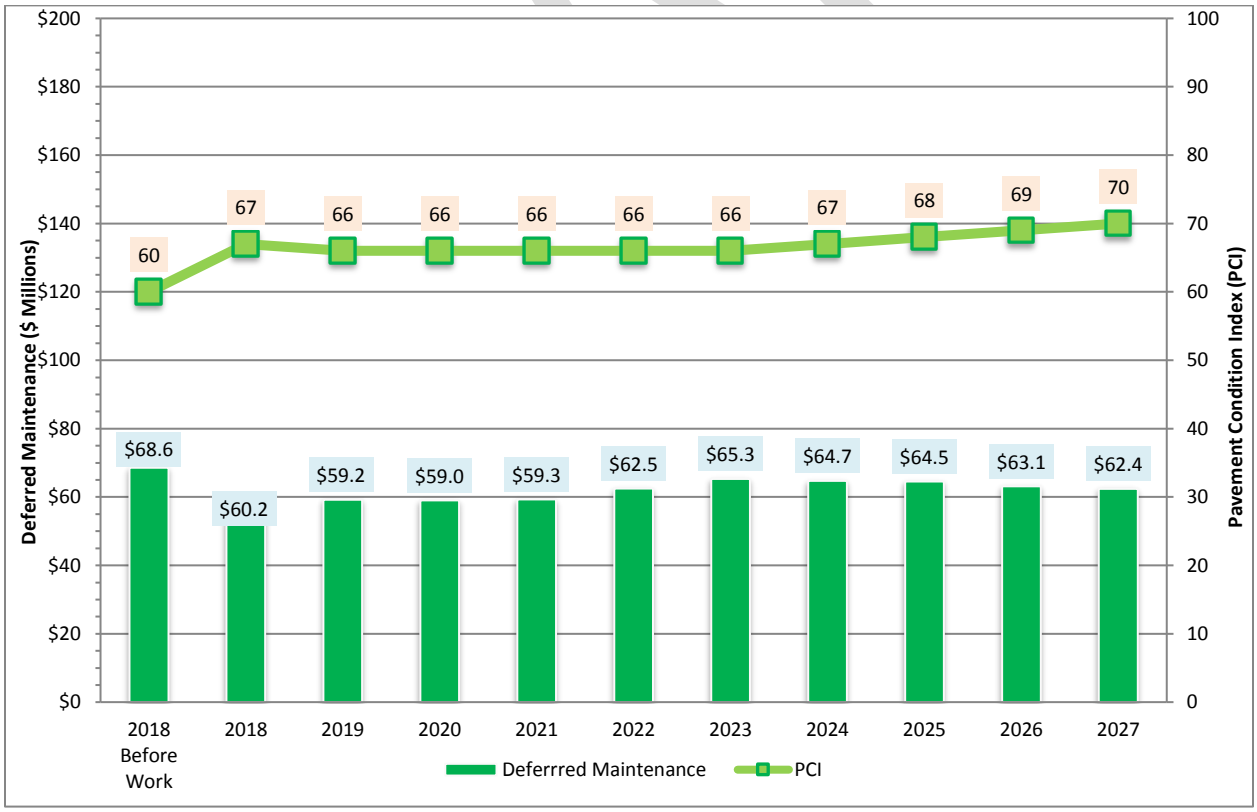

Figure 9: PCI vs Deferred Maintenance for Scenario 3

Scenario 4: Increase PCI to 70 (Residential)

This scenario seeks to improve the network PCI to 70 over the next 10 years and also requires $8.4 million per year. However residentials are prioritized over arterials and collectors. Approximately 75.3 percent of the pavement network would be in the "Good/Very Good" condition with 12.6 percent in the "Very Poor/Failed" category. The deferred maintenance would grow to $72.7 million from $68.6 million by 2027 and the RSL would be 22 years. Table 8 below summarizes the results, and Figure 10 illustrates the predicted change in PCI and deferred maintenance over the next 10 years.

Table 8: Summary Results for Scenario 4

Year	2018	2019	2020	2021	2022	2023	2024	2025	2026	2027	Total
Budget ($Millions)	8.4	8.4	8.4	8.4	8.4	8.4	8.4	8.4	8.4	8.4	84.0
Deferred Maint. ($ Millions)	60.2	59.2	59.0	59.5	62.0	66.5	67.4	68.6	71.8	72.7	--
Network PCI	67	67	68	68	69	68	68	69	70	70	--
Arterials PCI	61	60	57	56	57	54	53	53	50	48	
Collector PCI	58	55	54	57	56	57	56	59	66	73	
Residentials PCI	67	75	78	79	79	82	82	81	81	82	
Remaining Service Life (Years)	17.5	18.3	19.0	19	20	20	21	21	21	22	--

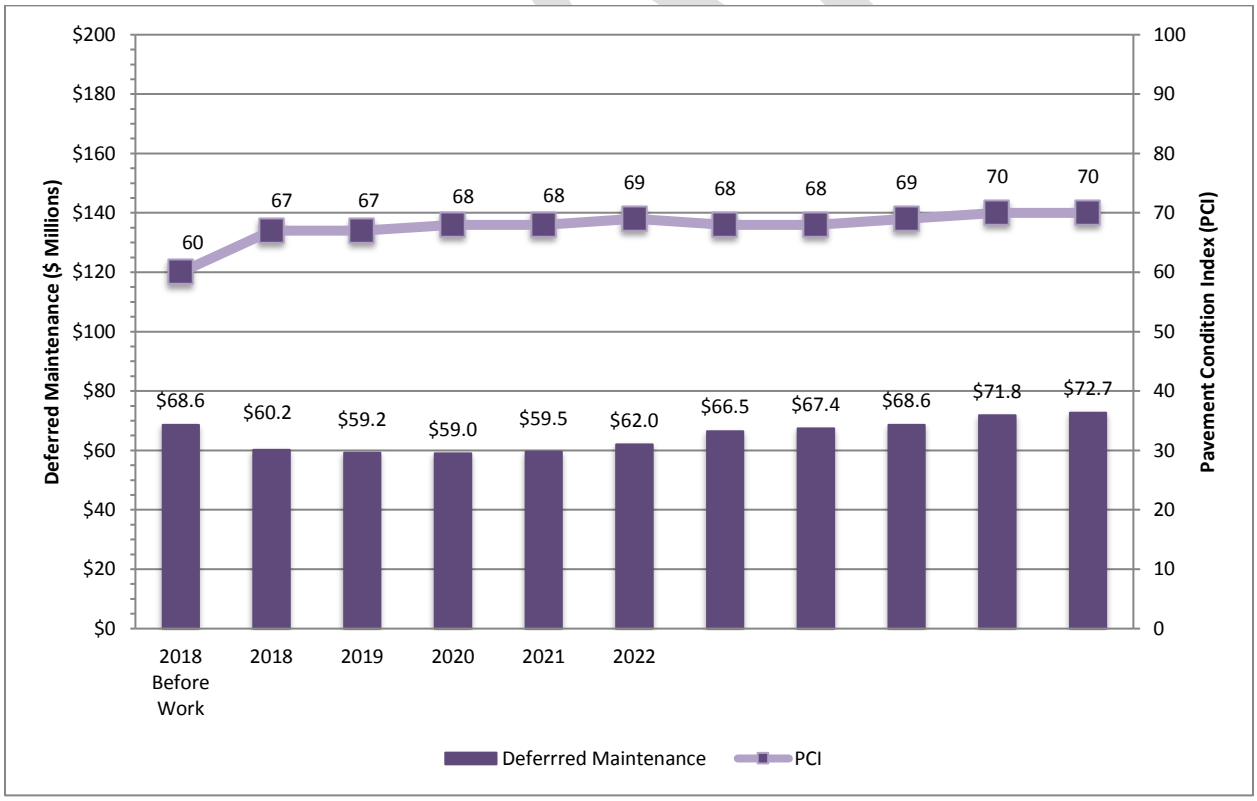

Figure 10: PCI vs Deferred Maintenance for Scenario 4

Scenario Comparisons

The following two figures graphically illustrate the annual changes in PCI and deferred maintenance for each scenario. Figure 11 below illustrates the changes in PCI over time for all four scenarios; clearly, the City's funding is not adequate to improve the current PCI.

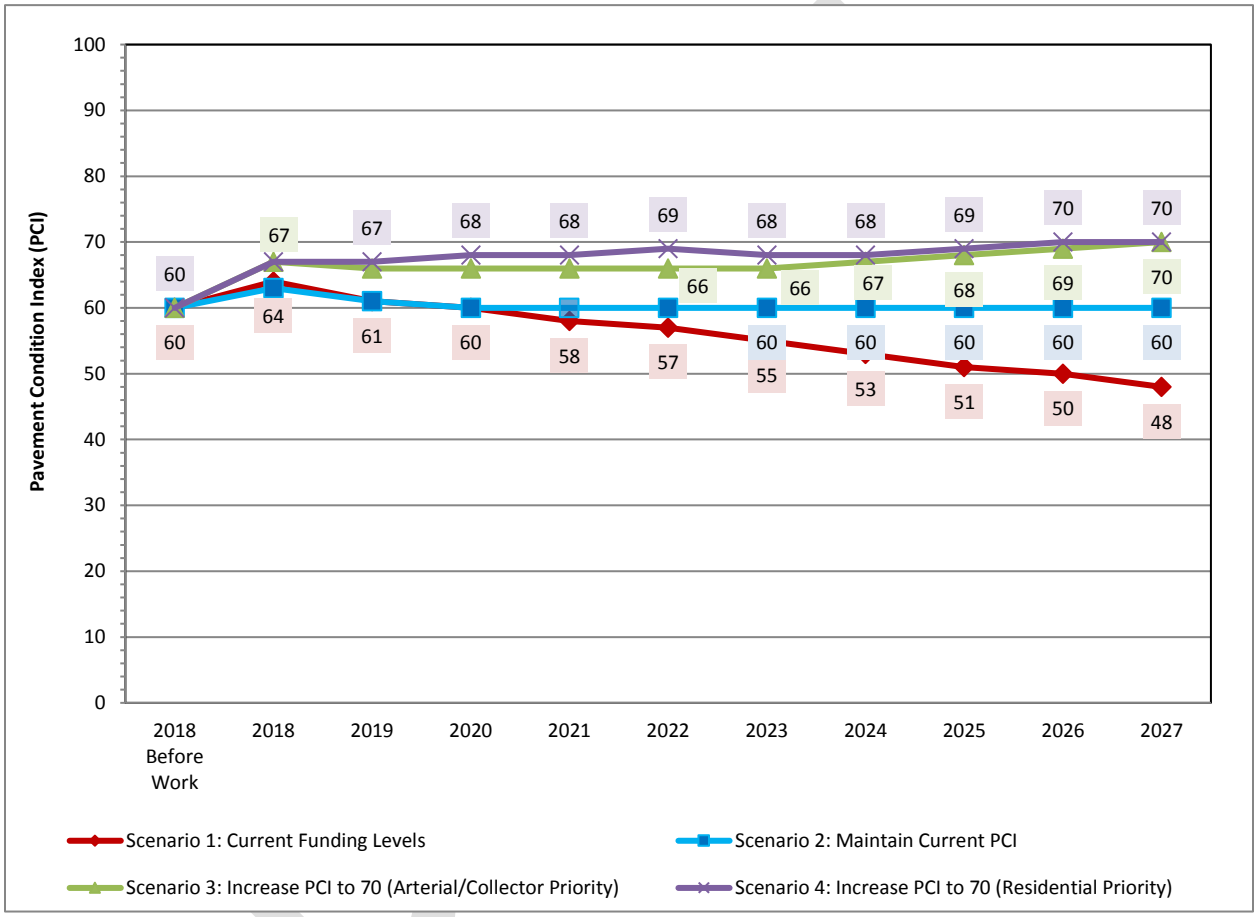

Figure 11: Annual Pavement Condition Index by Scenarios 1 to 4

Figure 12 on the next page illustrates the change in deferred maintenance over time for each budget scenario analyzed. As previously noted, Scenarios 1, 2 and 4 would result in an increase of the deferred maintenance; Scenario 3 would see a decrease to $62.4 million because arterials and collectors treatments are more costly than residentials treatments.

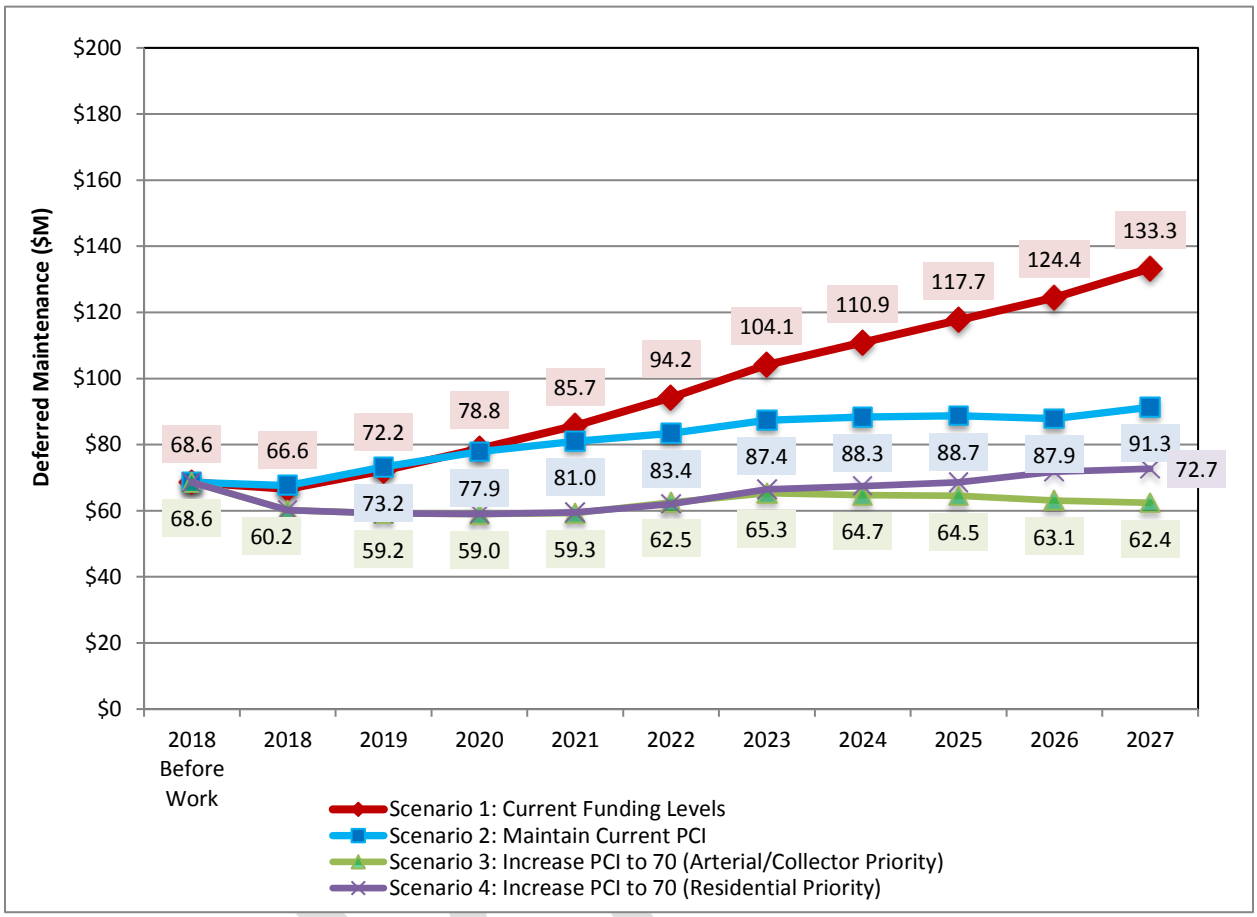

Figure 12: Annual Deferred Maintenance by Scenarios 1 to 4

Figure 13 on the next page illustrates the pavement condition changes under each scenario. Currently, only 37.1 percent of the network is in "Good/Very Good" condition, 32.7 percent in "Fair", and 30.2 percent in "Poor" or "Very Poor/Failed" condition.

Scenarios 3 and 4 will result in more than three quarters of the pavement network in "Good/Very Good" condition. Note that if the City prioritizes arterials and collectors, an annual budget of $8.4 million could eliminate the "Poor" condition. Scenario 1 shows that 27.2 percent of the network will be in "Very Poor/Failed" condition by 2027.

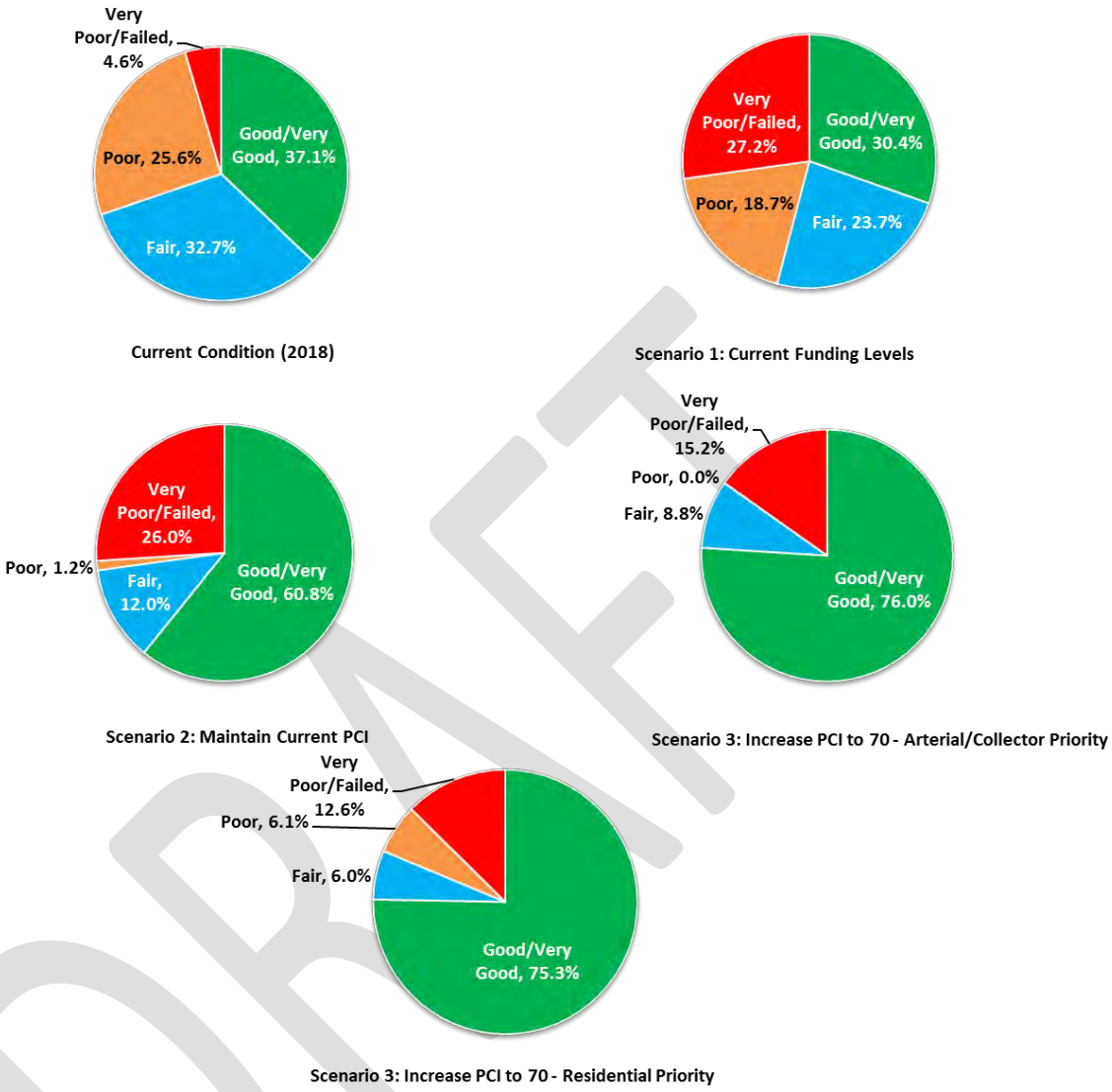

Figure 13: Pavement Condition Changes under Each Scenario

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Discussion

To summarize, City of West Sacramento has a substantial investment of $276.5 million in the street network. Overall, the City's network is in "Fair" condition with an average PCI of 60. Of the 185.4 centerline miles of paved streets , approximately 37.1 percent are currently in "Good/Very Good" condition with 30.2 percent in "Poor" or "Very Poor/Failed" conditions.

The analyses indicate that the City needs to spend approximately $121.3 million in pavement maintenance over the next 10 years to essentially repair all streets. By doing so, streets then can be maintained in good condition with on-going preventive maintenance.

Conclusions

A. Pavement Budget

The recommended scenario for the City of West Sacramento is Scenario 3 (Increase PCI to 70), with a budget of $84 million over the next 10 years. This budget scenario is preferable to Scenario 4 because it would eliminate streets in the "Poor" condition. It also will result in three quarters of the network in "Good/Very Good" condition.

B. Pavement Maintenance Strategies

The City's pavement maintenance strategies include cape seals, slurry seals, and HMA overlays. Since more than two thirds of the pavements are currently in "Good/Very Good" to "Fair" condition, it is important to preserve them. Life-extending surface seals are cost-effective for pavements currently in "Fair" to "Good/Very Good" condition.

NCE recommends that the City continue with the preventive maintenance program as this is necessary to maintain the portion of the street network that is in "Good/ Very Good" condition and avoid increasing the deferred maintenance even more.

C. Re-inspection Strategies

In order to continue monitoring the street network, and make appropriate decisions, it is recommended that arterial and collector streets be inspected every two years and residential streets every four to five years.

D. Maintenance and Rehabilitation Decision Tree

The M&R treatment strategies and associated unit costs should be reviewed and updated annually to reflect new construction techniques/costs so that the budget analysis results can continue to be reliable and accurate.

A significant unknown fact is the future cost of rehabilitation; we recommend that City carefully monitor future construction costs and be ready to adapt to any changes or price increases if necessary.

E. Next Steps

To summarize, we recommend that the City undertake the following steps:

- Maintain a preventive maintenance strategy.
- Pursue additional pavement funding sources to ensure that Scenario 3 is feasible. Examples of some funding sources are listed below:

Federal Funding Sources

- Community Development Block Grants
- Congestion Mitigation & Air Quality Improvement
- Surface Transportation Block Grant Program
- Highway Safety Improvement Program

State Funding Sources

- Active Transportation Program which now includes the Bicycle Transportation Account (BTA) and Safe Routes to Schools
- State Transportation Improvement Program
- AB 2766 (vehicle surcharge)
- Vehicle License Fees
- CalRecycle grants
- State Water Resource Control Board
- Transportation Development Act
- Traffic Safety Fund
- Transportation Uniform Mitigation Fee

Local/Regional Funding Sources

- Local sales taxes
- Development impact fees
- General fund
- Various assessment districts – lighting, maintenance, flood control, special assessments, community facility districts
- Traffic impact fees
- Utilities e.g., stormwater, water, wastewater enterprise funds
- Flood Control Districts
- Parcel/property taxes
- Vehicle registration fees
- Vehicle code fines

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- Underground impact fees
- Solid waste funds
- Transient Occupancy Taxes

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APPENDIX A

Section Description Inventory

Section Description Inventory Report

This report lists a variety of section description information for each of the City's pavement sections. It lists the street and section identifiers, limits, functional class, surface type, number of lanes, lengths, widths, Inspected 2018 PCI, and area identifier.

All of the City's pavement sections are included in the report. The report is sorted alphabetically by Street Name and Section ID. The field descriptions in this report are listed below:

COLUMN	DESCRIPTION
Street ID	Street Identification - A code up to ten characters/digits to identify the street. Generally, the street name is truncated to six characters. The Street ID should be unique for each street.
Section ID	Section Identification - A code up to ten characters/digits to identify the section number. The Section ID must be unique for each section of one street.
Street Name	Street Name - The name of the street as indicated by street signs in the field.
Beg Location	Beginning limit of the section.
End Location	Ending limit of the section.
# of Lanes	Number of travel lanes.
Functional Class (FC)	Functional Classification (A = Arterial, C = Collector, R = Residential).
Length (ft)	Length of the section in feet.
Width (ft)	Average width of the section in feet.
Area (ft^2)	Area of section in square feet.
Surface Type (ST)	Surface Type (A = AC, O = AC/AC, P = PCC).
PCI Date	The last inspection date or rehabilitation date.
PCI	Average PCI for the section. The value is the calculated PCI based on the most recent field inspections or maintenance data (i.e. 2018 inspections).

Section Description Inventory
Sorted by Street Name

Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
ASTREE	010	A ST	SIXTH ST	FIFTH ST	2	R	410	33	13,530	S	5/30/2018	78
ASTREE	020	A ST	FIFTH ST	E END	2	R	120	32	3,840	A	4/30/2018	30
ABCORD	010	ABACO RD	SAN SALVADOR ST	SOUTHPORT PKWY	2	R	695	33	22,935	A	4/30/2018	69
ACRNCT	010	ACORN CT	END	PARK BL	2	R	410	31	15,349	A	4/30/2018	51
AIROCT	010	AIRONS CT	W CDS	CANVASBACK WY	2	R	233	36	10,188	A	4/30/2018	75
AIROCT	020	AIRONS CT	CANVASBACK WY	E CDS	2	R	223	36	9,828	A	4/30/2018	67
ABAMAV	010	ALABAMA AV	ALAMEDA ST	REGENT ST	2	R	1,530	28	42,840	A	4/30/2018	23
ABAMAV	020	ALABAMA AV	ALABAMA AV	ALABAMA AV	2	R	510	23	11,730	A	4/30/2018	15
ABAMAV	030	ALABAMA AV	ALABAMA AV	FIFTEENTH ST	2	R	142	30	4,260	A	4/30/2018	12
ABAMAV	040	ALABAMA AV	FIFTEENTH ST	THIRTEENT ST	2	R	275	34	9,350	A	4/30/2018	25
ABAMAV	050	ALABAMA AV	THIRTEENT ST	ELEVENTH AV	2	R	480	34	16,320	A	4/30/2018	28
ABAMAV	060	ALABAMA AV	STONE BL	NINETEENT ST	2	R	350	36	12,600	A	4/30/2018	15
ABAMAV	070	ALABAMA AV	NINETEENT ST	REGENT ST	2	R	700	36	25,200	A	4/30/2018	32
ALBACT	010	ALABAMA CT	END S	STONE BL	2	R	160	31	7,599	S	4/30/2018	89
ALAEST	010	ALAMEDA ST	PARK BL	MARYLAND AV	2	R	280	40	11,200	A	4/30/2018	21
ALAEST	020	ALAMEDA ST	MARYLAND AV	PENNSYLVANIA AV	2	R	570	40	22,800	A	4/30/2018	28
ALAEST	030	ALAMEDA ST	PENNSYLVA AV	JEFFERSON BL	2	R	925	34	31,450	A	4/30/2018	24
ALAMPL	010	ALAMO PL	END W	END E	2	R	418	32	19,902	A	4/30/2018	71
ALDEWY	010	ALDER WY	REDWOOD AV	LINDEN RD	2	R	1,530	15	22,950	S	4/30/2018	71
ALDEWY	020	ALDER WY	LINDEN RD	LIMEWOOD WY	2	R	180	33	5,940	A	4/30/2018	75
ALEUST	010	ALEUTIAN ISL ST	BALLENA B RD	PILOT POI RD	2	R	600	33	19,800	A	4/30/2018	71
ALANAV	010	ALLAN AV	HIGGINS RD	HART AV	2	R	2,010	20	40,200	S	4/30/2018	38
ALANAV	020	ALLAN AV	END	BLACKER RD	2	R	370	18	6,660	S	4/30/2018	15
ALANAV	030	ALLAN AV	NICHOLS CT	LINDEN RD	2	R	1,020	31	31,620	A	4/30/2018	61
ALANAV	040	ALLAN AV	NICHOLS CT	END S	2	R	850	34	29,255	A	4/30/2018	60
ALANAV	050	ALLAN AV	DAVIS RD	MARSHALL RD	2	R	1,260	24	30,240	A	4/30/2018	63
ALMNRD	010	ALMANOR RD	BRIDGEWAY LAKES DR	BASS ST	2	R	370	32	11,840	A	4/30/2018	91
ALMOST	010	ALMOND ST	LIMEWOOD WY	PEPPERTRE RD	2	R	590	36	21,240	A	4/30/2018	71
ALPIST	010	ALPINE ST	BRIDGEWAY LAKES	STONEYFORD RD	2	R	249	32	7,968	A	4/30/2018	85
ALTUCT	010	ALTURAS CT	COLUMBUS RD	END N	2	R	244	33	11,227	A	4/30/2018	90
ANCPCT	010	ANACAPA ISLAND CT	N CDS	SANTA CRUZ RD	2	R	219	33	10,405	A	4/30/2018	71
ANDRCT	010	ANDRUS ISL CT	SHERMAN I RD	S CDS	2	R	394	33	16,286	A	4/30/2018	76
ANGLCT	010	ANGEL CT	W CDS	BETTY WY	2	R	358	33	13,842	A	4/30/2018	59
ANGLCT	020	ANGEL CT	BETTY WY	E CDS	2	R	303	33	12,027	A	4/30/2018	52
ANNAST	010	ANNA ST	BRYTE AV	MYRTLE AV	2	C	710	36	25,560	A	4/30/2018	27
ANNAST	020	ANNA ST	MYRTLE AV	TODHUNTER AV	2	C	695	36	25,020	A	4/30/2018	49
ANNAST	030	ANNA ST	TODHUNTER AV	HOLLAND DR	2	C	1,110	35	38,850	A	4/30/2018	72
ANNAST	040	ANNA ST	HOLLAND DR	KEGLE DR	2	C	1,065	35	37,275	A	4/30/2018	60
ANNAST	050	ANNA ST	KEGLE DR	HARDY DR	2	C	1,000	35	35,000	A	4/30/2018	57
ANTGPL	010	ANTIGUA PL	W CDS	MARTINIQU ST	2	R	354	33	14,860	A	4/30/2018	71

FC (Functional Classification): A (Arterial), C (Collector), R (Residential/Local), O (Other)
Surface Type: A (AC), O (AC/AC), P (PCC), G (Gravel)

Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
ANTIAV	010	ANTIOCH AV	BEVAN RD	TAPLEY RD	2	R	2,300	20	46,000	A	4/30/2018	29
ANTIAV	020	ANTIOCH AV	TAPLEY RD	HARMON RD	2	R	1,780	17	30,260	A	4/30/2018	25
APACST	010	APACHE ST	END S	TRINITY ST	2	R	1,064	32	34,048	A	4/30/2018	66
ARCBRD	010	ARCATA BAY RD	RYER ISL ST	VENICE ST	2	R	290	33	9,570	A	4/30/2018	75
ARLNRD	010	ARLINGTON RD	END S	CROWN AV	2	R	1,610	18	28,980	S	4/30/2018	62
ARLNRD	020	ARLINGTON RD	CROWN AV	END N	2	R	600	18	10,800	S	4/30/2018	66
ARMFAV	010	ARMFIELD AV	FISHER AV	JEFFERSON BL	2	R	700	24	16,800	A	4/30/2018	56
ARROST	010	ARROWHEAD ST	HENSHAW RD	S END	2	R	1,245	32	39,840	A	4/30/2018	86
ARTHDR	010	ARTHUR DR	ANNA ST	HOBSON AV	2	R	1,365	35	47,775	A	4/30/2018	82
ARTHDR	020	ARTHUR DR	HOBSON AV	FREMONT BL	2	R	460	36	16,560	A	4/30/2018	59
ARUBST	010	ARUBA ST	PUERTO RI RD	ST JOHN RD	2	R	1,065	33	35,145	A	4/30/2018	62
ASHAVE	010	ASH AV	WILLOW AV	PROCTOR AV	2	R	280	32	8,960	A	5/24/2018	48
ASHAVE	020	ASH AV	PROCTOR AV	EVERGREEN AV	2	R	275	32	8,800	A	5/24/2018	31
ASTRAV	010	ASTER AV	MANCHESTE ST	END E	2	R	965	32	30,880	S	4/30/2018	66
ATHECT	010	ATHERTON CT	END S	STONEGATE DR	2	R	330	33	14,065	A	4/30/2018	83
ALBYCT	010	AYLESBURY CT	W CDS	MARECA WY	2	R	383	32	14,360	S	4/30/2018	38
AZLAPL	010	AZALEA PL	RIVERMONT ST	E CDS	2	R	154	32	8,194	A	4/30/2018	76
BSTREE	010	B (WILLIAM) ST	SIXTH ST	FIFTH ST	2	R	375	32	12,000	A	4/30/2018	73
BSTREE	020	B (WILLIAM) ST	FIFTH ST	E END	2	R	920	32	29,440	A	4/30/2018	64
BCONST	010	BACON ISL ST	N CDS	GRIZZLY RD	2	R	454	33	18,160	A	4/30/2018	70
BLDPCT	010	BALDPATE CT	W CDS	TEAL DR	2	R	258	32	10,360	A	4/30/2018	100
BALIPL	010	BALI PL	FIJI ISL ST	E CDS	2	R	199	33	9,745	A	4/30/2018	68
BALBRD	010	BALLENA BAY RD	ALEUTIAN ST	TACOMA NA ST	2	R	290	33	9,570	A	4/30/2018	67
BALBRD	020	BALLENA BAY RD	TACOMA NA ST	E CDS	2	R	679	33	25,585	A	4/30/2018	68
BALLDR	020	BALLPARK DR	IRONWORKS AV	DREVER ST	2	R	563	30	16,890	A	4/30/2018	85
BALSST	010	BALSAM ST	EVERGREEN AV	BUCKEYE DR	2	R	405	32	12,960	A	4/30/2018	62
BNDLCT	010	BANDALIN CT	MARECA WY	S CDS	2	R	288	33	11,532	S	4/30/2018	58
BERYPL	010	BARBERRY PL	W CDS	RIVERMONT ST	2	R	269	32	11,874	A	4/30/2018	82
BAROCT	010	BARONA CT	END S	CORANADA ST	2	R	289	33	12,712	A	4/30/2018	83
BAROST	010	BARONA ST	CORANADO RD	LA JOLLA ST	2	R	1,440	33	47,520	A	4/30/2018	85
BASSST	010	BASS ST	ALMANOR RD	BRIDGEWAY LAKES DR	2	R	495	32	15,840	A	4/30/2018	88
BAYBST	010	BAYBERRY ST	PRINCETON	GREENBRIER RD	2	R	260	32	8,320	A	4/30/2018	65
BAYBST	015	BAYBERRY ST	GREENBRIAR RD	CORNADO ST	2	R	360	32	11,520	A	4/30/2018	70
BAYBST	020	BAYBERRY ST	CORANADO ST	HIGHLAND	2	R	610	32	19,520	A	4/30/2018	77
BAYSDRD	010	BAYSIDE RD	SHELL ST	END	2	R	885	32	28,320	A	4/30/2018	95
BEACBL	010	BEACON BL	HARBOR BL	HALYARD DR	4	R	375	56	21,000	A	5/30/2018	63
BEACBL	020	BEACON BL	HALYARD DR	INDUSTRIAL BL	2	R	1,485	20	29,700	A	4/30/2018	31
BEACBL	030	BEACON BL	INDUSTRIA BL	HALYARD DR	2	R	1,485	20	29,700	A	4/30/2018	49
BEARCT	010	BEAR RIVER CT	BERRY CRE RD	END E	2	R	264	33	11,887	A	4/30/2018	79
BEARRD	010	BEAR RIVER RD	VILLAGE PW	BERRY CRE RD	2	R	230	33	7,590	A	4/30/2018	77

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Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
BEARDR	010	BEARDSLEY DR	ELKHORN P	CUMMINS WAY	2	R	1,705	28	47,740	A	4/30/2018	75
BENTPL	010	BENTON PL	END S	END N @TRINITY	2	R	1,298	32	44,799	A	4/30/2018	88
BENTST	010	BENTON ST	S END	CHEROKEE RD	2	R	405	32	12,960	A	4/30/2018	78
BENTST	020	BENTON ST	CHEROKEE RD	TRINITY WY	2	R	655	32	20,960	A	4/30/2018	85
BERUCT	010	BERMUDA CT	N CDS	HAITI RD	2	R	189	33	9,415	A	4/30/2018	69
BERRRD	010	BERRY CREEK RD	ROARING C ST	BEAR RIVER RD	2	R	1,170	33	38,610	A	4/30/2018	87
BERRPL	010	BERRYESSA PL	BRIDGEWY DR	END E	2	R	554	32	20,994	A	4/30/2018	81
BETHWY	010	BETHEL WY	CONSTITUTION AV	BRANNAN WY	2	R	475	30	14,250	A	4/30/2018	90
BETTWY	010	BETTY WY	SUMMERFIELD DR	CLAUDIA CT	2	R	960	36	34,560	A	4/30/2018	55
BETTWY	020	BETTY WY	CLAUDIA CT	LINDEN RD	2	R	1,435	36	51,660	A	4/30/2018	63
BEVARD	010	BEVAN RD	OTIS AV	E END	2	R	811	32	25,952	A	4/30/2018	83
BEVARD	030	BEVAN RD	JEFFERSON BL	GREGORY AV	2	R	2,430	13	31,590	S	4/30/2018	18
BEVARD	040	BEVAN RD	GREGORY AV	PARTRIDGE AV	2	R	1,935	14	27,090	A	4/30/2018	21
BIGBST	010	BIG BEAR ST	HENSHAW RD	N CDS	2	R	459	32	17,954	A	4/30/2018	84
BINDDR	010	BIG ISLAND RD	KAUAI RD	HALF MOON CR	2	R	695	33	22,935	A	4/30/2018	68
BINGAV	010	BIRMINGHAM AV	MANCHESTE ST	KINSINGTO ST	2	R	800	32	25,600	S	4/30/2018	74
BLACRD	010	BLACK BUTTE RD	STONEYFORD RD	HUME ST	2	R	1,041	32	33,312	A	4/30/2018	88
BLCKRD	010	BLACKER RD	ALLAN AV	JEFFERSON BL	2	R	710	22	15,620	S	4/30/2018	48
BLROST	010	BLUE ROCK ST	GREENBRIER RD	END	2	R	440	32	14,080	A	4/30/2018	95
BTMNAV	010	BOATMAN AV	INDUSTRIA BL	TERMINAL ST	2	R	1,100	40	44,000	A	4/30/2018	51
BODBRD	010	BODEGA BAY RD	DRAKES BA ST	FARALLON RD	2	R	655	33	28,672	A	4/30/2018	72
BONRRD	010	BONAIRE RD	DOMINICA ST	GUADELUPE ST	2	R	860	33	28,380	A	4/30/2018	75
BONACT	010	BONITA CT	END	FERNWOOD ST	2	R	141	32	4,512	A	4/30/2018	62
BOWIST	010	BOWEN ISL ST	SOLOMON I RD	OAKLAND B DR	2	R	605	33	19,965	A	4/30/2018	76
BOWLDR	010	BOWLEN DR	CHARLES ST	ANNA ST	2	R	553	32	17,696	A	4/30/2018	70
BRADWY	010	BRADFORD WY	CONSTITUTION AV	END	2	R	165	30	4,950	A	4/30/2018	95
BRADWY	020	BRADFORD WY	CONSTITUTION AV	END	2	R	530	30	15,900	A	4/30/2018	96
BREAWY	010	BRENDA WY	NANCY LN	LINDEN RD	2	R	630	36	22,680	A	4/30/2018	57
BRIADR	010	BRIARWOOD DR	EVERGREEN AV	OAKMONT WY	2	R	350	36	12,600	A	5/24/2018	67
BRIDST	010	BRIDGE ST	FIFTEENTH ST	RIVERFRONT ST	2	R	801	44	35,244	P	2/28/2018	99
BRIDDR	010	BRIDGEWY LAKES DR	HENSHAW RD	BERRYESSA PL	2	R	1,490	32	47,680	A	4/30/2018	86
BRIDDR	020	BRIDGEWY LAKES DR	BERRYESSA PL	SOUTHPORT PW	2	R	2,015	32	64,480	A	4/30/2018	87
BRIDDR	030	BRIDGEWY LAKES DR	SOUTHPORT PW	CAMANCHE ST	2	R	1,875	32	60,000	A	4/30/2018	91
BRIDDR	040	BRIDGEWY LAKES DR	CAMANCHE ST	OTIS AV	2	R	1,635	32	52,320	A	4/30/2018	92
BRIDCT	010	BRIDLE CT	STABLE DR	END N	2	R	219	32	10,271	A	4/30/2018	84
BRIST	010	BRISBANE ST	PRESIDIO RD	CYPRESS RD	2	R	300	33	9,900	A	4/30/2018	81
BRTTPL	010	BRITTON PL	END W	SOUTHPORT PW	2	R	489	32	15,648	A	4/30/2018	76
BROKCT	010	BROCK CT	CANVASBAC WY	E CDS	2	R	223	32	9,240	A	4/30/2018	68
BROSRD	010	BROTHER ISL RD	TREASURE RD	GOLDEN GATE DR	2	R	505	33	16,665	A	4/30/2018	73
BRWSCT	010	BROWNS ISL CT	SUISUN BAY RD	S CDS	2	R	294	33	12,880	A	4/30/2018	67

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BRYTAV	010	BRYTE AV	SACRAMENTO AVE	MAY ST	2	C	1,570	32	50,240	S	4/30/2018	67
BRYTAV	020	BRYTE AV	MAY ST	RIVERBANK RD	2	C	1,370	32	43,840	S	4/30/2018	43
BUCKDR	010	BUCKEYE DR	SYCAMORE ST	POPLAR AV	2	R	846	32	31,280	A	4/30/2018	31
BUWSAV	010	BURROWS AV	JEFFERSON BL	COP	2	R	494	17	8,398	A	4/30/2018	30
BUWSAV	020	BURROWS AV	COP	MAILBOX #3175	2	R	2,626	16	41,856	A	4/30/2018	33
BUWSAV	030	BURROWS AV	MAILBOX #3175	SOUTH RIVER RD	2	R	2,363	18	42,534	A	4/30/2018	60
BURRST	010	BURROWS ST	TODHUNTER AV	FOURNESS DR	2	R	760	32	24,320	A	4/30/2018	80
BUTLCT	010	BUTLER CT	W CDS	CANVASBACK WY	2	R	238	32	9,720	A	4/30/2018	63
BUTLCT	020	BUTLER CT	CANVASBACK WY	E CDS	2	R	228	32	9,400	A	4/30/2018	75
CSTREE	020	C ST	SIXTH ST	FIFTH ST	4	A	380	49	19,820	A	4/30/2018	50
CSTREE	030	C ST	FIFTH ST	THIRD ST	3	A	785	56	43,960	A	4/30/2018	43
CSTREE	040	C ST	THIRD ST	SECOND ST	2	A	410	43	17,630	A	4/30/2018	56
CABLCT	010	CABLE CT	GATEWAY DR	END	2	R	99	44	6,563	S	4/30/2018	64
CALIST	010	CALIFORNIA ST	EIGHTH ST	C (ELIZAB ST	2	R	750	32	24,000	A	4/30/2018	71
CALLCT	010	CALL CT	MARECA WY	S CDS	2	R	143	33	6,747	A	4/30/2018	80
CAMAST	010	CAMANCHE ST	DORENA PL	BRIDGEWY DR	2	R	265	32	8,480	A	4/30/2018	88
CANAST	010	CANAL ST	GALVESTON ST	HOUSTON ST	2	R	445	36	16,020	A	4/30/2018	27
CANNWY	010	CANNA WY	ELEVENTH ST	WEBSTER ST	2	R	560	32	17,920	A	4/30/2018	42
CNVKWY	010	CANVASBACK WY	HIGGINS RD	AIRONS CT	2	R	660	36	23,760	S	4/30/2018	64
CNVKWY	020	CANVASBACK WY	AIRONS CT	LINDEN RD	2	R	1,355	33	48,780	A	4/30/2018	65
CNVKWY	030	CANVASBACK WY	LINDEN RD	S CDS	2	R	133	33	6,417	A	4/30/2018	70
CARNDR	010	CARLIN DR	OATES DR	SOUTHPORT PW	3	C	1,880	52	97,760	A	4/30/2018	45
CARNDR	020	CARLIN DR	SOUTHPORT PW	RAMCO ST	3	C	1,080	52	56,160	A	4/30/2018	60
CRMLRD	010	CARMEL BAY RD	MORROW BA ST	E CDS	2	R	954	33	34,660	A	4/30/2018	74
CARMCT	010	CARMEL CT	MERCED WY	END A	2	R	290	32	11,382	A	4/30/2018	62
CRLNAV	010	CAROLINA AV	STONE BL	NINETEENT ST	2	R	640	26	16,640	A	4/30/2018	23
CARRST	010	CARRIE ST	BRYTE AV	TODHUNTER AV	2	C	1,400	33	46,200	A	4/30/2018	39
CARRST	020	CARRIE ST	TODHUNTER AV	HOLLAND DR	2	C	1,115	36	40,140	A	4/30/2018	40
CARRST	030	CARRIE ST	HOLLAND DR	KEGLE DR	2	C	1,470	36	52,920	A	4/30/2018	40
CRRGCT	010	CARRIGAN CT	DEERWOOD ST	END	2	R	130	31	6,669	A	4/30/2018	54
CASCST	010	CASCADE ST	CHINOOK RD	SPOKANE RD	2	R	710	33	22,720	A	4/30/2018	81
CASSDR	010	CASSELMAN DR	ELKHORN P	CUMMINS WY	2	R	1,705	28	47,740	A	4/30/2018	77
CSTCCT	010	CASTAIC CT	END S	BRITTON PL	2	R	374	32	15,231	A	4/30/2018	80
CASTCT	010	CASTLE CT	W CDS	WESTACRE RD	2	R	178	32	8,353	A	4/30/2018	93
CATIRD	010	CAT ISL RD	SAN SALVADOR ST	LONG ISL ST	2	R	500	33	16,500	A	4/30/2018	85
CATLRD	010	CATALINA ISL RD	KELLYS IS DR	GOLDEN GA DR	2	R	180	33	5,940	A	4/30/2018	71
CATLRD	020	CATALINA ISL RD	GOLDEN GA DR	SANTA BARBARA ST	2	R	530	33	17,490	A	4/30/2018	69
CATLRD	030	CATALINA ISL RD	SANTA BARBARA ST	RYER ISL ST	2	R	550	33	18,150	A	4/30/2018	78
CATLRD	040	CATALINA ISL RD	RYER ISL ST	E CDS	2	R	259	33	11,725	A	4/30/2018	70
CYMNST	010	CAYMAN ISLAND ST	OAKLAND BAY DR	ST JOHN RD	2	R	1,190	33	39,270	A	4/30/2018	61

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CEBRST	010	CEBRIAN ST	DEL MONTE ST	2280 STONE (ON CEBRIAN)	2	C	1,367	54	73,818	A	4/30/2018	28
CEBRST	020	CEBRIAN ST	2280 STONE (ON CEBRIAN)	STONE BL	2	C	950	53	50,350	A	4/30/2018	34
CEDRCT	010	CEDAR CT	N CDS	TAMARACK RD	2	R	275	15	5,748	A	4/30/2018	66
CEDAST	010	CEDAR ST	N END	WEST CAPI AV	2	R	675	32	21,600	A	4/30/2018	84
CEDERD	010	CEDARBROOK RD	END W	NEWBERRY ST	2	R	269	32	11,879	A	4/30/2018	79
CEDERD	020	CEDARBROOK RD	NEWBERRY ST	MARLIN ST	2	R	650	32	20,800	A	4/30/2018	82
CENTST	010	CENTRAL ST	BRIDGE ST	GARDEN ST	2	R	331	37	12,247	P	2/28/2018	99
CENTST	020	CENTRAL ST	GARDEN ST	MILL ST	2	R	345	47	16,215	P	2/28/2018	99
CHANDR	010	CHANNEL DR	END W	RR TRACKS	2	R	1,046	46	50,528	A	4/30/2018	40
CHANDR	020	CHANNEL DR	RR TRACKS	ENTERPRIS BL	2	R	919	46	42,274	A	4/30/2018	52
CHANDR	030	CHANNEL DR	ENTERPRIS BL	3771 CHANNAL DR	2	R	1,864	46	85,744	A	4/30/2018	52
CHANDR	040	CHANNEL DR	3771 CHANNAL DR	SEAPORT BL	2	R	1,140	46	52,440	A	4/30/2018	55
CHAPWY	010	CHAPARRAL WY	MANZANITA WY	MICHIGAN BL	2	R	1,020	32	32,640	A	4/30/2018	72
CHARST	010	CHARLES ST	ARTHUR DR	BOWLEN DR	2	R	800	32	25,600	A	4/30/2018	62
CHELRD	010	CHELAN RD	END W	SWIFT ST	2	R	410	33	13,530	A	4/30/2018	79
CHERRD	010	CHEROKEE RD	APACHE ST	STONEGATE DR	2	R	415	32	13,280	A	4/30/2018	63
CHERPL	010	CHERRY PL	BRIDGEWY DR	END E	2	R	494	32	19,074	A	4/30/2018	87
CHILRD	010	CHINA HAT ISL RD	MIDWAY IS ST	MAUI ST	2	R	550	33	18,150	A	4/30/2018	75
CHINRD	010	CHINOOK RD	CASCADE ST	MONTE VISTA	2	R	1,115	33	35,680	A	4/30/2018	81
CICLST	010	CIRCLE ST	ALABAMA ST	JEFFERSON BL	2	R	490	23	11,270	A	4/30/2018	25
CITRST	010	CITRUS ST	ELM ST	E CDS	2	R	1,060	32	36,387	A	4/30/2018	76
CLRDST	010	CLARENDON ST	GARDENIA AV	GRANDE VI AV	2	R	688	34	23,392	A	4/30/2018	76
CLRDST	020	CLARENDON ST	GRANDE VI AV	NORFOLK AV	2	R	1,300	34	44,200	S	4/30/2018	67
CLRKBRLNTR	010	CLARKSBURG BRANCH LINE TRA	SOUTH RIVIER ROAD	1,955 FT SOUTH OF LINDEN RD	1	O	6,430	10	64,300	A	4/30/2018	97
CLUDCT	010	CLAUDIA CT	W CDS	BETTY WY	2	R	373	33	14,337	A	4/30/2018	51
CLUDCT	020	CLAUDIA CT	BETTY WY	E CDS	2	R	388	33	14,832	A	4/30/2018	44
CLIKCT	010	CLICKER CT	W CDS	CANVASBACK WY	2	R	263	32	10,520	A	4/30/2018	75
CLIKCT	020	CLICKER CT	CANVASBACK WY	E CDS	2	R	218	32	9,080	A	4/30/2018	70
COITPL	010	COIT PL	END	HEARST ST	2	R	419	33	17,002	S	4/30/2018	69
COKEST	010	COKE ST	EVERGREEN AV	SELLERS WY	2	R	485	40	19,400	A	5/24/2018	28
COLDRD	010	COLD SPRINGS RD	BERRY CRE RD	ROARING C ST	2	R	855	33	28,215	A	4/30/2018	86
CLEECT	010	COLLEEN CT	TERESA LN	E CDS	2	R	243	36	10,548	A	4/30/2018	40
COLLST	010	COLLINS ST	TOPAZ RD	N CDS	2	R	1,394	32	47,874	A	4/30/2018	85
COLURD	010	COLUMBUS RD	N BEACH AV	230' E/ NBEACH	2	R	230	33	7,590	A	4/30/2018	84
COLURD	020	COLUMBUS RD	230' E/ NBEACH	COLUMBUS RD	2	R	725	33	23,925	A	4/30/2018	87
COLURD	030	COLUMBUS RD	COLUMBUS RD	MESA GRANDE ST	2	R	762	33	25,146	A	4/30/2018	87
COLURD	040	COLUMBUS RD	MESA GRANDE ST	COLUMBUS RD	2	R	610	33	20,130	A	4/30/2018	89
COLSARD	010	COLUSA RD	APACHE ST	STONEGATE DR	2	R	415	32	13,280	A	4/30/2018	72
COLSARD	020	COLUSA RD	STONEGATE DR	END E	2	R	618	32	19,776	A	4/30/2018	63
COMMDR	010	COMMERCE DR	W CDS	E CDS	2	R	1,451	46	77,210	A	4/30/2018	51

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CONDST	010	CONDOR ST	END N	END S	2	R	750	23	17,250	A	4/30/2018	89
CNSTAV	010	CONSTITUTION AV	LINDEN RD	HIGGINS RD	2	R	1,630	36	58,680	A	4/30/2018	88
COKIRD	010	COOK ISL RD	MALCOLM I ST	HORNBY IS ST	2	R	470	33	15,510	A	4/30/2018	86
COPRRD	010	COOPER ISL RD	FIJI ISL ST	GOLDEN GA DR	2	R	1,170	33	38,610	A	4/30/2018	69
CORAST	010	CORANADO ST	CORANADO ST	BARONA ST	2	R	1,070	33	35,310	A	4/30/2018	94
CORARD	010	CORONADO RD	BAYBERRY ST	CORANADO ST	2	R	510	32	16,320	A	4/30/2018	91
CORIRD	010	CORTES ISL RD	MALCOLM I ST	HORNBY IS ST	2	R	485	33	16,005	A	4/30/2018	88
COTRD	010	CORTINA RD	REDDING RD	BARONA ST	2	R	545	33	17,985	A	4/30/2018	86
COSTCT	010	COSTA CT	CANVASBAC WY	E CDS	2	R	193	32	8,280	A	4/30/2018	67
COYARD	010	COYOTE RD	SOUTHPORT PW	SPALDING CT	2	R	1,250	32	40,000	A	4/30/2018	81
COYARD	020	COYOTE RD	SPALDING CT	OTIS AV	2	R	1,225	32	39,200	A	4/30/2018	86
CRESTST	010	CRESTED ST	MUSCOVY RD	MALLARD RD	2	R	757	32	24,224	A	4/30/2018	87
CRWNAV	010	CROWN AV	ARLINGTON RD	KINSTINGTON ST	2	R	850	32	27,200	S	4/30/2018	66
CRYSCT	010	CRYSTAL CT	LAGOON LN	END	2	R	228	32	9,398	S	4/30/2018	72
CUBARD	010	CUBA RD	ARUBA ST	CAYMAN IS ST	2	R	280	33	9,240	A	4/30/2018	71
CUMMWY	010	CUMMINS WY	ARTHUR DR	INGLEWOOD DR	2	C	1,565	36	56,340	A	4/30/2018	48
CUMMWY	020	CUMMINS WY	INGLEWOOD DR	ELDER DR	2	C	1,020	36	36,720	A	4/30/2018	42
CUMMWY	030	CUMMINS WY	ELDER DR	SIXTH ST	2	C	2,090	36	75,240	A	4/30/2018	65
CYPRRD	010	CYPRESS RD	BRISBANE ST	NBEACH AV	2	R	470	33	15,510	A	4/30/2018	82
DSTREE	010	D ST	W CDS	FOURTH ST	2	R	318	34	11,426	A	4/30/2018	73
DSTREE	020	D ST	FOURTH ST	THIRD ST	2	R	385	49	18,865	A	4/30/2018	67
DSTREE	030	D ST	THIRD ST	SECOND ST	2	R	380	34	12,920	A	4/30/2018	68
DNBYCT	010	DANBURY CT	FALLBROOK ST	END	2	R	210	32	9,184	A	4/30/2018	62
DAVIRD	010	DAVIS RD	SEYMORE AV	JEFFERSON BL	2	A	1,500	19	28,500	S	4/30/2018	30
DAVIRD	020	DAVIS RD	JEFFERSON BL	GREGORY AV	2	A	115	25	2,875	A	4/30/2018	20
DAVIRD	030	DAVIS RD	GREGORY AV	PARTRIDGE AV	2	A	1,655	20	33,100	S	4/30/2018	22
DAVIRD	040	DAVIS RD	PARTRIDGE AV	ANTIOCH AV	2	A	1,620	20	32,400	A	4/30/2018	18
DAVIRD	050	DAVIS RD	ANTIOCH AV	1800' E/O ANTIOCH	2	A	1,800	19	34,200	S	4/30/2018	20
DAVIRD	060	DAVIS RD	1800' E/O ANTIOCH	SOUTH RIV RD	4	A	1,895	19	36,005	S	4/30/2018	25
DRWD	010	DEERWOOD CR	DEERWOOD ST	END E	2	R	190	31	8,529	A	4/30/2018	50
DRWDST	010	DEERWOOD ST	PARK BL	DEERWOOD CR	2	R	1,150	31	35,650	S	4/30/2018	79
DRWDST	020	DEERWOOD ST	DEERWOD CR	GRANDE VIEW AV	2	R	1,295	31	40,145	S	4/30/2018	82
DELMST	010	DEL MONTE ST	SHORE ST	HARBOR BL	2	C	750	30	22,500	A	4/30/2018	34
DELMST	020	DEL MONTE ST	HARBOR BL	SHORE ST	2	C	810	30	24,300	A	4/30/2018	38
DELMST	030	DEL MONTE ST	SHORE ST	TERMINAL ST	2	C	1,400	60	84,000	A	4/30/2018	36
DELMST	040	DEL MONTE ST	TERMINAL ST	RR CROSSI	2	C	375	42	15,750	A	4/30/2018	34
DELMST	050	DEL MONTE ST	RR CROSSI	CEBRIAN ST	2	C	375	42	15,750	A	4/30/2018	30
DELAAV	010	DELAWARE AV	STONE BL	SEVENTEENTH ST	2	R	1,739	32	55,648	A	4/30/2018	15
DELAAV	020	DELAWARE AV	SEVENTEEN ST	FIFTEENTH ST	2	R	1,553	31	48,143	A	4/30/2018	28
DELAAV	030	DELAWARE AV	FIFTEENTH ST	ALABAMA AV	2	R	1,024	31	31,744	A	4/30/2018	49

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Surface Type: A (AC), O (AC/AC), P (PCC), G (Gravel)

Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
DLWRCT	010	DELAWARE CT	END S	STONE BL	2	R	160	31	7,599	S	4/30/2018	81
DELTLN	010	DELTA LN	W END	RISKE LN	2	A	545	41	22,345	A	4/30/2018	86
RISKLN	020	DELTA LN	DELTA LN	TOWER BRIDGE GATEWAY	2	A	643	44	28,292	A	4/30/2018	67
DENECT	010	DENISE CT	W CDS	BETTY WY	2	R	343	33	13,347	A	4/30/2018	34
DENECT	020	DENISE CT	BETTY WY	E CDS	2	R	318	33	12,522	A	4/30/2018	31
DEVOAV	010	DEVON AV	MANCHESTE ST	KINSINGTO ST	2	R	740	32	23,680	S	4/30/2018	68
DEVOAV	020	DEVON AV	KINSINGTO ST	JEFFERSON BL	2	R	350	36	12,600	S	4/30/2018	79
DBLOST	010	DIABLO ST	KINSINGTO ST	ESSEX AV	2	R	593	36	21,348	S	4/30/2018	65
DIANDR	010	DIANE DR	LESLIE LN	LINDEN RD	2	R	520	36	18,720	A	4/30/2018	74
DOBRDR	010	DOBROS DR	BRYTE AV	DOBROS DR	2	R	370	53	19,610	A	5/30/2018	41
DOBRDR	020	DOBROS DR	N END	S END	2	R	295	53	15,635	A	4/30/2018	37
DOLPCT	010	DOLPHIN CT	HIGHLAND DR	END N	2	R	109	54	7,130	A	4/30/2018	78
DOMAST	010	DOMINICA ST	N CDS	BONAIRE RD	2	R	469	33	18,655	A	4/30/2018	72
DONNRD	010	DONNER RD	SHASTA WY	END E	2	R	610	32	19,520	A	4/30/2018	87
DORAAV	010	DORAN AV	MARIGOLD ST	GARNET ST	2	R	643	32	20,576	A	4/30/2018	45
DOREPL	010	DORENA PL	END W	CAMANCHE ST	2	R	724	32	26,431	A	4/30/2018	89
DOUGST	010	DOUGLAS ST	SACRAMENT AV	FREMONT BLVD	2	C	640	36	23,040	A	4/30/2018	53
DOUGST	020	DOUGLAS ST	FREMONT BL	CUMMINS WY	2	C	1,220	34	41,480	A	4/30/2018	63
DOUGST	030	DOUGLAS ST	CUMMINS WY	LIGHTHOUS DR	2	C	710	36	25,560	A	4/30/2018	80
DOUGST	040	DOUGLAS ST	LIGHTHOUS DR	END	2	C	1,145	36	41,220	A	4/30/2018	26
DRAKST	010	DRAKES BAY ST	TREASURE RD	BODEGA BA RD	2	R	325	33	10,725	A	4/30/2018	75
DRVRST	010	DREVER ST	SOULE ST	END N	2	R	900	41	36,900	A	4/30/2018	63
DRVRST	020	DREVER ST	JEFFERSON BL	SOULE ST	2	R	1,090	41	44,690	A	5/24/2018	66
DREWST	010	DREW ST	TODHUNTER AV	FOURNESS DR	2	R	865	31	26,815	A	5/23/2018	76
DRFTCT	010	DRIFTWOOD CT	END	INDEPENDE AV	2	R	213	33	9,055	A	4/30/2018	43
DUETDR	010	DUET DR	CONSTITUT AV	PARADISE WY	2	R	1,055	36	37,900	S	4/30/2018	80
DULUST	010	DULUTH ST	GALVESTON ST	HOUSTON ST	2	R	875	33	28,875	A	4/30/2018	28
DULUST	020	DULUTH ST	HOUSTON ST	HARBOR BL	2	R	760	36	27,360	A	4/30/2018	22
ESTREE	010	E ST	WEST ST	RR TRACKS	2	R	1,417	38	53,846	A	4/30/2018	59
ESTREE	020	E ST	RR TRACKS	SEVENTH ST	2	R	463	36	16,668	A	4/30/2018	80
ESTREE	030	E ST	SEVENTH ST	WIDTH CHANGE	2	R	613	30	18,390	A	4/30/2018	85
ESTREE	040	E ST	WIDTH CHANGE	SECOND ST	2	R	1,417	37	52,429	A	4/30/2018	62
ESTREE	050	E ST	SECOND ST	E END	2	R	210	66	13,866	A	4/30/2018	89
EAGLST	010	EAGLE ST	LEWISTON RD	END N	2	R	639	32	23,711	A	4/30/2018	74
EDWACT	010	EDGEWATER CT	BAYSIDE RD	END	2	R	250	32	8,000	A	4/30/2018	96
EGRERD	010	EGRET RD	STONEGATE	CRESTED ST	2	R	273	32	8,736	A	4/30/2018	89
EITHST	010	EIGHTEENTH ST	PARK BL	DELAWARE AV	2	R	560	32	17,920	A	4/30/2018	33
EITHST	020	EIGHTEENTH ST	ALABAMA AV	VIRGINIA AV	2	R	270	32	8,640	A	4/30/2018	16
ELDEDR	010	ELDER DR	FREMONT BL	CUMMINS WY	2	R	1,185	32	37,920	A	4/30/2018	64
ELVTST	010	ELEVENTH ST	WEBSTER ST	MEADOW DR	2	R	805	20	15,500	A	4/30/2018	27

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ELVTST	020	ELEVENTH ST	MEADOW RD	PARK BL	2	R	270	32	8,640	A	4/30/2018	40
ELVTST	030	ELEVENTH ST	PARK BL	JEFFERSON BL	2	R	1,375	34	46,750	A	4/30/2018	43
ELKVST	010	ELK VALLEY ST	VILLAGE PW	BERRY CRE RD	2	R	240	33	7,920	A	4/30/2018	84
ELKHPL	010	ELKHORN PLAZA	DOUGLAS ST	REUTER DR	2	R	755	45	33,975	A	4/30/2018	83
ELLIST	010	ELLIOT ST	ARTHUR DR	KEGLE DR	2	R	660	35	23,100	A	4/30/2018	56
ELSNCT	010	ELSINORE CT	BRIDGEWAY LAKES DR	S CDS	2	R	184	32	9,154	A	4/30/2018	91
EMBADR	010	EMBARCADERO DR	RIVERSIDE PW	EMBARCADERO DR	2	R	1,090	45	49,050	A	4/30/2018	70
EMBADR	020	EMBARCADERO DR	EMBARCADERO DR	RIVERSIDE PW	2	R	1,214	45	54,630	A	4/30/2018	75
ENTEBL	010	ENTERPRISE BL	WEST CAPI AV	I80 EB ONRAMP	4	A	750	80	60,000	A	4/30/2018	61
ENTEBL	020	ENTERPRISE BL	I80 EB ONRAMP	LAKE RD	7	A	255	84	21,420	A	4/30/2018	32
ENTEBL	030	ENTERPRISE BL	LAKE RD	COP 490' S/O INDUSTRIA BL	4	A	1,260	60	75,600	A	4/30/2018	14
ENTEBL	040	ENTERPRISE BL	COP 490' S/O INDUSTRIA BL	SEAPORT BL	5	A	1,673	76	127,148	A	4/30/2018	52
ENTEBL	050	ENTERPRISE BL	SEAPORT BL	CHANNEL DR	2	A	1,975	44	86,900	A	4/30/2018	52
ESSXAV	010	ESSEX AV	DIABLO ST	KINSINGTO ST	2	R	420	36	15,120	S	4/30/2018	65
ESTUCT	010	ESTUARY CT	CORANADO ST	END	2	R	270	32	8,640	A	4/30/2018	96
ECLDST	010	EUCLID ST	DELAWARE AV	ALABAMA AV	2	R	560	32	17,920	A	4/30/2018	35
EVRGAV	010	EVERGREEN AV	PINE ST	HARBOR BL	2	C	1,960	37	72,520	A	4/30/2018	42
EVRGAV	020	EVERGREEN AV	HARBOR BL	COKE ST	2	C	1,830	41	75,030	A	4/30/2018	43
EVRGAV	030	EVERGREEN AV	COKE ST	SYCAMORE AV	2	C	1,560	36	56,160	A	4/30/2018	60
EVRGAV	040	EVERGREEN AV	SYCAMORE AV	POPLAR AV	2	C	880	31	27,280	A	4/30/2018	51
EVRGAV	050	EVERGREEN AV	POPLAR AV	WESTACRE RD	2	C	940	31	29,140	A	4/30/2018	15
FSTREE	010	F ST	JEFFERSON BL	WEST ST	2	C	1,255	42	53,830	A	4/30/2018	32
FSTREE	020	F ST	WEST ST	RR TRACKS	2	C	1,420	40	56,800	A	4/30/2018	54
FSTREE	030	F ST	RR TRACKS	FIFTH ST	2	C	1,280	44	56,320	A	4/30/2018	50
FSTREE	040	F ST	FIFTH ST	THIRD ST	2	C	805	37	29,785	A	4/30/2018	74
FAIRDR	010	FAIRWAY DR	FREMONT BL	CUMMINS WY	2	R	1,299	32	41,568	A	4/30/2018	73
FLBKST	010	FALLBROOK ST	PARK BL	GRANDE VIEW AV	2	R	700	36	25,200	A	4/30/2018	87
FLBKST	020	FALLBROOK ST	GRANDE VIEW AV	SONORS AV	2	R	573	32	18,336	A	4/30/2018	74
FARNRD	010	FARALLON RD	BALLENA B RD	BODEGA BA RD	2	R	590	33	19,470	A	4/30/2018	71
FATH	010	FATHOM PL	GEARY ST	END	2	R	309	35	13,372	S	4/30/2018	55
FRNDCR	010	FERNDALE CR	END S	LAKEWOOD DR	2	R	130	31	6,669	A	4/30/2018	48
FRNWST	005	FERNWOOD ST	N END	MEADOW RD	2	R	650	32	20,800	A	4/30/2018	58
FRNWST	010	FERNWOOD ST	MEADOW RD	SONORA AV	2	R	255	32	8,160	A	4/30/2018	54
FRNWST	020	FERNWOOD ST	SONORA AV	FALLBROOK ST	2	R	965	32	30,880	A	4/30/2018	51
FRNWST	030	FERNWOOD ST	FALLBROOK ST	S END	2	R	295	32	9,440	A	4/30/2018	99
FIFTST	010	FIFTEENTH ST	JEFFERSON BL	SOUTH RIV RD	2	A	895	48	39,380	A	4/30/2018	80
FIFNST	010	FIFTEENTH ST	PARK BL	MARYLAND AV	2	A	300	43	12,900	S	4/30/2018	51
FIFNST	020	FIFTEENTH ST	MARYLAND AV	VIRGINIA AV	2	A	1,345	34	45,730	S	4/30/2018	60
FIFNST	030	FIFTEENTH ST	VIRGINIA AV	JEFFERSON BL	2	A	310	40	12,400	A	4/30/2018	27
FIFHST	010	FIFTH ST	A ST	C ST	4	A	980	60	64,800	A	4/30/2018	59

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FIFHST	020	FIFTH ST	C ST	E ST	4	A	980	60	60,240	A	4/30/2018	41
FIFHST	030	FIFTH ST	E ST	TOWER BRIDGE GATEWAY	4	A	1,528	61	93,208	A	4/30/2018	43
FIF5ST	190	FIFTH ST	15TH	I-80 ON RAMP	2	A	751	44	33,044	A	4/30/2018	63
FIF5ST	200	FIFTH ST	I-80 ON RAMP	PAVEMENT CHANGE	2	A	385	51	19,635	A	4/30/2018	39
FIF5ST	210	FIFTH ST	PAVEMENT CHANGE	TOWER CT	2	A	275	41	11,275	A	4/30/2018	72
FIF5ST	220	FIFTH ST	TOWER CT	RISKE LN	2	A	1,890	86	162,540	P	2/28/2018	99
FIF5ST	230	FIFTH ST	RISKE LN	TOWER BRIDGE GATEWAY	2	A	1,097	64	70,208	P	2/28/2018	99
FIF5ST	240	FIFTH ST	TOWER BRIDGE GATEWAY	WEST CAPI AV	2	A	74	75	5,550	A	2/28/2018	93
FIJIST	010	FIJI ISL ST	COOPER IS RD	OAKLAND BAY DR	2	R	1,150	33	37,950	A	4/30/2018	70
FILLPL	010	FILLMORE PL	END	GATEWAY DR	2	R	364	37	16,291	S	4/30/2018	68
FIRST	010	FIRST ST	W END	SUTTER AV	2	R	1,000	23	23,000	A	4/30/2018	42
FISHAV	010	FISHER AV	ARMFIELD AV	GOODELL AV	2	R	1,645	24	39,480	A	4/30/2018	59
FISHAV	020	FISHER AV	GOODELL AV	END W	2	R	596	24	18,621	A	4/30/2018	50
FLSMCT	010	FOLSOM CT	N CDS	BRIDGEWAY LAKES DR	2	R	174	32	8,834	A	4/30/2018	91
FSTRCT	010	FOSTER CT	N CDS	BRIDGEWAY LAKES DR	2	R	174	32	8,834	A	4/30/2018	90
FOTNDR	010	FOUNTAIN DR	LIGHTHOUSE DR	ROUNDABOUT	2	R	1,532	70	107,240	A	4/30/2018	72
FOURDR	010	FOURNESS DR	HOBSON AV	ANNA ST	2	R	1,330	32	42,560	A	4/30/2018	61
FRTHST	010	FOURTH ST	D ST	F ST	2	R	925	36	33,300	A	4/30/2018	61
FOURST	010	FOURTH ST	N END	B ST	2	R	495	36	17,820	A	4/30/2018	73
FRTHST	020	FOURTH ST	F ST	G ST	2	R	360	41	14,760	A	4/30/2018	70
FOURST	020	FOURTH ST	B ST	C ST	2	R	475	46	21,850	A	4/30/2018	80
FRTHST	030	FOURTH ST	G ST	S END	2	R	558	24	13,392	A	4/30/2018	91
FOURST	030	FOURTH ST	C ST	S CDS	2	R	323	37	12,930	A	4/30/2018	26
FOWLRD	010	FOWLER RD	BRIDGEWY DR	BIG BEAR ST	2	R	760	32	24,320	A	4/30/2018	82
FRSRDR	010	FRASER ISL DR	SAN SALVADOR ST	LONG ISL ST	2	R	435	33	14,355	A	4/30/2018	83
FREBDR	010	FREEBOARD DR	END	STARBOARD DR	2	R	440	28	15,104	A	4/30/2018	57
FREMBL	010	FREMONT BL	ARTHUR DR	KEGLE DR	2	R	650	36	23,400	A	4/30/2018	58
FREMBL	020	FREMONT BL	KEGLE DR	HARDY DR	2	R	775	36	27,900	A	4/30/2018	70
FREMBL	030	FREMONT BL	HARDY DR	DOUGLAS ST	2	R	1,025	35	35,875	A	4/30/2018	78
FREN	010	FRENCH AV	DAVIS RD	1500' N/O DAVIS	2	R	1,500	24	36,000	S	4/30/2018	33
FREN	020	FRENCH AV	1500' N/O DAVIS	HARMON RD	2	R	1,500	24	36,000	A	4/30/2018	25
FRNTST	010	FRONT ST	SHORE ST	TERMINAL ST	2	R	1,295	31	40,145	A	4/30/2018	56
GSTREE	010	G ST	W END	SIXTH ST	2	R	911	27	24,597	A	4/30/2018	74
GSTREE	020	G ST	SIXTH ST	FIFTH ST	2	R	400	36	14,400	A	4/30/2018	79
GSTREE	030	G ST	FIFTH ST	THIRD ST	2	R	785	34	26,690	A	4/30/2018	79
GDWLCT	010	GADWALL CT	END W	SUMMERFIE DR	2	R	238	33	9,880	A	4/30/2018	22
GLIORD	010	GALIANO ISL RD	PENDER IS ST	BOWEN ISL DR	2	R	270	33	8,910	A	4/30/2018	87
GALVST	010	GALVESTON ST	DULUTH ST	CANAL ST	2	R	1,505	36	54,180	A	4/30/2018	25
GARDST	010	GARDEN ST	RIVERFRONT	FIFTH ST	2	R	653	34	22,202	P	2/28/2018	99
RISKLN	030	GARDEN ST	TOWER BRIDGE GATEWAY	WEST CAPI AV	2	A	1,000	80	80,000	A	4/30/2018	34

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GRDIAV	010	GARDENIA AV	DEERWOOD ST	CLARENDON ST	2	R	288	31	8,928	A	4/30/2018	75
GARNST	010	GARNET ST	WEST CAPI AV	DORAN AV	2	R	1,083	32	34,656	A	4/30/2018	49
GARYCT	010	GARY CT	SUMMERFIE DR	END E	2	R	123	50	6,150	S	4/30/2018	45
GATEDR	010	GATEWAY DR	JEFFERSON BL	N BEACH AV	2	C	1,190	32	38,080	A	4/30/2018	43
GATEDR	020	GATEWAY DR	N BEACH AV	SAUSALITO RD	2	R	1,475	36	53,100	S	4/30/2018	63
GEARST	010	GEARY ST	END S	RANDOLPH RD	2	R	1,248	37	47,898	S	4/30/2018	70
GLADAV	010	GLADYS AV	END	SEYMORE AV	2	R	771	32	24,747	S	4/30/2018	13
GLENST	010	GLENWOOD ST	ALAMO PL	ORINDA PL	2	R	640	37	23,680	A	4/30/2018	75
GLIDAV	010	GLIDE AV	RICE AV	GLIDE CT	2	R	1,220	22	26,840	S	4/30/2018	80
GLIDAV	020	GLIDE AV	GLIDE CT	WEST CAPI AV	2	R	1,330	24	31,920	S	4/30/2018	71
GLDKCT	010	GOLD CREEK CT	BRIDGEWAY LAKES DR	S CDS	2	R	299	32	12,834	A	4/30/2018	95
GOLDST	010	GOLD HILL ST	SANDY POI RD	QUAIL RD	2	R	530	32	16,960	A	4/30/2018	84
GOGADR	010	GOLDEN GATE DR	OAKLAND B DR	PETER ISL DR	2	R	1,065	33	35,145	A	4/30/2018	66
GOGADR	020	GOLDEN GATE DR	PETER ISL DR	HALF MOON CIR	2	C	1,020	33	33,660	A	4/30/2018	68
GOGADR	030	GOLDEN GATE DR	HALF MOON CR	PROMENADE WY	2	C	375	44	16,500	A	4/30/2018	63
GOGADR	040	GOLDEN GATE DR	PROMENADE WY	HALF MOON CR	2	C	320	44	14,080	A	4/30/2018	66
GOGADR	050	GOLDEN GATE DR	HALF MOON CR	SHERMAN I RD	2	C	1,475	34	50,150	A	4/30/2018	65
GOGADR	060	GOLDEN GATE DR	SHERMAN I RD	OAKLAND B DR	2	C	1,425	33	47,025	A	4/30/2018	60
GOGADR	080	GOLDEN GATE DR	MARSHALL RD	OAKLAND BAY DR	2	C	708	33	23,364	A	4/30/2018	45
GLEYCT	010	GOLDENEYE CT	W CDS	MARECA WY	2	R	408	33	15,492	A	4/30/2018	91
GRHMRD	010	GRAHAM ISL RD	STUART IS ST	E CDS	2	R	738	33	27,532	A	4/30/2018	86
GRVIAV	010	GRANDE VISTA AV	DEERWOOD ST	FALLBROOK ST	2	R	960	34	32,640	S	4/30/2018	74
GRANCT	010	GRANT CT	N BEACH AV	END	2	R	244	31	10,915	A	4/30/2018	54
GREERD	010	GREENBRIER RD	HAMPTON ST	BLUE ROCK ST	2	R	1,310	32	41,920	A	4/30/2018	72
GREERD	020	GREENBRIER RD	BLUE ROCK ST	END	2	R	1,176	32	37,632	A	4/30/2018	95
GREMAV	010	GREENMEADOW AV	GARNET ST	PINE ST	2	R	725	32	23,200	A	4/30/2018	55
GREEAV	010	GREENWOOD AV	FREMONT BL	CUMMINS WY	2	R	1,200	32	38,400	A	4/30/2018	74
GREGAV	010	GREGORY AV	SOUTH RIV RD	1300' N/ SO. RIVER	2	A	1,300	34	44,234	A	4/30/2018	68
GREGAV	020	GREGORY AV	1300' N/ SO. RIVER	BEVAN RD	2	A	1,525	34	51,850	A	4/30/2018	72
GREGAV	030	GREGORY AV	BEVAN RD	1500' N/ BEVAN RD	2	A	1,500	32	48,000	A	4/30/2018	69
GREGAV	040	GREGORY AV	1500' N/ BEVAN	JEFFERSON BL	2	A	1,830	32	58,560	A	4/30/2018	63
GRYLCT	010	GREYCALLS CT	END	TEAL DR	2	R	248	32	7,936	S	4/30/2018	52
GRIZRD	010	GRIZZLY BAY RD	GOLDEN GATE DR	E CDS	2	R	1,319	33	46,811	A	4/30/2018	71
GUAPST	010	GUADELUPE ST	OAKLAND BAY DR	MARSHALL RD	2	R	760	33	25,080	A	4/30/2018	77
GUFIST	010	GULF ISL ST	GRAHAM IS RD	S CDS	2	R	539	33	20,965	A	4/30/2018	86
HAITIRD	010	HAITI RD	GUADELUPE ST	DOMINICA ST	2	R	890	33	29,370	A	4/30/2018	80
HAMOCR	010	HALF MOON BAY CR	TACOMA NARROWS ST	GOLDEN GATE DR	2	R	1,150	36	41,400	A	4/30/2018	76
HAMOCR	020	HALF MOON BAY CR	TACOMA NARROWS ST	GOLDEN GATE DR	2	R	1,160	36	41,760	A	4/30/2018	72
HAMOCR	030	HALF MOON BAY CR	GOLDEN GATE DR	SANTA CRUZ RD	2	R	180	36	6,480	A	4/30/2018	75
HALYRD	010	HALYARD DR	INDUSTRIA BL	BEACON BL	2	R	645	35	22,575	A	4/30/2018	41

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HAKYRD	020	HALYARD DR	BEACON BL	HARBOR BL	2	R	890	36	32,040	A	4/30/2018	41
HAMPST	010	HAMPTON ST	PRINCETON RD	END N	2	R	474	32	18,431	A	4/30/2018	73
HARBBL	010	HARBOR BL	CITY LIMIT	I80	2	A	2,890	32	92,480	A	4/30/2018	59
HARBBL	020	HARBOR BL	I80	RIVERBANK RD	2	A	560	26	14,560	A	4/30/2018	74
HARBBL	030	HARBOR BL	RIVERBANK RD	SUNSET AV	2	A	780	34	26,520	A	4/30/2018	58
HARBBL	040	HARBOR BL	SUNSET AV	REED AV	2	A	990	26	25,740	A	4/30/2018	32
HARBBL	050	HARBOR BL	REED AV	COP 382' S/O RIVERPOINT CT	4	A	1,301	60	78,060	A	4/30/2018	81
HARBBL	060	HARBOR BL	COP 382' S/O RIVERPOINT CT	RICE AV	4	A	702	60	42,120	P	2/28/2018	99
HARBBL	080	HARBOR BL	DULUTH ST	WEST CAPI AV	5	A	2,165	70	151,550	A	4/30/2018	73
HARBBL	090	HARBOR BL	WEST CAPI AV	EVERGREEN AV	5	A	880	76	66,880	A	4/30/2018	40
HARBBL	100	HARBOR BL	EVERGREEN AV	180 BRIDGE	6	A	570	82	46,740	A	4/30/2018	61
HARBBL	110	HARBOR BL	180 BRIDGE	I80 EB ONRAMP	8	A	364	108	39,312	A	4/30/2018	54
HARBBL	120	HARBOR BL	I80 EB ONRAMP	BEACON BL	7	A	655	99	64,845	A	4/30/2018	66
HARBBL	130	HARBOR BL	BEACON BL	DEL MONTE ST	6	A	620	76	47,120	A	4/30/2018	34
HARBBL	140	HARBOR BL	DEL MONTE ST	INDUSTRIA BL	5	A	385	77	29,645	A	4/30/2018	34
HARBBL	150	HARBOR BL	INDUSTRIAL BL	STONE BL	2	A	1,030	18	18,540	A	4/30/2018	8
HARPPL	010	HARBOR POINT PL	STILLWATE RD	E CDS	2	R	250	44	14,014	A	4/30/2018	69
HARDDR	010	HARDY DR	END	CUMMINS WY	2	R	1,335	36	48,060	A	4/30/2018	57
HARDDR	020	HARDY DR	CUMMINS WY	END	2	R	970	36	34,920	A	4/30/2018	75
HARMRD	010	HARMON RD	JEFFERSON BL	PARTRIDGE AV	2	R	1,360	21	28,560	A	4/30/2018	20
HARMRD	020	HARMON RD	PARTRIDGE AV	ANTIOCH AV	2	R	1,250	20	25,000	A	4/30/2018	14
HARTAV	010	HART AV	ALLAN AV	HIGGINS RD	2	R	1,615	22	35,530	S	4/30/2018	39
HAVAST	010	HAVASU ST	SILVERWOO RD	LEWISTON RD	2	R	270	32	8,640	A	4/30/2018	85
HVHR	010	HAVERHILL ST	GRANDE VI AV	MEADOW RD	2	R	810	34	27,540	A	4/30/2018	49
HVHR	020	HAVERHILL ST	MEADOW RD	NORFOLK AV	2	R	1,088	32	34,816	A	4/30/2018	58
HAWACT	010	HAWAII CT	BIG ISLAND RD	S END	2	R	290	33	9,570	A	4/30/2018	72
HEADRD	010	HEADSLANE RD	GEARY ST	HEARST ST	2	R	1,040	36	37,440	S	4/30/2018	71
HEARST	010	HEARST ST	UNION SQ RD	RANDOLPH RD	2	R	770	36	27,720	S	4/30/2018	72
HEMLST	010	HEMLOCK ST	EVERGREEN AV	BUCKEYE DR	2	R	430	32	13,760	A	4/30/2018	46
HENSRD	010	HENSHAW RD	BRIDGEWAY LAKES DR	SOUTHPORT PW	2	R	1,435	32	45,920	A	4/30/2018	83
HENSRD	020	HENSHAW RD	SOUTHPORT PW	EAGLE ST	2	R	200	32	6,400	A	4/30/2018	85
HICKWY	010	HICKORY WY	W END	ORCHARD WY	2	R	660	32	21,120	A	4/30/2018	43
HIGGRD	010	HIGGINS RD	LINDEN RD	211' E/ SUMMERFIELD DR	2	C	1,120	36	40,320	S	4/30/2018	36
HIGGRD	020	HIGGINS RD	211' E/ SUMMERFIELD DR	311' E/ CONSTITUTION	2	C	710	24	17,040	S	4/30/2018	42
HIGGRD	030	HIGGINS RD	CONSTITUTION AV	PARADISE WY	2	C	728	32	23,296	S	4/30/2018	47
HIGGRD	040	HIGGINS RD	PARADISE WY	JEFFERSON BL	2	C	1,030	34	35,020	S	4/30/2018	48
HIGHDR	010	HIGHLAND DR	CHELAN RD	REDONDO RD	2	R	445	33	14,685	A	4/30/2018	81
HIGHDR	020	HIGHLAND DR	REDONDO RD	LAKE WASH BL	2	R	255	44	11,220	A	4/30/2018	91
HIGHDR	030	HIGHLAND DR	LAKE WASH BL	PRINCETON RD	2	R	265	45	11,925	A	4/30/2018	82
HIGHDR	040	HIGHLAND DR	PRINCETON RD	MIRA LOMA CT	2	R	1,515	32	48,480	A	4/30/2018	78

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HIGHDR	050	HIGHLAND DR	MIRA LOMA CT	DOLPHIN CT	2	R	560	32	17,920	A	4/30/2018	81
HIGHDR	060	HIGHLAND DR	DOLPHIN CT	BAYBERRY ST	2	R	435	32	13,920	A	4/30/2018	79
HIGHDR	070	HIGHLAND DR	BAYBERRY CT	CORANADO ST	2	R	900	32	28,800	A	4/30/2018	88
HILAAV	010	HILARY AV	DAVIS RD	HARMON RD	2	R	1,125	17	19,125	A	4/30/2018	24
HILLCT	010	HILL CT	DREW ST	S CDS	2	R	190	32	8,641	A	4/30/2018	73
HOBAVN	010	HOBSON (N) AV	HOBSON AV	RIVERBANK RD	2	R	2,120	32	67,840	A	4/30/2018	46
HOBSAV	010	HOBSON AV	WATER ST	SOLANO ST	2	R	395	36	14,220	A	5/23/2018	25
HOBSAV	020	HOBSON AV	SOLANO ST	BRYTE AV	2	R	795	36	28,620	S	5/25/2018	51
HOBSAV	030	HOBSON AV	BRYTE AV	TODHUNTER AV	2	R	1,110	36	39,960	S	4/30/2018	74
HOBSAV	040	HOBSON AV	TODHUNTER AV	FOURNESS DR	2	R	1,075	31	33,325	S	4/30/2018	66
HOBSAV	050	HOBSON AV	FOURNESS DR	KEGLE DR	2	R	985	32	31,520	S	4/30/2018	76
HOLLRD	010	HOLLAND DR	ANNA ST	CARRIE ST	2	R	1,210	32	38,720	A	4/30/2018	49
HOLLST	010	HOLLY ST	PECAN ST	MAPLE ST	2	R	795	32	25,440	A	4/30/2018	84
HOOPRD	010	HOOPA RD	STONEGATE	END E	2	R	543	32	17,376	A	4/30/2018	83
HOPLST	010	HOPLAND ST	LASSIK ST	HOOPA ST	2	R	939	32	30,048	A	4/30/2018	70
HORBST	010	HORNBY ISL ST	OAKLAND BAY DR	MARSHALL RD	2	R	1,480	35	51,800	A	4/30/2018	83
HOUSST	010	HOUSTON ST	DULUTH ST	CANAL ST	2	R	1,825	27	49,275	A	4/30/2018	28
HOUSST	020	HOUSTON ST	CANAL ST	WEST CAPI AV	2	R	300	27	8,100	S	5/30/2018	25
HGHSCR	010	HUGHES CR	END	PARK BL	2	R	450	21	9,450	S	4/30/2018	53
HUMEST	010	HUME ST	BRIDGEWAY LAKES	ROCK CREEK RD	2	R	548	32	17,536	A	4/30/2018	83
HUNTRD	010	HUNTINGTON RD	W CDS	ARROWHEAD ST	2	R	254	32	11,394	A	4/30/2018	79
HUNTRD	020	HUNTINGTON RD	ARROWHEAD ST	COLLINS ST	2	R	855	32	27,360	A	4/30/2018	89
INDPAV	010	INDEPENDENCE AV	DUET DR	DRIFTWOOD CT	2	R	785	36	28,260	S	4/30/2018	58
INDPAV	020	INDEPENDENCE AV	DRIFTWOOD CT	SAFFLOWER PL	2	R	480	42	20,160	S	4/30/2018	67
INDPAV	030	INDEPENDENCE AV	SAFFLOWER PL	LINDEN RD	2	R	340	60	20,400	S	4/30/2018	55
INDUBL	010	INDUSTRIAL BL	ENTERPRIS BL	OVERLAND CT	4	A	1,045	76	79,420	A	4/30/2018	55
INDUBL	020	INDUSTRIAL BL	OVERLAND CT	MAIN USPS SIGN	4	A	1,585	76	120,460	A	4/30/2018	54
INDUBL	030	INDUSTRIAL BL	MAIN USPS SIGN	PARKWAY BL	4	A	1,250	76	95,000	A	4/30/2018	56
INDUBL	040	INDUSTRIAL BL	PARKWAY BL	3500 INDUSTRIAL BL	4	A	912	76	69,312	A	4/30/2018	50
INDUBL	050	INDUSTRIAL BL	3500 INDUSTRIAL BL	3380 INDUSTRIAL BL	4	A	1,201	56	67,256	A	4/30/2018	58
INDUBL	060	INDUSTRIAL BL	3380 INDUSTRIAL BL	BEACON BL	4	A	1,204	56	67,424	A	4/30/2018	63
INDUBL	070	INDUSTRIAL BL	BEACON BL	HARBOR BL	4	A	1,775	56	99,400	A	4/30/2018	61
INDUBL	090	INDUSTRIAL BL	BOATMAN AV	TERMINAL ST	5	A	790	76	60,040	A	4/30/2018	40
INDUBL	100	INDUSTRIAL BL	TERMINAL ST	STONE BL	5	A	1,095	76	83,220	A	4/30/2018	59
INGLDR	010	INGLEWOOD DR	FREMONT BL	CUMMINS WY	2	R	1,190	32	38,080	A	4/30/2018	78
IRISPL	010	IRIS PL	W CDS	RIVERMONT ST	2	R	269	32	11,874	A	4/30/2018	85
IRONWY	010	IRON WOOD WY	SPRUCE ST	LINDEN RD	2	R	1,120	36	40,320	S	4/30/2018	55
IRWKSAV	010	IRONWORKS AV	TOWER BRIDGE GATEWAY	BALLPARK DR	2	R	735	25	18,375	A	4/30/2018	84
ISABST	010	ISABELLA ST	KAWEAH ST	COYOTE RD	2	R	730	32	23,360	A	4/30/2018	91
JCKNST	010	JACKSON ST	JEFFERSON BL	DREVER ST	2	R	355	46	16,331	A	5/24/2018	37

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JCKNST	020	JACKSON ST	DREVER ST	SOULE ST	2	R	227	46	10,442	A	4/30/2018	7
JAQNLN	010	JACQUELYN LN	SUMMERFIELD DR	JANET DR	2	R	275	36	9,900	A	4/30/2018	57
JAMAST	010	JAMAICA ST	OAKLAND BAY DR	MARSHALL RD	2	R	640	33	21,120	A	4/30/2018	79
JANTDR	010	JANET DR	JACQUELYN LN	BETTY WY	2	R	655	36	23,580	A	4/30/2018	60
JANTDR	020	JANET DR	BETTY WY	E CDS	2	R	498	33	18,462	A	4/30/2018	28
JASMAV	010	JASMINE AV	LISBON AV	RIVERBANK RD	2	R	1,370	32	43,840	S	4/30/2018	49
JAVACT	010	JAVA CT	N CDS	MANDURA ST	2	R	314	33	13,540	A	4/30/2018	71
JEFRBL	010	JEFFERSON BL	SACRAMENTO AV	TRIANGLE CT	4	A	985	81	79,785	A	4/30/2018	67
JEFRBL	020	JEFFERSON BL	TRIANGLE CT	MICHIGAN BL	4	A	1,480	84	124,320	A	4/30/2018	52
JEFRBL	030	JEFFERSON BL	MICHIGAN BL	WEST CAPI AV	5	A	815	76	61,940	A	4/30/2018	39
JEFRBL	040	JEFFERSON BL	WEST CAPI AV	MERKLEY AV	5	A	505	60	30,300	A	4/30/2018	9
JEFRBL	050	JEFFERSON BL	MERKLEY AV	CAPITOL MALL ONRAMP	4	A	792	65	51,480	A	4/30/2018	33
JEFRBL	060	JEFFERSON BL	CAPITOL MALL ONRAMP	PARK BL	6	A	400	80	32,000	A	4/30/2018	54
JEFRBL	070	JEFFERSON BL	PARK BL	FIFTEENTH ST	5	A	1,815	80	145,200	A	4/30/2018	45
JEFRBL	080	JEFFERSON BL	FIFTEENTH ST	VERMONT AV	5	A	1,610	84	135,240	A	4/30/2018	35
JEFRBL	090	JEFFERSON BL	VERMONT AV	STONE BL	5	A	1,685	84	141,540	A	4/30/2018	39
JEFRBL	100	JEFFERSON BL	STONE BL	SOUTH RIVER RD	4	A	1,320	64	84,480	A	4/30/2018	58
JEFRBL	110	JEFFERSON BL	SOUTH RIV RD	GATEWAY DR	4	A	1,570	62	106,460	A	4/30/2018	74
JEFRBL	120	JEFFERSON BL	GATEWAY DR	LAKE WASHINGTON BL	4	A	1,820	62	121,240	A	4/30/2018	68
JEFRBL	130	JEFFERSON BL	LAKE WASHINGTON BL	LINDEN RD	6	A	1,130	100	113,000	A	4/30/2018	57
JEFRBL	140	JEFFERSON BL	LINDEN RD	HIGGINS RD	4	A	1,600	62	104,720	A	4/30/2018	66
JEFRBL	150	JEFFERSON BL	HIGGINS RD	PERKINS RD	5	A	1,265	76	100,940	A	4/30/2018	67
JEFRBL	160	JEFFERSON BL	PERKINS RD	LINDEN RD	5	A	2,046	76	155,496	A	4/30/2018	71
JEFRBL	170	JEFFERSON BL	LINDEN RD	HARMON RD	4	A	635	74	46,990	A	4/30/2018	70
JEFRBL	180	JEFFERSON BL	HARMON RD	339' S/O HARMON RD	2	A	339	76	25,764	A	4/30/2018	63
JEFRBL	190	JEFFERSON BL	339' S/O HARMON RD	MARSHALL RD	2	A	1,035	42	43,470	A	4/30/2018	46
JEFRBL	200	JEFFERSON BL	MARSHALL RD	GREGORY AV	2	A	1,090	46	50,140	A	4/30/2018	35
JEFRBL	210	JEFFERSON BL	GREGORY AV	MAILBOX #3815	2	A	2,089	25	52,225	A	4/30/2018	21
JEFRBL	220	JEFFERSON BL	MAILBOX #3815	BEVAN RD	2	A	2,020	25	50,500	A	4/30/2018	30
JEFRBL	230	JEFFERSON BL	BEVAN RD	SOUTHPORT PW	2	A	1,740	24	41,760	A	4/30/2018	30
JEFRBL	240	JEFFERSON BL	SOUTHPORT PW	CANAL S/O SOUTHPORT PW	2	A	1,578	25	39,450	A	4/30/2018	25
JEFRBL	250	JEFFERSON BL	CANAL S/O SOUTHPORT PW	BURROWS AV	2	A	1,955	24	46,920	A	4/30/2018	23
JEFRBL	260	JEFFERSON BL	BURROWS AV	ARMFIELD AV	2	A	1,815	24	43,560	A	4/30/2018	29
JEFRBL	270	JEFFERSON BL	ARMFIELD AV	CITY LIMIT	2	A	1,540	24	36,960	A	4/30/2018	48
JENNCT	010	JENNINGS CT	LEWISTON RD	END N	2	R	234	32	10,542	A	4/30/2018	75
JOANST	010	JOAN ST	KEGLE DR	HARDY DR	2	R	950	32	30,400	A	4/30/2018	69
JULIDR	010	JULIAN DR	FREMONT BL	CUMMINS WY	2	R	1,200	32	38,400	A	4/30/2018	70
KATYCR	010	KATHY CR	NANCY LN	BRENDA WY	2	R	875	36	31,500	A	4/30/2018	67
KAUIRD	010	KAUAI RD	HALF MOON CR	S CDS	2	R	1,084	33	38,950	A	4/30/2018	72
KAWEST	010	KAWEAH ST	OTIS AV	COYOTE RD	2	R	730	32	23,360	A	4/30/2018	92

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KEGLRD	010	KEGLE DR	SACRAMENT AV	FREMONT BL	2	C	590	45	26,550	S	4/30/2018	33
KEGLRD	020	KEGLE DR	FREMONT BL	SHARIAN ST	2	C	1,440	36	51,840	A	4/30/2018	45
KEGLRD	030	KEGLE DR	SHARIAN ST	END	2	C	1,930	36	69,480	A	4/30/2018	53
KLYSDR	010	KELLYS ISL DR	HALF MOON CR	CATALINA RD	2	R	680	33	22,440	A	4/30/2018	70
KENTSRD	010	KENTS CT	OLIVE CT	END	2	R	268	33	8,844	S	4/30/2018	48
KILYCT	010	KIMBERLY CT	BETTY WY	E CDS	2	R	333	33	13,017	A	4/30/2018	34
KINECT	010	KING EDWARD CT	N CDS	KING EDWARD RD	2	R	209	33	10,075	A	4/30/2018	69
KINERD	010	KING EDWARD RD	BACON ISL ST	RYER ISL ST	2	R	680	33	22,440	A	4/30/2018	83
KSGTST	010	KINSINGTON ST	ASTER AV	DEVON DR	2	R	830	36	29,880	S	4/30/2018	63
KSGTST	020	KINSINGTON ST	DEVON DR	MANCHESTER ST	2	R	1,295	36	46,620	S	4/30/2018	49
KLAMRD	010	KLAMATH RD	CASCADE ST	SWIFT ST	2	R	650	33	21,450	A	4/30/2018	87
KODKPL	010	KODIAK ISL PL	ALEUTIAN ST	E CDS	2	R	244	33	11,230	A	4/30/2018	57
KOVRDR	010	KOVR DR	RIVERPOIN DR	E CDS	2	R	450	36	20,454	A	4/30/2018	76
LAJOST	010	LA JOLLA ST	REDDING RD	STONEGATE ST	2	R	895	33	29,535	A	4/30/2018	81
LGOOLN	010	LAGOON LN	INDEPENDE AV	MEADOW LA CR	2	R	950	36	34,200	S	4/30/2018	78
LAKEDR	010	LAKE RD	END W	ENTERPRIS BL	2	R	603	44	29,229	A	4/30/2018	48
LAKEDR	020	LAKE RD	ENTERPRIS BL	END E	2	R	1,590	28	44,520	A	4/30/2018	57
LAKEBL	010	LAKE WASHIGNTON BL	VILLAGE PW	REDWOOD AV	2	A	1,950	30	58,500	A	4/30/2018	78
LAKEBL	020	LAKE WASHIGNTON BL	REDWOOD AV	HIGHLAND ST	2	A	1,424	38	59,533	A	4/30/2018	71
LAKEBL	030	LAKE WASHIGNTON BL	HIGHLAND ST	SILVERADO ST	2	A	665	38	28,270	A	4/30/2018	66
LAKEBL	040	LAKE WASHIGNTON BL	SILVERADO ST	STONEGATE DR	2	A	610	51	31,110	A	4/30/2018	65
LAKEBL	050	LAKE WASHIGNTON BL	STONEGATE DR	STOPLIGHT AT SOUTHPORT TOV	2	A	690	73	50,370	A	4/30/2018	69
LAKEBL	060	LAKE WASHIGNTON BL	STOPLIGHT AT SOUTHPORT TC	SOUTHPORT PW	4	A	916	110	100,760	S	4/30/2018	65
LAKEBL	070	LAKE WASHIGNTON BL	JEFFERSON BL	SOUTHPORT PW	6	A	1,745	82	143,090	A	4/30/2018	71
LAKEBL	090	LAKE WASHIGNTON BL	SOUTHPORT PW	STONE BL	4	A	2,640	62	163,680	S	4/30/2018	61
LAWODR	010	LAKEWOOD DR	DEERWOOD ST	PARK BL	2	R	1,110	31	34,410	A	4/30/2018	71
LANICT	010	LANAI CT	BIG ISLAND RD	S CDS	2	R	354	33	14,860	A	4/30/2018	67
LASSST	010	LASSEN ST	DONNER RD	WHITNEY PL	2	R	547	32	17,504	A	4/30/2018	83
LASKST	010	LASSIK ST	HOOPA RD	MODOC CT	2	R	1,237	33	40,821	A	4/30/2018	70
LAURLN	020	LAUREL LN	WESTACRE RD	ROCKROSE RD	2	R	1,050	32	33,600	A	4/30/2018	56
LEMORD	010	LEMON TREE RD	END W	MONTE VISTA	2	R	224	33	10,567	A	4/30/2018	87
LEMORD	020	LEMON TREE RD	MONTE VISTA	REDWOOD AV	2	R	745	36	26,820	A	4/30/2018	75
LEMORD	030	LEMON TREE RD	REDWOOD AV	PEACHTREE ST	2	R	185	36	6,660	A	4/30/2018	81
LEMORD	040	LEMON TREE RD	PEACHTREE ST	LIMEWOOD RD	2	R	890	36	32,040	A	4/30/2018	73
LESLLN	010	LESLIE LN	SHIRLEY ST	DIANE DR	2	R	530	36	19,080	A	4/30/2018	66
LETTST	010	LETTERMAN ST	GEARY ST	UNION SQ RD	2	R	530	36	19,080	S	4/30/2018	65
LEWIRD	010	LEWISTON RD	EAGLE ST	TAHOE ST	2	R	1,310	32	41,920	A	4/30/2018	85
LIHODR	010	LIGHTHOUSE DR	KEGLE DR	SPLIT @ #808 LIGHTHOUSE DR	2	C	1,653	43	71,079	A	4/30/2018	66
LIHODR	020	LIGHTHOUSE DR	SPLIT @ #808 LIGHTHOUSE DR	DOUGLAS ST	2	C	481	44	23,564	A	4/30/2018	69
LIHODR	030	LIGHTHOUSE DR	DOUGLAS ST	FOUNTAIN DR	2	C	1,330	38	56,540	A	4/30/2018	79

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LIHODR	040	LIGHTHOUSE DR	FOUNTAIN DR	MARINA WY	5	C	370	73	27,010	A	4/30/2018	65
LIHODR	050	LIGHTHOUSE DR	MARINA WY	A ST	4	C	1,510	60	90,600	A	4/30/2018	67
LILALN	010	LILAC LN	MANZANITA WY	MICHIGAN BL	2	R	1,150	32	36,800	A	4/30/2018	78
LILYPL	010	LILY PL	W CDS	POPPY ST	2	R	514	32	19,714	A	4/30/2018	81
LIMEWY	010	LIMEWOOD WY	END W	ALMOND ST	2	R	1,079	36	41,858	A	4/30/2018	70
LINDRD	010	LINDEN RD	SOUTH RIVER RD.	ASANTE LN	2	A	1,000	32	32,000	A	4/30/2018	33
LINDRD	020	LINDEN RD	ASANTE LN	BASTONE CT	2	A	1,000	20	20,000	A	4/30/2018	37
LINDRD	030	LINDEN RD	BASTONE CT	REDWOOD AV	4	A	980	30	29,400	S	4/30/2018	47
LINDRD	040	LINDEN RD	REDWOOD WY	IRONWOOD WY	4	A	695	42	29,190	A	4/30/2018	56
LINDRD	050	LINDEN RD	IRONWOOD WY	STONEGATE DR	4	A	1,930	52	100,360	A	4/30/2018	62
LINDRD	060	LINDEN RD	MOJAVE DR	CLARKSBURG BRANCHLINE	4	A	800	44	35,200	S	4/30/2018	61
LINDRD	070	LINDEN RD	CLARKSBURG BRANCHLINE	JEFFERSON BD	4	A	1,312	44	57,728	A	4/30/2018	42
LINDRD	N150	LINDEN RD	JEFFERSON BD	CANVASBACK WY	2	A	1,207	27	40,993	S	4/30/2018	73
LINDRD	N160	LINDEN RD	CANVASBACK WY	BRENDA WY	3	A	1,045	36	41,822	S	4/30/2018	80
LINDRD	N170	LINDEN RD	BRENDA WY	HIGGINS RD	2	A	1,121	34	43,437	S	4/30/2018	67
LINDRD	N180	LINDEN RD	HIGGINS RD	SUMMERFIELD DR	2	A	1,811	30	63,675	S	4/30/2018	55
LINDRD	N190	LINDEN RD	SUMMERFIELD DR	CONSTITUTION AV	2	A	935	30	30,151	S	4/30/2018	58
LINDRD	N200	LINDEN RD	CONSTITUTION AV	BRIDGE ST	2	A	1,864	30	60,122	S	4/30/2018	65
LINDRD	N210	LINDEN RD	BRIDGE ST	JEFFERSON BD	2	A	1,203	30	40,292	S	4/30/2018	72
LINDRD	S080	LINDEN RD	JEFFERSON BD	COFFEEBERRY	2	A	1,252	32	44,266	S	4/30/2018	70
LINDRD	S090	LINDEN RD	COFFEEBERRY	CONSTITUTION AV	2	A	1,787	30	57,812	S	4/30/2018	73
LINDRD	S100	LINDEN RD	CONSTITUTION AV	SUMMERFIELD DR	2	A	1,050	29	34,652	S	4/30/2018	80
LINDRD	S110	LINDEN RD	SUMMERFIELD DR	BETTY WY	3	A	1,811	34	63,675	S	4/30/2018	56
LINDRD	S120	LINDEN RD	BETTY WY	BRENDA WY	3	A	1,198	34	40,732	S	4/30/2018	70
LINDRD	S130	LINDEN RD	BRENDA WY	CANVASBACK WY	3	A	1,044	34	35,496	S	4/30/2018	70
LINDRD	S140	LINDEN RD	CANVASBACK WY	JEFFERSON BD	2	A	1,207	35	46,447	S	4/30/2018	71
LISBAV	010	LISBON AV	SUNSET AV	WATER ST	2	R	1,000	36	36,000	S	4/30/2018	36
LISBAV	020	LISBON AV	WATER ST	BRYTE SV	2	R	1,070	36	38,520	S	4/30/2018	56
LISBAV	030	LISBON AV	BRYTE AV	TODHUNTER AV	2	R	1,365	34	46,410	S	4/30/2018	50
LONECT	010	LONE PINE CT	END S	BARONA ST	2	R	344	33	14,527	A	4/30/2018	81
LNGIST	010	LONG ISL ST	N CDS	CAT ISL RD	2	R	524	33	20,470	A	4/30/2018	87
LNGCST	010	LONGCROFT ST	GRANDE VI AV	MEADOW RD	2	R	825	32	26,400	A	4/30/2018	75
LNGCST	020	LONGCROFT ST	MEADOW RD	END N	2	R	211	32	6,752	A	4/30/2018	46
LOPZRD	010	LOPEZ ISLAND RD	MALCOLM I ST	HORNBY IS ST	2	R	485	33	16,005	A	4/30/2018	61
MACKRD	010	MACKINAC RD	WHITE FIS RD	SAGINAW ST	2	R	270	33	8,910	A	4/30/2018	73
MADRAV	020	MADRONE AV	WESTACRE RD	ROCKROSE RD	2	R	1,030	32	32,960	A	4/30/2018	48
MALCST	010	MALCOLM ISL ST	LOPEZ ISL RD	OAKLAND B DR	2	R	1,265	33	41,745	A	4/30/2018	81
MALICT	010	MALIBU CT	REDONDO RD	END N	2	R	344	33	14,527	A	4/30/2018	80
MALLRD	010	MALLARD RD	WIGEON ST	STONEGATE	2	R	415	24	9,960	A	4/30/2018	89
MALLRD	020	MALLARD RD	STONEGATE	CONDOR ST	2	R	380	23	8,740	A	4/30/2018	89

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Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
MANCST	010	MANCHESTER ST	JEFFERSON BL	DEVON AV	2	R	1,260	31	39,060	S	4/30/2018	70
MANCST	020	MANCHESTER ST	DEVON AV	ASTER AV	2	R	788	32	26,004	S	4/30/2018	65
MANDCT	010	MANDARIN CT	TEAL DR	END	2	R	228	33	9,550	A	4/30/2018	41
MANDST	010	MANDURA ST	FIJI ISL ST	SUMATRA	2	R	655	33	21,615	A	4/30/2018	68
MANZWY	010	MANZANITA WY	W CDS	WASHINGTON AV	2	R	101	35	5,946	A	4/30/2018	91
MANZWY	020	MANZANITA WY	WASHINGTON AV	POPLAR ST	2	R	595	32	19,040	A	4/30/2018	79
MANZWY	030	MANZANITA WY	POPLAR ST	WESTACRE RD	2	R	875	35	30,625	A	4/30/2018	90
MAPLST	010	MAPLE ST	RICE AV	HOLLY ST	2	C	455	32	14,560	A	4/30/2018	96
MAPLST	020	MAPLE ST	HOLLY ST	MICHIGAN BL	2	C	1,040	32	33,280	A	4/30/2018	85
MAPLST	030	MAPLE ST	MICHIGAN BL	WEST CAPI AV	2	C	1,250	32	40,000	A	4/30/2018	79
MARECT	010	MARE ISL CT	BROTHER I RD	END	2	R	214	33	10,237	A	4/30/2018	69
MAREWY	010	MARECA WY	CANVASBAC WY	HIGGINS RD	2	R	1,750	36	63,000	S	4/30/2018	52
MARIST	010	MARIGOLD ST	PALOMAR AV	DORAN AV	2	R	805	32	25,760	A	4/30/2018	46
MARIDR	010	MARINA GREENS DR	RANDOLPH RD	SOUTH RIV RD	2	R	340	37	12,580	S	4/30/2018	76
MARIDR	020	MARINA GREENS DR	SOUTH RIV RD	MARINA GR DR	1	R	360	17	6,120	S	4/30/2018	80
MARNWY	010	MARINA WY	N END	LIGHTHOUSE DR	2	R	405	31	12,555	A	4/30/2018	73
MARKST	010	MARKET ST	RIVERFRONT	GATE	2	R	79	22	1,738	A	4/30/2018	89
MARKST	020	MARKET ST	GATE	FIFTH	2	R	79	22	1,738	A	4/30/2018	38
MARLST	010	MARLIN ST	GREENBRIER RD	CEDARBROOK RD	2	R	278	32	8,896	A	4/30/2018	97
MARSRD	010	MARSHALL RD	W END	SOUTHPORT PW	2	A	1,540	32	49,280	A	4/30/2018	68
MARSRD	020	MARSHALL RD	SOUTHPORT PW	MARTINIQU ST	2	A	1,110	32	35,520	A	4/30/2018	74
MARSRD	030	MARSHALL RD	MARTINIQU ST	GUADELUPE ST	2	A	1,010	32	32,320	A	4/30/2018	69
MARSRD	040	MARSHALL RD	GUADELUPE ST	GOLDEN GA DR	2	A	1,060	32	33,920	A	4/30/2018	42
MARSRD	050	MARSHALL RD	GOLDEN GA DR	SEYMORE AV	2	A	1,000	32	32,000	A	4/30/2018	44
MARSRD	060	MARSHALL RD	SEYMORE AV	ALLAN AV	2	A	1,385	33	45,705	A	4/30/2018	47
MARSRD	070	MARSHALL RD	ALLAN AV	JEFFERSON BL	2	A	905	32	28,960	A	4/30/2018	65
MARSST	010	MARSTON ST	HOLLAND DR	KEGLE DR	2	R	1,295	36	46,620	A	4/30/2018	43
MARQST	010	MARTINIQUE ST	MARSHALL RD	OAKLAND B DR	2	R	1,185	33	37,105	A	4/30/2018	74
MARTST	010	MARTIS ST	BEVAN RD	SAN VINEN RD	2	R	1,065	32	34,080	A	4/30/2018	90
MARTST	020	MARTIS ST	SAN VINEN RD	PILLSBURY RD	2	R	905	32	28,960	A	4/30/2018	90
MARYAV	010	MARYLAND AV	STONE BL	NINETEENT ST	2	R	950	32	30,400	A	4/30/2018	25
MARYAV	020	MARYLAND AV	NINETEENT ST	SEVENTEENTH ST	2	R	1,110	31	34,410	A	4/30/2018	22
MARYAV	030	MARYLAND AV	SEVENTEEN ST	FIFTEENTH ST	2	R	1,430	31	44,330	A	4/30/2018	26
MARYAV	040	MARYLAND AV	ELEVENTH ST	WEBSTER ST	2	R	731	35	25,585	A	4/30/2018	51
MARYCT	010	MARYLAND CT	END S	STONE BL	2	R	160	31	7,599	S	4/30/2018	82
MASSDR	010	MASSIE DR	END	OATES DR	2	R	623	53	34,942	S	4/30/2018	66
MAUIST	010	MAUI ST	OAKLAND B DR	HALF MOON CR	2	R	1,110	33	36,630	A	4/30/2018	75
MAYSTR	010	MAY ST	BRYTE AV	MYRTLE AV	2	R	690	33	22,770	S	5/24/2018	48
MAYSTR	020	MAY ST	MYRTLE AV	TODHUNTER AV	2	R	700	33	23,100	S	5/24/2018	78
MCKIWY	010	MCKINLEY WY	N CDS	OXFORD ST	2	R	231	32	10,049	A	4/30/2018	58

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MEADCR	010	MEADOW LARK CR	PHESANT H DR	LAGOON LN	2	R	640	33	21,120	S	4/30/2018	73
MEADCR	020	MEADOW LARK CR	LAGOON LN	STARLING LN	2	R	1,528	33	50,424	S	4/30/2018	72
MEADRD	010	MEADOW RD	WESTACRE RD	ELEVENTH ST	2	R	710	31	22,010	A	4/30/2018	34
MEADRD	020	MEADOW RD	WEBSTER ST	END	2	R	221	42	9,372	S	5/30/2018	35
MEADRD	030	MEADOW RD	ELEVENTH ST	WEBSTER ST	2	R	825	32	26,400	S	4/30/2018	40
MEADRD	040	MEADOW RD	CLARENDON ST	WESTACRE RD	2	R	1,475	36	53,100	A	4/30/2018	70
MENDCT	010	MENDOCINO CT	END S	COYOTE RD	2	R	304	32	12,991	A	4/30/2018	76
MENDCT	020	MENDOCINO CT	COYOTE RD	DORENA PL	2	R	290	32	9,280	A	4/30/2018	81
MERCWY	010	MERCED WY	MOJAVE DR	RUBICON WY	2	R	995	36	35,820	S	4/30/2018	60
MERGCT	010	MERGANSERS CT	END	MARECA WY	2	R	258	33	13,702	S	4/30/2018	80
MERGCT	020	MERGANSERS CT	MARECA WY	END	2	R	218	33	9,307	S	4/30/2018	33
MERKAV	010	MERKLEY AV	SYCAMORE AV	WESTACRE RD	2	C	1,830	32	58,560	A	4/30/2018	62
MERKAV	020	MERKLEY AV	WESTACRE RD	JEFFERSON BL	3	C	1,300	60	78,000	A	4/30/2018	61
MERKAV	030	MERKLEY AV	JEFFERSON BL	EL RANCHO CT	2	C	980	40	39,200	A	4/30/2018	59
MERKAV	040	MERKLEY AV	EL RANCHO CT	159' N/O EL RANCHO CT	2	C	159	60	9,540	A	4/30/2018	88
MERKAV	050	MERKLEY AV	159' N/O EL RANCHO CT	327' N/O EL RANCHO CT	2	C	168	30	5,040	P	2/28/2018	100
MERKAV	060	MERKLEY AV	327' N/O EL RANCHO CT	WEST CAPI AV	2	C	253	30	7,590	A	4/30/2018	89
MERRCT	010	MERRIT ISL CT	END	SHERMAN I RD	2	R	319	33	13,702	A	4/30/2018	70
MESAST	010	MESA GRANDE ST	COLUMBUS RD	STONEGATE DR	2	R	175	33	5,775	A	4/30/2018	87
MICHBL	010	MICHIGAN BL	PECAN ST	COP 211' E/O MAPLE ST	2	C	1,263	32	40,416	A	4/30/2018	70
MICHBL	020	MICHIGAN BL	COP 211' E/O MAPLE ST	WESTACRE RD	2	C	1,890	45	85,050	A	4/30/2018	64
MICHBL	030	MICHIGAN BL	WESTACRE RD	ROCKROSE RD	2	C	1,270	45	57,150	A	4/30/2018	68
MICHBL	040	MICHIGAN BL	ROCKROSE RD	JEFFERSON BL	2	C	165	54	8,910	A	4/30/2018	82
MIDDCT	010	MIDDLE BASS ILS CT	END	N BASS IS RD	2	R	284	33	9,372	A	4/30/2018	66
MIDDST	010	MIDWAY ISL ST	MOLOKAI RD	END N	2	R	520	33	17,160	A	4/30/2018	73
MIKOST	010	MIKON ST	YOLO ST	E END	2	R	880	36	31,680	A	4/30/2018	65
MILLST	010	MILL ST	RIVERFRONT	FIFTH ST	2	R	545	36	19,620	P	2/28/2018	99
MILLCT	010	MILLBROOK CT	END S	QUAIL RD	2	R	60	64	4,287	A	4/30/2018	89
MILLPL	010	MILLERTON PL	ARROWHEAD ST	END E	2	R	374	32	11,968	A	4/30/2018	86
MILTST	010	MILTON ST	ARTHUR DR	BOWLEN DR	2	R	885	36	31,860	A	4/30/2018	65
MIRACT	010	MIRA LOMA CT	END S	HIGHLAND DR	2	R	134	32	7,551	A	4/30/2018	77
MODACT	010	MODOC CT	LASSIK ST	END E	2	R	198	32	8,352	A	4/30/2018	64
MOJADR	010	MOJAVE DR	END S	439' S/ LINDEN RD	2	R	1,550	33	51,150	A	4/30/2018	76
MOJADR	020	MOJAVE DR	439' S/ LINDEN RD	LINDEN RD	2	R	439	32	14,048	A	4/30/2018	60
MOJADR	030	MOJAVE DR	LINDEN RD	SPOKANE RD	2	R	840	33	27,720	A	4/30/2018	86
MOLORD	010	MOLOKAI RD	MIDWAY IS ST	MAUI ST	2	R	560	33	18,480	A	4/30/2018	60
MONCT	010	MONTE VISTA CT	MONTE VISTA	END W	2	R	224	32	10,431	A	4/30/2018	60
MONTST	010	MONTE VISTA ST	CHINOOK RD	MONTE VISTA	2	R	670	32	21,440	A	4/30/2018	84
MONTCT	010	MONTEREY CT	GREENBRIA RD	END N	2	R	204	32	9,686	A	4/30/2018	65
MORGST	010	MORNING GLORY ST	PALOMAR AV	DORAN AV	2	R	800	32	25,600	A	4/30/2018	58

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MORRST	010	MORROW BAY ST	TWITCHEL RD	CARMEL BA RD	2	R	270	33	8,910	A	4/30/2018	82
MORSCT	010	MORSE CT	W CDS	FOURNESS DR	2	R	415	32	15,841	A	4/30/2018	65
MOSSCR	010	MOSSWOOD CR	END	LAKEWOOD DR	2	R	180	31	8,124	A	4/30/2018	42
MULVPL	010	MULVANY PL	RAMCO ST	END	2	R	518	40	23,279	S	4/30/2018	57
MUSCRD	010	MUSCOVY RD	WIGEON ST	CONDOR ST	2	R	1,000	32	32,000	A	4/30/2018	85
MYRTAV	010	MYRTLE AV	ANNA ST	RIVERBANK RD	2	R	1,940	32	62,080	A	4/30/2018	34
NBASRD	010	N BASS ISL RD	HALF MOON CIR	WHITE FIS RD	2	R	575	33	18,975	A	4/30/2018	74
NBEAAV	010	N BEACH AV	COLUMBUS RD	STONEGATE DR	2	C	400	33	13,200	A	4/30/2018	85
NBEAAV	020	N BEACH AV	STONEGATE DR	GATEWAY DR	2	C	735	33	24,255	S	4/30/2018	34
NANCLN	010	NANCY LN	VIOLET DR	BRENDA WY	2	R	450	36	16,200	S	4/30/2018	62
NAUTCT	010	NAUTICA CT	END W	BAYBERRY ST	2	R	559	32	21,151	A	4/30/2018	73
NEPTCT	010	NEPTUNE CT	END S	HIGHLAND DR	2	R	349	32	14,431	A	4/30/2018	80
NEWHPL	010	NEW HOGAN PL	OTIS AV	MARTIS ST	2	R	175	32	5,600	A	4/30/2018	82
NEWHPL	020	NEW HOGAN PL	MARTIS ST	END E	2	R	189	32	9,311	A	4/30/2018	81
NEWIST	010	NEW ISLE ST	PHILIP IS RD	ABACO RD	2	R	290	33	9,570	A	4/30/2018	87
NWEBST	010	NEWBERRY ST	CEDARBROO RD	GREENBRIA RD	2	R	270	32	8,640	A	4/30/2018	85
NICHCT	010	NICHOLS CT	END	ALLAN AV	2	R	498	32	18,126	A	4/30/2018	69
NINEST	010	NINETEENTH ST	DEERWOOD ST	PARK BL	2	R	570	31	17,670	A	4/30/2018	59
NINEST	020	NINETEENTH ST	PARK BL	PENNSYLVANIA AV	2	R	900	32	28,800	A	4/30/2018	30
NINEST	030	NINETEENTH ST	PENNSYLVA AV	VERMONT AV	2	R	1,145	32	36,640	A	4/30/2018	32
NINEST	040	NINETEENTH ST	VERMONT AV	JEFFERSON BL	2	R	430	23	9,890	A	4/30/2018	20
NORFAV	010	NORFOLK AV	CLARENDON ST	FERNWOOD ST	2	R	1,265	32	40,480	A	4/30/2018	67
NORFAV	020	NORFOLK AV	FERNWOOD ST	END	2	R	161	34	5,474	A	4/30/2018	37
NORPDR	010	NORTHPORT DR	COMMERCE DR	WEST CAPI AV	2	R	1,450	46	66,700	A	4/30/2018	36
NUNECT	010	NUNEZ CT	END	MARECA WY	2	R	153	33	7,162	S	4/30/2018	75
OAKSTR	010	OAK ST	REED AV	ELM ST	2	R	240	32	7,680	A	4/30/2018	78
OAKSTR	020	OAK ST	ELM ST	S CDS	2	R	1,152	32	38,102	A	4/30/2018	76
OAKLDR	010	OAKLAND BAY DR	MALCOLM I ST	SOUTHPORT PW	2	C	1,455	32	46,560	A	4/30/2018	86
OAKLDR	020	OAKLAND BAY DR	SOUTHPORT PW	MARTINIQUE ST	2	C	1,240	33	40,920	A	4/30/2018	58
OAKLDR	030	OAKLAND BAY DR	MARTINIQU ST	TACOMA N ST	2	C	1,340	33	47,520	A	4/30/2018	82
OAKLDR	040	OAKLAND BAY DR	TACOMA NA ST	GOLDEN GA DR	2	C	715	33	23,595	A	4/30/2018	87
OAKMWY	010	OAKMONT WY	W END	BRIARWOOD DR	2	R	510	36	18,360	A	4/30/2018	48
OAKMWY	020	OAKMONT WY	BRIARWOOD DR	ORCHARD WY	2	R	145	32	4,640	A	4/30/2018	47
OATEDR	010	OATES DR	RAMOS DR	CARLIN DR	2	R	1,550	52	80,600	S	4/30/2018	47
OLIVCT	010	OLIVE CT	SUMMERFIE DR	END	2	R	613	33	22,255	S	4/30/2018	51
ORCHWY	010	ORCHARD WY	OAKMONT WY	HICKORY WY	2	R	310	32	9,920	A	4/30/2018	47
ORINPL	010	ORINDA PL	END W	END. E	2	R	536	37	25,378	A	4/30/2018	74
OTAY	010	OTAY ST	SAN VINEN RD	PILLSBURY RD	2	R	720	32	23,040	A	4/30/2018	93
OTISAV	010	OTIS AV	SOUTHPORT PW	KAWEAH ST	2	C	1,715	32	54,880	A	4/30/2018	84
OTISAV	020	OTIS AV	KAWEAH ST	BRIDGEWAY DR	2	C	1,520	32	48,620	A	4/30/2018	88

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OTISAV	030	OTIS AV	BRIDGEWY DR	MARSHALL RD	2	C	1,730	32	55,360	A	4/30/2018	74
OVERCT	010	OVERLAND CT	END	INDUSTRIA BL	2	R	978	41	61,208	A	4/30/2018	35
OXFOST	010	OXFORD ST	WASHINGTON AV	POPLAR ST	2	R	558	32	19,960	A	4/30/2018	71
PALOAV	010	PALOMAR AV	W END	PINE ST	2	R	1,440	32	46,080	A	4/30/2018	44
PARAWY	010	PARADISE WY	HIGGINS RD	LAGOON LN	2	R	560	36	20,160	S	4/30/2018	66
PARKBL	010	PARK BL	JEFFERSON BL	ELEVENTH ST	3	C	1,395	75	104,625	A	4/30/2018	46
PARKBL	020	PARK BL	ELEVENTH ST	FIFTEENTH ST	2	C	960	68	65,280	A	4/30/2018	37
PARKBL	030	PARK BL	FIFTEENTH ST	EIGHTEENTH ST	2	C	1,955	68	132,940	A	4/30/2018	38
PARKBL	040	PARK BL	EIGHTEENT ST	STONE BL	2	C	1,675	68	113,900	A	4/30/2018	41
PARKCR	010	PARK CR	END	PARK BL	2	C	125	30	6,568	S	4/30/2018	41
PARWBL	010	PARKWAY BL	END	SEAPORT BL	2	C	743	46	36,997	A	4/30/2018	21
PARWBL	020	PARKWAY BL	SEAPORT BL	PARKWAY PL	2	C	1,565	46	71,990	A	4/30/2018	42
PARWBL	030	PARKWAY BL	PARKWAY PL	INDUSTRIA BL	2	C	730	46	33,580	A	4/30/2018	28
PARWPL	010	PARKWAY PL	END	PARKWAY BL	2	C	403	46	21,354	A	4/30/2018	54
PARTAV	010	PARTRIDGE AV	BEVAN RD	TAPLEY RD	2	R	2,110	18	37,980	A	4/30/2018	46
PARTAV	020	PARTRIDGE AV	TAPLEY RD	DAVIS RD	2	R	1,080	18	19,440	A	4/30/2018	35
PARTAV	030	PARTRIDGE AV	DAVIS RD	HARMON RD	2	R	1,500	21	31,500	A	4/30/2018	26
PEACST	010	PEACHTREE ST	LEMON TRE RD	PEPPERTRE RD	2	R	380	37	14,060	A	4/30/2018	74
PECAST	010	PECAN ST	HOLLY ST	MICHIGAN BL	2	R	1,090	32	34,880	A	4/30/2018	63
PECAST	020	PECAN ST	MICHIGAN BL	WEST CAPI AV	2	R	1,275	32	40,800	A	4/30/2018	77
PEGLCT	010	PEGLER CT	END	ALLAN AV	2	R	108	48	6,142	A	4/30/2018	65
PEKICT	010	PEKINS CT	END	TEAL DR	2	R	227	32	7,264	S	4/30/2018	60
PEKICT	020	PEKINS CT	TEAL DR	END	2	R	83	63	4,975	S	4/30/2018	24
PENDST	010	PENDER ISL ST	GALIANO I RD	END N	2	R	563	33	21,754	A	4/30/2018	85
PENNAV	010	PENNSYLVANIA AV	STONE BL	EUCLID ST	2	R	845	32	27,040	A	4/30/2018	33
PENNAV	020	PENNSYLVANIA AV	REGENT ST	SEVENTEEN ST	2	R	415	32	13,280	A	4/30/2018	28
PENNAV	030	PENNSYLVANIA AV	SEVENTEEN ST	ALAMEDA ST	4	R	850	32	27,200	A	4/30/2018	32
PENNAV	040	PENNSYLVANIA AV	ALAMEDA ST	FIFTEENTH ST	2	R	620	31	19,220	A	4/30/2018	32
PENNCT	010	PENNSYLVANIA CT	END S	STONE BL	2	R	160	31	7,599	A	4/30/2018	83
PEPPCT	010	PEPPERTREE CT	END S	PEPPERTRE RD	2	R	134	52	8,471	A	4/30/2018	66
PEPPRD	010	PEPPERTREE RD	PEACHTREE ST	ALMOND ST	2	R	870	36	31,320	A	4/30/2018	73
PERECT	010	PEREZ CT	END	ALLAN AV	2	R	318	32	12,366	A	4/30/2018	67
PERKRD	010	PERKINS RD	JEFFERSON BLVD	END	2	R	1,300	22	28,600	A	4/30/2018	15
PERRCT	010	PERRIS CT	END S	SQUAW RD	2	R	279	32	12,191	A	4/30/2018	68
PETERD	010	PETER ISL RD	FIJI ISL ST	GOLDEN GA DR	2	R	1,080	33	35,640	A	4/30/2018	70
PHELCT	010	PHELPS CT	PITZER CR	END	2	R	248	32	10,126	S	4/30/2018	76
PHEADR	010	PHESANT HOLLOW DR	HIGGINS RD	STARLING LN	2	R	1,600	33	52,800	S	4/30/2018	75
PHEADR	020	PHESANT HOLLOW DR	JEFFERSON BL	STARLING LN	2	R	195	33	6,435	S	4/30/2018	69
PHILST	010	PHILLIP ISL RD	SAN SALVA ST	NEW ISLE ST	2	R	505	33	16,665	A	4/30/2018	86
PIERST	010	PIERCE ST	HOLLAND DR	KEGLE DR	2	R	1,135	36	40,860	S	4/30/2018	72

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PILLRD	010	PILLSBURY RD	MARTIS ST	PROSSER ST	2	R	495	32	15,840	A	4/30/2018	86
PILORD	010	PILOT POINT RD	ALEUTIAN ST	TACOMA NA ST	2	R	635	33	20,955	A	4/30/2018	74
PINEST	010	PINE ST	WEST CAPI AV	EVERGREEN AV	2	R	780	38	29,640	A	4/30/2018	38
PINTCT	010	PINTAIL CT	END	SUMMERFIE DR	2	R	158	33	7,240	S	4/30/2018	35
PINTCT	020	PINTAIL CT	SUMMERFIE DR	END	2	R	228	33	9,550	A	4/30/2018	30
PITZCR	010	PITZER CR	ALLAN AV	ALLEN AV	2	R	1,680	36	60,480	S	4/30/2018	67
POMOCT	010	POMO CT	RUMSEY ST	END E	2	R	186	32	8,054	A	4/30/2018	64
POPLAV	010	POPLAR AV	WEST CAPI AV	EVERGREEN AV	2	C	1,223	32	39,136	A	4/30/2018	42
POPLAV	020	POPLAR AV	EVERGREEN AV	BUCKEYE DR	2	C	345	32	11,040	A	5/24/2018	41
POPLST	010	POPLAR ST	MANZANITA WY	MICHIGAN BL	2	C	1,225	36	44,100	A	4/30/2018	67
POPPST	010	POPPY ST	RIVERMONT ST	LILY PL	2	R	1,255	32	40,160	A	4/30/2018	83
PORTST	010	PORT ST	SHORE ST	TERMINAL ST	2	R	1,370	46	63,020	A	4/30/2018	57
POLAST	010	PORTOLA ST	PRESIDIO RD	CYPRESS RD	2	R	350	33	11,550	A	4/30/2018	81
PORTAV	010	PORTSMOUTH AV	WESTACRE RD	MICHIGAN BL	2	R	1,030	32	32,960	A	4/30/2018	92
PORTCT	010	PORTSMOUTH CT	W CDS	WESTACRE RD	2	R	119	32	6,465	A	4/30/2018	95
POWEPL	010	POWELL PL	SANSOME ST	END	2	R	269	32	11,871	S	4/30/2018	56
PRESRD	010	PRESIDIO RD	BRISBANE ST	N BEACH AV	2	R	570	33	18,810	A	4/30/2018	75
PRINRD	010	PRINCETON RD	HIGHLAND DR	460' E/ HIGHLAND	2	R	460	32	14,720	A	4/30/2018	84
PRINRD	020	PRINCETON RD	460' E/ HIGHLAND	BAYBERRY ST	2	R	753	32	24,096	A	4/30/2018	75
PROCAV	010	PROCTOR AV	SYCAMORE ST	ASH AV	2	R	1,315	32	42,080	A	4/30/2018	59
PRONWY	010	PROMENADE WY	GOLDEN GA DR	SOUTHPORT PW	4	C	1,260	50	63,000	A	4/30/2018	54
PROMWY	020	PROMENADE WY	SOUTHPORT PW	RAMOS DR	2	C	900	52	47,700	S	4/30/2018	69
PROSST	010	PROSSER ST	END S	PILLSBURY RD	2	R	1,165	32	37,280	A	4/30/2018	88
PUERRD	010	PUERTO RICO RD	ARUBA ST	CAYMAN IS ST	2	R	285	33	9,405	A	4/30/2018	70
PYRAPL	010	PYRAMID PL	END W	COLLINS ST	2	R	314	32	13,311	A	4/30/2018	85
QUAILCT	010	QUAIL CT	QUAIL RD	END N	2	R	124	38	7,447	A	4/30/2018	81
QUAILRD	010	QUAIL RD	SILVERADO ST	HIGHLAND DR	2	R	1,130	32	36,160	A	4/30/2018	75
RALECT	010	RALEY CT	END	ENTERPRIS BL	2	R	413	40	19,515	A	4/30/2018	39
RAMCST	010	RAMCO ST	END	CARLIN DR	3	C	2,205	52	114,660	S	4/30/2018	57
RAMCST	020	RAMCO ST	CARLIN DR	S END	2	C	1,585	52	82,420	S	4/30/2018	52
RAMOCT	010	RAMONA CT	CORTINA RD	END N	2	R	214	33	10,237	A	4/30/2018	80
RAMODR	010	RAMOS DR	OATES DR	PROMENADE WY	2	R	825	53	43,725	S	4/30/2018	64
RANDRD	010	RANDOLPH RD	GEARY ST	HEARST ST	2	R	780	37	28,860	S	4/30/2018	76
REDDRD	010	REDDING RD	LA JOLLA ST	VILLAGE PW	2	R	1,150	33	37,950	A	4/30/2018	85
REDORD	010	REDONDO RD	STONEGATE DR	HIGHLAND DR	2	R	1,130	33	37,290	A	4/30/2018	85
REDWAV	010	REDWOOD AV	END	LINDEN RD	2	R	1,844	18	33,192	S	4/30/2018	69
REDWAV	020	REDWOOD AV	LINDEN RD	LAKE WASHINGTON BL	2	R	1,045	32	33,440	A	4/30/2018	71
REDWAV	030	REDWOOD AV	LAKE WASHINGTON BL	GREENBRIA RD	2	R	475	32	15,200	A	4/30/2018	79
REDWCT	010	REDWOOD CT	BREENBRIER RD	END	2	R	85	32	2,720	A	4/30/2018	87
REEDAV	010	REED AV	RIVERSIDE	STILLWATE RD	4	A	2,000	62	137,044	S	4/30/2018	63

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REEDAV	020	REED AV	STILLWATER RD	I80 WB ONRAMP	5	A	335	65	24,175	A	4/30/2018	44
REEDAV	030	REED AV	I80 WB ONRAMP	I80	6	A	531	78	41,418	A	4/30/2018	67
REEDAV	040	REED AV	I80	I80 EB ONRAMP	4	A	380	66	25,080	A	4/30/2018	42
REEDAV	050	REED AV	I80 EB ONRAMP	IKEA CT	4	A	650	80	52,000	A	4/30/2018	53
REEDAV	060	REED AV	IKEA CT	HARBOR BL	4	A	995	60	65,700	A	4/30/2018	36
REEDAV	070	REED AV	HARBOR BL	SUNSET AV	4	A	950	77	73,150	S	4/30/2018	62
REGEST	010	REGENT ST	DELAWARE AV	ALABAMA AV	2	R	560	31	17,360	A	4/30/2018	31
RENECT	010	RENEE CT	END	BETTY WY	2	R	348	33	13,510	S	4/30/2018	68
RENECT	020	RENEE CT	BETTY WY	END	2	R	298	33	11,860	S	4/30/2018	31
REUTDR	010	REUTER DR	SACRAMENT AV	CUMMINS WY	2	R	1,825	28	51,100	A	4/30/2018	78
RICEAV	010	RICE AV	HARBOR BL	RICE AV	2	C	1,474	26	38,324	S	4/30/2018	47
RICEAV	020	RICE AV	RICE AV	MAPLE ST	2	C	984	40	39,360	A	4/30/2018	84
RICEAV	030	RICE AV	MAPLE ST	E CDS	2	C	457	41	20,569	A	4/30/2018	75
RICHST	010	RICH ST	ARTHUR DR	KEGLE DR	2	R	855	35	29,925	A	4/30/2018	69
RISKLN	010	RISKE LN	BALLPARK DR	DELTA LN	2	A	642	31	19,902	A	4/30/2018	42
RISKLN	040	RISKE LN	SOUTH RIV RD	BALLPARK DR	2	A	389	40	15,560	A	4/30/2018	30
RIVERD	040	RIVERBANK RD	TODHUNTER AV	END	2	C	1,680	36	60,480	S	4/30/2018	44
RIVEST	010	RIVERFRONT ST	MILL ST	BRIDGE ST	2	A	901	38	34,238	P	5/24/2018	100
RIVEST	020	RIVERFRONT ST	BRIDGE ST	BALLPARK DR	2	A	1,505	38	57,190	P	5/24/2018	100
RIVEST	030	RIVERFRONT ST	BALLPARK DR	TOWER BRIDGE GTWY	2	A	550	46	25,300	P	5/24/2018	99
RIVMST	010	RIVERMONTE ST	MARSHALL RD	LILY PL	2	R	1,825	32	58,400	A	4/30/2018	84
REVEDR	010	RIVERPOINT DR	KOVR DR	REED AV	4	R	650	60	39,000	A	4/30/2018	72
RIVEPW	010	RIVERSIDE PW	REED AV	EMBARCADERO DR	2	C	1,030	45	46,350	A	4/30/2018	44
RIVEPW	020	RIVERSIDE PW	EMBARCADERO DR	CDS @ #875	2	C	1,259	45	56,655	A	4/30/2018	67
RIVEPW	030	RIVERSIDE PW	CDS @ #875	EMBARCADERO DR	2	C	1,646	45	74,070	A	4/30/2018	66
RIVEPW	040	RIVERSIDE PW	EMBARCADERO DR	STILLWATER RD	2	C	890	45	40,050	A	4/30/2018	68
ROARST	010	ROARING CREEK ST	END S	COLD SPRINGS RD	2	R	570	33	18,810	A	4/30/2018	87
ROCKCRD	010	ROCK CREEK RD	HUME ST	BRIDEWAY LAKES	2	R	791	32	25,312	A	4/30/2018	86
ROCKRD	020	ROCKROSE RD	WESTACRE RD	MICHIGAN BL	2	R	1,270	35	44,450	S	4/30/2018	61
ROGEST	010	ROGERS ST	HOLLAND DR	KEGLE DR	2	R	1,370	36	49,320	A	4/30/2018	48
ROGUCR	010	ROGUE RIVER CR	MOJAVE DR	MOJAVE DR	2	R	1,180	33	38,940	A	4/30/2018	85
ROGUCT	010	ROGUE RIVER CT	ROGUE RIV CR	END E	2	R	164	33	8,587	A	4/30/2018	85
ROLLCT	010	ROLLINS CT	LEWISTON RD	END N	2	R	319	32	137,967	A	4/30/2018	76
ROMACT	010	ROMA CT	END	LEMON TRE RD	2	R	139	53	8,782	A	4/30/2018	74
ROSECT	010	ROSE CT	END	ELEVENTH ST	2	R	141	29	6,793	A	4/30/2018	45
ROUNCT	010	ROUNDHILL CT	PRINCETON RD	END N	2	R	129	46	7,965	A	4/30/2018	71
ROVECT	010	ROVEN CT	END	SUMMERFIE DR	2	R	208	33	8,890	A	4/30/2018	38
ROVECT	020	ROVEN CT	SUMMERFIE DR	END	2	R	158	33	7,240	A	4/30/2018	61
RUBIWY	010	RUBICON WY	SHASTA WY	MERCED WY	2	R	1,075	32	34,400	S	4/30/2018	66
RUBIWY	020	RUBICON WY	MERCED WY	LINDEN RD	2	R	280	36	10,080	S	4/30/2018	68

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RUMPL	010	RUMSEY PL	RUMSEY ST	END N	2	R	214	32	9,312	A	4/30/2018	67
RUMSST	010	RUMSEY ST	POMO CT	END N	2	R	1,049	32	36,938	A	4/30/2018	69
RYERST	010	RYER ISL ST	SUISUN BAY	ARCATA BAY RD	2	R	805	33	26,565	A	4/30/2018	76
RYERST	020	RYER ISL ST	ARCATA BAY RD	CATALINA RD	2	R	1,370	33	45,210	A	4/30/2018	76
SACRAV	010	SACRAMENTO AV	SUNSET AV	BRYTE AV	5	A	1,565	78	122,070	A	4/30/2018	59
SACRAV	020	SACRAMENTO AV	BRYTE AV	TODHUNTER AV	5	A	935	78	72,930	A	4/30/2018	63
SACRAV	030	SACRAMENTO AV	TODHUNTER AV	SIMON TER	5	A	930	78	72,540	A	4/30/2018	63
SACRAV	040	SACRAMENTO AV	SIMON TER	JEFFERSON BL	5	A	995	78	77,610	A	4/30/2018	59
SACRAV	050	SACRAMENTO AV	JEFFERSON BL	COP E/O JEFFERSON BL	2	A	687	60	41,220	A	4/30/2018	45
SACRAV	060	SACRAMENTO AV	COP E/O JEFFERSON BL	DOUGLAS ST	2	A	1,100	52	57,200	A	4/30/2018	69
SACRAV	070	SACRAMENTO AV	DOUGLAS ST	EIGHTH ST	2	A	1,105	38	41,990	A	4/30/2018	68
SACRAV	080	SACRAMENTO AV	EIGHTH ST	SIXTH ST	2	A	619	38	23,522	A	4/30/2018	74
SAGIST	010	SAGINAW ST	END	CATALINA RD	2	R	789	33	29,212	A	4/30/2018	74
SALMLN	010	SALMON LN	VILLAGE PKWY	SCULPIN LN	2	R	1,380	20	27,600	A	4/30/2018	96
SANCCT	010	SAN CARLOS CT	REDONDO RD	END N	2	R	344	33	14,527	A	4/30/2018	84
SANMST	010	SAN MIGUEL ST	SANTA CRU RD	SAN NICOL ST	2	R	230	33	7,590	A	4/30/2018	37
SANNST	010	SAN NICOLAS ST	SAN MIGUE ST	SANTA BAR ST	2	R	600	33	19,800	A	4/30/2018	72
SANRCT	010	SAN REMO CT	REDONDO RD	END N	2	R	254	33	11,557	A	4/30/2018	81
SANST	010	SAN SALVADOR ST	MARSHALL RD	OAKLAND BAY DR	2	R	1,420	33	46,680	A	4/30/2018	84
SANVRD	010	SAN VINCENT RD	MARTIS ST	PROSSER ST	2	R	1,110	32	35,520	A	4/30/2018	89
SANDLN	010	SANDPIPER LN	PHESANT H DR	STARLING LN	2	R	510	33	16,830	S	4/30/2018	69
SANDCT	010	SANDY POINT CT	GOLD HILL ST	END E	2	R	179	32	8,991	A	4/30/2018	77
SANDRD	010	SANDY POINT RD	SILVERADO ST	GOLD HILL ST	2	R	480	32	15,360	A	4/30/2018	81
SANSST	010	SANSOME ST	SAUSALITO RD	GATEWAY DR	2	R	605	36	21,780	S	4/30/2018	64
SANSST	020	SANSOME ST	GATEWAY DR	UNION SQ RD	2	R	395	36	14,220	S	4/30/2018	74
SANBST	010	SANTA BARBARA ST	CATALINA RD	SAN NICOLAS	2	R	220	33	7,260	A	4/30/2018	78
SANCRD	010	SANTA CRUZ RD	GOLDEN GA DR	CATALINA RD	2	R	1,224	33	43,567	A	4/30/2018	74
SANTST	010	SANTIAM ST	LINDEN RD	CHINOOK RD	2	R	215	33	7,095	A	4/30/2018	68
SAUSRD	010	SAUSALITO RD	SANSOME ST	GATEWAY DR	2	R	955	37	35,335	S	4/30/2018	59
SAWBCT	010	SAWBILLS CT	END	TEAL DR	2	R	208	32	8,846	S	4/30/2018	39
SEABCT	010	SEABROOK CT	N CDS	MICHIGAN BL	2	R	191	32	8,769	A	4/30/2018	89
SEAPBL	010	SEAPORT BL	END	ENTERPRIS BL	2	C	1,073	60	68,356	A	4/30/2018	54
SEAPBL	020	SEAPORT BL	ENTERPRIS BL	CHANNEL DR	2	C	2,235	60	134,100	A	4/30/2018	34
SEAPBL	030	SEAPORT BL	CHANNEL DR	PARKWAY BL	2	C	2,245	46	103,270	A	4/30/2018	36
SENDST	010	SECOND ST	N END	THIRD ST	2	R	1,230	32	39,360	A	4/30/2018	71
SENDST	020	SECOND ST	D ST	E ST	2	R	440	37	16,280	A	4/30/2018	71
SELLWY	010	SELLERS WY	W CDS	E END	2	R	655	40	28,027	A	4/30/2018	40
SEVEST	010	SEVENTEENTH ST	PARK BL	DELAWARE AV	2	R	565	32	18,080	A	4/30/2018	20
SEVEST	020	SEVENTEENTH ST	DELAWARE AV	ALABAMA AV	2	R	902	40	36,080	A	4/30/2018	35
SEVEST	030	SEVENTEENTH ST	ALABAMA AV	VERMONT AV	2	R	590	32	18,880	A	4/30/2018	32

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SEVEST	040	SEVENTEENTH ST	VERMONT AV	JEFFERSON BL	2	R	270	23	6,210	A	4/30/2018	54
SVNTST	010	SEVENTH ST	E ST	F ST	2	R	440	35	15,400	A	4/30/2018	49
SVNTST	020	SEVENTH ST	F ST	G ST	2	R	320	32	10,240	A	4/30/2018	72
SEYMAV	010	SEYMORE AV	END	GLADYS AV	2	R	575	12	6,900	A	4/30/2018	4
SEYMAV	020	SEYMORE AV	GLADYS AV	MARSHALL RD	2	R	1,435	17	24,395	S	4/30/2018	11
SEYMAV	030	SEYMORE AV	MARSHALL RD	STABLE DR	2	R	222	32	10,367	A	4/30/2018	75
SEYMRCT	010	SEYMOUR CT	STABLE DR	END N	2	R	195	32	6,240	A	4/30/2018	61
SHARST	010	SHARIAN ST	KEGLE DR	HARDY DR	2	R	920	32	29,440	A	4/30/2018	69
SHARCT	010	SHARON CT	END	BETTY WY	2	R	518	33	19,120	S	4/30/2018	38
SHASTWY	010	SHASTA WY	S END	RUBICON WAY	2	R	1,070	32	34,240	S	4/30/2018	76
SHASTWY	020	SHASTA WY	RUBICON WY	MERCED WAY	2	R	560	32	17,920	S	4/30/2018	66
SHAVCT	010	SHAVER CT	END S	SQUAW RD	2	R	170	32	8,703	A	4/30/2018	69
SHELST	010	SHELL ST	GREENBRIER RD	END	2	R	530	32	16,960	A	4/30/2018	93
SHERRD	010	SHERMAN ISL RD	GOLDEN GA DR	VENICE ST	2	R	1,060	32	33,920	A	4/30/2018	77
SHIRST	010	SHIRLEY ST	LESLIE LN	BETTY WY	2	R	470	36	16,920	S	4/30/2018	61
SHORST	010	SHORE ST	DEL MONTE ST	FRONTST	2	R	1,710	46	78,660	A	4/30/2018	49
SHOST	010	SHORT ST	LISBON AV	RIVERBANK RD	2	R	760	32	24,320	A	5/24/2018	48
SHOVCT	010	SHOVELER CT	CANVASBAC WY	END	2	R	228	36	9,486	A	4/30/2018	58
SIERRD	010	SIERRA RD	LASSEN ST	MOJAVE DR	2	R	925	32	29,600	A	4/30/2018	89
SIERRD	020	SIERRA RD	MOJAVE DR	RUMSEY ST	2	R	430	32	13,760	A	4/30/2018	68
SILVST	010	SILVERADO ST	LAKE WASH BL	SANDY POI RD	2	R	205	32	6,560	A	4/30/2018	85
SILVST	020	SILVERADO ST	SANDYPOINT RD	QUAIL RD	2	R	225	32	7,200	A	4/30/2018	74
SILWRD	010	SILVERWOOD RD	END W	SOUTHPORT PW	2	R	1,524	32	52,031	A	4/30/2018	85
SILWRD	020	SILVERWOOD RD	SOUTHPORT PW	TAHOW ST	2	R	1,405	32	44,940	A	4/30/2018	86
SIXTST	010	SIXTEENTH ST	PARK BL	ALABAMA ST	2	R	1,125	31	34,872	A	4/30/2018	32
SIXTST	020	SIXTEENTH ST	ALABAMA AV	VERMONT AV	2	R	550	23	12,650	A	4/30/2018	7
SXTHST	015	SIXTH ST	C ST	SACRAMENTO AV	2	R	365	38	13,870	A	4/30/2018	62
SXTHST	020	SIXTH ST	E ST	F ST	2	A	445	32	14,240	A	4/30/2018	65
SXTHST	030	SIXTH ST	F ST	G ST	2	A	340	32	10,880	A	4/30/2018	46
SMILAV	010	SMILAX AV	LISBON AV	RIVERBANK RD	2	R	1,210	32	38,720	A	4/30/2018	43
SOLAST	010	SOLANO ST	SACRAMENT AV	LISBON AV	2	R	1,540	36	55,440	S	4/30/2018	63
SOLAST	020	SOLANO ST	LISBON AV	RIVERBANK RD	2	R	835	36	30,060	S	4/30/2018	60
SOLOST	010	SOLOMON ISL RD	STUART IS ST	BOWEN ST	2	R	1,205	33	40,095	A	4/30/2018	86
SOMEDR	010	SOMERSET DR	TODHUNTER AV	TODHUNTER AV	2	R	1,318	36	47,448	A	4/30/2018	61
SONOAV	010	SONORA AV	FALLBROOK ST	FERNWOOD ST	2	R	457	32	14,624	A	4/30/2018	85
SOULST	010	SOULE ST	FIFTEENTH ST	JACKSON ST	2	R	850	30	25,500	A	4/30/2018	53
SOULST	020	SOULE ST	JACKSON ST	DREVER ST	2	R	780	30	23,400	A	4/30/2018	39
SOULST	030	SOULE ST	DREVER ST	END	2	R	420	24	12,511	A	4/30/2018	35
SOUTRD	170	SOUTH RIVER RD	END	1800 SOUTH RIV RD	2	A	1,700	28	52,270	A	4/30/2018	73
SOUTRD	180	SOUTH RIVER RD	1800 SOUTH RIV RD	15TH ST	2	A	2,077	28	58,156	A	4/30/2018	75

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Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
SOLARD	010	SOUTHERNLAND RD	TURLOCK ST	PROSSER ST	2	R	665	32	21,280	A	4/30/2018	92
SOUTPW	010	SOUTHPORT PW	LAKE WASH BL	1700' W/O Lake Wa	4	A	1,700	60	102,000	S	4/30/2018	29
SOUTPW	020	SOUTHPORT PW	1700' W/O Lake Wa	3400' W/O Lake Wa	4	A	1,700	50	85,000	S	4/30/2018	22
SOUTPW	030	SOUTHPORT PW	3400' W/O Lake Wa	RAMCO ST	2	A	1,850	50	92,500	S	4/30/2018	18
SOUTPW	040	SOUTHPORT PW	1250' E/O RAMCO	RAMCO ST	4	A	1,250	79	98,750	A	4/30/2018	38
SOUTPW	050	SOUTHPORT PW	RAMCO ST	CARLIN DR	6	A	1,905	79	150,495	S	4/30/2018	47
SOUTPW	060	SOUTHPORT PW	CARLIN DR	PROMENADE WY	4	A	2,215	55	121,825	S	4/30/2018	44
SOUTPW	070	SOUTHPORT PW	PROMENADE WY	1900' W/O PROMENADE	4	A	1,900	55	104,500	A	4/30/2018	69
SOUTPW	080	SOUTHPORT PW	1900' W/O PROMENADE	OAKLAND BAY DR	4	A	1,955	55	107,525	A	4/30/2018	68
SOUTPW	090	SOUTHPORT PW	OAKLAND BAY DR	ABACO RD	2	A	950	55	52,250	A	4/30/2018	72
SOUTPW	100	SOUTHPORT PW	ABACO RD	MARSHALL RD	5	A	445	69	38,705	A	4/30/2018	63
SOUTPW	110	SOUTHPORT PW	MARSHALL RD	BRITTON PL	2	A	1,855	36	66,780	A	4/30/2018	82
SOUTPW	120	SOUTHPORT PW	BRITTON PL	SILVERWOOD RD	2	A	1,860	46	85,500	A	4/30/2018	82
SOUTPW	130	SOUTHPORT PW	SILVERWOO RD	OTIS AV	2	A	1,570	36	56,520	A	4/30/2018	76
SOUTPW	140	SOUTHPORT PW	OTIS AV	JEFFERSON BL	2	A	1,435	36	51,660	A	4/30/2018	79
SPALCT	010	SPALDING CT	END S	COYOTE RD	2	R	254	32	11,182	A	4/30/2018	83
SPOKRD	010	SPOKANE RD	STONEGATE DR	HIGHLAND DR	2	R	1,135	33	37,455	A	4/30/2018	84
SPOORD	010	SPOONBILL RD	CONDOR ST	STONEGATE	2	R	453	32	14,496	A	4/30/2018	90
SPRUST	010	SPRUCE ST	END	LINDEN RD	2	R	925	38	35,150	S	4/30/2018	62
SQUACT	010	SQUAW CT	END S	SQUAW CT	2	R	94	77	6,541	A	4/30/2018	51
SQUARD	010	SQUAW RD	SILVERWOO RD	OTIS V	2	R	1,440	32	46,080	A	4/30/2018	85
STCRRD	010	ST CROIX RD	MARTINIQU ST	JAMAICA ST	2	R	765	33	25,245	A	4/30/2018	77
STJORD	010	ST JOHN 10	ARUBA ST	MARTINIQUE ST	2	R	900	33	29,700	A	4/30/2018	68
STLUPL	010	ST. LUCIA PL	END	MARTINIQU ST	2	R	339	33	11,187	A	4/30/2018	74
STABDR	010	STABLE DR	MARSHALL RD	877' W/ MARSHALL	2	R	877	32	28,064	A	4/30/2018	78
STABDR	020	STABLE DR	877' W/ MARSHALL	1443' W/ MARSHALL	2	R	566	32	18,112	A	4/30/2018	63
STABDR	030	STABLE DR	1443' W/ MARSHALL	MARSHALL RD	2	R	1,459	32	46,688	A	4/30/2018	71
STARDR	010	STARBOARD DR	BEACON BL	FREEBOARD DR	2	R	670	34	22,780	A	4/30/2018	39
STARLN	010	STARLING LN	END S	PHEASANT H DR	2	R	839	33	27,687	S	4/30/2018	73
STARLN	020	STARLING LN	PHESANT H DR	END N	2	R	550	33	18,150	S	4/30/2018	69
STIWRD	010	STILLWATER RD	REED AV	HARBOR POINT PL	4	C	380	60	22,800	A	4/30/2018	28
STIWRD	020	STILLWATER RD	HARBOR POINT PL	1500 S/O HARBOR POINT PL	4	C	1,500	60	99,000	A	4/30/2018	52
STIWRD	030	STILLWATER RD	1500 S/O HARBOR POINT PL	RIVERSIDE PW	4	C	1,170	60	76,200	A	4/30/2018	62
STONBL	010	STONE BL	INDUSTRIA BL	CEBRIAN ST	2	C	390	28	10,920	A	4/30/2018	49
STONBL	020	STONE BL	CEBRIAN ST	PARK BL	4	C	390	59	23,010	A	4/30/2018	66
STONBL	030	STONE BL	PARK BL	ALABAMA CT	2	C	1,734	36	62,424	A	4/30/2018	74
STONBL	040	STONE BL	115' E/O ALABAMA CT	CAROLINA AV	2	C	723	41	29,643	A	4/30/2018	63
STONBL	050	STONE BL	CAROLINA AV	JEFFERSON BL	2	C	286	40	11,440	A	4/30/2018	89
STONBL	060	STONE CT	END S	STONE BL	2	C	181	32	5,792	A	4/30/2018	77
STONAV	010	STONEGATE AV	COLUSA RD	END	2	C	1,165	32	37,280	A	4/30/2018	70

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STONDR	010	STONEGATE DR	VILLAGE PW	LA JOLLA ST	2	A	725	32	23,200	A	4/30/2018	96
STONDRS	010	STONEGATE DR	LINDEN RD	MALLARD RD	2	C	179	32	5,728	A	4/30/2018	81
STONDR	020	STONEGATE DR	LA JOLLA ST	MESA GRAN ST	2	A	850	32	27,200	A	4/30/2018	80
STONDRS	020	STONEGATE DR	MALLARD RD	MUSCOVY RD	2	C	770	32	24,640	A	4/30/2018	83
STONDR	030	STONEGATE DR	MESA GRAN ST	N BEACH AV	2	A	655	32	20,960	A	4/30/2018	82
STONDRS	030	STONEGATE DR	MUSCOVY RD	COLUSA RD	2	C	1,460	32	46,720	A	4/30/2018	76
STONDR	040	STONEGATE DR	N BEACH CT	LAKE WASHINGTON BL	2	A	1,215	32	38,880	A	4/30/2018	84
STONDR	050	STONEGATE DR	LAKE WASH BL	REDONDO RD	2	A	365	36	13,140	A	4/30/2018	82
STONDR	060	STONEGATE DR	REDONDO RD	LINDEN RD	2	A	1,155	32	36,960	A	4/30/2018	80
STONRD	010	STONEYFORD RD	ALPINE ST	BLACK BUTTE RD	2	R	384	32	12,288	A	4/30/2018	80
STUACT	010	STUART CT	N CDS	HOBSON AV	2	R	340	35	14,127	A	4/30/2018	87
STUAST	010	STUART ISL ST	GRAHAM IS RD	SOLOMON I RD	2	R	245	33	8,085	A	4/30/2018	89
STRLN	010	STURGEON LN	VILLAGE PKWY	END	2	R	2,145	20	42,900	A	4/30/2018	84
SUISRD	010	SUISUN BAY RD	GOLDEN GA DR	END E	2	R	1,509	33	52,972	A	4/30/2018	79
SULLCT	010	SULLIVAN CT	W CDS	FOURNESS DR	2	R	455	32	17,121	A	4/30/2018	63
SUMAST	010	SUMATRA ST	MANDURA ST	TORTOLA RD	2	R	640	33	21,120	A	4/30/2018	70
SUMMCT	010	SUMMERFIELD CT	HIGGINS RD	END	2	R	130	36	4,680	A	4/30/2018	35
SUMMDR	010	SUMMERFIELD DR	JACQUELYN LN	LINDEN RD	2	R	1,545	36	55,620	S	4/30/2018	51
SUMMDR	020	SUMMERFIELD DR	LINDEN RD	HIGGINS RD	2	R	1,491	36	53,676	A	4/30/2018	54
SUMMDR	030	SUMMERFIELD DR	HIGGINS RD	END	2	R	178	32	7,798	A	4/30/2018	31
SUNSAV	010	SUNSET AV	SACRAMENT AV	HARPER AV	2	R	1,750	32	56,000	A	4/30/2018	63
SUSACT	010	SUSAN CT	VIOLET DR	END	2	R	288	36	10,176	A	4/30/2018	70
SUTTST	010	SUTTER ST	W CAPITAL AV	FIRST ST	2	R	1,100	15	16,500	A	4/30/2018	0
SUTTST	020	SUTTER ST	FIRST ST	N END OF PAVEMENT	2	R	695	26	18,070	A	4/30/2018	26
SWANST	010	SWAN ISL ST	HAITI RD	OAKLAND B DR	2	R	280	33	9,240	A	4/30/2018	89
SWIFTST	010	SWIFT ST	CHINOOK RD	CHELAN RD	2	R	530	33	17,490	A	4/30/2018	86
SYCAST	010	SYCAMORE ST	WEST CAPI AV	EVERGREEN AV	2	C	1,000	34	34,000	A	4/30/2018	54
SYCAST	020	SYCAMORE ST	EVERGREEN AV	BUCKEYE DR	2	C	478	32	17,400	A	4/30/2018	45
TABEST	010	TABER ST	HOLLAND DR	KEGLE DR	2	R	1,210	36	43,560	S	4/30/2018	65
TACOST	010	TACOMA NARROWS ST	OAKLAND B DR	HALF MOON CIR	2	R	950	36	34,200	A	4/30/2018	74
TAHOST	010	TAHOE ST	END S	SQUAW RD	2	R	214	32	10,111	A	4/30/2018	72
TAHOST	020	TAHOE ST	SQUAW RD	COYOTE RD	2	R	1,550	32	49,600	A	4/30/2018	86
TAMARD	010	TAMARACK RD	REDWOOD AV	END. E	2	R	1,208	17	20,536	S	4/30/2018	71
TAPLRD	010	TAPLEY RD	PARTRIDGE AV	ANTIOCH AV	2	R	1,155	18	20,790	A	4/30/2018	22
TEALDR	010	TEAL DR	HIGGINS RD	SUMMERFIELD DR	2	R	1,340	36	48,240	S	4/30/2018	58
TEALDR	020	TEAL DR	SUMMERFIE DR	MANDRIN CT	2	R	220	33	7,260	A	4/30/2018	32
TENTST	010	TENTH ST	MARYLAND AV	VIRGINIA AV	2	R	420	31	13,020	A	5/24/2018	62
TERELN	010	TERESA LN	DIANE DR	SHIRLEY ST	2	R	510	36	18,360	S	4/30/2018	57
TERMST	005	TERMINAL ST	S END	INDUSTRIAL BL	2	C	466	58	27,028	A	4/30/2018	65
TERMST	010	TERMINAL ST	INDUSTRIA BL	BOATMAN AV	2	C	580	61	35,380	A	4/30/2018	52

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TERMST	020	TERMINAL ST	BOATMAN AV	DEL MONTE ST	2	C	1,235	60	74,100	A	4/30/2018	44
TERMST	030	TERMINAL ST	DEL MONTE ST	PORT ST	2	C	1,030	46	47,380	S	4/30/2018	37
TERMST	040	TERMINAL ST	PORT ST	FRONT ST	2	C	820	46	37,720	A	4/30/2018	45
THRDST	010	THIRD ST	B ST	C ST	2	C	490	46	22,540	A	4/30/2018	62
THRDST	020	THIRD ST	C ST	SECOND ST	3	C	350	46	16,100	A	4/30/2018	44
THRDST	030	THIRD ST	SECOND ST	E ST	2	C	615	46	28,290	A	4/30/2018	35
THRDST	040	THIRD ST	E ST	G ST	2	C	845	46	38,870	A	4/30/2018	49
THRDST	050	THIRD ST	G ST	TOWER BRIDGE GATEWAY	5	C	720	59	42,480	A	4/30/2018	74
THIRST	010	THIRTEENTH ST	DELAWARE AV	ALABAMA AV	2	R	590	32	18,880	A	5/24/2018	61
THIRST	020	THIRTEENTH ST	ALABANA AV	VIRGINIA AV	2	R	500	32	16,000	A	4/30/2018	52
THIRST	030	THIRTEENTH ST	VIRGINIA AV	JEFFERSON BVD	2	R	340	32	10,880	A	4/30/2018	68
THOMPL	010	THOMAS PL	RAMCO ST	END	2	R	533	40	20,764	S	4/30/2018	56
THORD	010	THORP RD	JEFFERSON BL	UNNAMED RD	2	R	5,860	20	117,200	A	4/30/2018	67
TIDERD	010	TIDEWATER RD	CORANADO ST	END	2	R	110	32	3,520	A	4/30/2018	96
TOBACT	010	TOBAGO CT	TOBAGO ST	END	2	R	154	33	8,257	A	4/30/2018	77
TOBAST	010	TOBAGO ST	TRINIDAD RD	ST CROIX RD	2	R	665	33	21,945	A	4/30/2018	80
TODHAV	010	TODHUNTER AV	SACRAMENTO AVE	MAY ST	2	C	2,103	36	75,708	S	4/30/2018	38
TOPARD	010	TOPAZ RD	END W	ARROWHEAD ST	2	R	1,348	33	50,834	A	4/30/2018	84
TORTRD	010	TORTOLA RD	FIJI ISL ST	GOLDEN GATE DR	2	R	990	33	32,670	A	4/30/2018	73
TOUCPL	010	TOUCHSTONE PL	LINDEN RD	END	2	R	590	32	18,880	A	4/30/2018	9
TOWRST	010	TOWER ST	W END	FIFTH ST	2	R	240	21	5,040	A	4/30/2018	91
TRADCR	010	TRADEWINDS CR	END	PORT ST	2	R	313	41	14,251	A	5/30/2018	47
TREARD	010	TREASURE ISLAND RD	TACOMA NA ST	END E	2	R	989	33	35,812	A	4/30/2018	73
TRIACT	010	TRIANGLE CT	W CDS	JEFFERSON BL	2	R	1,508	40	63,718	A	4/30/2018	53
TRINRD	010	TRINIDAD RD	TOBAGO ST	JAMAICA ST	2	R	450	33	14,850	A	4/30/2018	82
TRINWY	010	TRINITY WY	E END	MOJAVE DR	2	R	1,025	32	33,706	S	4/30/2018	64
TRINWY	020	TRINITY WY	MOJAVE DR	APACHE ST	2	R	1,132	32	36,224	S	4/30/2018	69
TULEST	010	TULE ST	WALKER RD	SAN VINEN RD	2	R	740	32	23,680	A	4/30/2018	93
TURLST	010	TURLOCK ST	BEVAN RD	SAN VICENT	2	R	910	32	29,120	A	4/30/2018	90
TWITRD	010	TWITCHEL ISL RD	GOLDEN GA DR	END E	2	R	1,049	33	37,792	A	4/30/2018	79
UNIORD	010	UNION SQUARE RD	GEARY ST	HEARST ST	2	R	1,200	37	44,400	S	4/30/2018	74
VENIST	010	VENICE ST	END	SHERMAN ISLAND	2	R	734	33	27,397	A	4/30/2018	72
VERMAV	010	VERMONT AV	STONE BL	SEVENTEEN ST	2	R	1,050	26	27,300	A	4/30/2018	22
VERMAV	020	VERMONT AV	SEVENTEEN ST	JEFFERSON BL	2	R	955	26	24,830	A	4/30/2018	20
VICTCT	010	VICTORIA ISL CT	LOPEZ ISL RD	END N	2	R	319	33	13,702	A	4/30/2018	80
VILGPW	005	VILLAGE PW	S River Rd	Stonegate Dr	2	A	2,205	65	143,325	A	4/30/2018	97
VILGPW	010	VILLAGE PW	STONEGATE DR	REDDING RD	4	A	750	56	46,320	A	4/30/2018	91
VILGPW	020	VILLAGE PW	REDDING RD	ELK VALLE ST	4	A	555	69	38,295	A	4/30/2018	80
VILGPW	030	VILLAGE PW	ELK VALLE ST	LAKE WASHINGTON BLVD	2	A	2,546	28	71,288	A	4/30/2018	75
VILGPW	040	VILLAGE PW	LAKE WASHINGTON BLVD	LINDEN RD	2	A	1,075	32	34,400	A	4/30/2018	93

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VILGPW	050	VILLAGE PW	LINDEN RD	DAVIS RD	2	A	6,900	26	179,400	A	4/30/2018	94
VILGPW	060	VILLAGE PW	DAVIS RD	SALMON LN	2	A	2,510	26	65,260	A	4/30/2018	93
VILGPW	070	VILLAGE PW	SALMON LN	STURGEON LN	2	A	2,830	32	90,560	A	4/30/2018	94
VILGPW	080	VILLAGE PW	STURGEON LN	GREGORY AV	2	A	1,950	34	66,300	A	4/30/2018	95
VINECT	010	VINE HILL CT	END S	PEPPERTRE RD	2	R	139	53	8,782	A	4/30/2018	74
VIOLDR	010	VIOLET DR	NANCY LN	LINDEN RD	2	R	1,070	38	40,660	S	4/30/2018	75
VIRGAV	010	VIRGINIA AV	STONE BL	NINETEENT ST	2	R	450	31	13,950	A	4/30/2018	6
VIRGAV	020	VIRGINIA AV	EIGHTEENT ST	SEVENTEEN ST	2	R	560	32	17,920	A	4/30/2018	32
VIRGAV	030	VIRGINIA AV	SEVENTEEN ST	ALAMEDA ST	2	R	1,225	26	31,850	A	4/30/2018	21
VIRGAV	040	VIRGINIA AV	ALAMEDA ST	FIFTEENTH ST	2	R	1,105	26	28,730	A	4/30/2018	24
VIRGAV	050	VIRGINIA AV	FIFTEENTH ST	ELEVENTH ST	2	R	570	32	18,240	A	4/30/2018	44
VIRGAV	060	VIRGINIA AV	ELEVENTH ST	TENTH ST	2	R	285	32	9,120	A	4/30/2018	34
VIRGAV	070	VIRGINIA AV	TENTH ST	WEBSTER ST	2	R	425	32	13,600	A	4/30/2018	34
WAKECT	010	WAKE ISL CT	CHINA HAT RD	END	2	R	284	33	12,547	A	4/30/2018	75
WALKRD	010	WALKER RD	MARTIS ST	TURLOCK ST	2	R	535	32	17,120	A	4/30/2018	90
WALNST	010	WALNUT ST	HOLLY ST	MICHIGAN BL	2	R	1,035	32	33,120	A	4/30/2018	76
WALNST	020	WALNUT ST	MICHIGAN BL	WEST CAPI AV	2	R	1,300	32	41,600	A	4/30/2018	80
WALNST	030	WALNUT ST	WEST CAPI AV	EVERGREEN AV	2	R	930	20	18,600	A	4/30/2018	23
WASHAV	010	WASHINGTON AV	MANZANITA WY	OXFORD ST	2	R	618	32	21,880	A	4/30/2018	80
WASHCT	010	WASHOE CT	RUMSEY ST	END E	2	R	215	32	10,250	A	4/30/2018	68
WATEST	010	WATER ST	END	RIVERBANK RD	2	R	2,025	32	64,800	A	4/30/2018	42
WAFRPL	010	WATERFRONT PL	THIRD ST	WATERFRONT PL	3	C	395	42	16,590	A	4/30/2018	61
WAFRPL	020	WATERFRONT PL	WATERFRONT PL	S CDS	2	R	308	37	16,299	A	4/30/2018	59
WEBSST	010	WEBSTER ST	ELEVENTH ST	PARK BL	2	R	1,090	30	32,900	A	5/30/2018	49
WEBSST	020	WEBSTER ST	PARK BL	JEFFERSON BL	2	R	600	36	21,600	A	5/24/2018	73
WELLWY	010	WELLAND WY	EIGHTH ST	SIXTH ST	2	R	895	32	28,640	A	4/30/2018	80
WECAAV	010	WEST CAPITOL AV	ENTERPRISE BL	I80 EB OFFRAMP	4	A	560	66	39,360	A	4/30/2018	35
WECAAV	020	WEST CAPITOL AV	I80 EB OFFRAMP	# OF LANES CHANGE	4	A	1,150	72	82,800	A	4/30/2018	41
WECAAV	030	WEST CAPITOL AV	# OF LANES CHANGE	# OF LANES CHANGE @ MB #42	3	A	1,046	58	60,668	A	4/30/2018	39
WECAAV	040	WEST CAPITOL AV	# OF LANES CHANGE @ MB #4	CHANGE OF WIDTH	4	A	1,347	62	83,514	A	4/30/2018	40
WECAAV	050	WEST CAPITOL AV	CHANGE OF WIDTH	NORTHPORT DR	5	A	540	78	42,120	A	4/30/2018	41
WECAAV	060	WEST CAPITOL AV	NORTHPORT DR	I80	4	A	605	72	43,560	A	4/30/2018	37
WECAAV	070	WEST CAPITOL AV	I80	PINE ST	2	A	1,475	44	64,900	A	4/30/2018	50
WECAAV	080	WEST CAPITOL AV	PINE ST	HOUSTON ST	3	A	1,305	66	86,130	A	4/30/2018	40
WECAAV	090	WEST CAPITOL AV	HOUSTON ST	HARBOR BL	4	A	720	78	56,160	A	4/30/2018	39
WECAAV	140	WEST CAPITOL AV	JEFFERSON BL	MERKLEY AV	4	A	1,320	71	97,320	A	4/30/2018	87
WECAAV	150	WEST CAPITOL AV	MERKLEY AV	GARDENT ST	4	A	1,006	56	59,936	A	4/30/2018	91
WECAAV	170	WEST CAPITOL AV	300' N/O GARDENT ST	FIFTH ST	2	A	1,145	46	52,670	A	4/30/2018	46
WSTREE	010	WEST ST	E ST	F ST	2	R	365	37	13,505	A	4/30/2018	35
WEACRD	010	WESTACRE RD	MANZANITA WY	MICHIGAN BL	2	A	905	42	32,580	A	4/30/2018	78

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Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
WEACRD	040	WESTACRE RD	WEST CAPI AV	MERKLEY AV	3	A	440	42	18,480	A	4/30/2018	41
WEACRD	050	WESTACRE RD	MERKLEY AV	WILLOW AV	2	A	275	40	11,000	A	4/30/2018	23
WEACRD	060	WESTACRE RD	WILLOW AV	410' S/O EVERGREEN (I50)	2	A	979	35	34,265	A	4/30/2018	31
WEACRD	070	WESTACRE RD	410' S/O EVERGREEN (I50)	PARK BL	2	A	1,679	40	67,160	A	4/30/2018	49
WESTCR	010	WESTWOOD CR	END	LAKEWOOD DR	2	R	180	31	8,219	A	4/30/2018	45
WHITRD	010	WHITE FISH BAY RD	END	N BASS IS RD	2	R	374	33	15,517	A	4/30/2018	71
WHITPL	010	WHITNEY PL	LASSEN ST	END W	2	R	142	32	4,224	A	4/30/2018	87
WIGEST	010	WIGEON ST	MALLARD RD	MUSCOVY RD	2	R	791	25	19,775	A	4/30/2018	68
WILLST	010	WILLIAM ST	EIGHTH ST	SIXTH ST	2	R	890	32	28,480	A	4/30/2018	81
WILLAV	010	WILLOW AV	SYCAMORE ST	WESTACRE RD	2	R	1,845	32	59,040	A	4/30/2018	48
WINDCT	010	WINDEMERE CT	W CDS	TODHUNTER AV	2	R	338	35	13,706	A	4/30/2018	43
WINDCR	010	WINDWARD CR	END	PORT ST	2	R	283	40	12,814	A	5/30/2018	56
WRENST	010	WREN ST	SPOONBILL ST	MUSCOVY RD	2	R	422	23	9,706	A	4/30/2018	89
YOLOST	010	YOLO ST	RIVERBANK RD	LISBON AV	2	R	950	36	34,200	A	4/30/2018	65
YOLOST	020	YOLO ST	LISBON AVE	HOBSON AV	2	R	1,380	36	49,680	A	4/30/2018	48
YOLOST	030	YOLO ST	HOBSON AV	REED AV	2	R	350	36	12,600	A	4/30/2018	60
YOLOST	040	YOLO ST	REED AV	S END	2	R	510	36	18,360	A	5/30/2018	73
YUORPL	010	YUROK PL	TRINITY WY	END	2	R	200	36	7,200	A	4/30/2018	71

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Section Description Inventory
Sorted by Descending PCI

Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
BLDPCT	010	BALDPATE CT	W CDS	TEAL DR	2	R	258	32	10,360	A	4/30/2018	100
MERKAV	050	MERKLEY AV	159' N/O EL RANCHO CT	327' N/O EL RANCHO CT	2	C	168	30	5,040	P	2/28/2018	100
RIVEST	010	RIVERFRONT ST	MILL ST	BRIDGE ST	2	A	901	38	34,238	P	5/24/2018	100
RIVEST	020	RIVERFRONT ST	BRIDGE ST	BALLPARK DR	2	A	1,505	38	57,190	P	5/24/2018	100
BRIDST	010	BRIDGE ST	FIFTEENTH ST	RIVERFRONT ST	2	R	801	44	35,244	P	2/28/2018	99
CENTST	010	CENTRAL ST	BRIDGE ST	GARDEN ST	2	R	331	37	12,247	P	2/28/2018	99
CENTST	020	CENTRAL ST	GARDEN ST	MILL ST	2	R	345	47	16,215	P	2/28/2018	99
FRNWST	030	FERNWOOD ST	FALLBROOK ST	S END	2	R	295	32	9,440	A	4/30/2018	99
FIF5ST	220	FIFTH ST	TOWER CT	RISKE LN	2	A	1,890	86	162,540	P	2/28/2018	99
FIF5ST	230	FIFTH ST	RISKE LN	TOWER BRIDGE GATEWAY	2	A	1,097	64	70,208	P	2/28/2018	99
GARDST	010	GARDEN ST	RIVERFRONT	FIFTH ST	2	R	653	34	22,202	P	2/28/2018	99
HARBBL	060	HARBOR BL	COP 382' S/O RIVERPOINT CT	RICE AV	4	A	702	60	42,120	P	2/28/2018	99
MILLST	010	MILL ST	RIVERFRONT	FIFTH ST	2	R	545	36	19,620	P	2/28/2018	99
RIVEST	030	RIVERFRONT ST	BALLPARK DR	TOWER BRIDGE GTWY	2	A	550	46	25,300	P	5/24/2018	99
CLRKBRLNTR	010	CLARKSBURG BRANCH LINE TRA	SOUTH RIVIER ROAD	1,955 FT SOUTH OF LINDEN RD	1	O	6,430	10	64,300	A	4/30/2018	97
MARLST	010	MARLIN ST	GREENBRIER RD	CEDARBROOK RD	2	R	278	32	8,896	A	4/30/2018	97
VILGPW	005	VILLAGE PW	S River Rd	Stonegate Dr	2	A	2,205	65	143,325	A	4/30/2018	97
BRADWY	020	BRADFORD WY	CONSTITUTION AV	END	2	R	530	30	15,900	A	4/30/2018	96
EDWACT	010	EDGEWATER CT	BAYSIDE RD	END	2	R	250	32	8,000	A	4/30/2018	96
ESTUCT	010	ESTUARY CT	CORANADO ST	END	2	R	270	32	8,640	A	4/30/2018	96
MAPLST	010	MAPLE ST	RICE AV	HOLLY ST	2	C	455	32	14,560	A	4/30/2018	96
SALMLN	010	SALMON LN	VILLAGE PKWY	SCULPIN LN	2	R	1,380	20	27,600	A	4/30/2018	96
STONDR	010	STONEGATE DR	VILLAGE PW	LA JOLLA ST	2	A	725	32	23,200	A	4/30/2018	96
TIDERD	010	TIDEWATER RD	CORANADO ST	END	2	R	110	32	3,520	A	4/30/2018	96
BAYSDRD	010	BAYSIDE RD	SHELL ST	END	2	R	885	32	28,320	A	4/30/2018	95
BLROST	010	BLUE ROCK ST	GREENBRIER RD	END	2	R	440	32	14,080	A	4/30/2018	95
BRADWY	010	BRADFORD WY	CONSTITUTION AV	END	2	R	165	30	4,950	A	4/30/2018	95
GLDKCT	010	GOLD CREEK CT	BRIDGEWAY LAKES DR	S CDS	2	R	299	32	12,834	A	4/30/2018	95
GREERD	020	GREENBRIER RD	BLUE ROCK ST	END	2	R	1,176	32	37,632	A	4/30/2018	95
PORTCT	010	PORTSMOUTH CT	W CDS	WESTACRE RD	2	R	119	32	6,465	A	4/30/2018	95
VILGPW	080	VILLAGE PW	STURGEON LN	GREGORY AV	2	A	1,950	34	66,300	A	4/30/2018	95
CORAST	010	CORANADO ST	CORANADO ST	BARONA ST	2	R	1,070	33	35,310	A	4/30/2018	94
VILGPW	050	VILLAGE PW	LINDEN RD	DAVIS RD	2	A	6,900	26	179,400	A	4/30/2018	94
VILGPW	070	VILLAGE PW	SALMON LN	STURGEON LN	2	A	2,830	32	90,560	A	4/30/2018	94
CASTCT	010	CASTLE CT	W CDS	WESTACRE RD	2	R	178	32	8,353	A	4/30/2018	93
FIF5ST	240	FIFTH ST	TOWER BRIDGE GATEWAY	WEST CAPI AV	2	A	74	75	5,550	A	2/28/2018	93
OTAY	010	OTAY ST	SAN VINEN RD	PILLSBURY RD	2	R	720	32	23,040	A	4/30/2018	93
SHELST	010	SHELL ST	GREENBRIER RD	END	2	R	530	32	16,960	A	4/30/2018	93
TULEST	010	TULE ST	WALKER RD	SAN VINEN RD	2	R	740	32	23,680	A	4/30/2018	93
VILGPW	040	VILLAGE PW	LAKE WASHINGTON BLVD	LINDEN RD	2	A	1,075	32	34,400	A	4/30/2018	93

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VILGPW	060	VILLAGE PW	DAVIS RD	SALMON LN	2	A	2,510	26	65,260	A	4/30/2018	93
BRIDDR	040	BRIDGEWY LAKES DR	CAMANCHE ST	OTIS AV	2	R	1,635	32	52,320	A	4/30/2018	92
KAWEST	010	KAWEAH ST	OTIS AV	COYOTE RD	2	R	730	32	23,360	A	4/30/2018	92
PORTAV	010	PORTSMOUTH AV	WESTACRE RD	MICHIGAN BL	2	R	1,030	32	32,960	A	4/30/2018	92
SOLARD	010	SOUTHERNLAND RD	TURLOCK ST	PROSSER ST	2	R	665	32	21,280	A	4/30/2018	92
ALMNRD	010	ALMANOR RD	BRIDGEWAY LAKES DR	BASS ST	2	R	370	32	11,840	A	4/30/2018	91
BRIDDR	030	BRIDGEWY LAKES DR	SOUTHPORT PW	CAMANCHE ST	2	R	1,875	32	60,000	A	4/30/2018	91
CORARD	010	CORONADO RD	BAYBERRY ST	CORANADO ST	2	R	510	32	16,320	A	4/30/2018	91
ELSNCT	010	ELSINORE CT	BRIDGEWAY LAKES DR	S CDS	2	R	184	32	9,154	A	4/30/2018	91
FLSMCT	010	FOLSOM CT	N CDS	BRIDGEWAY LAKES DR	2	R	174	32	8,834	A	4/30/2018	91
FRTHST	030	FOURTH ST	G ST	S END	2	R	558	24	13,392	A	4/30/2018	91
GLEYCT	010	GOLDENEYE CT	W CDS	MARECA WY	2	R	408	33	15,492	A	4/30/2018	91
HIGHDR	020	HIGHLAND DR	REDONDO RD	LAKE WASH BL	2	R	255	44	11,220	A	4/30/2018	91
ISABST	010	ISABELLA ST	KAWEAH ST	COYOTE RD	2	R	730	32	23,360	A	4/30/2018	91
MANZWY	010	MANZANITA WY	W CDS	WASHINGTON AV	2	R	101	35	5,946	A	4/30/2018	91
TOWRST	010	TOWER ST	W END	FIFTH ST	2	R	240	21	5,040	A	4/30/2018	91
VILGPW	010	VILLAGE PW	STONEGATE DR	REDDING RD	4	A	750	56	46,320	A	4/30/2018	91
WECAAV	150	WEST CAPITOL AV	MERKLEY AV	GARDENT ST	4	A	1,006	56	59,936	A	4/30/2018	91
ALTUCT	010	ALTURAS CT	COLUMBUS RD	END N	2	R	244	33	11,227	A	4/30/2018	90
BETHWY	010	BETHEL WY	CONSTITUTION AV	BRANNAN WY	2	R	475	30	14,250	A	4/30/2018	90
FSTRCT	010	FOSTER CT	N CDS	BRIDGEWAY LAKES DR	2	R	174	32	8,834	A	4/30/2018	90
MANZWY	030	MANZANITA WY	POPLAR ST	WESTACRE RD	2	R	875	35	30,625	A	4/30/2018	90
MARTST	010	MARTIS ST	BEVAN RD	SAN VINEN RD	2	R	1,065	32	34,080	A	4/30/2018	90
MARTST	020	MARTIS ST	SAN VINEN RD	PILLSBURY RD	2	R	905	32	28,960	A	4/30/2018	90
SPOORD	010	SPOONBILL RD	CONDOR ST	STONEGATE	2	R	453	32	14,496	A	4/30/2018	90
TURLST	010	TURLOCK ST	BEVAN RD	SAN VICENT	2	R	910	32	29,120	A	4/30/2018	90
WALKRD	010	WALKER RD	MARTIS ST	TURLOCK ST	2	R	535	32	17,120	A	4/30/2018	90
ALBACT	010	ALABAMA CT	END S	STONE BL	2	R	160	31	7,599	S	4/30/2018	89
COLURD	040	COLUMBUS RD	MESA GRANDE ST	COLUMBUS RD	2	R	610	33	20,130	A	4/30/2018	89
CONDST	010	CONDOR ST	END N	END S	2	R	750	23	17,250	A	4/30/2018	89
DOREPL	010	DORENA PL	END W	CAMANCHE ST	2	R	724	32	26,431	A	4/30/2018	89
ESTREE	050	E ST	SECOND ST	E END	2	R	210	66	13,866	A	4/30/2018	89
EGRERD	010	EGRET RD	STONEGATE	CRESTED ST	2	R	273	32	8,736	A	4/30/2018	89
HUNTRD	020	HUNTINGTON RD	ARROWHEAD ST	COLLINS ST	2	R	855	32	27,360	A	4/30/2018	89
MALLRD	010	MALLARD RD	WIGEON ST	STONEGATE	2	R	415	24	9,960	A	4/30/2018	89
MALLRD	020	MALLARD RD	STONEGATE	CONDOR ST	2	R	380	23	8,740	A	4/30/2018	89
MARKST	010	MARKET ST	RIVERFRONT	GATE	2	R	79	22	1,738	A	4/30/2018	89
MERKAV	060	MERKLEY AV	327' N/O EL RANCHO CT	WEST CAPI AV	2	C	253	30	7,590	A	4/30/2018	89
MILLCT	010	MILLBROOK CT	END S	QUAIL RD	2	R	60	64	4,287	A	4/30/2018	89
SANVRD	010	SAN VINCENT RD	MARTIS ST	PROSSER ST	2	R	1,110	32	35,520	A	4/30/2018	89

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SEABCT	010	SEABROOK CT	N CDS	MICHIGAN BL	2	R	191	32	8,769	A	4/30/2018	89
SIERRD	010	SIERRA RD	LASSEN ST	MOJAVE DR	2	R	925	32	29,600	A	4/30/2018	89
STONBL	050	STONE BL	CAROLINA AV	JEFFERSON BL	2	C	286	40	11,440	A	4/30/2018	89
STUAST	010	STUART ISL ST	GRAHAM IS RD	SOLOMON I RD	2	R	245	33	8,085	A	4/30/2018	89
SWANST	010	SWAN ISL ST	HAITI RD	OAKLAND B DR	2	R	280	33	9,240	A	4/30/2018	89
WRENST	010	WREN ST	SPOONBILL ST	MUSCOVY RD	2	R	422	23	9,706	A	4/30/2018	89
BASSST	010	BASS ST	ALMANOR RD	BRIDGEWAY LAKES DR	2	R	495	32	15,840	A	4/30/2018	88
BENTPL	010	BENTON PL	END S	END N @TRINITY	2	R	1,298	32	44,799	A	4/30/2018	88
BLACRD	010	BLACK BUTTE RD	STONEYFORD RD	HUME ST	2	R	1,041	32	33,312	A	4/30/2018	88
CAMAST	010	CAMANCHE ST	DORENA PL	BRIDGEWY DR	2	R	265	32	8,480	A	4/30/2018	88
CNSTAV	010	CONSTITUTION AV	LINDEN RD	HIGGINS RD	2	R	1,630	36	58,680	A	4/30/2018	88
CORIRD	010	CORTES ISL RD	MALCOLM I ST	HORNBY IS ST	2	R	485	33	16,005	A	4/30/2018	88
HIGHDR	070	HIGHLAND DR	BAYBERRY CT	CORANADO ST	2	R	900	32	28,800	A	4/30/2018	88
MERKAV	040	MERKLEY AV	EL RANCHO CT	159' N/O EL RANCHO CT	2	C	159	60	9,540	A	4/30/2018	88
OTISAV	020	OTIS AV	KAWEAH ST	BRIDGEWAY DR	2	C	1,520	32	48,620	A	4/30/2018	88
PROSST	010	PROSSER ST	END S	PILLSBURY RD	2	R	1,165	32	37,280	A	4/30/2018	88
BERRRD	010	BERRY CREEK RD	ROARING C ST	BEAR RIVER RD	2	R	1,170	33	38,610	A	4/30/2018	87
BRIDDR	020	BRIDGEWY LAKES DR	BERRYESSA PL	SOUTHPORT PW	2	R	2,015	32	64,480	A	4/30/2018	87
CHERPL	010	CHERRY PL	BRIDGEWY DR	END E	2	R	494	32	19,074	A	4/30/2018	87
COLURD	020	COLUMBUS RD	230' E/ NBEACH	COLUMBUS RD	2	R	725	33	23,925	A	4/30/2018	87
COLURD	030	COLUMBUS RD	COLUMBUS RD	MESA GRANDE ST	2	R	762	33	25,146	A	4/30/2018	87
CRESTST	010	CRESTED ST	MUSCOVY RD	MALLARD RD	2	R	757	32	24,224	A	4/30/2018	87
DONNRD	010	DONNER RD	SHASTA WY	END E	2	R	610	32	19,520	A	4/30/2018	87
FLBKST	010	FALLBROOK ST	PARK BL	GRANDE VIEW AV	2	R	700	36	25,200	A	4/30/2018	87
GLIORD	010	GALIANO ISL RD	PENDER IS ST	BOWEN ISL DR	2	R	270	33	8,910	A	4/30/2018	87
KLAMRD	010	KLAMATH RD	CASCADE ST	SWIFT ST	2	R	650	33	21,450	A	4/30/2018	87
LEMORD	010	LEMON TREE RD	END W	MONTE VISTA	2	R	224	33	10,567	A	4/30/2018	87
LNGIST	010	LONG ISL ST	N CDS	CAT ISL RD	2	R	524	33	20,470	A	4/30/2018	87
MESAST	010	MESA GRANDE ST	COLUMBUS RD	STONEGATE DR	2	R	175	33	5,775	A	4/30/2018	87
NEWIST	010	NEW ISLE ST	PHILIP IS RD	ABACO RD	2	R	290	33	9,570	A	4/30/2018	87
OAKLDR	040	OAKLAND BAY DR	TACOMA NA ST	GOLDEN GA DR	2	C	715	33	23,595	A	4/30/2018	87
REDWCT	010	REDWOOD CT	BREENBRIER RD	END	2	R	85	32	2,720	A	4/30/2018	87
ROARST	010	ROARING CREEK ST	END S	COLD SPRINGS RD	2	R	570	33	18,810	A	4/30/2018	87
STUACT	010	STUART CT	N CDS	HOBSON AV	2	R	340	35	14,127	A	4/30/2018	87
WECAAV	140	WEST CAPITOL AV	JEFFERSON BL	MERKLEY AV	4	A	1,320	71	97,320	A	4/30/2018	87
WHITPL	010	WHITNEY PL	LASSEN ST	END W	2	R	142	32	4,224	A	4/30/2018	87
ARROST	010	ARROWHEAD ST	HENSHAW RD	S END	2	R	1,245	32	39,840	A	4/30/2018	86
BRIDDR	010	BRIDGEWY LAKES DR	HENSHAW RD	BERRYESSA PL	2	R	1,490	32	47,680	A	4/30/2018	86
COLDRD	010	COLD SPRINGS RD	BERRY CRE RD	ROARING C ST	2	R	855	33	28,215	A	4/30/2018	86
COKIRD	010	COOK ISL RD	MALCOLM I ST	HORNBY IS ST	2	R	470	33	15,510	A	4/30/2018	86

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COTRD	010	CORTINA RD	REDDING RD	BARONA ST	2	R	545	33	17,985	A	4/30/2018	86
COYARD	020	COYOTE RD	SPALDING CT	OTIS AV	2	R	1,225	32	39,200	A	4/30/2018	86
DELTLN	010	DELTA LN	W END	RISKE LN	2	A	545	41	22,345	A	4/30/2018	86
GRHMRD	010	GRAHAM ISL RD	STUART IS ST	E CDS	2	R	738	33	27,532	A	4/30/2018	86
GUFIST	010	GULF ISL ST	GRAHAM IS RD	S CDS	2	R	539	33	20,965	A	4/30/2018	86
MILLPL	010	MILLERTON PL	ARROWHEAD ST	END E	2	R	374	32	11,968	A	4/30/2018	86
MOJADR	030	MOJAVE DR	LINDEN RD	SPOKANE RD	2	R	840	33	27,720	A	4/30/2018	86
OAKLDR	010	OAKLAND BAY DR	MALCOLM I ST	SOUTHPORT PW	2	C	1,455	32	46,560	A	4/30/2018	86
PHILST	010	PHILLIP ISL RD	SAN SALVA ST	NEW ISLE ST	2	R	505	33	16,665	A	4/30/2018	86
PILLRD	010	PILLSBURY RD	MARTIS ST	PROSSER ST	2	R	495	32	15,840	A	4/30/2018	86
ROCKCRD	010	ROCK CREEK RD	HUME ST	BRIDEWAY LAKES	2	R	791	32	25,312	A	4/30/2018	86
SILWRD	020	SILVERWOOD RD	SOUTHPORT PW	TAHOW ST	2	R	1,405	32	44,940	A	4/30/2018	86
SOLOST	010	SOLOMON ISL RD	STUART IS ST	BOWEN ST	2	R	1,205	33	40,095	A	4/30/2018	86
SWIFTST	010	SWIFT ST	CHINOOK RD	CHELAN RD	2	R	530	33	17,490	A	4/30/2018	86
TAHOST	020	TAHOE ST	SQUAW RD	COYOTE RD	2	R	1,550	32	49,600	A	4/30/2018	86
ALPIST	010	ALPINE ST	BRIDGEWAY LAKES	STONEYFORD RD	2	R	249	32	7,968	A	4/30/2018	85
BALLDR	020	BALLPARK DR	IRONWORKS AV	DREVER ST	2	R	563	30	16,890	A	4/30/2018	85
BAROST	010	BARONA ST	CORANADO RD	LA JOLLA ST	2	R	1,440	33	47,520	A	4/30/2018	85
BENTST	020	BENTON ST	CHEROKEE RD	TRINITY WY	2	R	655	32	20,960	A	4/30/2018	85
CATIRD	010	CAT ISL RD	SAN SALVADOR ST	LONG ISL ST	2	R	500	33	16,500	A	4/30/2018	85
COLLST	010	COLLINS ST	TOPAZ RD	N CDS	2	R	1,394	32	47,874	A	4/30/2018	85
ESTREE	030	E ST	SEVENTH ST	WIDTH CHANGE	2	R	613	30	18,390	A	4/30/2018	85
HAVAST	010	HAVASU ST	SILVERWOO RD	LEWISTON RD	2	R	270	32	8,640	A	4/30/2018	85
HENSRD	020	HENSHAW RD	SOUTHPORT PW	EAGLE ST	2	R	200	32	6,400	A	4/30/2018	85
IRISPL	010	IRIS PL	W CDS	RIVERMONT ST	2	R	269	32	11,874	A	4/30/2018	85
LEWIRD	010	LEWISTON RD	EAGLE ST	TAHOE ST	2	R	1,310	32	41,920	A	4/30/2018	85
MAPLST	020	MAPLE ST	HOLLY ST	MICHIGAN BL	2	C	1,040	32	33,280	A	4/30/2018	85
MUSCRD	010	MUSCOVY RD	WIGEON ST	CONDOR ST	2	R	1,000	32	32,000	A	4/30/2018	85
NBEAAV	010	N BEACH AV	COLUMBUS RD	STONEGATE DR	2	C	400	33	13,200	A	4/30/2018	85
NWEBST	010	NEWBERRY ST	CEDARBROO RD	GREENBRIA RD	2	R	270	32	8,640	A	4/30/2018	85
PENDST	010	PENDER ISL ST	GALIANO I RD	END N	2	R	563	33	21,754	A	4/30/2018	85
PYRAPL	010	PYRAMID PL	END W	COLLINS ST	2	R	314	32	13,311	A	4/30/2018	85
REDDRD	010	REDDING RD	LA JOLLA ST	VILLAGE PW	2	R	1,150	33	37,950	A	4/30/2018	85
REDORD	010	REDONDO RD	STONEGATE DR	HIGHLAND DR	2	R	1,130	33	37,290	A	4/30/2018	85
ROGUCR	010	ROGUE RIVER CR	MOJAVE DR	MOJAVE DR	2	R	1,180	33	38,940	A	4/30/2018	85
ROGUCT	010	ROGUE RIVER CT	ROGUE RIV CR	END E	2	R	164	33	8,587	A	4/30/2018	85
SILVST	010	SILVERADO ST	LAKE WASH BL	SANDY POI RD	2	R	205	32	6,560	A	4/30/2018	85
SILWRD	010	SILVERWOOD RD	END W	SOUTHPORT PW	2	R	1,524	32	52,031	A	4/30/2018	85
SONOAV	010	SONORA AV	FALLBROOK ST	FERNWOOD ST	2	R	457	32	14,624	A	4/30/2018	85
SQUARD	010	SQUAW RD	SILVERWOO RD	OTIS V	2	R	1,440	32	46,080	A	4/30/2018	85

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Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
BIGBST	010	BIG BEAR ST	HENSHAW RD	N CDS	2	R	459	32	17,954	A	4/30/2018	84
BRIDCT	010	BRIDLE CT	STABLE DR	END N	2	R	219	32	10,271	A	4/30/2018	84
CEDAST	010	CEDAR ST	N END	WEST CAPI AV	2	R	675	32	21,600	A	4/30/2018	84
COLURD	010	COLUMBUS RD	N BEACH AV	230' E/ NBEACH	2	R	230	33	7,590	A	4/30/2018	84
ELKVST	010	ELK VALLEY ST	VILLAGE PW	BERRY CRE RD	2	R	240	33	7,920	A	4/30/2018	84
GOLDST	010	GOLD HILL ST	SANDY POI RD	QUAIL RD	2	R	530	32	16,960	A	4/30/2018	84
HOLLST	010	HOLLY ST	PECAN ST	MAPLE ST	2	R	795	32	25,440	A	4/30/2018	84
IRWKSAV	010	IRONWORKS AV	TOWER BRIDGE GATEWAY	BALLPARK DR	2	R	735	25	18,375	A	4/30/2018	84
MONTST	010	MONTE VISTA ST	CHINOOK RD	MONTE VISTA	2	R	670	32	21,440	A	4/30/2018	84
OTISAV	010	OTIS AV	SOUTHPORT PW	KAWEAH ST	2	C	1,715	32	54,880	A	4/30/2018	84
PRINRD	010	PRINCETON RD	HIGHLAND DR	460' E/ HIGHLAND	2	R	460	32	14,720	A	4/30/2018	84
RICEAV	020	RICE AV	RICE AV	MAPLE ST	2	C	984	40	39,360	A	4/30/2018	84
RIVMST	010	RIVERMONTE ST	MARSHALL RD	LILY PL	2	R	1,825	32	58,400	A	4/30/2018	84
SANCCT	010	SAN CARLOS CT	REDONDO RD	END N	2	R	344	33	14,527	A	4/30/2018	84
SANST	010	SAN SALVADOR ST	MARSHALL RD	OAKLAND BAY DR	2	R	1,420	33	46,680	A	4/30/2018	84
SPOKRD	010	SPOKANE RD	STONEGATE DR	HIGHLAND DR	2	R	1,135	33	37,455	A	4/30/2018	84
STONDR	040	STONEGATE DR	N BEACH CT	LAKE WASHINGTON BL	2	A	1,215	32	38,880	A	4/30/2018	84
STRLN	010	STURGEON LN	VILLAGE PKWY	END	2	R	2,145	20	42,900	A	4/30/2018	84
TOPARD	010	TOPAZ RD	END W	ARROWHEAD ST	2	R	1,348	33	50,834	A	4/30/2018	84
ATHECT	010	ATHERTON CT	END S	STONEGATE DR	2	R	330	33	14,065	A	4/30/2018	83
BAROCT	010	BARONA CT	END S	CORANADA ST	2	R	289	33	12,712	A	4/30/2018	83
BEVARD	010	BEVAN RD	OTIS AV	E END	2	R	811	32	25,952	A	4/30/2018	83
ELKHPL	010	ELKHORN PLAZA	DOUGLAS ST	REUTER DR	2	R	755	45	33,975	A	4/30/2018	83
FRSRDR	010	FRASER ISL DR	SAN SALVADOR ST	LONG ISL ST	2	R	435	33	14,355	A	4/30/2018	83
HENSRD	010	HENSHAW RD	BRIDGEWAY LAKES DR	SOUTHPORT PW	2	R	1,435	32	45,920	A	4/30/2018	83
HOOPRD	010	HOOPA RD	STONEGATE	END E	2	R	543	32	17,376	A	4/30/2018	83
HORBST	010	HORNBY ISL ST	OAKLAND BAY DR	MARSHALL RD	2	R	1,480	35	51,800	A	4/30/2018	83
HUMEST	010	HUME ST	BRIDGEWAY LAKES	ROCK CREEK RD	2	R	548	32	17,536	A	4/30/2018	83
KINERD	010	KING EDWARD RD	BACON ISL ST	RYER ISL ST	2	R	680	33	22,440	A	4/30/2018	83
LASSST	010	LASSEN ST	DONNER RD	WHITNEY PL	2	R	547	32	17,504	A	4/30/2018	83
PENNCT	010	PENNSYLVANIA CT	END S	STONE BL	2	R	160	31	7,599	A	4/30/2018	83
POPPST	010	POPPY ST	RIVERMONT ST	LILY PL	2	R	1,255	32	40,160	A	4/30/2018	83
SPALCT	010	SPALDING CT	END S	COYOTE RD	2	R	254	32	11,182	A	4/30/2018	83
STONDRS	020	STONEGATE DR	MALLARD RD	MUSCOVY RD	2	C	770	32	24,640	A	4/30/2018	83
ARTHDR	010	ARTHUR DR	ANNA ST	HOBSON AV	2	R	1,365	35	47,775	A	4/30/2018	82
BERYPL	010	BARBERRY PL	W CDS	RIVERMONT ST	2	R	269	32	11,874	A	4/30/2018	82
CEDERD	020	CEDARBROOK RD	NEWBERRY ST	MARLIN ST	2	R	650	32	20,800	A	4/30/2018	82
CYPRRD	010	CYPRESS RD	BRISBANE ST	NBEACH AV	2	R	470	33	15,510	A	4/30/2018	82
DRWDST	020	DEERWOOD ST	DEERWOD CR	GRANDE VIEW AV	2	R	1,295	31	40,145	S	4/30/2018	82
FOWLRD	010	FOWLER RD	BRIDGEWY DR	BIG BEAR ST	2	R	760	32	24,320	A	4/30/2018	82

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HIGHDR	030	HIGHLAND DR	LAKE WASH BL	PRINCETON RD	2	R	265	45	11,925	A	4/30/2018	82
MARYCT	010	MARYLAND CT	END S	STONE BL	2	R	160	31	7,599	S	4/30/2018	82
MICHBL	040	MICHIGAN BL	ROCKROSE RD	JEFFERSON BL	2	C	165	54	8,910	A	4/30/2018	82
MORRST	010	MORROW BAY ST	TWITCHEL RD	CARMEL BA RD	2	R	270	33	8,910	A	4/30/2018	82
NEWHPL	010	NEW HOGAN PL	OTIS AV	MARTIS ST	2	R	175	32	5,600	A	4/30/2018	82
OAKLDR	030	OAKLAND BAY DR	MARTINIQU ST	TACOMA N ST	2	C	1,340	33	47,520	A	4/30/2018	82
SOUTPW	110	SOUTHPORT PW	MARSHALL RD	BRITTON PL	2	A	1,855	36	66,780	A	4/30/2018	82
SOUTPW	120	SOUTHPORT PW	BRITTON PL	SILVERWOOD RD	2	A	1,860	46	85,500	A	4/30/2018	82
STONDR	030	STONEGATE DR	MESA GRAN ST	N BEACH AV	2	A	655	32	20,960	A	4/30/2018	82
STONDR	050	STONEGATE DR	LAKE WASH BL	REDONDO RD	2	A	365	36	13,140	A	4/30/2018	82
TRINRD	010	TRINIDAD RD	TOBAGO ST	JAMAICA ST	2	R	450	33	14,850	A	4/30/2018	82
BERRPL	010	BERRYESSA PL	BRIDGEWY DR	END E	2	R	554	32	20,994	A	4/30/2018	81
BRIST	010	BRISBANE ST	PRESIDIO RD	CYPRESS RD	2	R	300	33	9,900	A	4/30/2018	81
CASCST	010	CASCADE ST	CHINOOK RD	SPOKANE RD	2	R	710	33	22,720	A	4/30/2018	81
CHINRD	010	CHINOOK RD	CASCADE ST	MONTE VISTA	2	R	1,115	33	35,680	A	4/30/2018	81
COYARD	010	COYOTE RD	SOUTHPORT PW	SPALDING CT	2	R	1,250	32	40,000	A	4/30/2018	81
DLWRCT	010	DELAWARE CT	END S	STONE BL	2	R	160	31	7,599	S	4/30/2018	81
HARBBL	050	HARBOR BL	REED AV	COP 382' S/O RIVERPOINT CT	4	A	1,301	60	78,060	A	4/30/2018	81
HIGHDR	010	HIGHLAND DR	CHELAN RD	REDONDO RD	2	R	445	33	14,685	A	4/30/2018	81
HIGHDR	050	HIGHLAND DR	MIRA LOMA CT	DOLPHIN CT	2	R	560	32	17,920	A	4/30/2018	81
LAJOST	010	LA JOLLA ST	REDDING RD	STONEGATE ST	2	R	895	33	29,535	A	4/30/2018	81
LEMORD	030	LEMON TREE RD	REDWOOD AV	PEACHTREE ST	2	R	185	36	6,660	A	4/30/2018	81
LILYPL	010	LILY PL	W CDS	POPPY ST	2	R	514	32	19,714	A	4/30/2018	81
LONECT	010	LONE PINE CT	END S	BARONA ST	2	R	344	33	14,527	A	4/30/2018	81
MALCST	010	MALCOLM ISL ST	LOPEZ ISL RD	OAKLAND B DR	2	R	1,265	33	41,745	A	4/30/2018	81
MENDCT	020	MENDOCINO CT	COYOTE RD	DORENA PL	2	R	290	32	9,280	A	4/30/2018	81
NEWHPL	020	NEW HOGAN PL	MARTIS ST	END E	2	R	189	32	9,311	A	4/30/2018	81
POLAST	010	PORTOLA ST	PRESIDIO RD	CYPRESS RD	2	R	350	33	11,550	A	4/30/2018	81
QUAILCT	010	QUAIL CT	QUAIL RD	END N	2	R	124	38	7,447	A	4/30/2018	81
SANRCT	010	SAN REMO CT	REDONDO RD	END N	2	R	254	33	11,557	A	4/30/2018	81
SANDRD	010	SANDY POINT RD	SILVERADO ST	GOLD HILL ST	2	R	480	32	15,360	A	4/30/2018	81
STONDRS	010	STONEGATE DR	LINDEN RD	MALLARD RD	2	C	179	32	5,728	A	4/30/2018	81
WILLST	010	WILLIAM ST	EIGHTH ST	SIXTH ST	2	R	890	32	28,480	A	4/30/2018	81
BURRST	010	BURROWS ST	TODHUNTER AV	FOURNESS DR	2	R	760	32	24,320	A	4/30/2018	80
CALLCT	010	CALL CT	MARECA WY	S CDS	2	R	143	33	6,747	A	4/30/2018	80
CSTCCT	010	CASTAIC CT	END S	BRITTON PL	2	R	374	32	15,231	A	4/30/2018	80
DOUGST	030	DOUGLAS ST	CUMMINS WY	LIGHTHOUS DR	2	C	710	36	25,560	A	4/30/2018	80
DUETDR	010	DUET DR	CONSTITUT AV	PARADISE WY	2	R	1,055	36	37,900	S	4/30/2018	80
ESTREE	020	E ST	RR TRACKS	SEVENTH ST	2	R	463	36	16,668	A	4/30/2018	80
FIFTST	010	FIFTEENTH ST	JEFFERSON BL	SOUTH RIV RD	2	A	895	48	39,380	A	4/30/2018	80

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FOURST	020	FOURTH ST	B ST	C ST	2	R	475	46	21,850	A	4/30/2018	80
GLIDAV	010	GLIDE AV	RICE AV	GLIDE CT	2	R	1,220	22	26,840	S	4/30/2018	80
HAITIRD	010	HAITI RD	GUADELUPE ST	DOMINICA ST	2	R	890	33	29,370	A	4/30/2018	80
LINDRD	N160	LINDEN RD	CANVASBACK WY	BRENDA WY	3	A	1,045	36	41,822	S	4/30/2018	80
LINDRD	S100	LINDEN RD	CONSTITUTION AV	SUMMERFIELD DR	2	A	1,050	29	34,652	S	4/30/2018	80
MALICT	010	MALIBU CT	REDONDO RD	END N	2	R	344	33	14,527	A	4/30/2018	80
MARIDR	020	MARINA GREENS DR	SOUTH RIV RD	MARINA GR DR	1	R	360	17	6,120	S	4/30/2018	80
MERGCT	010	MERGANSERS CT	END	MARECA WY	2	R	258	33	13,702	S	4/30/2018	80
NEPTCT	010	NEPTUNE CT	END S	HIGHLAND DR	2	R	349	32	14,431	A	4/30/2018	80
RAMOCT	010	RAMONA CT	CORTINA RD	END N	2	R	214	33	10,237	A	4/30/2018	80
STONDR	020	STONEGATE DR	LA JOLLA ST	MESA GRAN ST	2	A	850	32	27,200	A	4/30/2018	80
STONDR	060	STONEGATE DR	REDONDO RD	LINDEN RD	2	A	1,155	32	36,960	A	4/30/2018	80
STONRD	010	STONEYFORD RD	ALPINE ST	BLACK BUTTE RD	2	R	384	32	12,288	A	4/30/2018	80
TOBAST	010	TOBAGO ST	TRINIDAD RD	ST CROIX RD	2	R	665	33	21,945	A	4/30/2018	80
VICTCT	010	VICTORIA ISL CT	LOPEZ ISL RD	END N	2	R	319	33	13,702	A	4/30/2018	80
VILGPW	020	VILLAGE PW	REDDING RD	ELK VALLE ST	4	A	555	69	38,295	A	4/30/2018	80
WALNST	020	WALNUT ST	MICHIGAN BL	WEST CAPI AV	2	R	1,300	32	41,600	A	4/30/2018	80
WASHAV	010	WASHINGTON AV	MANZANITA WY	OXFORD ST	2	R	618	32	21,880	A	4/30/2018	80
WELLWY	010	WELLAND WY	EIGHTH ST	SIXTH ST	2	R	895	32	28,640	A	4/30/2018	80
BEARCT	010	BEAR RIVER CT	BERRY CRE RD	END E	2	R	264	33	11,887	A	4/30/2018	79
CEDERD	010	CEDARBROOK RD	END W	NEWBERRY ST	2	R	269	32	11,879	A	4/30/2018	79
CHELRD	010	CHELAN RD	END W	SWIFT ST	2	R	410	33	13,530	A	4/30/2018	79
DRWDST	010	DEERWOOD ST	PARK BL	DEERWOOD CR	2	R	1,150	31	35,650	S	4/30/2018	79
DEVOAV	020	DEVON AV	KINSINGTO ST	JEFFERSON BL	2	R	350	36	12,600	S	4/30/2018	79
GSTREE	020	G ST	SIXTH ST	FIFTH ST	2	R	400	36	14,400	A	4/30/2018	79
GSTREE	030	G ST	FIFTH ST	THIRD ST	2	R	785	34	26,690	A	4/30/2018	79
HIGHDR	060	HIGHLAND DR	DOLPHIN CT	BAYBERRY ST	2	R	435	32	13,920	A	4/30/2018	79
HUNTRD	010	HUNTINGTON RD	W CDS	ARROWHEAD ST	2	R	254	32	11,394	A	4/30/2018	79
JAMAST	010	JAMAICA ST	OAKLAND BAY DR	MARSHALL RD	2	R	640	33	21,120	A	4/30/2018	79
LIHODR	030	LIGHTHOUSE DR	DOUGLAS ST	FOUNTAIN DR	2	C	1,330	38	56,540	A	4/30/2018	79
MANZWY	020	MANZANITA WY	WASHINGTON AV	POPLAR ST	2	R	595	32	19,040	A	4/30/2018	79
MAPLST	030	MAPLE ST	MICHIGAN BL	WEST CAPI AV	2	C	1,250	32	40,000	A	4/30/2018	79
REDWAV	030	REDWOOD AV	LAKE WASHINGTON BL	GREENBRIA RD	2	R	475	32	15,200	A	4/30/2018	79
SOUTPW	140	SOUTHPORT PW	OTIS AV	JEFFERSON BL	2	A	1,435	36	51,660	A	4/30/2018	79
SUISRD	010	SUISUN BAY RD	GOLDEN GA DR	END E	2	R	1,509	33	52,972	A	4/30/2018	79
TWITRD	010	TWITCHEL ISL RD	GOLDEN GA DR	END E	2	R	1,049	33	37,792	A	4/30/2018	79
ASTREE	010	A ST	SIXTH ST	FIFTH ST	2	R	410	33	13,530	S	5/30/2018	78
BENTST	010	BENTON ST	S END	CHEROKEE RD	2	R	405	32	12,960	A	4/30/2018	78
CATLRD	030	CATALINA ISL RD	SANTA BARBARA ST	RYER ISL ST	2	R	550	33	18,150	A	4/30/2018	78
DOLPCT	010	DOLPHIN CT	HIGHLAND DR	END N	2	R	109	54	7,130	A	4/30/2018	78

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FREMBL	030	FREMONT BL	HARDY DR	DOUGLAS ST	2	R	1,025	35	35,875	A	4/30/2018	78
HIGHDR	040	HIGHLAND DR	PRINCETON RD	MIRA LOMA CT	2	R	1,515	32	48,480	A	4/30/2018	78
INGLDR	010	INGLEWOOD DR	FREMONT BL	CUMMINS WY	2	R	1,190	32	38,080	A	4/30/2018	78
LGOOLN	010	LAGOON LN	INDEPENDE AV	MEADOW LA CR	2	R	950	36	34,200	S	4/30/2018	78
LAKEBL	010	LAKE WASHIGNTON BL	VILLAGE PW	REDWOOD AV	2	A	1,950	30	58,500	A	4/30/2018	78
LILALN	010	LILAC LN	MANZANITA WY	MICHIGAN BL	2	R	1,150	32	36,800	A	4/30/2018	78
MAYSTR	020	MAY ST	MYRTLE AV	TODHUNTER AV	2	R	700	33	23,100	S	5/24/2018	78
OAKSTR	010	OAK ST	REED AV	ELM ST	2	R	240	32	7,680	A	4/30/2018	78
REUTDR	010	REUTER DR	SACRAMENT AV	CUMMINS WY	2	R	1,825	28	51,100	A	4/30/2018	78
SANBST	010	SANTA BARBARA ST	CATALINA RD	SAN NICOLAS	2	R	220	33	7,260	A	4/30/2018	78
STABDR	010	STABLE DR	MARSHALL RD	877' W/ MARSHALL	2	R	877	32	28,064	A	4/30/2018	78
WEACRD	010	WESTACRE RD	MANZANITA WY	MICHIGAN BL	2	A	905	42	32,580	A	4/30/2018	78
BAYBST	020	BAYBERRY ST	CORANADO ST	HIGHLAND	2	R	610	32	19,520	A	4/30/2018	77
BEARRD	010	BEAR RIVER RD	VILLAGE PW	BERRY CRE RD	2	R	230	33	7,590	A	4/30/2018	77
CASSDR	010	CASSELMAN DR	ELKHORN P	CUMMINS WY	2	R	1,705	28	47,740	A	4/30/2018	77
GUAPST	010	GUADELUPE ST	OAKLAND BAY DR	MARSHALL RD	2	R	760	33	25,080	A	4/30/2018	77
MIRACT	010	MIRA LOMA CT	END S	HIGHLAND DR	2	R	134	32	7,551	A	4/30/2018	77
PECAST	020	PECAN ST	MICHIGAN BL	WEST CAPI AV	2	R	1,275	32	40,800	A	4/30/2018	77
SANDCT	010	SANDY POINT CT	GOLD HILL ST	END E	2	R	179	32	8,991	A	4/30/2018	77
SHERRD	010	SHERMAN ISL RD	GOLDEN GA DR	VENICE ST	2	R	1,060	32	33,920	A	4/30/2018	77
STCRRD	010	ST CROIX RD	MARTINIQU ST	JAMAICA ST	2	R	765	33	25,245	A	4/30/2018	77
STONBL	060	STONE CT	END S	STONE BL	2	C	181	32	5,792	A	4/30/2018	77
TOBACT	010	TOBAGO CT	TOBAGO ST	END	2	R	154	33	8,257	A	4/30/2018	77
ANDRCT	010	ANDRUS ISL CT	SHERMAN I RD	S CDS	2	R	394	33	16,286	A	4/30/2018	76
AZLAPL	010	AZALEA PL	RIVERMONT ST	E CDS	2	R	154	32	8,194	A	4/30/2018	76
BOWIST	010	BOWEN ISL ST	SOLOMON I RD	OAKLAND B DR	2	R	605	33	19,965	A	4/30/2018	76
BRTTPL	010	BRITTON PL	END W	SOUTHPORT PW	2	R	489	32	15,648	A	4/30/2018	76
CITRST	010	CITRUS ST	ELM ST	E CDS	2	R	1,060	32	36,387	A	4/30/2018	76
CLRDST	010	CLARENDON ST	GARDENIA AV	GRANDE VI AV	2	R	688	34	23,392	A	4/30/2018	76
DREWST	010	DREW ST	TODHUNTER AV	FOURNESS DR	2	R	865	31	26,815	A	5/23/2018	76
HAMOCR	010	HALF MOON BAY CR	TACOMA NARROWS ST	GOLDEN GATE DR	2	R	1,150	36	41,400	A	4/30/2018	76
HOBSAV	050	HOBSON AV	FOURNESS DR	KEGLE DR	2	R	985	32	31,520	S	4/30/2018	76
KOVRDR	010	KOVR DR	RIVERPOIN DR	E CDS	2	R	450	36	20,454	A	4/30/2018	76
MARIDR	010	MARINA GREENS DR	RANDOLPH RD	SOUTH RIV RD	2	R	340	37	12,580	S	4/30/2018	76
MENDCT	010	MENDOCINO CT	END S	COYOTE RD	2	R	304	32	12,991	A	4/30/2018	76
MOJADR	010	MOJAVE DR	END S	439' S/ LINDEN RD	2	R	1,550	33	51,150	A	4/30/2018	76
OAKSTR	020	OAK ST	ELM ST	S CDS	2	R	1,152	32	38,102	A	4/30/2018	76
PHELCT	010	PHELPS CT	PITZER CR	END	2	R	248	32	10,126	S	4/30/2018	76
RANDRD	010	RANDOLPH RD	GEARY ST	HEARST ST	2	R	780	37	28,860	S	4/30/2018	76
ROLLCT	010	ROLLINS CT	LEWISTON RD	END N	2	R	319	32	137,967	A	4/30/2018	76

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Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
RYERST	010	RYER ISL ST	SUISUN BAY	ARCATA BAY RD	2	R	805	33	26,565	A	4/30/2018	76
RYERST	020	RYER ISL ST	ARCATA BAY RD	CATALINA RD	2	R	1,370	33	45,210	A	4/30/2018	76
SHASTWY	010	SHASTA WY	S END	RUBICON WAY	2	R	1,070	32	34,240	S	4/30/2018	76
SOUTPW	130	SOUTHPORT PW	SILVERWOO RD	OTIS AV	2	A	1,570	36	56,520	A	4/30/2018	76
STONDRS	030	STONEGATE DR	MUSCOVY RD	COLUSA RD	2	C	1,460	32	46,720	A	4/30/2018	76
WALNST	010	WALNUT ST	HOLLY ST	MICHIGAN BL	2	R	1,035	32	33,120	A	4/30/2018	76
AIROCT	010	AIRONS CT	W CDS	CANVASBACK WY	2	R	233	36	10,188	A	4/30/2018	75
ALDEWY	020	ALDER WY	LINDEN RD	LIMEWOOD WY	2	R	180	33	5,940	A	4/30/2018	75
ARCBRD	010	ARCATA BAY RD	RYER ISL ST	VENICE ST	2	R	290	33	9,570	A	4/30/2018	75
BEARDR	010	BEARDSLEY DR	ELKHORN P	CUMMINS WAY	2	R	1,705	28	47,740	A	4/30/2018	75
BONRRD	010	BONAIRE RD	DOMINICA ST	GUADELUPE ST	2	R	860	33	28,380	A	4/30/2018	75
BUTLCT	020	BUTLER CT	CANVASBACK WY	E CDS	2	R	228	32	9,400	A	4/30/2018	75
CHILRD	010	CHINA HAT ISL RD	MIDWAY IS ST	MAUI ST	2	R	550	33	18,150	A	4/30/2018	75
CLIKCT	010	CLICKER CT	W CDS	CANVASBACK WY	2	R	263	32	10,520	A	4/30/2018	75
DRAKST	010	DRAKES BAY ST	TREASURE RD	BODEGA BA RD	2	R	325	33	10,725	A	4/30/2018	75
EMBADR	020	EMBARCADERO DR	EMBARCADERO DR	RIVERSIDE PW	2	R	1,214	45	54,630	A	4/30/2018	75
GRDIAV	010	GARDENIA AV	DEERWOOD ST	CLARENDON ST	2	R	288	31	8,928	A	4/30/2018	75
GLENST	010	GLENWOOD ST	ALAMO PL	ORINDA PL	2	R	640	37	23,680	A	4/30/2018	75
HAMOCR	030	HALF MOON BAY CR	GOLDEN GATE DR	SANTA CRUZ RD	2	R	180	36	6,480	A	4/30/2018	75
HARDDR	020	HARDY DR	CUMMINS WY	END	2	R	970	36	34,920	A	4/30/2018	75
JENNCT	010	JENNINGS CT	LEWISTON RD	END N	2	R	234	32	10,542	A	4/30/2018	75
LEMORD	020	LEMON TREE RD	MONTE VISTA	REDWOOD AV	2	R	745	36	26,820	A	4/30/2018	75
LNGCST	010	LONGCROFT ST	GRANDE VI AV	MEADOW RD	2	R	825	32	26,400	A	4/30/2018	75
MAUIST	010	MAUI ST	OAKLAND B DR	HALF MOON CR	2	R	1,110	33	36,630	A	4/30/2018	75
NUNECT	010	NUNEZ CT	END	MARECA WY	2	R	153	33	7,162	S	4/30/2018	75
PHEADR	010	PHESANT HOLLOW DR	HIGGINS RD	STARLING LN	2	R	1,600	33	52,800	S	4/30/2018	75
PRESRD	010	PRESIDIO RD	BRISBANE ST	N BEACH AV	2	R	570	33	18,810	A	4/30/2018	75
PRINRD	020	PRINCETON RD	460' E/ HIGHLAND	BAYBERRY ST	2	R	753	32	24,096	A	4/30/2018	75
QUAILRD	010	QUAIL RD	SILVERADO ST	HIGHLAND DR	2	R	1,130	32	36,160	A	4/30/2018	75
RICEAV	030	RICE AV	MAPLE ST	E CDS	2	C	457	41	20,569	A	4/30/2018	75
SEYMAV	030	SEYMORE AV	MARSHALL RD	STABLE DR	2	R	222	32	10,367	A	4/30/2018	75
SOUTRD	180	SOUTH RIVER RD	1800 SOUTH RIV RD	15TH ST	2	A	2,077	28	58,156	A	4/30/2018	75
VILGPW	030	VILLAGE PW	ELK VALLE ST	LAKE WASHINGTON BLVD	2	A	2,546	28	71,288	A	4/30/2018	75
VIOLDR	010	VIOLET DR	NANCY LN	LINDEN RD	2	R	1,070	38	40,660	S	4/30/2018	75
WAKECT	010	WAKE ISL CT	CHINA HAT RD	END	2	R	284	33	12,547	A	4/30/2018	75
BINGAV	010	BIRMINGHAM AV	MANCHESTE ST	KINSINGTO ST	2	R	800	32	25,600	S	4/30/2018	74
CRMLRD	010	CARMEL BAY RD	MORROW BA ST	E CDS	2	R	954	33	34,660	A	4/30/2018	74
DIANDR	010	DIANE DR	LESLIE LN	LINDEN RD	2	R	520	36	18,720	A	4/30/2018	74
EAGLST	010	EAGLE ST	LEWISTON RD	END N	2	R	639	32	23,711	A	4/30/2018	74
FSTREE	040	F ST	FIFTH ST	THIRD ST	2	C	805	37	29,785	A	4/30/2018	74

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FLBKST	020	FALLBROOK ST	GRANDE VIEW AV	SONORS AV	2	R	573	32	18,336	A	4/30/2018	74
GSTREE	010	G ST	W END	SIXTH ST	2	R	911	27	24,597	A	4/30/2018	74
GRVIAV	010	GRANDE VISTA AV	DEERWOOD ST	FALLBROOK ST	2	R	960	34	32,640	S	4/30/2018	74
GREEAV	010	GREENWOOD AV	FREMONT BL	CUMMINS WY	2	R	1,200	32	38,400	A	4/30/2018	74
HARBBL	020	HARBOR BL	I80	RIVERBANK RD	2	A	560	26	14,560	A	4/30/2018	74
HOBSAV	030	HOBSON AV	BRYTE AV	TODHUNTER AV	2	R	1,110	36	39,960	S	4/30/2018	74
JEFRBL	110	JEFFERSON BL	SOUTH RIV RD	GATEWAY DR	4	A	1,570	62	106,460	A	4/30/2018	74
MARSRD	020	MARSHALL RD	SOUTHPORT PW	MARTINIQU ST	2	A	1,110	32	35,520	A	4/30/2018	74
MARQST	010	MARTINIQUE ST	MARSHALL RD	OAKLAND B DR	2	R	1,185	33	37,105	A	4/30/2018	74
NBASRD	010	N BASS ISL RD	HALF MOON CIR	WHITE FIS RD	2	R	575	33	18,975	A	4/30/2018	74
ORINPL	010	ORINDA PL	END W	END. E	2	R	536	37	25,378	A	4/30/2018	74
OTISAV	030	OTIS AV	BRIDGEWY DR	MARSHALL RD	2	C	1,730	32	55,360	A	4/30/2018	74
PEACST	010	PEACHTREE ST	LEMON TRE RD	PEPPERTRE RD	2	R	380	37	14,060	A	4/30/2018	74
PILORD	010	PILOT POINT RD	ALEUTIAN ST	TACOMA NA ST	2	R	635	33	20,955	A	4/30/2018	74
ROMACT	010	ROMA CT	END	LEMON TRE RD	2	R	139	53	8,782	A	4/30/2018	74
SACRAV	080	SACRAMENTO AV	EIGHTH ST	SIXTH ST	2	A	619	38	23,522	A	4/30/2018	74
SAGIST	010	SAGINAW ST	END	CATALINA RD	2	R	789	33	29,212	A	4/30/2018	74
SANSST	020	SANSOME ST	GATEWAY DR	UNION SQ RD	2	R	395	36	14,220	S	4/30/2018	74
SANCRD	010	SANTA CRUZ RD	GOLDEN GA DR	CATALINA RD	2	R	1,224	33	43,567	A	4/30/2018	74
SILVST	020	SILVERADO ST	SANDYPOINT RD	QUAIL RD	2	R	225	32	7,200	A	4/30/2018	74
STLUPL	010	ST. LUCIA PL	END	MARTINIQU ST	2	R	339	33	11,187	A	4/30/2018	74
STONBL	030	STONE BL	PARK BL	ALABAMA CT	2	C	1,734	36	62,424	A	4/30/2018	74
TACOST	010	TACOMA NARROWS ST	OAKLAND B DR	HALF MOON CIR	2	R	950	36	34,200	A	4/30/2018	74
THRDST	050	THIRD ST	G ST	TOWER BRIDGE GATEWAY	5	C	720	59	42,480	A	4/30/2018	74
UNIORD	010	UNION SQUARE RD	GEARY ST	HEARST ST	2	R	1,200	37	44,400	S	4/30/2018	74
VINECT	010	VINE HILL CT	END S	PEPPERTRE RD	2	R	139	53	8,782	A	4/30/2018	74
BSTREE	010	B (WILLIAM) ST	SIXTH ST	FIFTH ST	2	R	375	32	12,000	A	4/30/2018	73
BROSRD	010	BROTHER ISL RD	TREASURE RD	GOLDEN GATE DR	2	R	505	33	16,665	A	4/30/2018	73
DSTREE	010	D ST	W CDS	FOURTH ST	2	R	318	34	11,426	A	4/30/2018	73
FAIRDR	010	FAIRWAY DR	FREMONT BL	CUMMINS WY	2	R	1,299	32	41,568	A	4/30/2018	73
FOURST	010	FOURTH ST	N END	B ST	2	R	495	36	17,820	A	4/30/2018	73
HAMPST	010	HAMPTON ST	PRINCETON RD	END N	2	R	474	32	18,431	A	4/30/2018	73
HARBBL	080	HARBOR BL	DULUTH ST	WEST CAPI AV	5	A	2,165	70	151,550	A	4/30/2018	73
HILLCT	010	HILL CT	DREW ST	S CDS	2	R	190	32	8,641	A	4/30/2018	73
LEMORD	040	LEMON TREE RD	PEACHTREE ST	LIMEWOOD RD	2	R	890	36	32,040	A	4/30/2018	73
LINDRD	N150	LINDEN RD	JEFFERSON BD	CANVASBACK WY	2	A	1,207	27	40,993	S	4/30/2018	73
LINDRD	S090	LINDEN RD	COFFEEBERRY	CONSTITUTION AV	2	A	1,787	30	57,812	S	4/30/2018	73
MACKRD	010	MACKINAC RD	WHITE FIS RD	SAGINAW ST	2	R	270	33	8,910	A	4/30/2018	73
MARNWY	010	MARINA WY	N END	LIGHTHOUSE DR	2	R	405	31	12,555	A	4/30/2018	73
MEADCR	010	MEADOW LARK CR	PHESANT H DR	LAGOON LN	2	R	640	33	21,120	S	4/30/2018	73

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MIDDST	010	MIDWAY ISL ST	MOLOKAI RD	END N	2	R	520	33	17,160	A	4/30/2018	73
NAUTCT	010	NAUTICA CT	END W	BAYBERRY ST	2	R	559	32	21,151	A	4/30/2018	73
PEPPRD	010	PEPPERTREE RD	PEACHTREE ST	ALMOND ST	2	R	870	36	31,320	A	4/30/2018	73
SOUTRD	170	SOUTH RIVER RD	END	1800 SOUTH RIV RD	2	A	1,700	28	52,270	A	4/30/2018	73
STARLN	010	STARLING LN	END S	PHEASANT H DR	2	R	839	33	27,687	S	4/30/2018	73
TORTRD	010	TORTOLA RD	FIJI ISL ST	GOLDEN GATE DR	2	R	990	33	32,670	A	4/30/2018	73
TREARD	010	TREASURE ISLAND RD	TACOMA NA ST	END E	2	R	989	33	35,812	A	4/30/2018	73
WEBSST	020	WEBSTER ST	PARK BL	JEFFERSON BL	2	R	600	36	21,600	A	5/24/2018	73
YOLOST	040	YOLO ST	REED AV	S END	2	R	510	36	18,360	A	5/30/2018	73
ANNAST	030	ANNA ST	TODHUNTER AV	HOLLAND DR	2	C	1,110	35	38,850	A	4/30/2018	72
BODBRD	010	BODEGA BAY RD	DRAKES BA ST	FARALLON RD	2	R	655	33	28,672	A	4/30/2018	72
CHAPWY	010	CHAPARRAL WY	MANZANITA WY	MICHIGAN BL	2	R	1,020	32	32,640	A	4/30/2018	72
COLSARD	010	COLUSA RD	APACHE ST	STONEGATE DR	2	R	415	32	13,280	A	4/30/2018	72
CRYSCT	010	CRYSTAL CT	LAGOON LN	END	2	R	228	32	9,398	S	4/30/2018	72
DOMAST	010	DOMINICA ST	N CDS	BONAIRE RD	2	R	469	33	18,655	A	4/30/2018	72
FIF5ST	210	FIFTH ST	PAVEMENT CHANGE	TOWER CT	2	A	275	41	11,275	A	4/30/2018	72
FOTNDR	010	FOUNTAIN DR	LIGHTHOUSE DR	ROUNDABOUT	2	R	1,532	70	107,240	A	4/30/2018	72
GREERD	010	GREENBRIER RD	HAMPTON ST	BLUE ROCK ST	2	R	1,310	32	41,920	A	4/30/2018	72
GREGAV	020	GREGORY AV	1300' N/ SO. RIVER	BEVAN RD	2	A	1,525	34	51,850	A	4/30/2018	72
HAMOCR	020	HALF MOON BAY CR	TACOMA NARROWS ST	GOLDEN GATE DR	2	R	1,160	36	41,760	A	4/30/2018	72
HAWACT	010	HAWAII CT	BIG ISLAND RD	S END	2	R	290	33	9,570	A	4/30/2018	72
HEARST	010	HEARST ST	UNION SQ RD	RANDOLPH RD	2	R	770	36	27,720	S	4/30/2018	72
KAUIRD	010	KAUAI RD	HALF MOON CR	S CDS	2	R	1,084	33	38,950	A	4/30/2018	72
LINDRD	N210	LINDEN RD	BRIDGE ST	JEFFERSON BD	2	A	1,203	30	40,292	S	4/30/2018	72
MEADCR	020	MEADOW LARK CR	LAGOON LN	STARLING LN	2	R	1,528	33	50,424	S	4/30/2018	72
PIERST	010	PIERCE ST	HOLLAND DR	KEGLE DR	2	R	1,135	36	40,860	S	4/30/2018	72
REVEDR	010	RIVERPOINT DR	KOVR DR	REED AV	4	R	650	60	39,000	A	4/30/2018	72
SANNST	010	SAN NICOLAS ST	SAN MIGUE ST	SANTA BAR ST	2	R	600	33	19,800	A	4/30/2018	72
SVNTST	020	SEVENTH ST	F ST	G ST	2	R	320	32	10,240	A	4/30/2018	72
SOUTPW	090	SOUTHPORT PW	OAKLAND BAY DR	ABACO RD	2	A	950	55	52,250	A	4/30/2018	72
TAHOST	010	TAHOE ST	END S	SQUAW RD	2	R	214	32	10,111	A	4/30/2018	72
VENIST	010	VENICE ST	END	SHERMAN ISLAND	2	R	734	33	27,397	A	4/30/2018	72
ALAMPL	010	ALAMO PL	END W	END E	2	R	418	32	19,902	A	4/30/2018	71
ALDEWY	010	ALDER WY	REDWOOD AV	LINDEN RD	2	R	1,530	15	22,950	S	4/30/2018	71
ALEUST	010	ALEUTIAN ISL ST	BALLENA B RD	PILOT POI RD	2	R	600	33	19,800	A	4/30/2018	71
ALMOST	010	ALMOND ST	LIMEWOOD WY	PEPPERTRE RD	2	R	590	36	21,240	A	4/30/2018	71
ANCPCT	010	ANACAPA ISLAND CT	N CDS	SANTA CRUZ RD	2	R	219	33	10,405	A	4/30/2018	71
ANTGPL	010	ANTIGUA PL	W CDS	MARTINIQU ST	2	R	354	33	14,860	A	4/30/2018	71
CALIST	010	CALIFORNIA ST	EIGHTH ST	C (ELIZAB ST	2	R	750	32	24,000	A	4/30/2018	71
CATLRD	010	CATALINA ISL RD	KELLYS IS DR	GOLDEN GA DR	2	R	180	33	5,940	A	4/30/2018	71

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CUBARD	010	CUBA RD	ARUBA ST	CAYMAN IS ST	2	R	280	33	9,240	A	4/30/2018	71
FARNRD	010	FARALLON RD	BALLENA B RD	BODEGA BA RD	2	R	590	33	19,470	A	4/30/2018	71
GLIDAV	020	GLIDE AV	GLIDE CT	WEST CAPI AV	2	R	1,330	24	31,920	S	4/30/2018	71
GRIZRD	010	GRIZZLY BAY RD	GOLDEN GATE DR	E CDS	2	R	1,319	33	46,811	A	4/30/2018	71
HEADRD	010	HEADSLANE RD	GEARY ST	HEARST ST	2	R	1,040	36	37,440	S	4/30/2018	71
JAVACT	010	JAVA CT	N CDS	MANDURA ST	2	R	314	33	13,540	A	4/30/2018	71
JEFRBL	160	JEFFERSON BL	PERKINS RD	LINDEN RD	5	A	2,046	76	155,496	A	4/30/2018	71
LAKEBL	020	LAKE WASHIGNTON BL	REDWOOD AV	HIGHLAND ST	2	A	1,424	38	59,533	A	4/30/2018	71
LAKEBL	070	LAKE WASHIGNTON BL	JEFFERSON BL	SOUTHPORT PW	6	A	1,745	82	143,090	A	4/30/2018	71
LAWODR	010	LAKEWOOD DR	DEERWOOD ST	PARK BL	2	R	1,110	31	34,410	A	4/30/2018	71
LINDRD	S140	LINDEN RD	CANVASBACK WY	JEFFERSON BD	2	A	1,207	35	46,447	S	4/30/2018	71
OXFOST	010	OXFORD ST	WASHINGTON AV	POPLAR ST	2	R	558	32	19,960	A	4/30/2018	71
REDWAV	020	REDWOOD AV	LINDEN RD	LAKE WASHINGTON BL	2	R	1,045	32	33,440	A	4/30/2018	71
ROUNCT	010	ROUNDHILL CT	PRINCETON RD	END N	2	R	129	46	7,965	A	4/30/2018	71
SENDST	010	SECOND ST	N END	THIRD ST	2	R	1,230	32	39,360	A	4/30/2018	71
SENDST	020	SECOND ST	D ST	E ST	2	R	440	37	16,280	A	4/30/2018	71
STABDR	030	STABLE DR	1443' W/ MARSHALL	MARSHALL RD	2	R	1,459	32	46,688	A	4/30/2018	71
TAMARD	010	TAMARACK RD	REDWOOD AV	END. E	2	R	1,208	17	20,536	S	4/30/2018	71
WHITRD	010	WHITE FISH BAY RD	END	N BASS IS RD	2	R	374	33	15,517	A	4/30/2018	71
YUORPL	010	YUROK PL	TRINITY WY	END	2	R	200	36	7,200	A	4/30/2018	71
BCONST	010	BACON ISL ST	N CDS	GRIZZLY RD	2	R	454	33	18,160	A	4/30/2018	70
BAYBST	015	BAYBERRY ST	GREENBRIAR RD	CORNADO ST	2	R	360	32	11,520	A	4/30/2018	70
BOWLDR	010	BOWLEN DR	CHARLES ST	ANNA ST	2	R	553	32	17,696	A	4/30/2018	70
CNVKWY	030	CANVASBACK WY	LINDEN RD	S CDS	2	R	133	33	6,417	A	4/30/2018	70
CATLRD	040	CATALINA ISL RD	RYER ISL ST	E CDS	2	R	259	33	11,725	A	4/30/2018	70
CLIKCT	020	CLICKER CT	CANVASBACK WY	E CDS	2	R	218	32	9,080	A	4/30/2018	70
EMBADR	010	EMBARCADERO DR	RIVERSIDE PW	EMBARCADERO DR	2	R	1,090	45	49,050	A	4/30/2018	70
FIJIST	010	FIJI ISL ST	COOPER IS RD	OAKLAND BAY DR	2	R	1,150	33	37,950	A	4/30/2018	70
FRTHST	020	FOURTH ST	F ST	G ST	2	R	360	41	14,760	A	4/30/2018	70
FREMBL	020	FREMONT BL	KEGLE DR	HARDY DR	2	R	775	36	27,900	A	4/30/2018	70
GEARST	010	GEARY ST	END S	RANDOLPH RD	2	R	1,248	37	47,898	S	4/30/2018	70
HOPLST	010	HOPLAND ST	LASSIK ST	HOOPA ST	2	R	939	32	30,048	A	4/30/2018	70
JEFRBL	170	JEFFERSON BL	LINDEN RD	HARMON RD	4	A	635	74	46,990	A	4/30/2018	70
JULIDR	010	JULIAN DR	FREMONT BL	CUMMINS WY	2	R	1,200	32	38,400	A	4/30/2018	70
KLYSDR	010	KELLYS ISL DR	HALF MOON CR	CATALINA RD	2	R	680	33	22,440	A	4/30/2018	70
LASKST	010	LASSIK ST	HOOPA RD	MODOC CT	2	R	1,237	33	40,821	A	4/30/2018	70
LIMEWY	010	LIMEWOOD WY	END W	ALMOND ST	2	R	1,079	36	41,858	A	4/30/2018	70
LINDRD	S080	LINDEN RD	JEFFERSON BD	COFFEEBERRY	2	A	1,252	32	44,266	S	4/30/2018	70
LINDRD	S120	LINDEN RD	BETTY WY	BRENDA WY	3	A	1,198	34	40,732	S	4/30/2018	70
LINDRD	S130	LINDEN RD	BRENDA WY	CANVASBACK WY	3	A	1,044	34	35,496	S	4/30/2018	70

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MANCST	010	MANCHESTER ST	JEFFERSON BL	DEVON AV	2	R	1,260	31	39,060	S	4/30/2018	70
MEADRD	040	MEADOW RD	CLARENDON ST	WESTACRE RD	2	R	1,475	36	53,100	A	4/30/2018	70
MERRCT	010	MERRIT ISL CT	END	SHERMAN I RD	2	R	319	33	13,702	A	4/30/2018	70
MICHBL	010	MICHIGAN BL	PECAN ST	COP 211' E/O MAPLE ST	2	C	1,263	32	40,416	A	4/30/2018	70
PETERD	010	PETER ISL RD	FIJI ISL ST	GOLDEN GA DR	2	R	1,080	33	35,640	A	4/30/2018	70
PUERRD	010	PUERTO RICO RD	ARUBA ST	CAYMAN IS ST	2	R	285	33	9,405	A	4/30/2018	70
STONAV	010	STONEGATE AV	COLUSA RD	END	2	C	1,165	32	37,280	A	4/30/2018	70
SUMAST	010	SUMATRA ST	MANDURA ST	TORTOLA RD	2	R	640	33	21,120	A	4/30/2018	70
SUSACT	010	SUSAN CT	VIOLET DR	END	2	R	288	36	10,176	A	4/30/2018	70
ABCORD	010	ABACO RD	SAN SALVADOR ST	SOUTHPORT PKWY	2	R	695	33	22,935	A	4/30/2018	69
BERUCT	010	BERMUDA CT	N CDS	HAITI RD	2	R	189	33	9,415	A	4/30/2018	69
CATLRD	020	CATALINA ISL RD	GOLDEN GA DR	SANTA BARBARA ST	2	R	530	33	17,490	A	4/30/2018	69
COITPL	010	COIT PL	END	HEARST ST	2	R	419	33	17,002	S	4/30/2018	69
COPRRD	010	COOPER ISL RD	FIJI ISL ST	GOLDEN GA DR	2	R	1,170	33	38,610	A	4/30/2018	69
GREGAV	030	GREGORY AV	BEVAN RD	1500' N/ BEVAN RD	2	A	1,500	32	48,000	A	4/30/2018	69
HARPPL	010	HARBOR POINT PL	STILLWATE RD	E CDS	2	R	250	44	14,014	A	4/30/2018	69
JOANST	010	JOAN ST	KEGLE DR	HARDY DR	2	R	950	32	30,400	A	4/30/2018	69
KINECT	010	KING EDWARD CT	N CDS	KING EDWARD RD	2	R	209	33	10,075	A	4/30/2018	69
LAKEBL	050	LAKE WASHIGNTON BL	STONEGATE DR	STOPLIGHT AT SOUTHPORT TOV	2	A	690	73	50,370	A	4/30/2018	69
LIHODR	020	LIGHTHOUSE DR	SPLIT @ #808 LIGHTHOUSE DR	DOUGLAS ST	2	C	481	44	23,564	A	4/30/2018	69
MARECT	010	MARE ISL CT	BROTHER I RD	END	2	R	214	33	10,237	A	4/30/2018	69
MARSRD	030	MARSHALL RD	MARTINIQU ST	GUADELUPE ST	2	A	1,010	32	32,320	A	4/30/2018	69
NICHCT	010	NICHOLS CT	END	ALLAN AV	2	R	498	32	18,126	A	4/30/2018	69
PHEADR	020	PHESANT HOLLOW DR	JEFFERSON BL	STARLING LN	2	R	195	33	6,435	S	4/30/2018	69
PROMWY	020	PROMENADE WY	SOUTHPORT PW	RAMOS DR	2	C	900	52	47,700	S	4/30/2018	69
REDWAV	010	REDWOOD AV	END	LINDEN RD	2	R	1,844	18	33,192	S	4/30/2018	69
RICHST	010	RICH ST	ARTHUR DR	KEGLE DR	2	R	855	35	29,925	A	4/30/2018	69
RUMSST	010	RUMSEY ST	POMO CT	END N	2	R	1,049	32	36,938	A	4/30/2018	69
SACRAV	060	SACRAMENTO AV	COP E/O JEFFERSON BL	DOUGLAS ST	2	A	1,100	52	57,200	A	4/30/2018	69
SANDLN	010	SANDPIPER LN	PHESANT H DR	STARLING LN	2	R	510	33	16,830	S	4/30/2018	69
SHARST	010	SHARIAN ST	KEGLE DR	HARDY DR	2	R	920	32	29,440	A	4/30/2018	69
SHAVCT	010	SHAVER CT	END S	SQUAW RD	2	R	170	32	8,703	A	4/30/2018	69
SOUTPW	070	SOUTHPORT PW	PROMENADE WY	1900' W/O PROMENADE	4	A	1,900	55	104,500	A	4/30/2018	69
STARLN	020	STARLING LN	PHESANT H DR	END N	2	R	550	33	18,150	S	4/30/2018	69
TRINWY	020	TRINITY WY	MOJAVE DR	APACHE ST	2	R	1,132	32	36,224	S	4/30/2018	69
BALIPL	010	BALI PL	FIJI ISL ST	E CDS	2	R	199	33	9,745	A	4/30/2018	68
BALBRD	020	BALLENA BAY RD	TACOMA NA ST	E CDS	2	R	679	33	25,585	A	4/30/2018	68
BINDDR	010	BIG ISLAND RD	KAUAI RD	HALF MOON CR	2	R	695	33	22,935	A	4/30/2018	68
BROKCT	010	BROCK CT	CANVASBAC WY	E CDS	2	R	223	32	9,240	A	4/30/2018	68
DSTREE	030	D ST	THIRD ST	SECOND ST	2	R	380	34	12,920	A	4/30/2018	68

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DEVOAV	010	DEVON AV	MANCHESTE ST	KINSINGTO ST	2	R	740	32	23,680	S	4/30/2018	68
FILLPL	010	FILLMORE PL	END	GATEWAY DR	2	R	364	37	16,291	S	4/30/2018	68
GOGADR	020	GOLDEN GATE DR	PETER ISL DR	HALF MOON CIR	2	C	1,020	33	33,660	A	4/30/2018	68
GREGAV	010	GREGORY AV	SOUTH RIV RD	1300' N/ SO. RIVER	2	A	1,300	34	44,234	A	4/30/2018	68
JEFRBL	120	JEFFERSON BL	GATEWAY DR	LAKE WASHINGTON BL	4	A	1,820	62	121,240	A	4/30/2018	68
MANDST	010	MANDURA ST	FIJI ISL ST	SUMATRA	2	R	655	33	21,615	A	4/30/2018	68
MARSRD	010	MARSHALL RD	W END	SOUTHPORT PW	2	A	1,540	32	49,280	A	4/30/2018	68
MICHBL	030	MICHIGAN BL	WESTACRE RD	ROCKROSE RD	2	C	1,270	45	57,150	A	4/30/2018	68
PERRCT	010	PERRIS CT	END S	SQUAW RD	2	R	279	32	12,191	A	4/30/2018	68
RENECT	010	RENEE CT	END	BETTY WY	2	R	348	33	13,510	S	4/30/2018	68
RIVEPW	040	RIVERSIDE PW	EMBARCADERO DR	STILLWATER RD	2	C	890	45	40,050	A	4/30/2018	68
RUBIWY	020	RUBICON WY	MERCED WY	LINDEN RD	2	R	280	36	10,080	S	4/30/2018	68
SACRAV	070	SACRAMENTO AV	DOUGLAS ST	EIGHTH ST	2	A	1,105	38	41,990	A	4/30/2018	68
SANTST	010	SANTIAM ST	LINDEN RD	CHINOOK RD	2	R	215	33	7,095	A	4/30/2018	68
SIERRD	020	SIERRA RD	MOJAVE DR	RUMSEY ST	2	R	430	32	13,760	A	4/30/2018	68
SOUTPW	080	SOUTHPORT PW	1900' W/O PROMENADE	OAKLAND BAY DR	4	A	1,955	55	107,525	A	4/30/2018	68
STJORD	010	ST JOHN 10	ARUBA ST	MARTINIQUE ST	2	R	900	33	29,700	A	4/30/2018	68
THIRST	030	THIRTEENTH ST	VIRGINIA AV	JEFFERSON BVD	2	R	340	32	10,880	A	4/30/2018	68
WASHCT	010	WASHOE CT	RUMSEY ST	END E	2	R	215	32	10,250	A	4/30/2018	68
WIGEST	010	WIGEON ST	MALLARD RD	MUSCOVY RD	2	R	791	25	19,775	A	4/30/2018	68
AIROCT	020	AIRONS CT	CANVASBACK WY	E CDS	2	R	223	36	9,828	A	4/30/2018	67
BALBRD	010	BALLENA BAY RD	ALEUTIAN ST	TACOMA NA ST	2	R	290	33	9,570	A	4/30/2018	67
BRIADR	010	BRIARWOOD DR	EVERGREEN AV	OAKMONT WY	2	R	350	36	12,600	A	5/24/2018	67
BRWSCT	010	BROWNS ISL CT	SUISUN BAY RD	S CDS	2	R	294	33	12,880	A	4/30/2018	67
BRYTAV	010	BRYTE AV	SACRAMENTO AVE	MAY ST	2	C	1,570	32	50,240	S	4/30/2018	67
CLRDST	020	CLARENDON ST	GRANDE VI AV	NORFOLK AV	2	R	1,300	34	44,200	S	4/30/2018	67
COSTCT	010	COSTA CT	CANVASBAC WY	E CDS	2	R	193	32	8,280	A	4/30/2018	67
DSTREE	020	D ST	FOURTH ST	THIRD ST	2	R	385	49	18,865	A	4/30/2018	67
RISKLN	020	DELTA LN	DELTA LN	TOWER BRIDGE GATEWAY	2	A	643	44	28,292	A	4/30/2018	67
INDPAV	020	INDEPENDENCE AV	DRIFTWOOD CT	SAFFLOWER PL	2	R	480	42	20,160	S	4/30/2018	67
JEFRBL	010	JEFFERSON BL	SACRAMENTO AV	TRIANGLE CT	4	A	985	81	79,785	A	4/30/2018	67
JEFRBL	150	JEFFERSON BL	HIGGINS RD	PERKINS RD	5	A	1,265	76	100,940	A	4/30/2018	67
KATYCR	010	KATHY CR	NANCY LN	BRENDA WY	2	R	875	36	31,500	A	4/30/2018	67
LANICT	010	LANAI CT	BIG ISLAND RD	S CDS	2	R	354	33	14,860	A	4/30/2018	67
LIHODR	050	LIGHTHOUSE DR	MARINA WY	A ST	4	C	1,510	60	90,600	A	4/30/2018	67
LINDRD	N170	LINDEN RD	BRENDA WY	HIGGINS RD	2	A	1,121	34	43,437	S	4/30/2018	67
NORFAV	010	NORFOLK AV	CLARENDON ST	FERNWOOD ST	2	R	1,265	32	40,480	A	4/30/2018	67
PERECT	010	PEREZ CT	END	ALLAN AV	2	R	318	32	12,366	A	4/30/2018	67
PITZCR	010	PITZER CR	ALLAN AV	ALLEN AV	2	R	1,680	36	60,480	S	4/30/2018	67
POPLST	010	POPLAR ST	MANZANITA WY	MICHIGAN BL	2	C	1,225	36	44,100	A	4/30/2018	67

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REEDAV	030	REED AV	I80 WB ONRAMP	I80	6	A	531	78	41,418	A	4/30/2018	67
RIVEPW	020	RIVERSIDE PW	EMBARCADERO DR	CDS @ #875	2	C	1,259	45	56,655	A	4/30/2018	67
RUMPL	010	RUMSEY PL	RUMSEY ST	END N	2	R	214	32	9,312	A	4/30/2018	67
THORD	010	THORP RD	JEFFERSON BL	UNNAMED RD	2	R	5,860	20	117,200	A	4/30/2018	67
APACST	010	APACHE ST	END S	TRINITY ST	2	R	1,064	32	34,048	A	4/30/2018	66
ARLNRD	020	ARLINGTON RD	CROWN AV	END N	2	R	600	18	10,800	S	4/30/2018	66
ASTRAV	010	ASTER AV	MANCHESTE ST	END E	2	R	965	32	30,880	S	4/30/2018	66
CEDRCT	010	CEDAR CT	N CDS	TAMARACK RD	2	R	275	15	5,748	A	4/30/2018	66
CRWNAV	010	CROWN AV	ARLINGTON RD	KINSTINGTON ST	2	R	850	32	27,200	S	4/30/2018	66
DRVRST	020	DREVER ST	JEFFERSON BL	SOULE ST	2	R	1,090	41	44,690	A	5/24/2018	66
GOGADR	010	GOLDEN GATE DR	OAKLAND B DR	PETER ISL DR	2	R	1,065	33	35,145	A	4/30/2018	66
GOGADR	040	GOLDEN GATE DR	PROMENADE WY	HALF MOON CR	2	C	320	44	14,080	A	4/30/2018	66
HARBBL	120	HARBOR BL	I80 EB ONRAMP	BEACON BL	7	A	655	99	64,845	A	4/30/2018	66
HOBSAV	040	HOBSON AV	TODHUNTER AV	FOURNESS DR	2	R	1,075	31	33,325	S	4/30/2018	66
JEFRBL	140	JEFFERSON BL	LINDEN RD	HIGGINS RD	4	A	1,600	62	104,720	A	4/30/2018	66
LAKEBL	030	LAKE WASHIGNTON BL	HIGHLAND ST	SILVERADO ST	2	A	665	38	28,270	A	4/30/2018	66
LESLLN	010	LESLIE LN	SHIRLEY ST	DIANE DR	2	R	530	36	19,080	A	4/30/2018	66
LIHODR	010	LIGHTHOUSE DR	KEGLE DR	SPLIT @ #808 LIGHTHOUSE DR	2	C	1,653	43	71,079	A	4/30/2018	66
MASSDR	010	MASSIE DR	END	OATES DR	2	R	623	53	34,942	S	4/30/2018	66
MIDDCT	010	MIDDLE BASS ILS CT	END	N BASS IS RD	2	R	284	33	9,372	A	4/30/2018	66
PARAWY	010	PARADISE WY	HIGGINS RD	LAGOON LN	2	R	560	36	20,160	S	4/30/2018	66
PEPPCT	010	PEPPERTREE CT	END S	PEPPERTRE RD	2	R	134	52	8,471	A	4/30/2018	66
RIVEPW	030	RIVERSIDE PW	CDS @ #875	EMBARCADERO DR	2	C	1,646	45	74,070	A	4/30/2018	66
RUBIWY	010	RUBICON WY	SHASTA WY	MERCED WY	2	R	1,075	32	34,400	S	4/30/2018	66
SHASTWY	020	SHASTA WY	RUBICON WY	MERCED WAY	2	R	560	32	17,920	S	4/30/2018	66
STONBL	020	STONE BL	CEBRIAN ST	PARK BL	4	C	390	59	23,010	A	4/30/2018	66
BAYBST	010	BAYBERRY ST	PRINCETON	GREENBRIER RD	2	R	260	32	8,320	A	4/30/2018	65
CNVKWY	020	CANVASBACK WY	AIRONS CT	LINDEN RD	2	R	1,355	33	48,780	A	4/30/2018	65
CUMMWY	030	CUMMINS WY	ELDER DR	SIXTH ST	2	C	2,090	36	75,240	A	4/30/2018	65
DBLOST	010	DIABLO ST	KINSINGTO ST	ESSEX AV	2	R	593	36	21,348	S	4/30/2018	65
ESSXAV	010	ESSEX AV	DIABLO ST	KINSINGTO ST	2	R	420	36	15,120	S	4/30/2018	65
GOGADR	050	GOLDEN GATE DR	HALF MOON CR	SHERMAN I RD	2	C	1,475	34	50,150	A	4/30/2018	65
LAKEBL	040	LAKE WASHIGNTON BL	SILVERADO ST	STONEGATE DR	2	A	610	51	31,110	A	4/30/2018	65
LAKEBL	060	LAKE WASHIGNTON BL	STOPLIGHT AT SOUTHPORT TC	SOUTHPORT PW	4	A	916	110	100,760	S	4/30/2018	65
LETTST	010	LETTERMAN ST	GEARY ST	UNION SQ RD	2	R	530	36	19,080	S	4/30/2018	65
LIHODR	040	LIGHTHOUSE DR	FOUNTAIN DR	MARINA WY	5	C	370	73	27,010	A	4/30/2018	65
LINDRD	N200	LINDEN RD	CONSTITUTION AV	BRIDGE ST	2	A	1,864	30	60,122	S	4/30/2018	65
MANCST	020	MANCHESTER ST	DEVON AV	ASTER AV	2	R	788	32	26,004	S	4/30/2018	65
MARSRD	070	MARSHALL RD	ALLAN AV	JEFFERSON BL	2	A	905	32	28,960	A	4/30/2018	65
MIKOST	010	MIKON ST	YOLO ST	E END	2	R	880	36	31,680	A	4/30/2018	65

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MILTST	010	MILTON ST	ARTHUR DR	BOWLEN DR	2	R	885	36	31,860	A	4/30/2018	65
MONTCT	010	MONTEREY CT	GREENBRIA RD	END N	2	R	204	32	9,686	A	4/30/2018	65
MORSCT	010	MORSE CT	W CDS	FOURNESS DR	2	R	415	32	15,841	A	4/30/2018	65
PEGLCT	010	PEGLER CT	END	ALLAN AV	2	R	108	48	6,142	A	4/30/2018	65
SXTHST	020	SIXTH ST	E ST	F ST	2	A	445	32	14,240	A	4/30/2018	65
TABEST	010	TABER ST	HOLLAND DR	KEGLE DR	2	R	1,210	36	43,560	S	4/30/2018	65
TERMST	005	TERMINAL ST	S END	INDUSTRIAL BL	2	C	466	58	27,028	A	4/30/2018	65
YOLOST	010	YOLO ST	RIVERBANK RD	LISBON AV	2	R	950	36	34,200	A	4/30/2018	65
BSTREE	020	B (WILLIAM) ST	FIFTH ST	E END	2	R	920	32	29,440	A	4/30/2018	64
CABLCT	010	CABLE CT	GATEWAY DR	END	2	R	99	44	6,563	S	4/30/2018	64
CNVKWY	010	CANVASBACK WY	HIGGINS RD	AIRONS CT	2	R	660	36	23,760	S	4/30/2018	64
ELDEDR	010	ELDER DR	FREMONT BL	CUMMINS WY	2	R	1,185	32	37,920	A	4/30/2018	64
MICHBL	020	MICHIGAN BL	COP 211' E/O MAPLE ST	WESTACRE RD	2	C	1,890	45	85,050	A	4/30/2018	64
MODACT	010	MODOC CT	LASSIK ST	END E	2	R	198	32	8,352	A	4/30/2018	64
POMOCT	010	POMO CT	RUMSEY ST	END E	2	R	186	32	8,054	A	4/30/2018	64
RAMODR	010	RAMOS DR	OATES DR	PROMENADE WY	2	R	825	53	43,725	S	4/30/2018	64
SANSST	010	SANSOME ST	SAUSALITO RD	GATEWAY DR	2	R	605	36	21,780	S	4/30/2018	64
TRINWY	010	TRINITY WY	E END	MOJAVE DR	2	R	1,025	32	33,706	S	4/30/2018	64
ALANAV	050	ALLAN AV	DAVIS RD	MARSHALL RD	2	R	1,260	24	30,240	A	4/30/2018	63
BEACBL	010	BEACON BL	HARBOR BL	HALYARD DR	4	R	375	56	21,000	A	5/30/2018	63
BETTWY	020	BETTY WY	CLAUDIA CT	LINDEN RD	2	R	1,435	36	51,660	A	4/30/2018	63
BUTLCT	010	BUTLER CT	W CDS	CANVASBACK WY	2	R	238	32	9,720	A	4/30/2018	63
CHERRD	010	CHEROKEE RD	APACHE ST	STONEGATE DR	2	R	415	32	13,280	A	4/30/2018	63
COLSARD	020	COLUSA RD	STONEGATE DR	END E	2	R	618	32	19,776	A	4/30/2018	63
DOUGST	020	DOUGLAS ST	FREMONT BL	CUMMINS WY	2	C	1,220	34	41,480	A	4/30/2018	63
DRVRST	010	DREVER ST	SOULE ST	END N	2	R	900	41	36,900	A	4/30/2018	63
FIF5ST	190	FIFTH ST	15TH	I-80 ON RAMP	2	A	751	44	33,044	A	4/30/2018	63
GATEDR	020	GATEWAY DR	N BEACH AV	SAUSALITO RD	2	R	1,475	36	53,100	S	4/30/2018	63
GOGADR	030	GOLDEN GATE DR	HALF MOON CR	PROMENADE WY	2	C	375	44	16,500	A	4/30/2018	63
GREGAV	040	GREGORY AV	1500' N/ BEVAN	JEFFERSON BL	2	A	1,830	32	58,560	A	4/30/2018	63
INDUBL	060	INDUSTRIAL BL	3380 INDUSTRIAL BL	BEACON BL	4	A	1,204	56	67,424	A	4/30/2018	63
JEFRBL	180	JEFFERSON BL	HARMON RD	339' S/O HARMON RD	2	A	339	76	25,764	A	4/30/2018	63
KSGTST	010	KINSINGTON ST	ASTER AV	DEVON DR	2	R	830	36	29,880	S	4/30/2018	63
PECAST	010	PECAN ST	HOLLY ST	MICHIGAN BL	2	R	1,090	32	34,880	A	4/30/2018	63
REEDAV	010	REED AV	RIVERSIDE	STILLWATE RD	4	A	2,000	62	137,044	S	4/30/2018	63
SACRAV	020	SACRAMENTO AV	BRYTE AV	TODHUNTER AV	5	A	935	78	72,930	A	4/30/2018	63
SACRAV	030	SACRAMENTO AV	TODHUNTER AV	SIMON TER	5	A	930	78	72,540	A	4/30/2018	63
SOLAST	010	SOLANO ST	SACRAMENT AV	LISBON AV	2	R	1,540	36	55,440	S	4/30/2018	63
SOUTPW	100	SOUTHPORT PW	ABACO RD	MARSHALL RD	5	A	445	69	38,705	A	4/30/2018	63
STABDR	020	STABLE DR	877' W/ MARSHALL	1443' W/ MARSHALL	2	R	566	32	18,112	A	4/30/2018	63

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Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
STONBL	040	STONE BL	115' E/O ALABAMA CT	CAROLINA AV	2	C	723	41	29,643	A	4/30/2018	63
SULLCT	010	SULLIVAN CT	W CDS	FOURNESS DR	2	R	455	32	17,121	A	4/30/2018	63
SUNSAV	010	SUNSET AV	SACRAMENT AV	HARPER AV	2	R	1,750	32	56,000	A	4/30/2018	63
ARLNRD	010	ARLINGTON RD	END S	CROWN AV	2	R	1,610	18	28,980	S	4/30/2018	62
ARUBST	010	ARUBA ST	PUERTO RI RD	ST JOHN RD	2	R	1,065	33	35,145	A	4/30/2018	62
BALSST	010	BALSAM ST	EVERGREEN AV	BUCKEYE DR	2	R	405	32	12,960	A	4/30/2018	62
BONACT	010	BONITA CT	END	FERNWOOD ST	2	R	141	32	4,512	A	4/30/2018	62
CARMCT	010	CARMEL CT	MERCED WY	END A	2	R	290	32	11,382	A	4/30/2018	62
CHARST	010	CHARLES ST	ARTHUR DR	BOWLEN DR	2	R	800	32	25,600	A	4/30/2018	62
DNBYCT	010	DANBURY CT	FALLBROOK ST	END	2	R	210	32	9,184	A	4/30/2018	62
ESTREE	040	E ST	WIDTH CHANGE	SECOND ST	2	R	1,417	37	52,429	A	4/30/2018	62
LINDRD	050	LINDEN RD	IRONWOOD WY	STONEGATE DR	4	A	1,930	52	100,360	A	4/30/2018	62
MERKAV	010	MERKLEY AV	SYCAMORE AV	WESTACRE RD	2	C	1,830	32	58,560	A	4/30/2018	62
NANCLN	010	NANCY LN	VIOLET DR	BRENDA WY	2	R	450	36	16,200	S	4/30/2018	62
REEDAV	070	REED AV	HARBOR BL	SUNSET AV	4	A	950	77	73,150	S	4/30/2018	62
SXTHST	015	SIXTH ST	C ST	SACRAMENTO AV	2	R	365	38	13,870	A	4/30/2018	62
SPRUST	010	SPRUCE ST	END	LINDEN RD	2	R	925	38	35,150	S	4/30/2018	62
STIWRD	030	STILLWATER RD	1500 S/O HARBOR POINT PL	RIVERSIDE PW	4	C	1,170	60	76,200	A	4/30/2018	62
TENTST	010	TENTH ST	MARYLAND AV	VIRGINIA AV	2	R	420	31	13,020	A	5/24/2018	62
THRDST	010	THIRD ST	B ST	C ST	2	C	490	46	22,540	A	4/30/2018	62
ALANAV	030	ALLAN AV	NICHOLS CT	LINDEN RD	2	R	1,020	31	31,620	A	4/30/2018	61
CYMNST	010	CAYMAN ISLAND ST	OAKLAND BAY DR	ST JOHN RD	2	R	1,190	33	39,270	A	4/30/2018	61
ENTEBL	010	ENTERPRISE BL	WEST CAPI AV	I80 EB ONRAMP	4	A	750	80	60,000	A	4/30/2018	61
FOURDR	010	FOURNESS DR	HOBSON AV	ANNA ST	2	R	1,330	32	42,560	A	4/30/2018	61
FRTHST	010	FOURTH ST	D ST	F ST	2	R	925	36	33,300	A	4/30/2018	61
HARBBL	100	HARBOR BL	EVERGREEN AV	180 BRIDGE	6	A	570	82	46,740	A	4/30/2018	61
INDUBL	070	INDUSTRIAL BL	BEACON BL	HARBOR BL	4	A	1,775	56	99,400	A	4/30/2018	61
LAKEBL	090	LAKE WASHIGNTON BL	SOUTHPORT PW	STONE BL	4	A	2,640	62	163,680	S	4/30/2018	61
LINDRD	060	LINDEN RD	MOJAVE DR	CLARKSBURG BRANCHLINE	4	A	800	44	35,200	S	4/30/2018	61
LOPZRD	010	LOPEZ ISLAND RD	MALCOLM I ST	HORNBY IS ST	2	R	485	33	16,005	A	4/30/2018	61
MERKAV	020	MERKLEY AV	WESTACRE RD	JEFFERSON BL	3	C	1,300	60	78,000	A	4/30/2018	61
ROCKRD	020	ROCKROSE RD	WESTACRE RD	MICHIGAN BL	2	R	1,270	35	44,450	S	4/30/2018	61
ROVECT	020	ROVEN CT	SUMMERFIE DR	END	2	R	158	33	7,240	A	4/30/2018	61
SEYMRCT	010	SEYMOUR CT	STABLE DR	END N	2	R	195	32	6,240	A	4/30/2018	61
SHIRST	010	SHIRLEY ST	LESLIE LN	BETTY WY	2	R	470	36	16,920	S	4/30/2018	61
SOMEDR	010	SOMERSET DR	TODHUNTER AV	TODHUNTER AV	2	R	1,318	36	47,448	A	4/30/2018	61
THIRST	010	THIRTEENTH ST	DELAWARE AV	ALABAMA AV	2	R	590	32	18,880	A	5/24/2018	61
WAFRPL	010	WATERFRONT PL	THIRD ST	WATERFRONT PL	3	C	395	42	16,590	A	4/30/2018	61
ALANAV	040	ALLAN AV	NICHOLS CT	END S	2	R	850	34	29,255	A	4/30/2018	60
ANNAST	040	ANNA ST	HOLLAND DR	KEGLE DR	2	C	1,065	35	37,275	A	4/30/2018	60

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BUWSAV	030	BURROWS AV	MAILBOX #3175	SOUTH RIVER RD	2	R	2,363	18	42,534	A	4/30/2018	60
CARNDR	020	CARLIN DR	SOUTHPORT PW	RAMCO ST	3	C	1,080	52	56,160	A	4/30/2018	60
EVRGAV	030	EVERGREEN AV	COKE ST	SYCAMORE AV	2	C	1,560	36	56,160	A	4/30/2018	60
FIFNST	020	FIFTEENTH ST	MARYLAND AV	VIRGINIA AV	2	A	1,345	34	45,730	S	4/30/2018	60
GOGADR	060	GOLDEN GATE DR	SHERMAN I RD	OAKLAND B DR	2	C	1,425	33	47,025	A	4/30/2018	60
JANTDR	010	JANET DR	JACQUELYN LN	BETTY WY	2	R	655	36	23,580	A	4/30/2018	60
MERCWY	010	MERCED WY	MOJAVE DR	RUBICON WY	2	R	995	36	35,820	S	4/30/2018	60
MOJADR	020	MOJAVE DR	439' S/ LINDEN RD	LINDEN RD	2	R	439	32	14,048	A	4/30/2018	60
MOLORD	010	MOLOKAI RD	MIDWAY IS ST	MAUI ST	2	R	560	33	18,480	A	4/30/2018	60
MONCT	010	MONTE VISTA CT	MONTE VISTA	END W	2	R	224	32	10,431	A	4/30/2018	60
PEKICT	010	PEKINS CT	END	TEAL DR	2	R	227	32	7,264	S	4/30/2018	60
SOLAST	020	SOLANO ST	LISBON AV	RIVERBANK RD	2	R	835	36	30,060	S	4/30/2018	60
YOLOST	030	YOLO ST	HOBSON AV	REED AV	2	R	350	36	12,600	A	4/30/2018	60
ANGLCT	010	ANGEL CT	W CDS	BETTY WY	2	R	358	33	13,842	A	4/30/2018	59
ARTHDR	020	ARTHUR DR	HOBSON AV	FREMONT BL	2	R	460	36	16,560	A	4/30/2018	59
ESTREE	010	E ST	WEST ST	RR TRACKS	2	R	1,417	38	53,846	A	4/30/2018	59
FIFHST	010	FIFTH ST	A ST	C ST	4	A	980	60	64,800	A	4/30/2018	59
FISHAV	010	FISHER AV	ARMFIELD AV	GOODELL AV	2	R	1,645	24	39,480	A	4/30/2018	59
HARBBL	010	HARBOR BL	CITY LIMIT	I80	2	A	2,890	32	92,480	A	4/30/2018	59
INDUBL	100	INDUSTRIAL BL	TERMINAL ST	STONE BL	5	A	1,095	76	83,220	A	4/30/2018	59
MERKAV	030	MERKLEY AV	JEFFERSON BL	EL RANCHO CT	2	C	980	40	39,200	A	4/30/2018	59
NINEST	010	NINETEENTH ST	DEERWOOD ST	PARK BL	2	R	570	31	17,670	A	4/30/2018	59
PROCAV	010	PROCTOR AV	SYCAMORE ST	ASH AV	2	R	1,315	32	42,080	A	4/30/2018	59
SACRAV	010	SACRAMENTO AV	SUNSET AV	BRYTE AV	5	A	1,565	78	122,070	A	4/30/2018	59
SACRAV	040	SACRAMENTO AV	SIMON TER	JEFFERSON BL	5	A	995	78	77,610	A	4/30/2018	59
SAUSRD	010	SAUSALITO RD	SANSOME ST	GATEWAY DR	2	R	955	37	35,335	S	4/30/2018	59
WAFRPL	020	WATERFRONT PL	WATERFRONT PL	S CDS	2	R	308	37	16,299	A	4/30/2018	59
BNDLCT	010	BANDALIN CT	MARECA WY	S CDS	2	R	288	33	11,532	S	4/30/2018	58
FRNWST	005	FERNWOOD ST	N END	MEADOW RD	2	R	650	32	20,800	A	4/30/2018	58
FREMBL	010	FREMONT BL	ARTHUR DR	KEGLE DR	2	R	650	36	23,400	A	4/30/2018	58
HARBBL	030	HARBOR BL	RIVERBANK RD	SUNSET AV	2	A	780	34	26,520	A	4/30/2018	58
HVHR	020	HAVERHILL ST	MEADOW RD	NORFOLK AV	2	R	1,088	32	34,816	A	4/30/2018	58
INDPAV	010	INDEPENDENCE AV	DUET DR	DRIFTWOOD CT	2	R	785	36	28,260	S	4/30/2018	58
INDUBL	050	INDUSTRIAL BL	3500 INDUSTRIAL BL	3380 INDUSTRIAL BL	4	A	1,201	56	67,256	A	4/30/2018	58
JEFRBL	100	JEFFERSON BL	STONE BL	SOUTH RIVER RD	4	A	1,320	64	84,480	A	4/30/2018	58
LINDRD	N190	LINDEN RD	SUMMERFIELD DR	CONSTITUTION AV	2	A	935	30	30,151	S	4/30/2018	58
MCKIWY	010	MCKINLEY WY	N CDS	OXFORD ST	2	R	231	32	10,049	A	4/30/2018	58
MORGST	010	MORNING GLORY ST	PALOMAR AV	DORAN AV	2	R	800	32	25,600	A	4/30/2018	58
OAKLDR	020	OAKLAND BAY DR	SOUTHPORT PW	MARTINIQUE ST	2	C	1,240	33	40,920	A	4/30/2018	58
SHOVCT	010	SHOVELER CT	CANVASBAC WY	END	2	R	228	36	9,486	A	4/30/2018	58

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TEALDR	010	TEAL DR	HIGGINS RD	SUMMERFIELD DR	2	R	1,340	36	48,240	S	4/30/2018	58
ANNAST	050	ANNA ST	KEGLE DR	HARDY DR	2	C	1,000	35	35,000	A	4/30/2018	57
BREAWY	010	BRENDA WY	NANCY LN	LINDEN RD	2	R	630	36	22,680	A	4/30/2018	57
FREBDR	010	FREEBOARD DR	END	STARBOARD DR	2	R	440	28	15,104	A	4/30/2018	57
HARDDR	010	HARDY DR	END	CUMMINS WY	2	R	1,335	36	48,060	A	4/30/2018	57
JAQNLN	010	JACQUELYN LN	SUMMERFIELD DR	JANET DR	2	R	275	36	9,900	A	4/30/2018	57
JEFRBL	130	JEFFERSON BL	LAKE WASHINGTON BL	LINDEN RD	6	A	1,130	100	113,000	A	4/30/2018	57
KODKPL	010	KODIAK ISL PL	ALEUTIAN ST	E CDS	2	R	244	33	11,230	A	4/30/2018	57
LAKEDR	020	LAKE RD	ENTERPRIS BL	END E	2	R	1,590	28	44,520	A	4/30/2018	57
MULVPL	010	MULVANY PL	RAMCO ST	END	2	R	518	40	23,279	S	4/30/2018	57
PORTST	010	PORT ST	SHORE ST	TERMINAL ST	2	R	1,370	46	63,020	A	4/30/2018	57
RAMCST	010	RAMCO ST	END	CARLIN DR	3	C	2,205	52	114,660	S	4/30/2018	57
TERELN	010	TERESA LN	DIANE DR	SHIRLEY ST	2	R	510	36	18,360	S	4/30/2018	57
ARMFAV	010	ARMFIELD AV	FISHER AV	JEFFERSON BL	2	R	700	24	16,800	A	4/30/2018	56
CSTREE	040	C ST	THIRD ST	SECOND ST	2	A	410	43	17,630	A	4/30/2018	56
ELLIST	010	ELLIOT ST	ARTHUR DR	KEGLE DR	2	R	660	35	23,100	A	4/30/2018	56
FRNTST	010	FRONT ST	SHORE ST	TERMINAL ST	2	R	1,295	31	40,145	A	4/30/2018	56
INDUBL	030	INDUSTRIAL BL	MAIN USPS SIGN	PARKWAY BL	4	A	1,250	76	95,000	A	4/30/2018	56
LAURLN	020	LAUREL LN	WESTACRE RD	ROCKROSE RD	2	R	1,050	32	33,600	A	4/30/2018	56
LINDRD	040	LINDEN RD	REDWOOD WY	IRONWOOD WY	4	A	695	42	29,190	A	4/30/2018	56
LINDRD	S110	LINDEN RD	SUMMERFIELD DR	BETTY WY	3	A	1,811	34	63,675	S	4/30/2018	56
LISBAV	020	LISBON AV	WATER ST	BRYTE SV	2	R	1,070	36	38,520	S	4/30/2018	56
POWEPL	010	POWELL PL	SANSOME ST	END	2	R	269	32	11,871	S	4/30/2018	56
THOMPL	010	THOMAS PL	RAMCO ST	END	2	R	533	40	20,764	S	4/30/2018	56
WINDCR	010	WINDWARD CR	END	PORT ST	2	R	283	40	12,814	A	5/30/2018	56
BETTWY	010	BETTY WY	SUMMERFIELD DR	CLAUDIA CT	2	R	960	36	34,560	A	4/30/2018	55
CHANDR	040	CHANNEL DR	3771 CHANNAL DR	SEAPORT BL	2	R	1,140	46	52,440	A	4/30/2018	55
FATH	010	FATHOM PL	GEARY ST	END	2	R	309	35	13,372	S	4/30/2018	55
GREMAV	010	GREENMEADOW AV	GARNET ST	PINE ST	2	R	725	32	23,200	A	4/30/2018	55
INDPAV	030	INDEPENDENCE AV	SAFFLOWER PL	LINDEN RD	2	R	340	60	20,400	S	4/30/2018	55
INDUBL	010	INDUSTRIAL BL	ENTERPRIS BL	OVERLAND CT	4	A	1,045	76	79,420	A	4/30/2018	55
IRONWY	010	IRON WOOD WY	SPRUCE ST	LINDEN RD	2	R	1,120	36	40,320	S	4/30/2018	55
LINDRD	N180	LINDEN RD	HIGGINS RD	SUMMERFIELD DR	2	A	1,811	30	63,675	S	4/30/2018	55
CRRGCT	010	CARRIGAN CT	DEERWOOD ST	END	2	R	130	31	6,669	A	4/30/2018	54
FSTREE	020	F ST	WEST ST	RR TRACKS	2	C	1,420	40	56,800	A	4/30/2018	54
FRNWST	010	FERNWOOD ST	MEADOW RD	SONORA AV	2	R	255	32	8,160	A	4/30/2018	54
GRANCT	010	GRANT CT	N BEACH AV	END	2	R	244	31	10,915	A	4/30/2018	54
HARBBL	110	HARBOR BL	180 BRIDGE	I80 EB ONRAMP	8	A	364	108	39,312	A	4/30/2018	54
INDUBL	020	INDUSTRIAL BL	OVERLAND CT	MAIN USPS SIGN	4	A	1,585	76	120,460	A	4/30/2018	54
JEFRBL	060	JEFFERSON BL	CAPITOL MALL ONRAMP	PARK BL	6	A	400	80	32,000	A	4/30/2018	54

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PARWPL	010	PARKWAY PL	END	PARKWAY BL	2	C	403	46	21,354	A	4/30/2018	54
PRONWY	010	PROMENADE WY	GOLDEN GA DR	SOUTHPORT PW	4	C	1,260	50	63,000	A	4/30/2018	54
SEAPBL	010	SEAPORT BL	END	ENTERPRIS BL	2	C	1,073	60	68,356	A	4/30/2018	54
SEVEST	040	SEVENTEENTH ST	VERMONT AV	JEFFERSON BL	2	R	270	23	6,210	A	4/30/2018	54
SUMMDR	020	SUMMERFIELD DR	LINDEN RD	HIGGINS RD	2	R	1,491	36	53,676	A	4/30/2018	54
SYCAST	010	SYCAMORE ST	WEST CAPI AV	EVERGREEN AV	2	C	1,000	34	34,000	A	4/30/2018	54
DOUGST	010	DOUGLAS ST	SACRAMENT AV	FREMONT BLVD	2	C	640	36	23,040	A	4/30/2018	53
HGHSCR	010	HUGHES CR	END	PARK BL	2	R	450	21	9,450	S	4/30/2018	53
KEGLRD	030	KEGLE DR	SHARIAN ST	END	2	C	1,930	36	69,480	A	4/30/2018	53
REEDAV	050	REED AV	I80 EB ONRAMP	IKEA CT	4	A	650	80	52,000	A	4/30/2018	53
SOULST	010	SOULE ST	FIFTEENTH ST	JACKSON ST	2	R	850	30	25,500	A	4/30/2018	53
TRIACT	010	TRIANGLE CT	W CDS	JEFFERSON BL	2	R	1,508	40	63,718	A	4/30/2018	53
ANGLCT	020	ANGEL CT	BETTY WY	E CDS	2	R	303	33	12,027	A	4/30/2018	52
CHANDR	020	CHANNEL DR	RR TRACKS	ENTERPRIS BL	2	R	919	46	42,274	A	4/30/2018	52
CHANDR	030	CHANNEL DR	ENTERPRIS BL	3771 CHANNAL DR	2	R	1,864	46	85,744	A	4/30/2018	52
ENTEBL	040	ENTERPRISE BL	COP 490' S/O INDUSTRIA BL	SEAPORT BL	5	A	1,673	76	127,148	A	4/30/2018	52
ENTEBL	050	ENTERPRISE BL	SEAPORT BL	CHANNEL DR	2	A	1,975	44	86,900	A	4/30/2018	52
GRYLCT	010	GREYCALLS CT	END	TEAL DR	2	R	248	32	7,936	S	4/30/2018	52
JEFRBL	020	JEFFERSON BL	TRIANGLE CT	MICHIGAN BL	4	A	1,480	84	124,320	A	4/30/2018	52
MAREWY	010	MARECA WY	CANVASBAC WY	HIGGINS RD	2	R	1,750	36	63,000	S	4/30/2018	52
RAMCST	020	RAMCO ST	CARLIN DR	S END	2	C	1,585	52	82,420	S	4/30/2018	52
STIWRD	020	STILLWATER RD	HARBOR POINT PL	1500 S/O HARBOR POINT PL	4	C	1,500	60	99,000	A	4/30/2018	52
TERMST	010	TERMINAL ST	INDUSTRIA BL	BOATMAN AV	2	C	580	61	35,380	A	4/30/2018	52
THIRST	020	THIRTEENTH ST	ALABANA AV	VIRGINIA AV	2	R	500	32	16,000	A	4/30/2018	52
ACRNCT	010	ACORN CT	END	PARK BL	2	R	410	31	15,349	A	4/30/2018	51
BTMNAV	010	BOATMAN AV	INDUSTRIA BL	TERMINAL ST	2	R	1,100	40	44,000	A	4/30/2018	51
CLUDCT	010	CLAUDIA CT	W CDS	BETTY WY	2	R	373	33	14,337	A	4/30/2018	51
COMMDR	010	COMMERCE DR	W CDS	E CDS	2	R	1,451	46	77,210	A	4/30/2018	51
EVRGAV	040	EVERGREEN AV	SYCAMORE AV	POPLAR AV	2	C	880	31	27,280	A	4/30/2018	51
FRNWST	020	FERNWOOD ST	SONORA AV	FALLBROOK ST	2	R	965	32	30,880	A	4/30/2018	51
FIFNST	010	FIFTEENTH ST	PARK BL	MARYLAND AV	2	A	300	43	12,900	S	4/30/2018	51
HOBSAV	020	HOBSON AV	SOLANO ST	BRYTE AV	2	R	795	36	28,620	S	5/25/2018	51
MARYAV	040	MARYLAND AV	ELEVENTH ST	WEBSTER ST	2	R	731	35	25,585	A	4/30/2018	51
OLIVCT	010	OLIVE CT	SUMMERFIE DR	END	2	R	613	33	22,255	S	4/30/2018	51
SQUACT	010	SQUAW CT	END S	SQUAW CT	2	R	94	77	6,541	A	4/30/2018	51
SUMMDR	010	SUMMERFIELD DR	JACQUELYN LN	LINDEN RD	2	R	1,545	36	55,620	S	4/30/2018	51
CSTREE	020	C ST	SIXTH ST	FIFTH ST	4	A	380	49	19,820	A	4/30/2018	50
DRWD	010	DEERWOOD CR	DEERWOOD ST	END E	2	R	190	31	8,529	A	4/30/2018	50
FSTREE	030	F ST	RR TRACKS	FIFTH ST	2	C	1,280	44	56,320	A	4/30/2018	50
FISHAV	020	FISHER AV	GOODELL AV	END W	2	R	596	24	18,621	A	4/30/2018	50

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INDUBL	040	INDUSTRIAL BL	PARKWAY BL	3500 INDUSTRIAL BL	4	A	912	76	69,312	A	4/30/2018	50
LISBAV	030	LISBON AV	BRYTE AV	TODHUNTER AV	2	R	1,365	34	46,410	S	4/30/2018	50
WECAAV	070	WEST CAPITOL AV	I80	PINE ST	2	A	1,475	44	64,900	A	4/30/2018	50
ANNAST	020	ANNA ST	MYRTLE AV	TODHUNTER AV	2	C	695	36	25,020	A	4/30/2018	49
BEACBL	030	BEACON BL	INDUSTRIA BL	HALYARD DR	2	R	1,485	20	29,700	A	4/30/2018	49
DELAAV	030	DELAWARE AV	FIFTEENTH ST	ALABAMA AV	2	R	1,024	31	31,744	A	4/30/2018	49
GARNST	010	GARNET ST	WEST CAPI AV	DORAN AV	2	R	1,083	32	34,656	A	4/30/2018	49
HVHR	010	HAVERHILL ST	GRANDE VI AV	MEADOW RD	2	R	810	34	27,540	A	4/30/2018	49
HOLLRD	010	HOLLAND DR	ANNA ST	CARRIE ST	2	R	1,210	32	38,720	A	4/30/2018	49
JASMAV	010	JASMINE AV	LISBON AV	RIVERBANK RD	2	R	1,370	32	43,840	S	4/30/2018	49
KSGTST	020	KINSINGTON ST	DEVON DR	MANCHESTER ST	2	R	1,295	36	46,620	S	4/30/2018	49
SVNTST	010	SEVENTH ST	E ST	F ST	2	R	440	35	15,400	A	4/30/2018	49
SHORST	010	SHORE ST	DEL MONTE ST	FRONTST	2	R	1,710	46	78,660	A	4/30/2018	49
STONBL	010	STONE BL	INDUSTRIA BL	CEBRIAN ST	2	C	390	28	10,920	A	4/30/2018	49
THRDST	040	THIRD ST	E ST	G ST	2	C	845	46	38,870	A	4/30/2018	49
WEBSST	010	WEBSTER ST	ELEVENTH ST	PARK BL	2	R	1,090	30	32,900	A	5/30/2018	49
WEACRD	070	WESTACRE RD	410' S/O EVERGREEN (I50)	PARK BL	2	A	1,679	40	67,160	A	4/30/2018	49
ASHAVE	010	ASH AV	WILLOW AV	PROCTOR AV	2	R	280	32	8,960	A	5/24/2018	48
BLCKRD	010	BLACKER RD	ALLAN AV	JEFFERSON BL	2	R	710	22	15,620	S	4/30/2018	48
CUMMWY	010	CUMMINS WY	ARTHUR DR	INGLEWOOD DR	2	C	1,565	36	56,340	A	4/30/2018	48
FRNDCR	010	FERNDALE CR	END S	LAKEWOOD DR	2	R	130	31	6,669	A	4/30/2018	48
HIGGRD	040	HIGGINS RD	PARADISE WY	JEFFERSON BL	2	C	1,030	34	35,020	S	4/30/2018	48
JEFRBL	270	JEFFERSON BL	ARMFIELD AV	CITY LIMIT	2	A	1,540	24	36,960	A	4/30/2018	48
KENTSRD	010	KENTS CT	OLIVE CT	END	2	R	268	33	8,844	S	4/30/2018	48
LAKEDR	010	LAKE RD	END W	ENTERPRIS BL	2	R	603	44	29,229	A	4/30/2018	48
MADRAV	020	MADRONE AV	WESTACRE RD	ROCKROSE RD	2	R	1,030	32	32,960	A	4/30/2018	48
MAYSTR	010	MAY ST	BRYTE AV	MYRTLE AV	2	R	690	33	22,770	S	5/24/2018	48
OAKMWY	010	OAKMONT WY	W END	BRIARWOOD DR	2	R	510	36	18,360	A	4/30/2018	48
ROGEST	010	ROGERS ST	HOLLAND DR	KEGLE DR	2	R	1,370	36	49,320	A	4/30/2018	48
SHOST	010	SHORT ST	LISBON AV	RIVERBANK RD	2	R	760	32	24,320	A	5/24/2018	48
WILLAV	010	WILLOW AV	SYCAMORE ST	WESTACRE RD	2	R	1,845	32	59,040	A	4/30/2018	48
YOLOST	020	YOLO ST	LISBON AVE	HOBSON AV	2	R	1,380	36	49,680	A	4/30/2018	48
HIGGRD	030	HIGGINS RD	CONSTITUTION AV	PARADISE WY	2	C	728	32	23,296	S	4/30/2018	47
LINDRD	030	LINDEN RD	BASTONE CT	REDWOOD AV	4	A	980	30	29,400	S	4/30/2018	47
MARSRD	060	MARSHALL RD	SEYMORE AV	ALLAN AV	2	A	1,385	33	45,705	A	4/30/2018	47
OAKMWY	020	OAKMONT WY	BRIARWOOD DR	ORCHARD WY	2	R	145	32	4,640	A	4/30/2018	47
OATEDR	010	OATES DR	RAMOS DR	CARLIN DR	2	R	1,550	52	80,600	S	4/30/2018	47
ORCHWY	010	ORCHARD WY	OAKMONT WY	HICKORY WY	2	R	310	32	9,920	A	4/30/2018	47
RICEAV	010	RICE AV	HARBOR BL	RICE AV	2	C	1,474	26	38,324	S	4/30/2018	47
SOUTPW	050	SOUTHPORT PW	RAMCO ST	CARLIN DR	6	A	1,905	79	150,495	S	4/30/2018	47

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TRADCR	010	TRADEWINDS CR	END	PORT ST	2	R	313	41	14,251	A	5/30/2018	47
HEMLST	010	HEMLOCK ST	EVERGREEN AV	BUCKEYE DR	2	R	430	32	13,760	A	4/30/2018	46
HOBAVN	010	HOBSON (N) AV	HOBSON AV	RIVERBANK RD	2	R	2,120	32	67,840	A	4/30/2018	46
JEFRBL	190	JEFFERSON BL	339' S/O HARMON RD	MARSHALL RD	2	A	1,035	42	43,470	A	4/30/2018	46
LNGCST	020	LONGCROFT ST	MEADOW RD	END N	2	R	211	32	6,752	A	4/30/2018	46
MARIST	010	MARIGOLD ST	PALOMAR AV	DORAN AV	2	R	805	32	25,760	A	4/30/2018	46
PARKBL	010	PARK BL	JEFFERSON BL	ELEVENTH ST	3	C	1,395	75	104,625	A	4/30/2018	46
PARTAV	010	PARTRIDGE AV	BEVAN RD	TAPLEY RD	2	R	2,110	18	37,980	A	4/30/2018	46
SXTHST	030	SIXTH ST	F ST	G ST	2	A	340	32	10,880	A	4/30/2018	46
WECAAV	170	WEST CAPITOL AV	300' N/O GARDENT ST	FIFTH ST	2	A	1,145	46	52,670	A	4/30/2018	46
CARNDR	010	CARLIN DR	OATES DR	SOUTHPORT PW	3	C	1,880	52	97,760	A	4/30/2018	45
DORAAV	010	DORAN AV	MARIGOLD ST	GARNET ST	2	R	643	32	20,576	A	4/30/2018	45
GARYCT	010	GARY CT	SUMMERFIE DR	END E	2	R	123	50	6,150	S	4/30/2018	45
GOGADR	080	GOLDEN GATE DR	MARSHALL RD	OAKLAND BAY DR	2	C	708	33	23,364	A	4/30/2018	45
JEFRBL	070	JEFFERSON BL	PARK BL	FIFTEENTH ST	5	A	1,815	80	145,200	A	4/30/2018	45
KEGLRD	020	KEGLE DR	FREMONT BL	SHARIAN ST	2	C	1,440	36	51,840	A	4/30/2018	45
ROSECT	010	ROSE CT	END	ELEVENTH ST	2	R	141	29	6,793	A	4/30/2018	45
SACRAV	050	SACRAMENTO AV	JEFFERSON BL	COP E/O JEFFERSON BL	2	A	687	60	41,220	A	4/30/2018	45
SYCAST	020	SYCAMORE ST	EVERGREEN AV	BUCKEYE DR	2	C	478	32	17,400	A	4/30/2018	45
TERMST	040	TERMINAL ST	PORT ST	FRONT ST	2	C	820	46	37,720	A	4/30/2018	45
WESTCR	010	WESTWOOD CR	END	LAKEWOOD DR	2	R	180	31	8,219	A	4/30/2018	45
CLUDCT	020	CLAUDIA CT	BETTY WY	E CDS	2	R	388	33	14,832	A	4/30/2018	44
MARSRD	050	MARSHALL RD	GOLDEN GA DR	SEYMORE AV	2	A	1,000	32	32,000	A	4/30/2018	44
PALOAV	010	PALOMAR AV	W END	PINE ST	2	R	1,440	32	46,080	A	4/30/2018	44
REEDAV	020	REED AV	STILLWATER RD	I80 WB ONRAMP	5	A	335	65	24,175	A	4/30/2018	44
RIVERD	040	RIVERBANK RD	TODHUNTER AV	END	2	C	1,680	36	60,480	S	4/30/2018	44
RIVEPW	010	RIVERSIDE PW	REED AV	EMBARCADERO DR	2	C	1,030	45	46,350	A	4/30/2018	44
SOUTPW	060	SOUTHPORT PW	CARLIN DR	PROMENADE WY	4	A	2,215	55	121,825	S	4/30/2018	44
TERMST	020	TERMINAL ST	BOATMAN AV	DEL MONTE ST	2	C	1,235	60	74,100	A	4/30/2018	44
THRDST	020	THIRD ST	C ST	SECOND ST	3	C	350	46	16,100	A	4/30/2018	44
VIRGAV	050	VIRGINIA AV	FIFTEENTH ST	ELEVENTH ST	2	R	570	32	18,240	A	4/30/2018	44
BRYTAV	020	BRYTE AV	MAY ST	RIVERBANK RD	2	C	1,370	32	43,840	S	4/30/2018	43
CSTREE	030	C ST	FIFTH ST	THIRD ST	3	A	785	56	43,960	A	4/30/2018	43
DRFTCT	010	DRIFTWOOD CT	END	INDEPENDE AV	2	R	213	33	9,055	A	4/30/2018	43
ELVTST	030	ELEVENTH ST	PARK BL	JEFFERSON BL	2	R	1,375	34	46,750	A	4/30/2018	43
EVRGAV	020	EVERGREEN AV	HARBOR BL	COKE ST	2	C	1,830	41	75,030	A	4/30/2018	43
FIFHST	030	FIFTH ST	E ST	TOWER BRIDGE GATEWAY	4	A	1,528	61	93,208	A	4/30/2018	43
GATEDR	010	GATEWAY DR	JEFFERSON BL	N BEACH AV	2	C	1,190	32	38,080	A	4/30/2018	43
HICKWY	010	HICKORY WY	W END	ORCHARD WY	2	R	660	32	21,120	A	4/30/2018	43
MARSST	010	MARSTON ST	HOLLAND DR	KEGLE DR	2	R	1,295	36	46,620	A	4/30/2018	43

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SMILAV	010	SMILAX AV	LISBON AV	RIVERBANK RD	2	R	1,210	32	38,720	A	4/30/2018	43
WINDCT	010	WINDEMERE CT	W CDS	TODHUNTER AV	2	R	338	35	13,706	A	4/30/2018	43
CANNWY	010	CANNA WY	ELEVENTH ST	WEBSTER ST	2	R	560	32	17,920	A	4/30/2018	42
CUMMWY	020	CUMMINS WY	INGLEWOOD DR	ELDER DR	2	C	1,020	36	36,720	A	4/30/2018	42
EVRGAV	010	EVERGREEN AV	PINE ST	HARBOR BL	2	C	1,960	37	72,520	A	4/30/2018	42
FIRST	010	FIRST ST	W END	SUTTER AV	2	R	1,000	23	23,000	A	4/30/2018	42
HIGGRD	020	HIGGINS RD	211' E/ SUMMERFIELD DR	311' E/ CONSTITUTION	2	C	710	24	17,040	S	4/30/2018	42
LINDRD	070	LINDEN RD	CLARKSBURG BRANCHLINE	JEFFERSON BD	4	A	1,312	44	57,728	A	4/30/2018	42
MARSRD	040	MARSHALL RD	GUADELUPE ST	GOLDEN GA DR	2	A	1,060	32	33,920	A	4/30/2018	42
MOSSCR	010	MOSSWOOD CR	END	LAKEWOOD DR	2	R	180	31	8,124	A	4/30/2018	42
PARWBL	020	PARKWAY BL	SEAPORT BL	PARKWAY PL	2	C	1,565	46	71,990	A	4/30/2018	42
POPLAV	010	POPLAR AV	WEST CAPI AV	EVERGREEN AV	2	C	1,223	32	39,136	A	4/30/2018	42
REEDAV	040	REED AV	I80	I80 EB ONRAMP	4	A	380	66	25,080	A	4/30/2018	42
RISKLN	010	RISKE LN	BALLPARK DR	DELTA LN	2	A	642	31	19,902	A	4/30/2018	42
WATEST	010	WATER ST	END	RIVERBANK RD	2	R	2,025	32	64,800	A	4/30/2018	42
DOBRDR	010	DOBROS DR	BRYTE AV	DOBROS DR	2	R	370	53	19,610	A	5/30/2018	41
FIFHST	020	FIFTH ST	C ST	E ST	4	A	980	60	60,240	A	4/30/2018	41
HALYRD	010	HALYARD DR	INDUSTRIA BL	BEACON BL	2	R	645	35	22,575	A	4/30/2018	41
HAKYRD	020	HALYARD DR	BEACON BL	HARBOR BL	2	R	890	36	32,040	A	4/30/2018	41
MANDCT	010	MANDARIN CT	TEAL DR	END	2	R	228	33	9,550	A	4/30/2018	41
PARKBL	040	PARK BL	EIGHTEENT ST	STONE BL	2	C	1,675	68	113,900	A	4/30/2018	41
PARKCR	010	PARK CR	END	PARK BL	2	C	125	30	6,568	S	4/30/2018	41
POPLAV	020	POPLAR AV	EVERGREEN AV	BUCKEYE DR	2	C	345	32	11,040	A	5/24/2018	41
WECAAV	020	WEST CAPITOL AV	I80 EB OFFRAMP	# OF LANES CHANGE	4	A	1,150	72	82,800	A	4/30/2018	41
WECAAV	050	WEST CAPITOL AV	CHANGE OF WIDTH	NORTHPORT DR	5	A	540	78	42,120	A	4/30/2018	41
WEACRD	040	WESTACRE RD	WEST CAPI AV	MERKLEY AV	3	A	440	42	18,480	A	4/30/2018	41
CARRST	020	CARRIE ST	TODHUNTER AV	HOLLAND DR	2	C	1,115	36	40,140	A	4/30/2018	40
CARRST	030	CARRIE ST	HOLLAND DR	KEGLE DR	2	C	1,470	36	52,920	A	4/30/2018	40
CHANDR	010	CHANNEL DR	END W	RR TRACKS	2	R	1,046	46	50,528	A	4/30/2018	40
CLEECT	010	COLLEEN CT	TERESA LN	E CDS	2	R	243	36	10,548	A	4/30/2018	40
ELVTST	020	ELEVENTH ST	MEADOW RD	PARK BL	2	R	270	32	8,640	A	4/30/2018	40
HARBBL	090	HARBOR BL	WEST CAPI AV	EVERGREEN AV	5	A	880	76	66,880	A	4/30/2018	40
INDUBL	090	INDUSTRIAL BL	BOATMAN AV	TERMINAL ST	5	A	790	76	60,040	A	4/30/2018	40
MEADRD	030	MEADOW RD	ELEVENTH ST	WEBSTER ST	2	R	825	32	26,400	S	4/30/2018	40
SELLWY	010	SELLERS WY	W CDS	E END	2	R	655	40	28,027	A	4/30/2018	40
WECAAV	040	WEST CAPITOL AV	# OF LANES CHANGE @ MB #4	CHANGE OF WIDTH	4	A	1,347	62	83,514	A	4/30/2018	40
WECAAV	080	WEST CAPITOL AV	PINE ST	HOUSTON ST	3	A	1,305	66	86,130	A	4/30/2018	40
CARRST	010	CARRIE ST	BRYTE AV	TODHUNTER AV	2	C	1,400	33	46,200	A	4/30/2018	39
FIF5ST	200	FIFTH ST	I-80 ON RAMP	PAVEMENT CHANGE	2	A	385	51	19,635	A	4/30/2018	39
HARTAV	010	HART AV	ALLAN AV	HIGGINS RD	2	R	1,615	22	35,530	S	4/30/2018	39

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JEFRBL	030	JEFFERSON BL	MICHIGAN BL	WEST CAPI AV	5	A	815	76	61,940	A	4/30/2018	39
JEFRBL	090	JEFFERSON BL	VERMONT AV	STONE BL	5	A	1,685	84	141,540	A	4/30/2018	39
RALECT	010	RALEY CT	END	ENTERPRIS BL	2	R	413	40	19,515	A	4/30/2018	39
SAWBCT	010	SAWBILLS CT	END	TEAL DR	2	R	208	32	8,846	S	4/30/2018	39
SOULST	020	SOULE ST	JACKSON ST	DREVER ST	2	R	780	30	23,400	A	4/30/2018	39
STARDR	010	STARBOARD DR	BEACON BL	FREEBOARD DR	2	R	670	34	22,780	A	4/30/2018	39
WECAAV	030	WEST CAPITOL AV	# OF LANES CHANGE	# OF LANES CHANGE @ MB #42	3	A	1,046	58	60,668	A	4/30/2018	39
WECAAV	090	WEST CAPITOL AV	HOUSTON ST	HARBOR BL	4	A	720	78	56,160	A	4/30/2018	39
ALANAV	010	ALLAN AV	HIGGINS RD	HART AV	2	R	2,010	20	40,200	S	4/30/2018	38
ALBYCT	010	AYLESBURY CT	W CDS	MARECA WY	2	R	383	32	14,360	S	4/30/2018	38
DELMST	020	DEL MONTE ST	HARBOR BL	SHORE ST	2	C	810	30	24,300	A	4/30/2018	38
MARKST	020	MARKET ST	GATE	FIFTH	2	R	79	22	1,738	A	4/30/2018	38
PARKBL	030	PARK BL	FIFTEENTH ST	EIGHTEENTH ST	2	C	1,955	68	132,940	A	4/30/2018	38
PINEST	010	PINE ST	WEST CAPI AV	EVERGREEN AV	2	R	780	38	29,640	A	4/30/2018	38
ROVECT	010	ROVEN CT	END	SUMMERFIE DR	2	R	208	33	8,890	A	4/30/2018	38
SHARCT	010	SHARON CT	END	BETTY WY	2	R	518	33	19,120	S	4/30/2018	38
SOUTPW	040	SOUTHPORT PW	1250' E/O RAMCO	RAMCO ST	4	A	1,250	79	98,750	A	4/30/2018	38
TODHAV	010	TODHUNTER AV	SACRAMENTO AVE	MAY ST	2	C	2,103	36	75,708	S	4/30/2018	38
DOBRDR	020	DOBROS DR	N END	S END	2	R	295	53	15,635	A	4/30/2018	37
JCKNST	010	JACKSON ST	JEFFERSON BL	DREVER ST	2	R	355	46	16,331	A	5/24/2018	37
LINDRD	020	LINDEN RD	ASANTE LN	BASTONE CT	2	A	1,000	20	20,000	A	4/30/2018	37
NORFAV	020	NORFOLK AV	FERNWOOD ST	END	2	R	161	34	5,474	A	4/30/2018	37
PARKBL	020	PARK BL	ELEVENTH ST	FIFTEENTH ST	2	C	960	68	65,280	A	4/30/2018	37
SANMST	010	SAN MIGUEL ST	SANTA CRU RD	SAN NICOL ST	2	R	230	33	7,590	A	4/30/2018	37
TERMST	030	TERMINAL ST	DEL MONTE ST	PORT ST	2	C	1,030	46	47,380	S	4/30/2018	37
WECAAV	060	WEST CAPITOL AV	NORTHPORT DR	I80	4	A	605	72	43,560	A	4/30/2018	37
DELMST	030	DEL MONTE ST	SHORE ST	TERMINAL ST	2	C	1,400	60	84,000	A	4/30/2018	36
HIGGRD	010	HIGGINS RD	LINDEN RD	211' E/ SUMMERFIELD DR	2	C	1,120	36	40,320	S	4/30/2018	36
LISBAV	010	LISBON AV	SUNSET AV	WATER ST	2	R	1,000	36	36,000	S	4/30/2018	36
NORPDR	010	NORTHPORT DR	COMMERCE DR	WEST CAPI AV	2	R	1,450	46	66,700	A	4/30/2018	36
REEDAV	060	REED AV	IKEA CT	HARBOR BL	4	A	995	60	65,700	A	4/30/2018	36
SEAPBL	030	SEAPORT BL	CHANNEL DR	PARKWAY BL	2	C	2,245	46	103,270	A	4/30/2018	36
ECLDST	010	EUCLID ST	DELAWARE AV	ALABAMA AV	2	R	560	32	17,920	A	4/30/2018	35
JEFRBL	080	JEFFERSON BL	FIFTEENTH ST	VERMONT AV	5	A	1,610	84	135,240	A	4/30/2018	35
JEFRBL	200	JEFFERSON BL	MARSHALL RD	GREGORY AV	2	A	1,090	46	50,140	A	4/30/2018	35
MEADRD	020	MEADOW RD	WEBSTER ST	END	2	R	221	42	9,372	S	5/30/2018	35
OVERCT	010	OVERLAND CT	END	INDUSTRIA BL	2	R	978	41	61,208	A	4/30/2018	35
PARTAV	020	PARTRIDGE AV	TAPLEY RD	DAVIS RD	2	R	1,080	18	19,440	A	4/30/2018	35
PINTCT	010	PINTAIL CT	END	SUMMERFIE DR	2	R	158	33	7,240	S	4/30/2018	35
SEVEST	020	SEVENTEENTH ST	DELAWARE AV	ALABAMA AV	2	R	902	40	36,080	A	4/30/2018	35

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SOULST	030	SOULE ST	DREVER ST	END	2	R	420	24	12,511	A	4/30/2018	35
SUMMCT	010	SUMMERFIELD CT	HIGGINS RD	END	2	R	130	36	4,680	A	4/30/2018	35
THRDST	030	THIRD ST	SECOND ST	E ST	2	C	615	46	28,290	A	4/30/2018	35
WECAAV	010	WEST CAPITOL AV	ENTERPRISE BL	I80 EB OFFRAMP	4	A	560	66	39,360	A	4/30/2018	35
WSTREE	010	WEST ST	E ST	F ST	2	R	365	37	13,505	A	4/30/2018	35
CEBRST	020	CEBRIAN ST	2280 STONE (ON CEBRIAN)	STONE BL	2	C	950	53	50,350	A	4/30/2018	34
DELMST	010	DEL MONTE ST	SHORE ST	HARBOR BL	2	C	750	30	22,500	A	4/30/2018	34
DELMST	040	DEL MONTE ST	TERMINAL ST	RR CROSSI	2	C	375	42	15,750	A	4/30/2018	34
DENECT	010	DENISE CT	W CDS	BETTY WY	2	R	343	33	13,347	A	4/30/2018	34
RISKLN	030	GARDEN ST	TOWER BRIDGE GATEWAY	WEST CAPI AV	2	A	1,000	80	80,000	A	4/30/2018	34
HARBBL	130	HARBOR BL	BEACON BL	DEL MONTE ST	6	A	620	76	47,120	A	4/30/2018	34
HARBBL	140	HARBOR BL	DEL MONTE ST	INDUSTRIA BL	5	A	385	77	29,645	A	4/30/2018	34
KILYCT	010	KIMBERLY CT	BETTY WY	E CDS	2	R	333	33	13,017	A	4/30/2018	34
MEADRD	010	MEADOW RD	WESTACRE RD	ELEVENTH ST	2	R	710	31	22,010	A	4/30/2018	34
MYRTAV	010	MYRTLE AV	ANNA ST	RIVERBANK RD	2	R	1,940	32	62,080	A	4/30/2018	34
NBEAAV	020	N BEACH AV	STONEGATE DR	GATEWAY DR	2	C	735	33	24,255	S	4/30/2018	34
SEAPBL	020	SEAPORT BL	ENTERPRIS BL	CHANNEL DR	2	C	2,235	60	134,100	A	4/30/2018	34
VIRGAV	060	VIRGINIA AV	ELEVENTH ST	TENTH ST	2	R	285	32	9,120	A	4/30/2018	34
VIRGAV	070	VIRGINIA AV	TENTH ST	WEBSTER ST	2	R	425	32	13,600	A	4/30/2018	34
BUWSAV	020	BURROWS AV	COP	MAILBOX #3175	2	R	2,626	16	41,856	A	4/30/2018	33
EITHST	010	EIGHTEENTH ST	PARK BL	DELAWARE AV	2	R	560	32	17,920	A	4/30/2018	33
FREN	010	FRENCH AV	DAVIS RD	1500' N/O DAVIS	2	R	1,500	24	36,000	S	4/30/2018	33
JEFRBL	050	JEFFERSON BL	MERKLEY AV	CAPITOL MALL ONRAMP	4	A	792	65	51,480	A	4/30/2018	33
KEGLRD	010	KEGLE DR	SACRAMENT AV	FREMONT BL	2	C	590	45	26,550	S	4/30/2018	33
LINDRD	010	LINDEN RD	SOUTH RIVER RD.	ASANTE LN	2	A	1,000	32	32,000	A	4/30/2018	33
MERGCT	020	MERGANSERS CT	MARECA WY	END	2	R	218	33	9,307	S	4/30/2018	33
PENNAV	010	PENNSYLVANIA AV	STONE BL	EUCLID ST	2	R	845	32	27,040	A	4/30/2018	33
ABAMAV	070	ALABAMA AV	NINETEENT ST	REGENT ST	2	R	700	36	25,200	A	4/30/2018	32
ENTEBL	020	ENTERPRISE BL	I80 EB ONRAMP	LAKE RD	7	A	255	84	21,420	A	4/30/2018	32
FSTREE	010	F ST	JEFFERSON BL	WEST ST	2	C	1,255	42	53,830	A	4/30/2018	32
HARBBL	040	HARBOR BL	SUNSET AV	REED AV	2	A	990	26	25,740	A	4/30/2018	32
NINEST	030	NINETEENTH ST	PENNSYLVA AV	VERMONT AV	2	R	1,145	32	36,640	A	4/30/2018	32
PENNAV	030	PENNSYLVANIA AV	SEVENTEEN ST	ALAMEDA ST	4	R	850	32	27,200	A	4/30/2018	32
PENNAV	040	PENNSYLVANIA AV	ALAMEDA ST	FIFTEENTH ST	2	R	620	31	19,220	A	4/30/2018	32
SEVEST	030	SEVENTEENTH ST	ALABAMA AV	VERMONT AV	2	R	590	32	18,880	A	4/30/2018	32
SIXTST	010	SIXTEENTH ST	PARK BL	ALABAMA ST	2	R	1,125	31	34,872	A	4/30/2018	32
TEALDR	020	TEAL DR	SUMMERFIE DR	MANDRIN CT	2	R	220	33	7,260	A	4/30/2018	32
VIRGAV	020	VIRGINIA AV	EIGHTEENT ST	SEVENTEEN ST	2	R	560	32	17,920	A	4/30/2018	32
ASHAVE	020	ASH AV	PROCTOR AV	EVERGREEN AV	2	R	275	32	8,800	A	5/24/2018	31
BEACBL	020	BEACON BL	HALYARD DR	INDUSTRIAL BL	2	R	1,485	20	29,700	A	4/30/2018	31

FC (Functional Classification): A (Arterial), C (Collector), R (Residential/Local), O (Other)
Surface Type: A (AC), O (AC/AC), P (PCC), G (Gravel)

Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
BUCKDR	010	BUCKEYE DR	SYCAMORE ST	POPLAR AV	2	R	846	32	31,280	A	4/30/2018	31
DENECT	020	DENISE CT	BETTY WY	E CDS	2	R	318	33	12,522	A	4/30/2018	31
REGEST	010	REGENT ST	DELAWARE AV	ALABAMA AV	2	R	560	31	17,360	A	4/30/2018	31
RENECT	020	RENEE CT	BETTY WY	END	2	R	298	33	11,860	S	4/30/2018	31
SUMMDR	030	SUMMERFIELD DR	HIGGINS RD	END	2	R	178	32	7,798	A	4/30/2018	31
WEACRD	060	WESTACRE RD	WILLOW AV	410' S/O EVERGREEN (I50)	2	A	979	35	34,265	A	4/30/2018	31
ASTREE	020	A ST	FIFTH ST	E END	2	R	120	32	3,840	A	4/30/2018	30
BUWSAV	010	BURROWS AV	JEFFERSON BL	COP	2	R	494	17	8,398	A	4/30/2018	30
DAVIRD	010	DAVIS RD	SEYMORE AV	JEFFERSON BL	2	A	1,500	19	28,500	S	4/30/2018	30
DELMST	050	DEL MONTE ST	RR CROSSI	CEBRIAN ST	2	C	375	42	15,750	A	4/30/2018	30
JEFRBL	220	JEFFERSON BL	MAILBOX #3815	BEVAN RD	2	A	2,020	25	50,500	A	4/30/2018	30
JEFRBL	230	JEFFERSON BL	BEVAN RD	SOUTHPORT PW	2	A	1,740	24	41,760	A	4/30/2018	30
NINEST	020	NINETEENTH ST	PARK BL	PENNSYLVANIA AV	2	R	900	32	28,800	A	4/30/2018	30
PINTCT	020	PINTAIL CT	SUMMERFIE DR	END	2	R	228	33	9,550	A	4/30/2018	30
RISKLN	040	RISKE LN	SOUTH RIV RD	BALLPARK DR	2	A	389	40	15,560	A	4/30/2018	30
ANTIAV	010	ANTIOCH AV	BEVAN RD	TAPLEY RD	2	R	2,300	20	46,000	A	4/30/2018	29
JEFRBL	260	JEFFERSON BL	BURROWS AV	ARMFIELD AV	2	A	1,815	24	43,560	A	4/30/2018	29
SOUTPW	010	SOUTHPORT PW	LAKE WASH BL	1700' W/O Lake Wa	4	A	1,700	60	102,000	S	4/30/2018	29
ABAMAV	050	ALABAMA AV	THIRTEENT ST	ELEVENTH AV	2	R	480	34	16,320	A	4/30/2018	28
ALAEST	020	ALAMEDA ST	MARYLAND AV	PENNSYLVANIA AV	2	R	570	40	22,800	A	4/30/2018	28
CEBRST	010	CEBRIAN ST	DEL MONTE ST	2280 STONE (ON CEBRIAN)	2	C	1,367	54	73,818	A	4/30/2018	28
COKEST	010	COKE ST	EVERGREEN AV	SELLERS WY	2	R	485	40	19,400	A	5/24/2018	28
DELAAV	020	DELAWARE AV	SEVENTEEN ST	FIFTEENTH ST	2	R	1,553	31	48,143	A	4/30/2018	28
DULUST	010	DULUTH ST	GALVESTON ST	HOUSTON ST	2	R	875	33	28,875	A	4/30/2018	28
HOUSST	010	HOUSTON ST	DULUTH ST	CANAL ST	2	R	1,825	27	49,275	A	4/30/2018	28
JANTDR	020	JANET DR	BETTY WY	E CDS	2	R	498	33	18,462	A	4/30/2018	28
PARWBL	030	PARKWAY BL	PARKWAY PL	INDUSTRIA BL	2	C	730	46	33,580	A	4/30/2018	28
PENNAV	020	PENNSYLVANIA AV	REGENT ST	SEVENTEEN ST	2	R	415	32	13,280	A	4/30/2018	28
STIWRD	010	STILLWATER RD	REED AV	HARBOR POINT PL	4	C	380	60	22,800	A	4/30/2018	28
ANNAST	010	ANNA ST	BRYTE AV	MYRTLE AV	2	C	710	36	25,560	A	4/30/2018	27
CANAST	010	CANAL ST	GALVESTON ST	HOUSTON ST	2	R	445	36	16,020	A	4/30/2018	27
ELVTST	010	ELEVENTH ST	WEBSTER ST	MEADOW DR	2	R	805	20	15,500	A	4/30/2018	27
FIFNST	030	FIFTEENTH ST	VIRGINIA AV	JEFFERSON BL	2	A	310	40	12,400	A	4/30/2018	27
DOUGST	040	DOUGLAS ST	LIGHTHOUS DR	END	2	C	1,145	36	41,220	A	4/30/2018	26
FOURST	030	FOURTH ST	C ST	S CDS	2	R	323	37	12,930	A	4/30/2018	26
MARYAV	030	MARYLAND AV	SEVENTEEN ST	FIFTEENTH ST	2	R	1,430	31	44,330	A	4/30/2018	26
PARTAV	030	PARTRIDGE AV	DAVIS RD	HARMON RD	2	R	1,500	21	31,500	A	4/30/2018	26
SUTTST	020	SUTTER ST	FIRST ST	N END OF PAVEMENT	2	R	695	26	18,070	A	4/30/2018	26
ABAMAV	040	ALABAMA AV	FIFTEENTH ST	THIRTEENT ST	2	R	275	34	9,350	A	4/30/2018	25
ANTIAV	020	ANTIOCH AV	TAPLEY RD	HARMON RD	2	R	1,780	17	30,260	A	4/30/2018	25

FC (Functional Classification): A (Arterial), C (Collector), R (Residential/Local), O (Other)
Surface Type: A (AC), O (AC/AC), P (PCC), G (Gravel)

Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
CICLST	010	CIRCLE ST	ALABAMA ST	JEFFERSON BL	2	R	490	23	11,270	A	4/30/2018	25
DAVIRD	060	DAVIS RD	1800' E/O ANTIOCH	SOUTH RIV RD	4	A	1,895	19	36,005	S	4/30/2018	25
FREN	020	FRENCH AV	1500' N/O DAVIS	HARMON RD	2	R	1,500	24	36,000	A	4/30/2018	25
GALVST	010	GALVESTON ST	DULUTH ST	CANAL ST	2	R	1,505	36	54,180	A	4/30/2018	25
HOBSAV	010	HOBSON AV	WATER ST	SOLANO ST	2	R	395	36	14,220	A	5/23/2018	25
HOUSST	020	HOUSTON ST	CANAL ST	WEST CAPI AV	2	R	300	27	8,100	S	5/30/2018	25
JEFRBL	240	JEFFERSON BL	SOUTHPORT PW	CANAL S/O SOUTHPORT PW	2	A	1,578	25	39,450	A	4/30/2018	25
MARYAV	010	MARYLAND AV	STONE BL	NINETEENT ST	2	R	950	32	30,400	A	4/30/2018	25
ALAEST	030	ALAMEDA ST	PENNSYLVA AV	JEFFERSON BL	2	R	925	34	31,450	A	4/30/2018	24
HILAAV	010	HILARY AV	DAVIS RD	HARMON RD	2	R	1,125	17	19,125	A	4/30/2018	24
PEKICT	020	PEKINS CT	TEAL DR	END	2	R	83	63	4,975	S	4/30/2018	24
VIRGAV	040	VIRGINIA AV	ALAMEDA ST	FIFTEENTH ST	2	R	1,105	26	28,730	A	4/30/2018	24
ABAMAV	010	ALABAMA AV	ALAMEDA ST	REGENT ST	2	R	1,530	28	42,840	A	4/30/2018	23
CRLNAV	010	CAROLINA AV	STONE BL	NINETEENT ST	2	R	640	26	16,640	A	4/30/2018	23
JEFRBL	250	JEFFERSON BL	CANAL S/O SOUTHPORT PW	BURROWS AV	2	A	1,955	24	46,920	A	4/30/2018	23
WALNST	030	WALNUT ST	WEST CAPI AV	EVERGREEN AV	2	R	930	20	18,600	A	4/30/2018	23
WEACRD	050	WESTACRE RD	MERKLEY AV	WILLOW AV	2	A	275	40	11,000	A	4/30/2018	23
DAVIRD	030	DAVIS RD	GREGORY AV	PARTRIDGE AV	2	A	1,655	20	33,100	S	4/30/2018	22
DULUST	020	DULUTH ST	HOUSTON ST	HARBOR BL	2	R	760	36	27,360	A	4/30/2018	22
GDWLCT	010	GADWALL CT	END W	SUMMERFIE DR	2	R	238	33	9,880	A	4/30/2018	22
MARYAV	020	MARYLAND AV	NINETEENT ST	SEVENTEENTH ST	2	R	1,110	31	34,410	A	4/30/2018	22
SOUTPW	020	SOUTHPORT PW	1700' W/O Lake Wa	3400' W/O Lake Wa	4	A	1,700	50	85,000	S	4/30/2018	22
TAPLRD	010	TAPLEY RD	PARTRIDGE AV	ANTIOCH AV	2	R	1,155	18	20,790	A	4/30/2018	22
VERMAV	010	VERMONT AV	STONE BL	SEVENTEEN ST	2	R	1,050	26	27,300	A	4/30/2018	22
ALAEST	010	ALAMEDA ST	PARK BL	MARYLAND AV	2	R	280	40	11,200	A	4/30/2018	21
BEVARD	040	BEVAN RD	GREGORY AV	PARTRIDGE AV	2	R	1,935	14	27,090	A	4/30/2018	21
JEFRBL	210	JEFFERSON BL	GREGORY AV	MAILBOX #3815	2	A	2,089	25	52,225	A	4/30/2018	21
PARWBL	010	PARKWAY BL	END	SEAPORT BL	2	C	743	46	36,997	A	4/30/2018	21
VIRGAV	030	VIRGINIA AV	SEVENTEEN ST	ALAMEDA ST	2	R	1,225	26	31,850	A	4/30/2018	21
DAVIRD	020	DAVIS RD	JEFFERSON BL	GREGORY AV	2	A	115	25	2,875	A	4/30/2018	20
DAVIRD	050	DAVIS RD	ANTIOCH AV	1800' E/O ANTIOCH	2	A	1,800	19	34,200	S	4/30/2018	20
HARMRD	010	HARMON RD	JEFFERSON BL	PARTRIDGE AV	2	R	1,360	21	28,560	A	4/30/2018	20
NINEST	040	NINETEENTH ST	VERMONT AV	JEFFERSON BL	2	R	430	23	9,890	A	4/30/2018	20
SEVEST	010	SEVENTEENTH ST	PARK BL	DELAWARE AV	2	R	565	32	18,080	A	4/30/2018	20
VERMAV	020	VERMONT AV	SEVENTEEN ST	JEFFERSON BL	2	R	955	26	24,830	A	4/30/2018	20
BEVARD	030	BEVAN RD	JEFFERSON BL	GREGORY AV	2	R	2,430	13	31,590	S	4/30/2018	18
DAVIRD	040	DAVIS RD	PARTRIDGE AV	ANTIOCH AV	2	A	1,620	20	32,400	A	4/30/2018	18
SOUTPW	030	SOUTHPORT PW	3400' W/O Lake Wa	RAMCO ST	2	A	1,850	50	92,500	S	4/30/2018	18
EITHST	020	EIGHTEENTH ST	ALABAMA AV	VIRGINIA AV	2	R	270	32	8,640	A	4/30/2018	16
ABAMAV	020	ALABAMA AV	ALABAMA AV	ALABAMA AV	2	R	510	23	11,730	A	4/30/2018	15

FC (Functional Classification): A (Arterial), C (Collector), R (Residential/Local), O (Other)
Surface Type: A (AC), O (AC/AC), P (PCC), G (Gravel)

Street ID	Section ID	Street Name	Begin Location	End Location	Lanes	FC	Length (ft)	Width (ft)	Area (sf)	Surface Type	PCI Date	PCI
ABAMAV	060	ALABAMA AV	STONE BL	NINETEENT ST	2	R	350	36	12,600	A	4/30/2018	15
ALANAV	020	ALLAN AV	END	BLACKER RD	2	R	370	18	6,660	S	4/30/2018	15
DELAAV	010	DELAWARE AV	STONE BL	SEVENTEENTH ST	2	R	1,739	32	55,648	A	4/30/2018	15
EVRGAV	050	EVERGREEN AV	POPLAR AV	WESTACRE RD	2	C	940	31	29,140	A	4/30/2018	15
PERKRD	010	PERKINS RD	JEFFERSON BLVD	END	2	R	1,300	22	28,600	A	4/30/2018	15
ENTEBL	030	ENTERPRISE BL	LAKE RD	COP 490' S/O INDUSTRIA BL	4	A	1,260	60	75,600	A	4/30/2018	14
HARMRD	020	HARMON RD	PARTRIDGE AV	ANTIOCH AV	2	R	1,250	20	25,000	A	4/30/2018	14
GLADAV	010	GLADYS AV	END	SEYMORE AV	2	R	771	32	24,747	S	4/30/2018	13
ABAMAV	030	ALABAMA AV	ALABAMA AV	FIFTEENTH ST	2	R	142	30	4,260	A	4/30/2018	12
SEYMAV	020	SEYMORE AV	GLADYS AV	MARSHALL RD	2	R	1,435	17	24,395	S	4/30/2018	11
JEFRBL	040	JEFFERSON BL	WEST CAPI AV	MERKLEY AV	5	A	505	60	30,300	A	4/30/2018	9
TOUCPL	010	TOUCHSTONE PL	LINDEN RD	END	2	R	590	32	18,880	A	4/30/2018	9
HARBBL	150	HARBOR BL	INDUSTRIAL BL	STONE BL	2	A	1,030	18	18,540	A	4/30/2018	8
JCKNST	020	JACKSON ST	DREVER ST	SOULE ST	2	R	227	46	10,442	A	4/30/2018	7
SIXTST	020	SIXTEENTH ST	ALABAMA AV	VERMONT AV	2	R	550	23	12,650	A	4/30/2018	7
VIRGAV	010	VIRGINIA AV	STONE BL	NINETEENT ST	2	R	450	31	13,950	A	4/30/2018	6
SEYMAV	010	SEYMORE AV	END	GLADYS AV	2	R	575	12	6,900	A	4/30/2018	4
SUTTST	010	SUTTER ST	W CAPITAL AV	FIRST ST	2	R	1,100	15	16,500	A	4/30/2018	0

FC (Functional Classification): A (Arterial), C (Collector), R (Residential/Local), O (Other)
Surface Type: A (AC), O (AC/AC), P (PCC), G (Gravel)

APPENDIX B

Maintenance and Rehabilitation Decision Tree

Maintenance and Rehabilitation (M&R) Decision Tree

This report presents the current maintenance and rehabilitation decision tree that exists in the database. The decision tree forms the basis for all of the budgetary computations that are included in this volume. ***Changes to the decision tree will make the results in the budget reports invalid.*** All pavement treatment unit costs relevant to the street types in the database were updated.

The decision tree lists the treatments and costs selected for preventive maintenance and rehabilitation activities. Each line represents a specific combination of functional classification and surface type.

The preventive maintenance portion of the report is identified as Condition Category I – Very Good. All preventive maintenance treatment listings are assigned only to sections in Condition Category I where the PCI ≥ 70. Sections with PCI values less than 70 are assigned to treatments listed in Categories II through V.

In the preventive maintenance category (PCI ≥ 70), a time sequence is used to identify the appropriate treatment and cost. Each preventive maintenance treatment description consists of three parts: 1) a CRACK treatment, 2) a SURFACE treatment, and 3) a RESTORATION treatment. These three parts allow the user to specify one of three different preventive maintenance treatments depending on the prior maintenance history of the section.

1. The CRACK treatment part can be used to specify the most frequent type of preventive maintenance activity planned (typically crack seals).
2. The SURFACE treatment part can be used to specify more extensive and less frequent preventive maintenance activities, such as slurry seals. For example, a crack seal can be specified on a 3-year cycle with a slurry seal specified after 5 years.
3. The RESTORATION part can be used to specify a surface restoration treatment (such as an overlay) to be performed after a specified number of surface treatments. For example, after a certain number of successive slurry seals, an overlay can be specified instead of another slurry seal.

Rehabilitation treatments are assigned to sections in Condition Categories II through V (PCI less than 70). Each line is defined by a specific combination of functional classification, surface type, and condition category.

COLUMN	DESCRIPTION
Functional Class	Functional Classification identifying the branch number.
Surface	Surface Type identifying the branch number.
Condition Category	Condition Category (I through V).
Treatment Type	First Row (Crack Treatment) indicates localized treatment (e.g. crack sealing). Second Row (Surface Treatment) indicates surface treatment (e.g. slurry sealing). Third Row (Restoration Treatment) indicates surface restoration (e.g. overlay).
Treatment	Name of treatments from the "Treatment Descriptions" report.

COLUMN	DESCRIPTION
Yrs. Between Crack Seals	First Row - number of years between successive treatment applications specified in the first row (i.e. CRACK treatment).
Yrs. Between Surface Seals	Second Row - number of years between successive treatment applications specified in the second row (i.e. SURFACE treatment).
Number of Sequential Seals	Number of times that the treatment application in the second row (i.e. SURFACE treatment) will be performed prior to performing the treatment application in the third row.

Note that the treatments assigned to each section should not be blindly followed in preparing a street maintenance program. Engineering judgment and project level analysis should be applied to ensure that the treatment is appropriate and cost effective for the section.

Decision Tree

Printed: 08/05/2018

Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Arterial	AC	I - Very Good	Crack Treatment	SEAL CRACKS	$1.50	3		
			Surface Treatment	SLURRY SEAL	$6.00		5	
			Restoration Treatment	DO NOTHING	$0.00			3
		II - Good, Non-Load Related		2" MILL AND FILL	$28.50		7	
		III - Good, Load Related		2" MILL AND FILL	$31.00			
		IV - Poor		3" MILL AND FILL	$47.50			
		V - Very Poor		RECONSTRUCT SURFACE / FDR	$77.00			
	AC/AC	I - Very Good	Crack Treatment	SEAL CRACKS	$1.50	3		
			Surface Treatment	SLURRY SEAL	$6.00		5	
			Restoration Treatment	DO NOTHING	$0.00			3
		II - Good, Non-Load Related		2" MILL AND FILL	$28.50		7	
		III - Good, Load Related		2" MILL AND FILL	$31.00			
		IV - Poor		3" MILL AND FILL	$47.50			
		V - Very Poor		RECONSTRUCT SURFACE / FDR	$77.00			
	AC/PCC	I - Very Good	Crack Treatment	SEAL CRACKS	$1.50	3		
			Surface Treatment	SLURRY SEAL	$6.00		5	
			Restoration Treatment	DO NOTHING	$0.00			3
		II - Good, Non-Load Related		2" MILL AND FILL	$28.50		7	
		III - Good, Load Related		2" MILL AND FILL	$31.00			
		IV - Poor		3" MILL AND FILL	$47.50			
		V - Very Poor		RECONSTRUCT STRUCTURE (AC)	$77.00			
	PCC	I - Very Good	Crack Treatment	DO NOTHING	$0.00	3		
			Surface Treatment	DO NOTHING	$0.00		99	
			Restoration Treatment	DO NOTHING	$0.00			100
		II - Good, Non-Load Related		DO NOTHING	$0.00			
		III - Good, Load Related		DO NOTHING	$0.00			
		IV - Poor		10% SLAB REPLACEMENT	$31.00			
		V - Very Poor		15% SLAB REPLACEMENT	$46.50			

Functional Class and Surface combination not used

Decision Tree

Printed: 08/05/2018

Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Arterial	ST	I - Very Good	Crack Treatment	DO NOTHING	$0.00	9		
			Surface Treatment	DO NOTHING	$0.00		99	
			Restoration Treatment	DO NOTHING	$0.00			100
		II - Good, Non-Load Related		CAPE SEAL	$11.00			
		III - Good, Load Related		CAPE SEAL	$11.00			
		IV - Poor		CAPE SEAL	$11.00			
		V - Very Poor		CAPE SEAL	$11.00			

Functional Class and Surface combination not used

Decision Tree

Printed: 08/05/2018

Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Collector	AC	I - Very Good	Crack Treatment	SEAL CRACKS	$1.50	4		
			Surface Treatment	SLURRY SEAL	$6.00		6	
			Restoration Treatment	DO NOTHING	$0.00			3
		II - Good, Non-Load Related		CAPE SEAL	$11.00		8	
		III - Good, Load Related		2" MILL AND FILL	$28.50			
		IV - Poor		2" MILL AND FILL	$31.50			
		V - Very Poor		3" MILL AND FILL	$45.50			
	AC/AC	I - Very Good	Crack Treatment	SEAL CRACKS	$1.50	4		
			Surface Treatment	SLURRY SEAL	$6.00		6	
			Restoration Treatment	DO NOTHING	$0.00			3
		II - Good, Non-Load Related		CAPE SEAL	$11.00		8	
		III - Good, Load Related		2" MILL AND FILL	$28.50			
		IV - Poor		2" MILL AND FILL	$31.50			
		V - Very Poor		3" MILL AND FILL	$45.50			
	AC/PCC	I - Very Good	Crack Treatment	SEAL CRACKS	$1.50	4		
			Surface Treatment	SLURRY SEAL	$6.00		6	
			Restoration Treatment	DO NOTHING	$0.00			3
		II - Good, Non-Load Related		CAPE SEAL	$11.00		8	
		III - Good, Load Related		2" MILL AND FILL	$28.50			
		IV - Poor		2" MILL AND FILL	$31.50			
		V - Very Poor		2" MILL AND FILL	$31.50			
	PCC	I - Very Good	Crack Treatment	DO NOTHING	$0.00	9		
			Surface Treatment	DO NOTHING	$0.00		99	
			Restoration Treatment	DO NOTHING	$0.00			100
		II - Good, Non-Load Related		DO NOTHING	$0.00			
		III - Good, Load Related		DO NOTHING	$0.00			
		IV - Poor		10% SLAB REPLACEMENT	$30.00			
		V - Very Poor		15% SLAB REPLACEMENT	$45.00			

Functional Class and Surface combination not used

Decision Tree

Printed: 08/05/2018

Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Collector	ST	I - Very Good	Crack Treatment	DO NOTHING	$0.00	9		
			Surface Treatment	DO NOTHING	$0.00		99	
			Restoration Treatment	DO NOTHING	$0.00			100
		II - Good, Non-Load Related		CAPE SEAL	$11.00			
		III - Good, Load Related		CAPE SEAL	$11.00			
		IV - Poor		CAPE SEAL	$11.00			
		V - Very Poor		CAPE SEAL	$11.00			

Functional Class and Surface combination not used

Decision Tree

Printed: 08/05/2018

Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Residential/Local	AC	I - Very Good	Crack Treatment	SEAL CRACKS	$1.50	4		
			Surface Treatment	SLURRY SEAL	$5.50		7	
			Restoration Treatment	DO NOTHING	$0.00			3
		II - Good, Non-Load Related		CAPE SEAL	$10.00		9	
		III - Good, Load Related		CAPE SEAL	$11.00			
		IV - Poor		THIN AC OVERLAY(2 INCHES)	$21.50			
		V - Very Poor		RECONSTRUCT SURFACE / FDR	$37.00			
	AC/AC	I - Very Good	Crack Treatment	SEAL CRACKS	$1.50	4		
			Surface Treatment	SLURRY SEAL	$5.50		7	
			Restoration Treatment	DO NOTHING	$0.00			3
		II - Good, Non-Load Related		CAPE SEAL	$10.00		9	
		III - Good, Load Related		CAPE SEAL	$11.00			
		IV - Poor		THIN AC OVERLAY(2 INCHES)	$21.50			
		V - Very Poor		RECONSTRUCT SURFACE / FDR	$37.00			
	AC/PCC	I - Very Good	Crack Treatment	SEAL CRACKS	$1.50	4		
			Surface Treatment	SLURRY SEAL	$5.50		7	
			Restoration Treatment	DO NOTHING	$0.00			3
		II - Good, Non-Load Related		CAPE SEAL	$10.00		9	
		III - Good, Load Related		CAPE SEAL	$11.00			
		IV - Poor		THIN AC OVERLAY(2 INCHES)	$21.50			
		V - Very Poor		THIN AC OVERLAY(2 INCHES)	$21.50			
	PCC	I - Very Good	Crack Treatment	DO NOTHING	$0.00	4		
			Surface Treatment	DO NOTHING	$0.00		99	
			Restoration Treatment	DO NOTHING	$0.00			100
		II - Good, Non-Load Related		DO NOTHING	$0.00			
		III - Good, Load Related		DO NOTHING	$0.00			
		IV - Poor		10% SLAB REPLACEMENT	$27.50			
		V - Very Poor		15% SLAB REPLACEMENT	$36.50			

Functional Class and Surface combination not used

Decision Tree

Printed: 08/05/2018

Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Residential/Local	ST	I - Very Good	Crack Treatment	DO NOTHING	$0.00	9		
			Surface Treatment	DO NOTHING	$0.00		99	
			Restoration Treatment	DO NOTHING	$0.00			100
		II - Good, Non-Load Related		CAPE SEAL	$10.00			
		III - Good, Load Related		CAPE SEAL	$10.00			
		IV - Poor		CAPE SEAL	$10.00			
		V - Very Poor		CAPE SEAL	$10.00			

Functional Class and Surface combination not used

Decision Tree

Printed: 08/05/2018

Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Other	AC	I - Very Good	Crack Treatment	SEAL CRACKS	$1.50	4		
			Surface Treatment	SLURRY SEAL	$5.50		8	
			Restoration Treatment	DO NOTHING	$0.00			3
		II - Good, Non-Load Related		CAPE SEAL	$10.00			
		III - Good, Load Related		CAPE SEAL	$11.00			
		IV - Poor		THIN AC OVERLAY(2 INCHES)	$21.50			
		V - Very Poor		RECONSTRUCT SURFACE / FDR	$37.00			
	AC/AC	I - Very Good	Crack Treatment	SEAL CRACKS	$1.50	4		
			Surface Treatment	SLURRY SEAL	$5.50		8	
			Restoration Treatment	DO NOTHING	$0.00			3
		II - Good, Non-Load Related		CAPE SEAL	$10.00			
		III - Good, Load Related		CAPE SEAL	$11.00			
		IV - Poor		THIN AC OVERLAY(2 INCHES)	$21.50			
		V - Very Poor		RECONSTRUCT SURFACE / FDR	$37.00			
	AC/PCC	I - Very Good	Crack Treatment	SEAL CRACKS	$1.50	4		
			Surface Treatment	SLURRY SEAL	$5.50		8	
			Restoration Treatment	DO NOTHING	$0.00			3
		II - Good, Non-Load Related		CAPE SEAL	$10.00			
		III - Good, Load Related		CAPE SEAL	$11.00			
		IV - Poor		THIN AC OVERLAY(2 INCHES)	$21.50			
		V - Very Poor		THIN AC OVERLAY(2 INCHES)	$21.50			
	PCC	I - Very Good	Crack Treatment	DO NOTHING	$0.00	9		
			Surface Treatment	DO NOTHING	$0.00		99	
			Restoration Treatment	DO NOTHING	$0.00			100
		II - Good, Non-Load Related		DO NOTHING	$0.00			
		III - Good, Load Related		DO NOTHING	$0.00			
		IV - Poor		10% SLAB REPLACEMENT	$27.50			
		V - Very Poor		15% SLAB REPLACEMENT	$36.50			

Functional Class and Surface combination not used

Decision Tree

Printed: 08/05/2018

Functional Class	Surface	Condition Category	Treatment Type	Treatment	Cost/Sq Yd, except Seal Cracks in LF:	Yrs Between Crack Seals	Yrs Between Surface Seals	# of Surface Seals before Overlay
Other	ST	I - Very Good	Crack Treatment	DO NOTHING	$0.00	9		
			Surface Treatment	DO NOTHING	$0.00		99	
			Restoration Treatment	DO NOTHING	$0.00			100
		II - Good, Non-Load Related		CAPE SEAL	$10.00			
		III - Good, Load Related		CAPE SEAL	$10.00			
		IV - Poor		CAPE SEAL	$10.00			
		V - Very Poor		CAPE SEAL	$10.00			

Functional Class and Surface combination not used

APPENDIX C

Budget Needs

Projected PCI / Cost Summary

Preventative Treatment / Cost Summary

Rehabilitation Treatment / Cost Summary

Budget Needs Reports

The purpose of this module is to answer the question: ***If the City had all the money in the world, what sections should be fixed and how much will it cost?*** Based on the Maintenance & Rehabilitation (M&R) decision tree and the PCIs of the sections, the program will then select a maintenance or rehabilitation action and compute the total costs over a period of ten years. The Budget Needs represents the "ideal world" funding levels, while the Budget Scenarios reports in the next section represent the most "cost effective" prioritization possible for the actual funding levels.

A budget needs analysis has been performed. The summary results from the analysis are shown below. An interest rate of 3% and an inflation factor of 3% were used to project the costs for the next ten years. This report shows the total ten-year budget that would be required to meet the City's standards as exemplified in the M&R decision tree.

As indicated in the report, with a budget of $121.3 million dollars over the next ten years the PCI of the street network will improve from the current level of 60 to 82 by 2027. If no treatments are programmed, the weighted average PCI is projected to deteriorate to 36 by 2027.

Budget Needs reports included in this volume are listed below:

- Projected PCI/Cost Summary
- Preventative Maintenance Treatment/Cost Summary
- Rehabilitation Treatment/Cost Summary

Needs - Projected PCI/Cost Summary

This report summarizes and projects the City's network PCI values over a ten-year period, both with and without treatments applied. These costs are based on those in the M&R decision tree. It also projects the costs over a ten-year period.

COLUMN	DESCRIPTION
Year	Year in the analysis period.
PCI Treated	Projected network average PCI with all needed treatments applied.
PCI Untreated	Projected network average PCI without any treatments applied.
PM Cost	Total preventive maintenance treatment cost.
Rehab Cost	Total rehabilitation treatment cost.
Cost	The budget required for each year in the analysis period to meet the City's standard as shown on the M&R decision tree.
Total Cost	Total budget required over a ten-year period.

Needs - Projected PCI/Cost Summary

Inflation Rate = 3.00 % Printed: 08/06/2018

Year	PCI Treated	PCI Untreated	PM Cost	Rehab Cost	Cost
2018	91	60	$4,858,854	$63,717,421	$68,576,275
2019	85	59	$321,585	$3,351,688	$3,673,273
2020	83	56	$572,892	$2,618,106	$3,190,998
2021	82	53	$164,187	$678,619	$842,806
2022	84	50	$61,656	$6,765,514	$6,827,170
2023	82	47	$6,391,758	$1,357,431	$7,749,189
2024	81	45	$3,334,399	$1,618,747	$4,953,146
2025	82	42	$8,248,380	$1,188,228	$9,436,608
2026	85	39	$784,809	$9,932,841	$10,717,650
2027	82	36	$498,622	$4,862,150	$5,360,772

% PM	PM Total Cost	Rehab Total Cost	Total Cost
20.80%	$25,237,142	$96,090,745	$121,327,887

Needs - Preventive Maintenance Treatment/Cost Summary

This report summarizes each preventive maintenance treatment type, quantity of pavement affected, and total costs over the ten-year period. It also summarizes the total quantities and costs over the next ten years.

COLUMN	DESCRIPTION
Treatment	Type of preventive maintenance treatments needed.
Year	Year in the analysis period.
Area Treated	Quantities in linear feet (Seal Cracks) or square yard (Microsurfacing).
Cost	Maintenance treatment cost.

Needs - Preventive Maintenance Treatment/Cost Summary

Inflation Rate = 3.00 % Printed: 08/06/2018

Treatment	Year	Area Treated		Cost
SEAL CRACKS	2018	127.12	ft.	$191
	2020	25.8	ft.	$42
	2021	10,165.76	ft.	$16,736
	2022	29,886.59	ft.	$50,731
	2023	6,965.88	ft.	$12,153
	2024	3,138.64	ft.	$5,642
	2025	876.53	ft.	$1,627
	2026	14,593.05	ft.	$27,810
	2027	1,189.25	ft.	$2,332
	Total	66,968.61		$117,264
SLURRY SEAL	2018	865,991.89	sq.yd.	$4,858,663
	2019	55,454.22	sq.yd.	$321,585
	2020	92,033.11	sq.yd.	$572,850
	2021	24,533	sq.yd.	$147,451
	2022	1,617.78	sq.yd.	$10,925
	2023	917,173.67	sq.yd.	$6,379,605
	2024	464,624.33	sq.yd.	$3,328,757
	2025	1,209,757.33	sq.yd.	$8,246,753
	2026	107,681.67	sq.yd.	$756,999
	2027	69,155.67	sq.yd.	$496,290
	Total	3,808,022.67		$25,119,878
	Total Quantity	3,874,991.28		$25,237,142

Needs - Rehabilitation Treatment/Cost Summary

This report summarizes each rehabilitation treatment type, quantity of pavement affected, and total costs over the ten-year period. It also summarizes the total quantities and costs over the next ten years.

COLUMN	DESCRIPTION
Treatment	Type of rehabilitation treatments needed.
Year	Year in the analysis period.
Area Treated	Quantities in square yard.
Cost	Rehabilitation treatment cost.

Needs - Rehabilitation Treatment/Cost Summary

Inflation Rate = 3.00 % Printed: 08/06/2018

Treatment	Year		Area Treated		Cost
2" MILL AND FILL	2018		717,267.22	sq.yd.	$21,392,079
	2019		45,012.11	sq.yd.	$1,321,332
	2020		46,102.11	sq.yd.	$1,393,929
	2023		13,220	sq.yd.	$482,758
	2024		30,945.56	sq.yd.	$1,163,948
	2025		10,381.67	sq.yd.	$402,198
	2026		31,102.78	sq.yd.	$1,241,105
		Total	894,031.44	sq.yd.	$27,397,349
3" MILL AND FILL	2018		348,609.11	sq.yd.	$16,534,341
		Total	348,609.11	sq.yd.	$16,534,341
CAPE SEAL	2018		1,044,636	sq.yd.	$10,930,935
	2019		188,485.89	sq.yd.	$2,030,356
	2020		111,721.22	sq.yd.	$1,224,177
	2021		60,242.22	sq.yd.	$678,619
	2022		572,106.44	sq.yd.	$6,765,514
	2023		73,078.78	sq.yd.	$874,673
	2024		36,415.78	sq.yd.	$454,799
	2025		57,842.33	sq.yd.	$731,320
	2026		631,192.56	sq.yd.	$8,437,963
	2027		285,215.11	sq.yd.	$3,766,856
		Total	3,060,936.33	sq.yd.	$35,895,212
RECONSTRUCT SURFACE / FDR	2018		140,608.89	sq.yd.	$7,815,211
		Total	140,608.89	sq.yd.	$7,815,211
THIN AC OVERLAY(2 INCHES)	2018		327,665.67	sq.yd.	$7,044,855
	2025		2,069	sq.yd.	$54,710
	2026		9,317.67	sq.yd.	$253,773
	2027		39,044.11	sq.yd.	$1,095,294
		Total	378,096.44	sq.yd.	$8,448,632

Total Cost $96,090,745

Scenarios 1 - 4

Scenario 1: Current Funding Levels with SB1

Cost Summary Report

Network Condition Summary Report

Scenarios - Cost Summary

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year	PM	Budget	Rehabilitation			Preventative Maintenance	Surplus PM	Deferred		Stop Gap
2018	20%	$2,000,000	II	$43,023	Non-Project	$415,445	$0	$66,577,421	Funded	$0
			III	$0					Unmet	$482,101
			IV	$1,143,769	Project	$0				
			V	$396,304						
			Total	$1,583,096						
			Project	$0						
2019	20%	$2,000,000	II	$400,902	Non-Project	$411,607	$0	$72,175,002	Funded	$0
			III	$264,612					Unmet	$25,066
			IV	$854,659	Project	$0				
			V	$60,291						
			Total	$1,580,464						
			Project	$0						
2020	20%	$2,000,000	II	$589,147	Non-Project	$400,150	$0	$78,820,497	Funded	$0
			III	$44,113					Unmet	$19,285
			IV	$661,362	Project	$0				
			V	$302,480						
			Total	$1,597,102						
			Project	$0						
2021	20%	$2,000,000	II	$681,412	Non-Project	$401,705	$0	$85,697,162	Funded	$0
			III	$0					Unmet	$14,468
			IV	$710,826	Project	$0				
			V	$181,009						
			Total	$1,573,247						
			Project	$0						
2022	20%	$2,000,000	II	$631,611	Non-Project	$294,749	$105,251	$94,247,735	Funded	$0
			III	$45,534					Unmet	$18,275
			IV	$724,578	Project	$0				
			V	$196,289						
			Total	$1,598,012						
			Project	$0						
2023	20%	$2,000,000	II	$547,896	Non-Project	$314,002	$85,998	$104,141,742	Funded	$0
			III	$99,422					Unmet	$1,004,712
			IV	$917,944	Project	$0				
			V	$0						
			Total	$1,565,262						
			Project	$0						

Year	PM	Budget	Rehabilitation			Preventative Maintenance	Surplus PM	Deferred		Stop Gap
2024	20%	$2,000,000	II	$258,601	Non-Project	$413,149	$0	$110,913,439	Funded	$0
			III	$194,247					Unmet	$45,578
			IV	$1,114,477	Project	$0				
			V	$0						
			Total	$1,567,325						
			Project	$0						
2025	20%	$2,000,000	II	$52,089	Non-Project	$422,992	$0	$117,700,626	Funded	$0
			III	$711,254					Unmet	$30,558
			IV	$809,461	Project	$0				
			V	$0						
			Total	$1,572,804						
			Project	$0						
2026	20%	$2,000,000	II	$0	Non-Project	$414,955	$0	$124,372,661	Funded	$0
			III	$1,086,150					Unmet	$33,119
			IV	$478,827	Project	$0				
			V	$0						
			Total	$1,564,977						
			Project	$0						
2027	20%	$2,000,000	II	$249,525	Non-Project	$386,598	$13,402	$133,261,201	Funded	$0
			III	$652,280					Unmet	$36,699
			IV	$655,992	Project	$0				
			V	$32,094						
			Total	$1,589,891						
			Project	$0						

Summary

Functional Class	Rehabilitation	Prev. Maint.	Funded Stop Gap	Unmet Stop Gap
Arterial	$7,094,204	$1,848,738	$0	$629,385
Collector	$3,918,791	$814,897	$0	$369,795
Other	$0	$0	$0	$0
Residential/Local	$4,779,185	$1,211,717	$0	$710,681
Grand Total:	$15,792,180	$3,875,352	$0	$1,709,861

Scenarios - Network Condition Summary

Interest: 3% Inflation: 3% Printed: 08/06/2018

Scenario: Current Funding Levels

Year	Budget	PM	Year	Budget	PM	Year	Budget	PM
2018	$2,000,000	20%	2022	$2,000,000	20%	2026	$2,000,000	20%
2019	$2,000,000	20%	2023	$2,000,000	20%	2027	$2,000,000	20%
2020	$2,000,000	20%	2024	$2,000,000	20%			
2021	$2,000,000	20%	2025	$2,000,000	20%			

Projected Network Average PCI by year

Year	Never Treated	With Selected Treatment	Treated Centerline Miles	Treated Lane Miles
2018	60	64	10.61	25.04
2019	59	61	7.68	17.60
2020	56	60	7.01	16.07
2021	53	58	8.05	18.21
2022	50	57	10.35	22.42
2023	47	55	10.87	23.45
2024	45	53	10.69	25.50
2025	42	51	10.23	25.29
2026	39	50	10.47	23.27
2027	37	48	9.38	20.80

Percent Network Area by Functional Class and Condition Category

Condition in base year 2018, prior to applying treatments.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	8.9%	2.6%	25.5%	0.2%	37.1%
II / III	11.9%	6.7%	14.1%	0.0%	32.7%
IV	9.4%	6.9%	9.3%	0.0%	25.6%
V	2.2%	0.3%	2.1%	0.0%	4.6%
Total	32.4%	16.4%	51.0%	0.2%	100.0%

Condition in year 2018 after schedulable treatments applied.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	10.5%	3.4%	26.6%	0.2%	40.6%
II / III	11.8%	6.7%	14.1%	0.0%	32.6%
IV	8.6%	6.1%	8.4%	0.0%	23.1%
V	1.6%	0.3%	1.8%	0.0%	3.7%
Total	32.4%	16.4%	51.0%	0.2%	100.0%

Condition in year 2027 after schedulable treatments applied.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	11.7%	3.9%	14.7%	0.2%	30.4%
II / III	1.7%	1.6%	20.5%	0.0%	23.7%
IV	7.5%	3.1%	8.1%	0.0%	18.7%
V	11.6%	7.8%	7.7%	0.0%	27.2%
Total	32.4%	16.4%	51.0%	0.2%	100.0%

Scenario 2: Maintain Current PCI

Cost Summary Report

Network Condition Summary Report

Scenarios - Cost Summary

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Maintain Current PCI

Year	PM	Budget	Rehabilitation			Preventative Maintenance	Surplus PM	Deferred		Stop Gap
2018	20%	$1,000,000	II	$0	Non-Project	$201,677	$0	$67,576,395	Funded	$0
			III	$0					Unmet	$501,343
			IV	$491,275	Project	$0				
			V	$306,601						
			Total	$797,876						
			Project	$0						
2019	20%	$2,000,000	II	$454,187	Non-Project	$400,207	$0	$73,197,027	Funded	$0
			III	$264,612					Unmet	$25,593
			IV	$727,299	Project	$0				
			V	$152,685						
			Total	$1,598,783						
			Project	$0						
2020	20%	$4,000,000	II	$1,439,460	Non-Project	$802,740	$0	$77,871,415	Funded	$0
			III	$0					Unmet	$19,914
			IV	$1,454,367	Project	$0				
			V	$302,480						
			Total	$3,196,307						
			Project	$0						
2021	20%	$6,000,000	II	$2,088,688	Non-Project	$1,199,168	$853	$80,956,081	Funded	$0
			III	$0					Unmet	$17,226
			IV	$2,530,324	Project	$0				
			V	$181,009						
			Total	$4,800,021						
			Project	$0						
2022	20%	$7,000,000	II	$3,293,437	Non-Project	$1,404,544	$0	$83,351,363	Funded	$0
			III	$0					Unmet	$14,690
			IV	$2,028,154	Project	$0				
			V	$273,737						
			Total	$5,595,328						
			Project	$0						
2023	20%	$7,500,000	II	$232,473	Non-Project	$1,498,507	$1,493	$87,400,065	Funded	$0
			III	$1,003,523					Unmet	$914,530
			IV	$4,755,237	Project	$0				
			V	$8,284						
			Total	$5,999,517						
			Project	$0						

Year	PM	Budget	Rehabilitation			Preventative Maintenance	Surplus PM	Deferred		Stop Gap
2024	20%	$8,500,000	II	$219,872	Non-Project	$696,258	$1,003,742	$88,263,599	Funded	$0
			III	$525,179					Unmet	$30,148
			IV	$6,054,337	Project	$0				
			V	$0						
			Total	$6,799,388						
			Project	$0						
2025	20%	$8,500,000	II	$215,323	Non-Project	$341,047	$1,358,953	$88,671,757	Funded	$0
			III	$497,211					Unmet	$17,313
			IV	$6,086,114	Project	$0				
			V	$0						
			Total	$6,798,648						
			Project	$0						
2026	20%	$8,500,000	II	$430,122	Non-Project	$1,081,878	$618,122	$87,897,041	Funded	$0
			III	$1,953,509					Unmet	$18,651
			IV	$4,103,590	Project	$0				
			V	$306,378						
			Total	$6,793,599						
			Project	$0						
2027	20%	$7,000,000	II	$0	Non-Project	$1,409,871	$0	$91,268,345	Funded	$0
			III	$1,883,286					Unmet	$16,779
			IV	$2,273,042	Project	$0				
			V	$1,433,222						
			Total	$5,589,550						
			Project	$0						

Summary

Functional Class	Rehabilitation	Prev. Maint.	Funded Stop Gap	Unmet Stop Gap
Arterial	$21,414,026	$3,360,861	$0	$584,164
Collector	$9,530,267	$382,941	$0	$366,258
Other	$0	$49,948	$0	$0
Residential/Local	$17,024,724	$5,242,147	$0	$625,765
Grand Total:	$47,969,017	$9,035,897	$0	$1,576,186

Scenarios - Network Condition Summary

Interest: 3% Inflation: 3% Printed: 08/06/2018

Scenario: Maintain Current PCI

Year	Budget	PM	Year	Budget	PM	Year	Budget	PM
2018	$1,000,000	20%	2022	$7,000,000	20%	2026	$8,500,000	20%
2019	$2,000,000	20%	2023	$7,500,000	20%	2027	$7,000,000	20%
2020	$4,000,000	20%	2024	$8,500,000	20%			
2021	$6,000,000	20%	2025	$8,500,000	20%			

Projected Network Average PCI by year

Year	Never Treated	With Selected Treatment	Treated Centerline Miles	Treated Lane Miles
2018	60	63	4.55	12.61
2019	59	61	8.82	21.14
2020	56	60	14.03	30.80
2021	53	60	19.82	43.23
2022	50	60	22.59	51.82
2023	47	60	24.78	56.77
2024	45	60	27.81	65.36
2025	42	60	29.64	68.94
2026	39	60	31.86	68.65
2027	37	60	40.26	92.54

Percent Network Area by Functional Class and Condition Category

Condition in base year 2018, prior to applying treatments.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	8.9%	2.6%	25.5%	0.2%	37.1%
II / III	11.9%	6.7%	14.1%	0.0%	32.7%
IV	9.4%	6.9%	9.3%	0.0%	25.6%
V	2.2%	0.3%	2.1%	0.0%	4.6%
Total	32.4%	16.4%	51.0%	0.2%	100.0%

Condition in year 2018 after schedulable treatments applied.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	10.4%	2.8%	25.5%	0.2%	38.9%
II / III	11.9%	6.7%	14.1%	0.0%	32.7%
IV	8.6%	6.6%	9.3%	0.0%	24.5%
V	1.6%	0.3%	2.0%	0.0%	3.9%
Total	32.4%	16.4%	51.0%	0.2%	100.0%

Condition in year 2027 after schedulable treatments applied.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	19.6%	8.1%	32.9%	0.2%	60.8%
II / III	0.0%	0.6%	11.3%	0.0%	12.0%
IV	1.2%	0.0%	0.0%	0.0%	1.2%
V	11.6%	7.7%	6.7%	0.0%	26.1%
Total	32.4%	16.4%	51.0%	0.2%	100.0%

Scenario 3: Increase PCI to 70 (Arterial/Collector Priority)

Cost Summary Report

Network Condition Summary Report

Scenarios - Cost Summary

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Increase PCI to 70 (Arterial/Collector Priority)

Year	PM	Budget	Rehabilitation			Preventative Maintenance	Surplus PM	Deferred		Stop Gap
2018	20%	$8,400,000	II	$2,109,375	Non-Project	$1,680,375	$0	$60,176,595	Funded	$0
			III	$456,959					Unmet	$441,901
			IV	$3,756,404	Project	$0				
			V	$396,304						
			Total	$6,719,042						
			Project	$0						
2019	20%	$8,400,000	II	$4,103,309	Non-Project	$1,684,663	$0	$59,174,431	Funded	$0
			III	$1,005,233					Unmet	$22,054
			IV	$1,546,357	Project	$0				
			V	$60,291						
			Total	$6,715,190						
			Project	$0						
2020	20%	$8,400,000	II	$2,196,645	Non-Project	$1,685,010	$0	$59,028,544	Funded	$0
			III	$384,265					Unmet	$14,405
			IV	$3,830,271	Project	$0				
			V	$302,480						
			Total	$6,713,661						
			Project	$0						
2021	20%	$8,400,000	II	$503,762	Non-Project	$1,033,479	$646,521	$59,258,616	Funded	$0
			III	$331,235					Unmet	$5,915
			IV	$5,703,783	Project	$0				
			V	$181,009						
			Total	$6,719,789						
			Project	$0						
2022	20%	$8,400,000	II	$110,814	Non-Project	$22,609	$1,657,391	$62,487,872	Funded	$0
			III	$87,043					Unmet	$5,577
			IV	$6,324,239	Project	$0				
			V	$196,289						
			Total	$6,718,385						
			Project	$0						
2023	20%	$8,400,000	II	$220,367	Non-Project	$1,044,447	$635,553	$65,320,840	Funded	$0
			III	$1,541,091					Unmet	$815,076
			IV	$4,952,506	Project	$0				
			V	$0						
			Total	$6,713,964						
			Project	$0						

Year	PM	Budget	Rehabilitation			Preventative Maintenance	Surplus PM	Deferred		Stop Gap
2024	20%	$8,400,000	II	$942,064	Non-Project	$1,426,576	$253,424	$64,727,646	Funded	$0
			III	$1,263,505					Unmet	$23,334
			IV	$3,446,169	Project	$0				
			V	$1,067,335						
			Total	$6,719,073						
			Project	$0						
2025	20%	$8,400,000	II	$201,574	Non-Project	$1,682,524	$0	$64,483,821	Funded	$0
			III	$1,112,849					Unmet	$14,572
			IV	$2,287,432	Project	$0				
			V	$3,115,482						
			Total	$6,717,337						
			Project	$0						
2026	20%	$8,400,000	II	$0	Non-Project	$1,680,194	$0	$63,131,418	Funded	$0
			III	$824,960					Unmet	$8,339
			IV	$965,038	Project	$0				
			V	$4,929,644						
			Total	$6,719,642						
			Project	$0						
2027	20%	$8,400,000	II	$0	Non-Project	$1,679,928	$72	$62,406,932	Funded	$0
			III	$2,482,848					Unmet	$9,606
			IV	$1,957,375	Project	$0				
			V	$2,279,369						
			Total	$6,719,592						
			Project	$0						

Summary

Functional Class	Rehabilitation	Prev. Maint.	Funded Stop Gap	Unmet Stop Gap
Arterial	$26,417,199	$5,950,931	$0	$494,490
Collector	$12,266,352	$593,052	$0	$327,750
Other	$0	$51,441	$0	$0
Residential/Local	$28,492,124	$7,024,381	$0	$538,541
Grand Total:	$67,175,675	$13,619,805	$0	$1,360,780

Scenarios - Network Condition Summary

Interest: 3% Inflation: 3% Printed: 08/06/2018

Scenario: Increase PCI to 70 (Arterial/Collector Priority)

Year	Budget	PM	Year	Budget	PM	Year	Budget	PM
2018	$8,400,000	20%	2022	$8,400,000	20%	2026	$8,400,000	20%
2019	$8,400,000	20%	2023	$8,400,000	20%	2027	$8,400,000	20%
2020	$8,400,000	20%	2024	$8,400,000	20%			
2021	$8,400,000	20%	2025	$8,400,000	20%			

Projected Network Average PCI by year

Year	Never Treated	With Selected Treatment	Treated Centerline Miles	Treated Lane Miles
2018	60	67	31.70	73.87
2019	59	66	24.06	55.77
2020	56	66	24.87	55.93
2021	53	66	23.10	53.96
2022	50	66	31.95	75.58
2023	47	66	33.99	84.83
2024	45	67	43.54	101.63
2025	42	68	37.17	78.28
2026	39	69	30.43	74.16
2027	37	70	32.16	81.31

Percent Network Area by Functional Class and Condition Category

Condition in base year 2018, prior to applying treatments.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	8.9%	2.6%	25.5%	0.2%	37.1%
II / III	11.9%	6.7%	14.1%	0.0%	32.7%
IV	9.4%	6.9%	9.3%	0.0%	25.6%
V	2.2%	0.3%	2.1%	0.0%	4.6%
Total	32.4%	16.4%	51.0%	0.2%	100.0%

Condition in year 2018 after schedulable treatments applied.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	13.5%	3.4%	29.6%	0.2%	46.6%
II / III	8.8%	6.7%	14.1%	0.0%	29.5%
IV	8.6%	6.1%	5.4%	0.0%	20.2%
V	1.6%	0.3%	1.8%	0.0%	3.7%
Total	32.4%	16.4%	51.0%	0.2%	100.0%

Condition in year 2027 after schedulable treatments applied.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	23.0%	10.3%	42.6%	0.2%	76.0%
II / III	0.0%	0.6%	8.1%	0.0%	8.8%
V	9.4%	5.6%	0.2%	0.0%	15.2%
Total	32.4%	16.4%	51.0%	0.2%	100.0%

Scenarios Criteria:

Scenario 4: Increase PCI to 70 (Residential Priority)

Cost Summary Report

Network Condition Summary Report

Scenarios - Cost Summary

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Increase PCI to 70 (Residential Priority)

Year	PM	Budget	Rehabilitation			Preventative Maintenance	Surplus PM	Deferred		Stop Gap
2018	20%	$8,400,000	II	$978,678	Non-Project	$1,682,508	$0	$60,176,981	Funded	$0
			III	$0					Unmet	$416,436
			IV	$3,543,152	Project	$0				
			V	$2,194,716						
			Total	$6,716,546						
			Project	$0						
2019	20%	$8,400,000	II	$2,394,509	Non-Project	$1,684,464	$0	$59,176,518	Funded	$0
			III	$80,085					Unmet	$15,720
			IV	$1,453,910	Project	$0				
			V	$2,785,233						
			Total	$6,713,737						
			Project	$0						
2020	20%	$8,400,000	II	$1,646,988	Non-Project	$1,679,772	$228	$59,030,483	Funded	$0
			III	$317,867					Unmet	$9,564
			IV	$3,181,048	Project	$0				
			V	$1,573,222						
			Total	$6,719,125						
			Project	$0						
2021	20%	$8,400,000	II	$3,103,158	Non-Project	$964,430	$715,570	$59,548,331	Funded	$0
			III	$331,235					Unmet	$1,890
			IV	$1,508,218	Project	$0				
			V	$1,765,273						
			Total	$6,707,884						
			Project	$0						
2022	20%	$8,400,000	II	$389,781	Non-Project	$26,504	$1,653,496	$61,978,267	Funded	$0
			III	$1,288,150					Unmet	$1,243
			IV	$3,062,327	Project	$0				
			V	$1,979,272						
			Total	$6,719,530						
			Project	$0						
2023	20%	$8,400,000	II	$2,530,382	Non-Project	$28,281	$1,651,719	$66,472,813	Funded	$0
			III	$963,206					Unmet	$630,125
			IV	$2,513,289	Project	$0				
			V	$706,186						
			Total	$6,713,063						
			Project	$0						

Year	PM	Budget	Rehabilitation			Preventative Maintenance	Surplus PM	Deferred		Stop Gap
2024	20%	$8,400,000	II	$529,071	Non-Project	$23,040	$1,656,960	$67,370,852	Funded	$0
			III	$1,960,709					Unmet	$2,151
			IV	$3,540,944	Project	$0				
			V	$685,621						
			Total	$6,716,345						
			Project	$0						
2025	20%	$8,400,000	II	$0	Non-Project	$1,686,489	$0	$68,563,041	Funded	$0
			III	$1,432,186					Unmet	$1,974
			IV	$1,354,415	Project	$0				
			V	$3,926,058						
			Total	$6,712,659						
			Project	$0						
2026	20%	$8,400,000	II	$0	Non-Project	$1,719,301	$0	$71,813,611	Funded	$0
			III	$3,031,202					Unmet	$376
			IV	$172,250	Project	$0				
			V	$3,476,360						
			Total	$6,679,812						
			Project	$0						
2027	20%	$8,400,000	II	$664,024	Non-Project	$1,688,870	$0	$72,740,633	Funded	$0
			III	$1,615,019					Unmet	$0
			IV	$789,255	Project	$0				
			V	$3,642,582						
			Total	$6,710,880						
			Project	$0						

Summary

Functional Class	Rehabilitation	Prev. Maint.	Funded Stop Gap	Unmet Stop Gap
Arterial	$10,171,413	$1,259,219	$0	$606,592
Collector	$23,680,723	$468,845	$0	$331,612
Other	$0	$49,948	$0	$0
Residential/Local	$33,257,445	$9,405,647	$0	$141,274
Grand Total:	$67,109,581	$11,183,659	$0	$1,079,479

Scenarios - Network Condition Summary

Interest: 3% Inflation: 3% Printed: 08/06/2018

Scenario: Increase PCI to 70 (Residential Priority)

Year	Budget	PM	Year	Budget	PM	Year	Budget	PM
2018	$8,400,000	20%	2022	$8,400,000	20%	2026	$8,400,000	20%
2019	$8,400,000	20%	2023	$8,400,000	20%	2027	$8,400,000	20%
2020	$8,400,000	20%	2024	$8,400,000	20%			
2021	$8,400,000	20%	2025	$8,400,000	20%			

Projected Network Average PCI by year

Year	Never Treated	With Selected Treatment	Treated Centerline Miles	Treated Lane Miles
2018	60	67	35.07	70.79
2019	59	67	35.60	73.87
2020	56	68	30.10	63.22
2021	53	68	19.21	44.68
2022	50	69	43.51	88.34
2023	47	68	40.13	85.29
2024	45	68	41.39	93.30
2025	42	69	29.51	60.91
2026	39	70	34.99	75.05
2027	37	70	26.99	55.86

Percent Network Area by Functional Class and Condition Category

Condition in base year 2018, prior to applying treatments.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	8.9%	2.6%	25.5%	0.2%	37.1%
II / III	11.9%	6.7%	14.1%	0.0%	32.7%
IV	9.4%	6.9%	9.3%	0.0%	25.6%
V	2.2%	0.3%	2.1%	0.0%	4.6%
Total	32.4%	16.4%	51.0%	0.2%	100.0%

Condition in year 2018 after schedulable treatments applied.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	9.6%	2.6%	33.7%	0.2%	46.0%
II / III	11.9%	6.7%	11.8%	0.0%	30.3%
IV	9.4%	6.9%	4.9%	0.0%	21.2%
V	1.6%	0.3%	0.6%	0.0%	2.5%
Total	32.4%	16.4%	51.0%	0.2%	100.0%

Condition in year 2027 after schedulable treatments applied.

Condition	Arterial	Collector	Res/Loc	Other	Total
I	14.2%	15.0%	46.0%	0.2%	75.3%
II / III	0.5%	0.6%	5.0%	0.0%	6.0%
IV	6.1%	0.0%	0.0%	0.0%	6.1%
V	11.6%	0.9%	0.0%	0.0%	12.5%
Total	32.4%	16.4%	51.0%	0.2%	100.0%

Scenarios Criteria:

APPENDIX D

Sections Selected for Treatment:
Current Funding Levels (Scenario 1)

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year	Budget	PM	Year	Budget	PM	Year	Budget	PM
2018	$2,000,000	20%	2022	$2,000,000	20%	2026	$2,000,000	20%
2019	$2,000,000	20%	2023	$2,000,000	20%	2027	$2,000,000	20%
2020	$2,000,000	20%	2024	$2,000,000	20%			
2021	$2,000,000	20%	2025	$2,000,000	20%			

Year: 2018

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
POPLAR AV	EVERGREEN AV	BUCKEYE DR	POPLAV	020	345	32	11,040	C	AC	40	42	100	$38,640	16,795	2" MILL AND FILL
										Treatment Total			$38,640		
ALLAN AV	END	BLACKER RD	ALANAV	020	370	18	6,660	R	ST	12	17	100	$7,400	25,056	CAPE SEAL
BEVAN RD	JEFFERSON BL	GREGORY AV	BEVARD	030	2,430	13	31,590	R	ST	15	20	100	$35,100	25,056	CAPE SEAL
BLACKER RD	ALLAN AV	JEFFERSON BL	BLCKRD	010	710	22	15,620	R	ST	47	48	100	$17,356	21,700	CAPE SEAL
BRYTE AV	MAY ST	RIVERBANK RD	BRYTAV	020	1,370	32	43,840	C	ST	42	44	100	$53,583	27,510	CAPE SEAL
DAVIS RD	GREGORY AV	PARTRIDGE AV	DAVIRD	030	1,655	20	33,100	A	ST	20	24	100	$40,456	41,414	CAPE SEAL
DAVIS RD	ANTIOCH AV	1800' E/O ANTIOCH	DAVIRD	050	1,800	19	34,200	A	ST	18	22	100	$41,800	41,414	CAPE SEAL
EIGHTH ST	WILLIAM ST	CUMMINS WAY	EIGHST	020	1,030	16	16,480	R	ST	0	43	100	$18,312	23,252	CAPE SEAL
GARY CT	SUMMERFIE DR	END E	GARYCT	010	123	50	6,150	R	ST	44	46	100	$6,834	22,631	CAPE SEAL
GLADYS AV	END	SEYMORE AV	GLADAV	010	771	32	24,747	R	ST	10	15	100	$27,497	25,056	CAPE SEAL
HART AV	ALLAN AV	HIGGINS RD	HARTAV	010	1,615	22	35,530	R	ST	37	40	100	$39,478	23,918	CAPE SEAL
HIGGINS RD	211' E/ SUMMERFIELD DR	311' E/ CONSTITUTIO N	HIGGRD	020	710	24	17,040	C	ST	41	43	100	$20,827	27,800	CAPE SEAL
HIGGINS RD	CONSTITUTION AV	PARADISE WY	HIGGRD	030	728	32	23,296	C	ST	46	47	100	$28,473	26,196	CAPE SEAL
HIGGINS RD	PARADISE WY	JEFFERSON BL	HIGGRD	040	1,030	34	35,020	C	ST	47	48	100	$42,803	25,824	CAPE SEAL
JASMINE AV	LISBON AV	RIVERBANK RD	JASMAV	010	1,370	32	43,840	R	ST	48	49	100	$48,712	21,315	CAPE SEAL
KENTS CT	OLIVE CT	END	KENTSRD	010	268	33	8,844	R	ST	47	48	100	$9,827	21,698	CAPE SEAL

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2018

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
KINSINGTON ST	DEVON DR	MANCHESTER ST	KSGTST	020	1,295	36	46,620	R	ST	48	49	100	$51,800	21,315	CAPE SEAL
LINDEN RD	BASTONE CT	REDWOOD AV	LINDRD	030	980	30	29,400	A	ST	45	48	100	$35,934	37,603	CAPE SEAL
LINDEN RD	MOJAVE DR	CLARKSBURG BRANCHLINE	LINDRD	060	800	44	35,200	A	ST	60	61	100	$43,023	25,870	CAPE SEAL
MAY ST	BRYTE AV	MYRTLE AV	MAYSTR	010	690	33	22,770	R	ST	47	49	100	$25,300	21,625	CAPE SEAL
MEADOW RD	ELEVENTH ST	WEBSTER ST	MEADRD	030	825	32	26,400	R	ST	38	41	100	$29,334	23,741	CAPE SEAL
OATES DR	RAMOS DR	CARLIN DR	OATEDR	010	1,550	52	80,600	R	ST	45	48	100	$89,556	22,750	CAPE SEAL
PARK CR	END	PARK BL	PARKCR	010	125	30	6,568	C	ST	40	42	100	$8,028	28,045	CAPE SEAL
RICE AV	HARBOR BL	RICE AV	RICEAV	010	1,474	26	38,324	C	ST	46	47	100	$46,841	26,195	CAPE SEAL
RIVERBANK RD	TODHUNTER AV	END	RIVERD	040	1,680	36	60,480	C	ST	43	45	100	$73,920	27,225	CAPE SEAL
SAWBILLS CT	END	TEAL DR	SAWBCT	010	208	32	8,846	R	ST	37	40	100	$9,829	23,917	CAPE SEAL
SEYMORE AV	GLADYS AV	MARSHALL RD	SEYMAV	020	1,435	17	24,395	R	ST	8	13	100	$27,106	25,056	CAPE SEAL
SOUTHPORT PW	1700' W/O Lake Wa	3400' W/O Lake Wa	SOUTPW	020	1,700	50	85,000	A	ST	20	24	100	$103,889	41,414	CAPE SEAL
SOUTHPORT PW	3400' W/O Lake Wa	RAMCO ST	SOUTPW	030	1,850	50	92,500	A	ST	15	20	100	$113,056	41,414	CAPE SEAL
SOUTHPORT PW	RAMCO ST	CARLIN DR	SOUTPW	050	1,905	79	150,495	A	ST	45	48	100	$183,939	38,666	CAPE SEAL
SOUTHPORT PW	CARLIN DR	PROMENADE WY	SOUTPW	060	2,215	55	121,825	A	ST	42	45	100	$148,898	39,235	CAPE SEAL
TODHUNTER AV	MAY ST	RIVERBANK RD	TODHAV	020	1,905	36	68,580	C	ST	0	43	100	$83,820	27,672	CAPE SEAL
										Treatment Total			$1,512,731		
ELEVENTH ST	MEADOW RD	PARK BL	ELVTST	020	270	32	8,640	R	AC	39	40	100	$20,640	21,286	THIN AC OVERLAY(2 INCHES)
OAKMONT WY	BRIARWOOD DR	ORCHARD WY	OAKMWY	020	145	32	4,640	R	AC	46	47	100	$11,085	20,447	THIN AC OVERLAY(2 INCHES)
										Treatment Total			$31,725		
BARBERRY PL	W CDS	RIVERMONT ST	BERYPL	010	269	32	11,874	R	AC	81	82	89	$7,257	28,605	SLURRY SEAL
HAITI RD	GUADELUPE ST	DOMINICA ST	HAITIRD	010	890	33	29,370	R	AC	79	80	87	$17,949	28,798	SLURRY SEAL
HARBOR BL	I80	RIVERBANK RD	HARBBL	020	560	26	14,560	A	AC	73	74	83	$9,707	26,088	SLURRY SEAL

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2018

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
HENSHAW RD	SOUTHPORT PW	EAGLE ST	HENSRD	020	200	32	6,400	R	AC	84	85	91	$3,912	26,104	SLURRY SEAL
HORNBY ISL ST	OAKLAND BAY DR	MARSHALL RD	HORBST	010	1,480	35	51,800	R	AC	82	83	90	$31,656	29,367	SLURRY SEAL
JAMAICA ST	OAKLAND BAY DR	MARSHALL RD	JAMAST	010	640	33	21,120	R	AC	78	79	87	$12,907	28,025	SLURRY SEAL
LEMON TREE RD	REDWOOD AV	PEACHTREE ST	LEMORD	030	185	36	6,660	R	AC	80	81	88	$4,070	29,607	SLURRY SEAL
MALIBU CT	REDONDO RD	END N	MALICT	010	344	33	14,527	R	AC	79	80	87	$8,878	27,996	SLURRY SEAL
MICHIGAN BL	ROCKROSE RD	JEFFERSON BL	MICHBL	040	165	54	8,910	C	AC	81	82	89	$5,940	19,857	SLURRY SEAL
OTIS AV	SOUTHPORT PW	KAWEAH ST	OTISAV	010	1,715	32	54,880	C	AC	83	84	91	$36,587	28,015	SLURRY SEAL
OTIS AV	BRIDGEWY DR	MARSHALL RD	OTISAV	030	1,730	32	55,360	C	AC	73	74	83	$36,907	23,240	SLURRY SEAL
POPPY ST	RIVERMONT ST	LILY PL	POPPST	010	1,255	32	40,160	R	AC	82	83	90	$24,543	29,367	SLURRY SEAL
SOUTHPORT PW	MARSHALL RD	BRITTON PL	SOUTPW	110	1,855	36	66,780	A	AC	81	82	89	$44,520	40,072	SLURRY SEAL
SOUTHPORT PW	BRITTON PL	SILVERWOOD RD	SOUTPW	120	1,860	46	85,500	A	AC	81	82	89	$57,000	40,072	SLURRY SEAL
STONEGATE DR	LA JOLLA ST	MESA GRAN ST	STONDR	020	850	32	27,200	A	AC	79	80	88	$18,134	37,714	SLURRY SEAL
STONEGATE DR	LAKE WASH BL	REDONDO RD	STONDR	050	365	36	13,140	A	AC	81	82	89	$8,760	22,615	SLURRY SEAL
STONEGATE DR	REDONDO RD	LINDEN RD	STONDR	060	1,155	32	36,960	A	AC	79	80	88	$24,640	39,385	SLURRY SEAL
STONEGATE DR	LINDEN RD	MALLARD RD	STONDRS	010	179	32	5,728	C	AC	80	81	88	$3,819	24,262	SLURRY SEAL
STONEGATE DR	MALLARD RD	MUSCOVY RD	STONDRS	020	770	32	24,640	C	AC	82	83	90	$16,427	25,920	SLURRY SEAL
SYCAMORE AV	EVERGREEN AV	EVERGREEN AV	SYCAAV	010	135	30	4,050	C	AC	77	78	86	$2,700	19,720	SLURRY SEAL
TOBAGO ST	TRINIDAD RD	ST CROIX RD	TOBAST	010	665	33	21,945	R	AC	79	80	87	$13,411	28,798	SLURRY SEAL
VILLAGE PW	REDDING RD	ELK VALLE ST	VILGPW	020	555	69	38,295	A	AC	79	80	88	$25,530	37,714	SLURRY SEAL
										Treatment Total			$415,254		
SACRAMENTO AV	EIGHTH ST	SIXTH ST	SACRAV	080	619	38	23,522	A	AC	73	74	77	$146	740,764	SEAL CRACKS
SILVERADO ST	SANDYPOINT RD	QUAIL RD	SILVST	020	225	32	7,200	R	AC	73	74	76	$45	508,552	SEAL CRACKS
										Treatment Total			$191		

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year 2018 Area Total 1,968,861 Year 2018 Total $1,998,541

Year: 2019

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
GATEWAY DR	JEFFERSON BL	N BEACH AV	GATEDR	010	1,190	32	38,080	C	AC	41	40	100	$137,279	16,456	2" MILL AND FILL
GOLDEN GATE DR	MARSHALL RD	OAKLAND BAY DR	GOGADR	080	708	33	23,364	C	AC	43	42	100	$84,228	16,343	2" MILL AND FILL
GREGORY AV	1500' N/ BEVAN	JEFFERSON BL	GREGAV	040	1,830	32	58,560	A	AC	62	61	100	$191,004	20,988	2" MILL AND FILL
THIRD ST	C ST	SECOND ST	THRDST	020	350	46	16,100	C	AC	42	41	100	$58,041	16,397	2" MILL AND FILL
										Treatment Total			$470,552		
DAVIS RD	1800' E/O ANTIOCH	SOUTH RIV RD	DAVIRD	060	1,895	19	36,005	A	ST	23	20	100	$45,327	40,208	CAPE SEAL
FIFTEENTH ST	PARK BL	MARYLAND AV	FIFNST	010	300	43	12,900	A	ST	50	49	100	$16,240	34,642	CAPE SEAL
HOBSON AV	SOLANO ST	BRYTE AV	HOBSAV	020	795	36	28,620	R	ST	50	49	100	$32,754	20,887	CAPE SEAL
HOUSTON ST	CANAL ST	WEST CAPI AV	HOUSST	020	300	27	8,100	R	ST	23	21	100	$9,270	24,326	CAPE SEAL
LAKE WASHIGNTON BL	STOPLIGHT AT SOUTHPORT TOWN CE	SOUTHPORT PW	LAKEBL	060	916	110	100,760	A	ST	64	63	100	$126,846	24,631	CAPE SEAL
LINDEN RD	CONSTITUTION AV	BRIDGE ST	LINDRD	N200	1,864	30	60,122	A	ST	64	63	100	$75,687	21,170	CAPE SEAL
LISBON AV	BRYTE AV	TODHUNTER AV	LISBAV	030	1,365	34	46,410	R	ST	49	48	100	$53,114	21,311	CAPE SEAL
OLIVE CT	SUMMERFIE DR	END	OLIVCT	010	613	33	22,255	R	ST	50	49	100	$25,470	20,959	CAPE SEAL
PEKINS CT	TEAL DR	END	PEKICT	020	83	63	4,975	R	ST	22	19	100	$5,694	24,326	CAPE SEAL
PHESANT HOLLOW DR	JEFFERSON BL	STARLING LN	PHEADR	020	195	33	6,435	R	ST	68	67	100	$7,365	14,703	CAPE SEAL
RAMCO ST	CARLIN DR	S END	RAMCST	020	1,585	52	82,420	C	ST	50	48	100	$103,758	27,296	CAPE SEAL
REED AV	RIVERSIDE	STILLWATE RD	REEDAV	010	2,000	62	137,044	A	ST	62	61	100	$172,524	23,675	CAPE SEAL
REED AV	HARBOR BL	SUNSET AV	REEDAV	070	950	77	73,150	A	ST	61	60	100	$92,088	24,910	CAPE SEAL
SUMMERFIELD DR	JACQUELYN LN	LINDEN RD	SUMMDR	010	1,545	36	55,620	R	ST	50	49	100	$63,654	20,959	CAPE SEAL
										Treatment Total			$829,791		
CANNA WY	ELEVENTH ST	WEBSTER ST	CANNWY	010	560	32	17,920	R	AC	41	40	100	$44,094	20,712	THIN AC OVERLAY(2 INCHES)
FIRST ST	W END	SUTTER AV	FIRST	010	1,000	23	23,000	R	AC	41	40	100	$56,593	20,712	THIN AC OVERLAY(2 INCHES)

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2019

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
MOSSWOOD CR	END	LAKEWOOD DR	MOSSCR	010	180	31	8,124	R	AC	41	40	100	$19,990	20,712	THIN AC OVERLAY(2 INCHES)
WATER ST	END	RIVERBANK RD	WATEST	010	2,025	32	64,800	R	AC	41	40	100	$159,444	20,683	THIN AC OVERLAY(2 INCHES)
										Treatment Total			$280,121		
CEDARBROOK RD	NEWBERRY ST	MARLIN ST	CEDERD	020	650	32	20,800	R	AC	81	81	88	$13,093	27,069	SLURRY SEAL
CORTINA RD	REDDING RD	BARONA ST	COTRD	010	545	33	17,985	R	AC	85	85	91	$11,321	26,667	SLURRY SEAL
DOUGLAS ST	CUMMINS WY	LIGHTHOUS DR	DOUGST	030	710	36	25,560	C	AC	79	78	86	$17,552	19,146	SLURRY SEAL
GOLD HILL ST	SANDY POI RD	QUAIL RD	GOLDST	010	530	32	16,960	R	AC	83	83	90	$10,676	27,571	SLURRY SEAL
LAKE WASHIGNTON BL	VILLAGE PW	REDWOOD AV	LAKEBL	010	1,950	30	58,500	A	AC	77	76	85	$40,170	35,630	SLURRY SEAL
LILY PL	W CDS	POPPY ST	LILYPL	010	514	32	19,714	R	AC	80	80	87	$12,409	27,172	SLURRY SEAL
MARSHALL RD	SOUTHPORT PW	MARTINIQU ST	MARSRD	020	1,110	32	35,520	A	AC	73	72	81	$24,391	35,198	SLURRY SEAL
MONTE VISTA ST	CHINOOK RD	MONTE VISTA	MONTST	010	670	32	21,440	R	AC	83	83	90	$13,496	27,571	SLURRY SEAL
OAKLAND BAY DR	MARTINIQU ST	TACOMA N ST	OAKLDR	030	1,340	33	47,520	C	AC	81	80	88	$32,631	19,373	SLURRY SEAL
RICE AV	RICE AV	MAPLE ST	RICEAV	020	984	40	39,360	C	AC	83	82	89	$27,028	19,283	SLURRY SEAL
SACRAMENTO AV	EIGHTH ST	SIXTH ST	SACRAV	080	619	38	23,522	A	AC	73	74	83	$16,152	25,325	SLURRY SEAL
SANTA BARBARA ST	CATALINA RD	SAN NICOLAS	SANBST	010	220	33	7,260	R	AC	77	77	85	$4,570	26,965	SLURRY SEAL
SAN CARLOS CT	REDONDO RD	END N	SANCCT	010	344	33	14,527	R	AC	83	83	90	$9,144	27,571	SLURRY SEAL
SOUTHPORT PW	OTIS AV	JEFFERSON BL	SOUTPW	140	1,435	36	51,660	A	AC	78	77	85	$35,474	35,353	SLURRY SEAL
SPOKANE RD	STONEGATE DR	HIGHLAND DR	SPOKRD	010	1,135	33	37,455	R	AC	83	83	90	$23,576	27,571	SLURRY SEAL
STONE CT	END S	STONE BL	STONBL	060	181	32	5,792	C	AC	76	75	83	$3,978	18,493	SLURRY SEAL
STONEGATE DR	MUSCOVY RD	COLUSA RD	STONDRS	030	1,460	32	46,720	C	AC	75	74	82	$32,082	19,129	SLURRY SEAL
TWITCHEL ISL RD	GOLDEN GA DR	END E	TWITRD	010	1,049	33	37,792	R	AC	78	78	86	$23,788	27,108	SLURRY SEAL
VILLAGE PW	ELK VALLE ST	LAKE WASHINGTON BLVD	VILGPW	030	2,546	28	71,288	A	AC	74	73	82	$48,952	32,259	SLURRY SEAL
WEST CAPITOL AV	GARDENT ST	300' N/O GARDENT ST	WECAAV	160	300	54	16,200	A	AC	80	79	87	$11,124	23,981	SLURRY SEAL
										Treatment Total			$411,607		
				Year 2019 Area Total			1,540,339			Year 2019 Total			$1,992,071		

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2020

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
CUMMINS WY	ARTHUR DR	INGLEWOOD DR	CUMMWY	010	1,565	36	56,340	C	AC	46	40	100	$209,199	15,925	2" MILL AND FILL
FIFTH ST	PAVEMENT CHANGE	TOWER CT	FIF5ST	210	275	41	11,275	A	AC	71	69	100	$37,879	16,394	2" MILL AND FILL
JEFFERSON BL	LINDEN RD	HIGGINS RD	JEFRBL	140	1,600	62	104,720	A	AC	65	61	100	$351,809	20,201	2" MILL AND FILL
LAKE WASHIGNTON BL	SILVERADO ST	STONEGATE DR	LAKEBL	040	610	51	31,110	A	AC	64	60	100	$104,515	20,442	2" MILL AND FILL
STONE BL	INDUSTRIA BL	CEBRIAN ST	STONBL	010	390	28	10,920	C	AC	47	42	100	$40,548	15,865	2" MILL AND FILL
										Treatment Total			$743,950		
DAVIS RD	SEYMORE AV	JEFFERSON BL	DAVIRD	010	1,500	19	28,500	A	ST	28	20	100	$36,955	39,037	CAPE SEAL
FATHOM PL	GEARY ST	END	FATH	010	309	35	13,372	R	ST	54	48	100	$15,763	21,767	CAPE SEAL
FRENCH AV	DAVIS RD	1500' N/O DAVIS	FREN	010	1,500	24	36,000	R	ST	31	24	100	$42,436	23,617	CAPE SEAL
F ST	FIFTH ST	THIRD ST	FSTREE	040	805	37	29,785	C	AC	73	69	78	$38,621	8,900	CAPE SEAL
GREYCALLS CT	END	TEAL DR	GRYLCT	010	248	32	7,936	R	ST	51	47	100	$9,355	20,889	CAPE SEAL
HUGHES CR	END	PARK BL	HGHSCR	010	450	21	9,450	R	ST	52	48	100	$11,140	20,553	CAPE SEAL
KEGLE DR	SACRAMENT AV	FREMONT BL	KEGLRD	010	590	45	26,550	C	ST	31	24	100	$34,427	28,106	CAPE SEAL
LINDEN RD	BRENDA WY	HIGGINS RD	LINDRD	N170	1,121	34	43,437	A	ST	66	63	100	$56,323	20,537	CAPE SEAL
MARECA WY	CANVASBAC WY	HIGGINS RD	MAREWY	010	1,750	36	63,000	R	ST	51	47	100	$74,263	20,889	CAPE SEAL
MERGANSERS CT	MARECA WY	END	MERGCT	020	218	33	9,307	R	ST	31	24	100	$10,971	23,617	CAPE SEAL
N BEACH AV	STONEGATE DR	GATEWAY DR	NBEAAV	020	735	33	24,255	C	ST	32	24	100	$31,451	28,106	CAPE SEAL
POWELL PL	SANSOME ST	END	POWEPL	010	269	32	11,871	R	ST	55	49	100	$13,994	22,008	CAPE SEAL
PROMENADE WY	SOUTHPORT PW	RAMCO ST	PROSWY	030	1,260	27	34,020	C	ST	65	62	100	$44,113	15,726	CAPE SEAL
RENEE CT	BETTY WY	END	RENECT	020	298	33	11,860	R	ST	29	21	100	$13,981	23,617	CAPE SEAL
SOUTHPORT PW	LAKE WASH BL	1700' W/O Lake Wa	SOUTPW	010	1,700	60	102,000	A	ST	27	18	100	$132,259	39,037	CAPE SEAL
										Treatment Total			$566,052		
CLAUDIA CT	BETTY WY	E CDS	CLUDCT	020	388	33	14,832	R	AC	43	40	100	$37,590	20,110	THIN AC OVERLAY(2 INCHES)

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2020

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
DORAN AV	MARIGOLD ST	GARNET ST	DORAAV	010	643	32	20,576	R	AC	44	40	100	$52,148	20,058	THIN AC OVERLAY(2 INCHES)
FERNDALE CR	END S	LAKEWOOD DR	FRNDCR	010	130	31	6,669	R	AC	47	44	100	$16,902	19,744	THIN AC OVERLAY(2 INCHES)
HEMLOCK ST	EVERGREEN AV	BUCKEYE DR	HEMLST	010	430	32	13,760	R	AC	45	41	100	$34,873	19,962	THIN AC OVERLAY(2 INCHES)
LONGCROFT ST	MEADOW RD	END N	LNGCST	020	211	32	6,752	R	AC	45	41	100	$17,113	19,962	THIN AC OVERLAY(2 INCHES)
MARIGOLD ST	PALOMAR AV	DORAN AV	MARIST	010	805	32	25,760	R	AC	45	41	100	$65,286	19,962	THIN AC OVERLAY(2 INCHES)
ORCHARD WY	OAKMONT WY	HICKORY WY	ORCHWY	010	310	32	9,920	R	AC	46	43	100	$25,141	19,852	THIN AC OVERLAY(2 INCHES)
ROSE CT	END	ELEVENTH ST	ROSECT	010	141	29	6,793	R	AC	44	40	100	$17,216	20,058	THIN AC OVERLAY(2 INCHES)
WESTWOOD CR	END	LAKEWOOD DR	WESTCR	010	180	31	8,219	R	AC	44	40	100	$20,831	20,058	THIN AC OVERLAY(2 INCHES)
										Treatment Total			$287,100		
ARROWHEAD ST	HENSHAW RD	S END	ARROST	010	1,245	32	39,840	R	AC	85	84	91	$25,830	26,511	SLURRY SEAL
BRIDGEWY LAKES DR	HENSHAW RD	BERRYESSA PL	BRIDDR	010	1,490	32	47,680	R	AC	85	84	91	$30,913	26,511	SLURRY SEAL
COLD SPRINGS RD	BERRY CRE RD	ROARING C ST	COLDRD	010	855	33	28,215	R	AC	85	84	91	$18,293	26,511	SLURRY SEAL
COYOTE RD	SPALDING CT	OTIS AV	COYARD	020	1,225	32	39,200	R	AC	85	84	91	$25,415	26,511	SLURRY SEAL
GRAND ST	RIVERFRONT	GATE	GRANST	010	86	38	3,268	R	AC	82	80	87	$2,119	19,538	SLURRY SEAL
JEFFERSON BL	SOUTH RIV RD	GATEWAY DR	JEFRBL	110	1,570	62	106,460	A	AC	73	70	79	$75,296	28,606	SLURRY SEAL
MAPLE ST	HOLLY ST	MICHIGAN BL	MAPLST	020	1,040	32	33,280	C	AC	84	81	88	$23,538	18,738	SLURRY SEAL
MILLERTON PL	ARROWHEAD ST	END E	MILLPL	010	374	32	11,968	R	AC	85	84	91	$7,760	26,511	SLURRY SEAL
N BEACH AV	COLUMBUS RD	STONEGATE DR	NBEAAV	010	400	33	13,200	C	AC	84	81	88	$9,336	18,738	SLURRY SEAL
OAKLAND BAY DR	MALCOLM I ST	SOUTHPORT PW	OAKLDR	010	1,455	32	46,560	C	AC	85	82	89	$32,931	18,721	SLURRY SEAL
OAKLAND BAY DR	TACOMA NA ST	GOLDEN GA DR	OAKLDR	040	715	33	23,595	C	AC	86	83	90	$16,688	18,437	SLURRY SEAL
PILLSBURY RD	MARTIS ST	PROSSER ST	PILLRD	010	495	32	15,840	R	AC	85	84	91	$10,270	26,511	SLURRY SEAL
SOUTHPORT PW	SILVERWOO RD	OTIS AV	SOUTPW	130	1,570	36	56,520	A	AC	75	73	81	$39,975	30,595	SLURRY SEAL

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2020

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
SOUTH RIVER RD	1800 SOUTH RIV RD	15TH ST	SOUTRD	180	2,077	28	58,156	A	AC	74	71	80	$41,132	24,427	SLURRY SEAL
THIRD ST	TOWER BRIDGE GATEWAY	BALLPARK DR	THRDST	060	540	46	24,840	C	AC	85	82	90	$17,569	18,638	SLURRY SEAL
WESTACRE RD	MANZANITA WY	MICHIGAN BL	WEACRD	010	905	42	32,580	A	AC	77	74	82	$23,043	24,595	SLURRY SEAL
										Treatment Total			$400,108		
SHELL ST	GREENBRIER RD	END	SHELST	010	530	32	16,960	R	AC	92	89	89	$42	389,458	SEAL CRACKS
										Treatment Total			$42		
				Year 2020 Area Total			1,377,151			Year 2020 Total			$1,997,252		

Year: 2021

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
JEFFERSON BL	SACRAMENTO AV	TRIANGLE CT	JEFRBL	010	985	81	79,785	A	AC	66	60	100	$276,081	20,061	2" MILL AND FILL
REED AV	I80 WB ONRAMP	I80	REEDAV	030	531	78	41,418	A	AC	66	60	100	$143,319	20,062	2" MILL AND FILL
TERMINAL ST	INDUSTRIA BL	BOATMAN AV	TERMST	010	580	61	35,380	C	AC	50	41	100	$135,313	15,453	2" MILL AND FILL
										Treatment Total			$554,713		
BRYTE AV	SACRAMENTO AVE	MAY ST	BRYTAV	010	1,570	32	50,240	C	ST	66	61	100	$67,099	16,048	CAPE SEAL
COSTA CT	CANVASBAC WY	E CDS	COSTCT	010	193	32	8,280	R	AC	66	63	73	$10,054	11,418	CAPE SEAL
HIGGINS RD	LINDEN RD	211' E/ SUMMERFIEL D DR	HIGGRD	010	1,120	36	40,320	C	ST	34	22	100	$53,850	27,288	CAPE SEAL
INDEPENDENCE AV	SAFFLOWER PL	LINDEN RD	INDPAV	030	340	60	20,400	R	ST	54	48	100	$24,769	20,086	CAPE SEAL
IRON WOOD WY	SPRUCE ST	LINDEN RD	IRONWY	010	1,120	36	40,320	R	ST	54	47	100	$48,955	20,839	CAPE SEAL
LINDEN RD	HIGGINS RD	SUMMERFIEL D DR	LINDRD	N180	1,811	30	63,675	A	ST	54	48	100	$85,042	33,200	CAPE SEAL
LINDEN RD	JEFFERSON BD	COFFEEBERR Y	LINDRD	S080	1,252	32	44,266	A	ST	69	64	100	$59,120	18,846	CAPE SEAL

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2021

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
LINDEN RD	SUMMERFIELD DR	BETTY WY	LINDRD	S110	1,811	34	63,675	A	ST	55	49	100	$85,042	32,556	CAPE SEAL
LINDEN RD	CANVASBACK WY	JEFFERSON BD	LINDRD	S140	1,207	35	46,447	A	ST	70	66	100	$62,033	19,730	CAPE SEAL
LISBON AV	SUNSET AV	WATER ST	LISBAV	010	1,000	36	36,000	R	ST	34	22	100	$43,710	22,929	CAPE SEAL
LISBON AV	WATER ST	BRYTE SV	LISBAV	020	1,070	36	38,520	R	ST	55	49	100	$46,769	19,697	CAPE SEAL
MEADOW RD	WEBSTER ST	END	MEADRD	020	221	42	9,372	R	ST	34	21	100	$11,379	22,929	CAPE SEAL
PINTAIL CT	END	SUMMERFIE DR	PINTCT	010	158	33	7,240	R	ST	33	20	100	$8,791	22,929	CAPE SEAL
PROMENADE WY	SOUTHPORT PW	RAMOS DR	PROMWY	020	900	52	47,700	C	ST	68	63	100	$63,706	14,375	CAPE SEAL
SAUSALITO RD	SANSOME ST	GATEWAY DR	SAUSRD	010	955	37	35,335	R	ST	58	47	100	$42,902	21,743	CAPE SEAL
TERMINAL ST	DEL MONTE ST	PORT ST	TERMST	030	1,030	46	47,380	C	ST	35	23	100	$63,279	27,288	CAPE SEAL
THOMAS PL	RAMCO ST	END	THOMPL	010	533	40	20,764	R	ST	55	49	100	$25,211	19,699	CAPE SEAL
										Treatment Total			$801,711		
ASH AV	WILLOW AV	PROCTOR AV	ASHAVE	010	280	32	8,960	R	AC	47	41	100	$23,390	19,398	THIN AC OVERLAY(2 INCHES)
DEERWOOD CR	DEERWOOD ST	END E	DRWD	010	190	31	8,529	R	AC	49	43	100	$22,265	19,196	THIN AC OVERLAY(2 INCHES)
MADRONE AV	WESTACRE RD	ROCKROSE RD	MADRAV	020	1,030	32	32,960	R	AC	47	41	100	$86,039	19,414	THIN AC OVERLAY(2 INCHES)
OAKMONT WY	W END	BRIARWOOD DR	OAKMWY	010	510	36	18,360	R	AC	47	41	100	$47,928	19,414	THIN AC OVERLAY(2 INCHES)
TRADEWINDS CR	END	PORT ST	TRADCR	010	313	41	14,251	R	AC	46	40	100	$37,201	19,488	THIN AC OVERLAY(2 INCHES)
										Treatment Total			$216,823		
CRESTED ST	MUSCOVY RD	MALLARD RD	CRESTST	010	757	32	24,224	R	AC	86	84	91	$16,177	25,845	SLURRY SEAL
DELTA LN	W END	RISKE LN	DELTLN	010	545	41	22,345	A	AC	85	80	87	$16,278	22,387	SLURRY SEAL
DONNER RD	SHASTA WY	END E	DONNRD	010	610	32	19,520	R	AC	86	84	91	$13,036	25,845	SLURRY SEAL
FIFTH ST	TOWER BRIDGE GATEWAY	WEST CAPI AV	FIF5ST	240	74	75	5,550	A	AC	91	85	92	$4,044	17,588	SLURRY SEAL
FIFTEENTH ST	JEFFERSON BL	SOUTH RIV RD	FIFTST	010	895	48	39,380	A	AC	79	74	82	$28,688	23,879	SLURRY SEAL
GRAND ST	GATE	FIFTH	GRANST	020	86	38	3,268	R	AC	82	78	86	$2,183	19,369	SLURRY SEAL

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2021

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
HARBOR BL	REED AV	COP 382' S/O RIVERPOINT CT	HARBBL	050	1,301	60	78,060	A	AC	80	75	83	$56,866	23,816	SLURRY SEAL
HAVASU ST	SILVERWOO RD	LEWISTON RD	HAVAST	010	270	32	8,640	R	AC	84	82	89	$5,770	25,413	SLURRY SEAL
LIGHTHOUSE DR	DOUGLAS ST	FOUNTAIN DR	LIHODR	030	1,330	38	56,540	C	AC	78	72	81	$41,189	16,687	SLURRY SEAL
MERKLEY AV	EL RANCHO CT	159' N/O EL RANCHO CT	MERKAV	040	159	60	9,540	C	AC	87	82	89	$6,950	18,172	SLURRY SEAL
MERKLEY AV	327' N/O EL RANCHO CT	WEST CAPI AV	MERKAV	060	253	30	7,590	C	AC	88	83	90	$5,530	17,886	SLURRY SEAL
OTIS AV	KAWEAH ST	BRIDGEWAY DR	OTISAV	020	1,520	32	48,620	C	AC	87	82	89	$35,419	18,172	SLURRY SEAL
ROCK CREEK RD	HUME ST	BRIDEWAY LAKES	ROCKCRD	010	791	32	25,312	R	AC	85	83	90	$16,903	26,192	SLURRY SEAL
SILVERWOOD RD	SOUTHPORT PW	TAHOW ST	SILWRD	020	1,405	32	44,940	R	AC	85	83	90	$30,010	26,192	SLURRY SEAL
STONE BL	CAROLINA AV	JEFFERSON BL	STONBL	050	286	40	11,440	C	AC	88	83	90	$8,334	17,886	SLURRY SEAL
STONEGATE DR	MESA GRAN ST	N BEACH AV	STONDR	030	655	32	20,960	A	AC	81	76	84	$15,270	23,723	SLURRY SEAL
STONEGATE DR	N BEACH CT	LAKE WASHINGTON BL	STONDR	040	1,215	32	38,880	A	AC	83	78	86	$28,324	23,244	SLURRY SEAL
TAHOE ST	SQUAW RD	COYOTE RD	TAHOST	020	1,550	32	49,600	R	AC	85	83	90	$33,122	26,192	SLURRY SEAL
VILLAGE PW	STONEGATE DR	REDDING RD	VILGPW	010	750	56	46,320	A	AC	90	84	91	$33,744	18,762	SLURRY SEAL
WHITNEY PL	LASSEN ST	END W	WHITPL	010	142	32	4,224	R	AC	86	84	91	$2,821	25,845	SLURRY SEAL
										Treatment Total			$400,658		
HARBOR BL	I80	RIVERBANK RD	HARBBL	020	560	26	14,560	A	AC	73	76	78	$90	707,021	SEAL CRACKS
SOUTHPORT PW	MARSHALL RD	BRITTON PL	SOUTPW	110	1,855	36	66,780	A	AC	81	86	87	$221	1,268,009	SEAL CRACKS
SOUTHPORT PW	BRITTON PL	SILVERWOOD RD	SOUTPW	120	1,860	46	85,500	A	AC	81	86	87	$282	1,268,009	SEAL CRACKS
STONEGATE DR	LA JOLLA ST	MESA GRAN ST	STONDR	020	850	32	27,200	A	AC	79	84	85	$106	1,162,319	SEAL CRACKS
STONEGATE DR	LAKE WASH BL	REDONDO RD	STONDR	050	365	36	13,140	A	AC	81	82	84	$57	746,057	SEAL CRACKS

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2021

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
STONEGATE DR	REDONDO RD	LINDEN RD	STONDR	060	1,155	32	36,960	A	AC	79	84	85	$142	1,218,622	SEAL CRACKS
VILLAGE PW	REDDING RD	ELK VALLE ST	VILGPW	020	555	69	38,295	A	AC	79	84	85	$149	1,162,319	SEAL CRACKS
										Treatment Total			$1,047		
				Year 2021 Area Total			1,706,965			Year 2021 Total			$1,974,952		

Year: 2022

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
ANNA ST	KEGLE DR	HARDY DR	ANNAST	050	1,000	35	35,000	C	AC	56	42	100	$137,875	14,913	2" MILL AND FILL
LAKE WASHIGNTON BL	STONEGATE DR	STOPLIGHT AT SOUTHPORT TOWN CE	LAKEBL	050	690	73	50,370	A	AC	68	60	100	$179,525	19,344	2" MILL AND FILL
MARSHALL RD	W END	SOUTHPORT PW	MARSRD	010	1,540	32	49,280	A	AC	67	60	100	$175,640	19,188	2" MILL AND FILL
MARSHALL RD	MARTINIQU ST	GUADELUPE ST	MARSRD	030	1,010	32	32,320	A	AC	68	61	100	$115,193	18,661	2" MILL AND FILL
										Treatment Total			$608,233		
ALLAN AV	HIGGINS RD	HART AV	ALANAV	010	2,010	20	40,200	R	ST	36	18	100	$50,273	22,262	CAPE SEAL
AYLESBURY CT	W CDS	MARECA WY	ALBYCT	010	383	32	14,360	R	ST	36	18	100	$17,959	22,262	CAPE SEAL
BANDALIN CT	MARECA WY	S CDS	BNDLCT	010	288	33	11,532	R	ST	57	49	100	$14,422	19,220	CAPE SEAL
DAVIS RD	GREGORY AV	PARTRIDGE AV	DAVIRD	030	1,655	20	33,100	A	ST	20	69	100	$45,534	12,513	CAPE SEAL
INDEPENDENCE AV	DUET DR	DRIFTWOOD CT	INDPAV	010	785	36	28,260	R	ST	57	49	100	$35,341	19,219	CAPE SEAL
LINDEN RD	JEFFERSON BD	CANVASBACK WY	LINDRD	N150	1,207	27	40,993	A	ST	72	67	100	$56,391	19,464	CAPE SEAL
LINDEN RD	SUMMERFIELD DR	CONSTITUTION AV	LINDRD	N190	935	30	30,151	A	ST	57	49	100	$41,477	31,769	CAPE SEAL
LINDEN RD	BETTY WY	BRENDA WY	LINDRD	S120	1,198	34	40,732	A	ST	69	62	100	$56,032	20,639	CAPE SEAL
LINDEN RD	BRENDA WY	CANVASBACK WY	LINDRD	S130	1,044	34	35,496	A	ST	69	62	100	$48,830	20,639	CAPE SEAL
MULVANY PL	RAMCO ST	END	MULVPL	010	518	40	23,279	R	ST	56	47	100	$29,112	19,598	CAPE SEAL
RAMCO ST	END	CARLIN DR	RAMCST	010	2,205	52	114,660	C	ST	56	47	100	$157,729	23,324	CAPE SEAL

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2022

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
SHARON CT	END	BETTY WY	SHARCT	010	518	33	19,120	R	ST	36	18	100	$23,911	22,262	CAPE SEAL
TEAL DR	HIGGINS RD	SUMMERFIELD DR	TEALDR	010	1,340	36	48,240	R	ST	57	49	100	$60,328	19,219	CAPE SEAL
TERESA LN	DIANE DR	SHIRLEY ST	TERELN	010	510	36	18,360	R	ST	56	47	100	$22,961	19,597	CAPE SEAL
TODHUNTER AV	SACRAMENTO AVE	MAY ST	TODHAV	010	2,103	36	75,708	C	ST	36	18	100	$104,146	26,493	CAPE SEAL
										Treatment Total			$764,446		
GARNET ST	WEST CAPI AV	DORAN AV	GARNST	010	1,083	32	34,656	R	AC	48	40	100	$93,181	18,963	THIN AC OVERLAY(2 INCHES)
HAVERHILL ST	GRANDE VI AV	MEADOW RD	HVHR	010	810	34	27,540	R	AC	48	40	100	$74,048	18,963	THIN AC OVERLAY(2 INCHES)
SEVENTEENTH ST	VERMONT AV	JEFFERSON BL	SEVEST	040	270	23	6,210	R	AC	53	45	100	$16,697	18,410	THIN AC OVERLAY(2 INCHES)
SEVENTH ST	E ST	F ST	SVNTST	010	440	35	15,400	R	AC	48	40	100	$41,407	18,963	THIN AC OVERLAY(2 INCHES)
										Treatment Total			$225,333		
BARONA ST	CORANADO RD	LA JOLLA ST	BAROST	010	1,440	33	47,520	R	AC	84	81	88	$32,685	24,899	SLURRY SEAL
BLACK BUTTE RD	STONEYFORD RD	HUME ST	BLACRD	010	1,041	32	33,312	R	AC	87	84	91	$22,913	25,211	SLURRY SEAL
COLLINS ST	TOPAZ RD	N CDS	COLLST	010	1,394	32	47,874	R	AC	84	81	88	$32,929	24,899	SLURRY SEAL
COLUMBUS RD	N BEACH AV	230' E/ NBEACH	COLURD	010	230	33	7,590	R	AC	83	80	87	$5,221	24,089	SLURRY SEAL
E ST	SEVENTH ST	WIDTH CHANGE	ESTREE	030	613	30	18,390	R	AC	84	81	88	$12,649	24,899	SLURRY SEAL
MAPLE ST	RICE AV	HOLLY ST	MAPLST	010	455	32	14,560	C	AC	95	88	94	$10,925	14,803	SLURRY SEAL
MARKET ST	RIVERFRONT	GATE	MARKST	010	79	22	1,738	R	AC	88	82	89	$1,196	14,442	SLURRY SEAL
PRINCETON RD	HIGHLAND DR	460' E/ HIGHLAND	PRINRD	010	460	32	14,720	R	AC	83	80	88	$10,125	24,945	SLURRY SEAL
REDWOOD CT	BREENBRIER RD	END	REDWCT	010	85	32	2,720	R	AC	86	81	88	$1,871	15,507	SLURRY SEAL
STONEGATE DR	VILLAGE PW	LA JOLLA ST	STONDR	010	725	32	23,200	A	AC	94	84	91	$17,408	17,821	SLURRY SEAL
VILLAGE PW	LAKE WASHINGTON BLVD	LINDEN RD	VILGPW	040	1,075	32	34,400	A	AC	92	83	90	$25,812	19,125	SLURRY SEAL
WEST CAPITOL AV	JEFFERSON BL	MERKLEY AV	WECAAV	140	1,320	71	97,320	A	AC	86	79	86	$73,024	22,351	SLURRY SEAL
WEST CAPITOL AV	MERKLEY AV	GARDENT ST	WECAAV	150	1,006	56	59,936	A	AC	90	82	89	$44,973	20,320	SLURRY SEAL

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Treatment Total $291,731

BARBERRY PL	W CDS	RIVERMONT ST	BERYPL	010	269	32	11,874	R	AC	81	86	87	$41	809,412	SEAL CRACKS
ELEVENTH ST	MEADOW RD	PARK BL	ELVTST	020	270	32	8,640	R	AC	39	87	88	$10	1,224,755	SEAL CRACKS
GREGORY AV	1500' N/ BEVAN	JEFFERSON BL	GREGAV	040	1,830	32	58,560	A	AC	62	86	87	$92	1,651,061	SEAL CRACKS
HAITI RD	GUADELUPE ST	DOMINICA ST	HAITIRD	010	890	33	29,370	R	AC	79	84	85	$116	801,543	SEAL CRACKS
HENSHAW RD	SOUTHPORT PW	EAGLE ST	HENSRD	020	200	32	6,400	R	AC	84	88	89	$18	763,639	SEAL CRACKS
HORNBY ISL ST	OAKLAND BAY DR	MARSHALL RD	HORBST	010	1,480	35	51,800	R	AC	82	87	88	$162	841,732	SEAL CRACKS
JAMAICA ST	OAKLAND BAY DR	MARSHALL RD	JAMAST	010	640	33	21,120	R	AC	78	83	84	$90	765,362	SEAL CRACKS
LAKE WASHIGNTON BL	VILLAGE PW	REDWOOD AV	LAKEBL	010	1,950	30	58,500	A	AC	77	81	82	$294	1,043,965	SEAL CRACKS
LEMON TREE RD	REDWOOD AV	PEACHTREE ST	LEMORD	030	185	36	6,660	R	AC	80	85	86	$25	835,097	SEAL CRACKS
MALIBU CT	REDONDO RD	END N	MALICT	010	344	33	14,527	R	AC	79	84	85	$58	775,085	SEAL CRACKS
MARSHALL RD	SOUTHPORT PW	MARTINIQU ST	MARSRD	020	1,110	32	35,520	A	AC	73	77	79	$219	983,382	SEAL CRACKS
MICHIGAN BL	ROCKROSE RD	JEFFERSON BL	MICHBL	040	165	54	8,910	C	AC	81	81	83	$43	562,442	SEAL CRACKS
OAKMONT WY	BRIARWOOD DR	ORCHARD WY	OAKMWY	020	145	32	4,640	R	AC	46	87	88	$6	1,224,755	SEAL CRACKS
OTIS AV	SOUTHPORT PW	KAWEAH ST	OTISAV	010	1,715	32	54,880	C	AC	83	86	87	$184	862,327	SEAL CRACKS
OTIS AV	BRIDGEWY DR	MARSHALL RD	OTISAV	030	1,730	32	55,360	C	AC	73	76	78	$362	576,725	SEAL CRACKS
POPLAR AV	EVERGREEN AV	BUCKEYE DR	POPLAV	020	345	32	11,040	C	AC	40	84	86	$31	835,269	SEAL CRACKS
POPPY ST	RIVERMONT ST	LILY PL	POPPST	010	1,255	32	40,160	R	AC	82	87	88	$126	841,732	SEAL CRACKS
SACRAMENTO AV	EIGHTH ST	SIXTH ST	SACRAV	080	619	38	23,522	A	AC	73	76	78	$149	688,003	SEAL CRACKS
SOUTHPORT PW	OTIS AV	JEFFERSON BL	SOUTPW	140	1,435	36	51,660	A	AC	78	81	83	$246	1,054,524	SEAL CRACKS
STONEGATE DR	LINDEN RD	MALLARD RD	STONDRS	010	179	32	5,728	C	AC	80	82	84	$26	684,429	SEAL CRACKS
STONEGATE DR	MALLARD RD	MUSCOVY RD	STONDRS	020	770	32	24,640	C	AC	82	84	86	$93	767,222	SEAL CRACKS
SYCAMORE AV	EVERGREEN AV	EVERGREEN AV	SYCAAV	010	135	30	4,050	C	AC	77	77	79	$25	504,577	SEAL CRACKS
TOBAGO ST	TRINIDAD RD	ST CROIX RD	TOBAST	010	665	33	21,945	R	AC	79	84	85	$87	801,543	SEAL CRACKS

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2022

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
VILLAGE PW	ELK VALLE ST	LAKE WASHINGTON BLVD	VILGPW	030	2,546	28	71,288	A	AC	74	77	79	$433	890,906	SEAL CRACKS
WEST CAPITOL AV	GARDENT ST	300' N/O GARDENT ST	WECAAV	160	300	54	16,200	A	AC	80	81	82	$82	724,960	SEAL CRACKS
										Treatment Total			$3,018		
				Year 2022 Area Total			1,925,241			Year 2022 Total			$1,892,761		

Year: 2023

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
ANNA ST	HOLLAND DR	KEGLE DR	ANNAST	040	1,065	35	37,275	C	AC	59	42	100	$151,242	14,501	2" MILL AND FILL
GREGORY AV	1300' N/ SO. RIVER	BEVAN RD	GREGAV	020	1,525	34	51,850	A	AC	71	60	100	$190,344	18,803	2" MILL AND FILL
LAKE WASHIGNTON BL	REDWOOD AV	HIGHLAND ST	LAKEBL	020	1,424	38	59,533	A	AC	70	60	100	$218,548	18,472	2" MILL AND FILL
MERKLEY AV	JEFFERSON BL	EL RANCHO CT	MERKAV	030	980	40	39,200	C	AC	58	41	100	$159,053	14,569	2" MILL AND FILL
WATERFRONT PL	THIRD ST	WATERFRONT PL	WAFRPL	010	395	42	16,590	C	AC	60	43	100	$67,314	14,435	2" MILL AND FILL
										Treatment Total			$786,501		
DAVIS RD	ANTIOCH AV	1800' E/O ANTIOCH	DAVIRD	050	1,800	19	34,200	A	ST	18	67	100	$48,458	14,689	CAPE SEAL
FIFTEENTH ST	MARYLAND AV	VIRGINIA AV	FIFNST	020	1,345	34	45,730	A	ST	59	48	100	$64,795	30,935	CAPE SEAL
LETTERMAN ST	GEARY ST	UNION SQ RD	LETTST	010	530	36	19,080	R	ST	64	44	100	$24,577	20,881	CAPE SEAL
LINDEN RD	BASTONE CT	REDWOOD AV	LINDRD	030	980	30	29,400	A	ST	45	67	100	$41,657	14,689	CAPE SEAL
LINDEN RD	BRIDGE ST	JEFFERSON BD	LINDRD	N210	1,203	30	40,292	A	ST	71	62	100	$57,090	20,233	CAPE SEAL
LINDEN RD	COFFEEBERRY	CONSTITUTION AV	LINDRD	S090	1,787	30	57,812	A	ST	72	63	100	$81,914	19,277	CAPE SEAL
MERCED WY	MOJAVE DR	RUBICON WY	MERCWY	010	995	36	35,820	R	ST	59	48	100	$46,140	18,715	CAPE SEAL
PARK CR	END	PARK BL	PARKCR	010	125	30	6,568	C	ST	40	67	100	$9,307	10,576	CAPE SEAL
PEKINS CT	END	TEAL DR	PEKICT	010	227	32	7,264	R	ST	59	48	100	$9,357	18,715	CAPE SEAL
RAMOS DR	OATES DR	PROMENADE WY	RAMODR	010	825	53	43,725	R	ST	63	43	100	$56,322	20,975	CAPE SEAL
RUBICON WY	SHASTA WY	MERCED WY	RUBIWY	010	1,075	32	34,400	R	ST	65	49	100	$44,311	20,265	CAPE SEAL

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2023

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
SANSOME ST	SAUSALITO RD	GATEWAY DR	SANSST	010	605	36	21,780	R	ST	63	43	100	$28,055	20,975	CAPE SEAL
SHASTA WY	RUBICON WY	MERCED WAY	SHASTWY	020	560	32	17,920	R	ST	65	49	100	$23,083	20,265	CAPE SEAL
SOLANO ST	LISBON AV	RIVERBANK RD	SOLAST	020	835	36	30,060	R	ST	59	48	100	$38,720	18,715	CAPE SEAL
TRINITY WY	E END	MOJAVE DR	TRINWY	010	1,025	32	33,706	R	ST	63	46	100	$43,417	20,579	CAPE SEAL
										Treatment Total			$617,203		
BOATMAN AV	INDUSTRIA BL	TERMINAL ST	BTMNAV	010	1,100	40	44,000	R	AC	50	40	100	$121,853	18,349	THIN AC OVERLAY(2 INCHES)
CLAUDIA CT	W CDS	BETTY WY	CLUDCT	010	373	33	14,337	R	AC	50	40	100	$39,705	18,351	THIN AC OVERLAY(2 INCHES)
										Treatment Total			$161,558		
BIG BEAR ST	HENSHAW RD	N CDS	BIGBST	010	459	32	17,954	R	AC	83	79	87	$12,720	23,429	SLURRY SEAL
ELK VALLEY ST	VILLAGE PW	BERRY CRE RD	ELKVST	010	240	33	7,920	R	AC	83	79	87	$5,611	23,429	SLURRY SEAL
HARBOR BL	I80	RIVERBANK RD	HARBBL	020	560	26	14,560	A	AC	73	74	83	$11,253	22,506	SLURRY SEAL
LEWISTON RD	EAGLE ST	TAHOE ST	LEWIRD	010	1,310	32	41,920	R	AC	84	80	88	$29,699	24,282	SLURRY SEAL
PYRAMID PL	END W	COLLINS ST	PYRAPL	010	314	32	13,311	R	AC	84	80	88	$9,431	24,282	SLURRY SEAL
REDDING RD	LA JOLLA ST	VILLAGE PW	REDDRD	010	1,150	33	37,950	R	AC	84	80	88	$26,886	24,282	SLURRY SEAL
SILVERWOOD RD	END W	SOUTHPORT PW	SILWRD	010	1,524	32	52,031	R	AC	84	80	88	$36,862	24,282	SLURRY SEAL
SOUTHPORT PW	MARSHALL RD	BRITTON PL	SOUTPW	110	1,855	36	66,780	A	AC	81	84	91	$51,611	33,290	SLURRY SEAL
SOUTHPORT PW	BRITTON PL	SILVERWOOD RD	SOUTPW	120	1,860	46	85,500	A	AC	81	84	91	$66,079	33,290	SLURRY SEAL
STONEGATE DR	LA JOLLA ST	MESA GRAN ST	STONDR	020	850	32	27,200	A	AC	79	82	90	$21,022	32,002	SLURRY SEAL
STONEGATE DR	LAKE WASH BL	REDONDO RD	STONDR	050	365	36	13,140	A	AC	81	80	87	$10,156	21,160	SLURRY SEAL
STONEGATE DR	REDONDO RD	LINDEN RD	STONDR	060	1,155	32	36,960	A	AC	79	83	90	$28,565	33,486	SLURRY SEAL
										Treatment Total			$309,895		
CANNA WY	ELEVENTH ST	WEBSTER ST	CANNWY	010	560	32	17,920	R	AC	41	87	88	$22	1,189,083	SEAL CRACKS
CEDARBROOK RD	NEWBERRY ST	MARLIN ST	CEDERD	020	650	32	20,800	R	AC	81	85	86	$80	758,968	SEAL CRACKS
CORTINA RD	REDDING RD	BARONA ST	COTRD	010	545	33	17,985	R	AC	85	88	89	$50	775,346	SEAL CRACKS

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2023

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
DOUGLAS ST	CUMMINS WY	LIGHTHOUS DR	DOUGST	030	710	36	25,560	C	AC	79	77	79	$158	489,837	SEAL CRACKS
FIFTH ST	PAVEMENT CHANGE	TOWER CT	FIF5ST	210	275	41	11,275	A	AC	71	86	87	$19	1,602,972	SEAL CRACKS
FIRST ST	W END	SUTTER AV	FIRST	010	1,000	23	23,000	R	AC	41	87	88	$28	1,189,083	SEAL CRACKS
GATEWAY DR	JEFFERSON BL	N BEACH AV	GATEDR	010	1,190	32	38,080	C	AC	41	84	86	$109	810,941	SEAL CRACKS
GOLDEN GATE DR	MARSHALL RD	OAKLAND BAY DR	GOGADR	080	708	33	23,364	C	AC	43	84	86	$67	810,941	SEAL CRACKS
GOLD HILL ST	SANDY POI RD	QUAIL RD	GOLDST	010	530	32	16,960	R	AC	83	87	88	$55	790,381	SEAL CRACKS
JEFFERSON BL	SOUTH RIV RD	GATEWAY DR	JEFRBL	110	1,570	62	106,460	A	AC	73	74	76	$772	751,506	SEAL CRACKS
JEFFERSON BL	LINDEN RD	HIGGINS RD	JEFRBL	140	1,600	62	104,720	A	AC	65	86	87	$169	1,602,972	SEAL CRACKS
LAKE WASHIGNTON BL	SILVERADO ST	STONEGATE DR	LAKEBL	040	610	51	31,110	A	AC	64	86	87	$51	1,602,972	SEAL CRACKS
LILY PL	W CDS	POPPY ST	LILYPL	010	514	32	19,714	R	AC	80	84	85	$81	752,324	SEAL CRACKS
MONTE VISTA ST	CHINOOK RD	MONTE VISTA	MONTST	010	670	32	21,440	R	AC	83	87	88	$70	790,381	SEAL CRACKS
MOSSWOOD CR	END	LAKEWOOD DR	MOSSCR	010	180	31	8,124	R	AC	41	87	88	$10	1,189,083	SEAL CRACKS
OAKLAND BAY DR	MARTINIQU ST	TACOMA N ST	OAKLDR	030	1,340	33	47,520	C	AC	81	79	81	$262	519,768	SEAL CRACKS
RICE AV	RICE AV	MAPLE ST	RICEAV	020	984	40	39,360	C	AC	83	81	83	$193	544,836	SEAL CRACKS
SANTA BARBARA ST	CATALINA RD	SAN NICOLAS	SANBST	010	220	33	7,260	R	AC	77	81	83	$37	717,840	SEAL CRACKS
SAN CARLOS CT	REDONDO RD	END N	SANCCT	010	344	33	14,527	R	AC	83	87	88	$48	790,381	SEAL CRACKS
SOUTHPORT PW	SILVERWOO RD	OTIS AV	SOUTPW	130	1,570	36	56,520	A	AC	75	76	78	$368	841,244	SEAL CRACKS
SOUTH RIVER RD	1800 SOUTH RIV RD	15TH ST	SOUTRD	180	2,077	28	58,156	A	AC	74	73	76	$436	626,409	SEAL CRACKS
SPOKANE RD	STONEGATE DR	HIGHLAND DR	SPOKRD	010	1,135	33	37,455	R	AC	83	87	88	$122	790,381	SEAL CRACKS
STONE CT	END S	STONE BL	STONBL	060	181	32	5,792	C	AC	76	74	77	$42	440,339	SEAL CRACKS
STONEGATE DR	MUSCOVY RD	COLUSA RD	STONDRS	030	1,460	32	46,720	C	AC	75	74	76	$341	452,336	SEAL CRACKS
THIRD ST	C ST	SECOND ST	THRDST	020	350	46	16,100	C	AC	42	84	86	$46	810,941	SEAL CRACKS
TWITCHEL ISL RD	GOLDEN GA DR	END E	TWITRD	010	1,049	33	37,792	R	AC	78	82	83	$177	729,977	SEAL CRACKS
WATER ST	END	RIVERBANK RD	WATEST	010	2,025	32	64,800	R	AC	41	87	88	$77	1,189,083	SEAL CRACKS
WESTACRE RD	MANZANITA WY	MICHIGAN BL	WEACRD	010	905	42	32,580	A	AC	77	76	78	$217	662,674	SEAL CRACKS

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

												Treatment Total		$4,107	
							Year 2023 Area Total		2,086,862			Year 2023 Total		$1,879,264	

Year: 2024

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
GOLDEN GATE DR	PROMENADE WY	HALF MOON CR	GOGADR	040	320	44	14,080	C	AC	65	48	100	$58,843	13,695	2" MILL AND FILL
MERKLEY AV	SYCAMORE AV	WESTACRE RD	MERKAV	010	1,830	32	58,560	C	AC	61	40	100	$244,733	14,160	2" MILL AND FILL
SOUTHPORT PW	OAKLAND BAY DR	ABACO RD	SOUTPW	090	950	55	52,250	A	AC	71	60	100	$197,566	18,027	2" MILL AND FILL
THIRD ST	B ST	C ST	THRDST	010	490	46	22,540	C	AC	61	40	100	$94,199	14,160	2" MILL AND FILL
										Treatment Total			$595,341		
ARLINGTON RD	END S	CROWN AV	ARLNRD	010	1,610	18	28,980	R	ST	61	46	100	$38,449	19,315	CAPE SEAL
FIFTEENTH ST	PARK BL	MARYLAND AV	FIFNST	010	300	43	12,900	A	ST	50	67	100	$18,827	14,261	CAPE SEAL
LAKE WASHIGNTON BL	SOUTHPORT PW	STONE BL	LAKEBL	090	2,640	62	163,680	A	ST	60	47	100	$238,875	30,431	CAPE SEAL
LINDEN RD	MOJAVE DR	CLARKSBURG BRANCHLINE	LINDRD	060	800	44	35,200	A	ST	60	65	100	$51,371	16,613	CAPE SEAL
LINDEN RD	CANVASBACK WY	BRENDA WY	LINDRD	N160	1,045	36	41,822	A	ST	78	65	100	$61,035	16,592	CAPE SEAL
NANCY LN	VIOLET DR	BRENDA WY	NANCLN	010	450	36	16,200	R	ST	61	48	100	$21,493	18,196	CAPE SEAL
ROCKROSE RD	WESTACRE RD	MICHIGAN BL	ROCKRD	020	1,270	35	44,450	R	ST	60	47	100	$58,973	18,565	CAPE SEAL
SHIRLEY ST	LESLIE LN	BETTY WY	SHIRST	010	470	36	16,920	R	ST	60	47	100	$22,449	18,564	CAPE SEAL
SOUTHPORT PW	1700' W/O Lake Wa	3400' W/O Lake Wa	SOUTPW	020	1,700	50	85,000	A	ST	20	65	100	$124,049	16,613	CAPE SEAL
SPRUCE ST	END	LINDEN RD	SPRUST	010	925	38	35,150	R	ST	61	48	100	$46,635	18,195	CAPE SEAL
										Treatment Total			$682,156		
CARRIGAN CT	DEERWOOD ST	END	CRRGCT	010	130	31	6,669	R	AC	53	40	100	$19,024	17,817	THIN AC OVERLAY(2 INCHES)
FERNWOOD ST	MEADOW RD	SONORA AV	FRNWST	010	255	32	8,160	R	AC	53	40	100	$23,277	17,817	THIN AC OVERLAY(2 INCHES)
GREENMEADOW AV	GARNET ST	PINE ST	GREMAV	010	725	32	23,200	R	AC	54	42	100	$66,178	17,718	THIN AC OVERLAY(2 INCHES)
JACQUELYN LN	SUMMERFIELD DR	JANET DR	JAQNLN	010	275	36	9,900	R	AC	56	44	100	$28,240	17,492	THIN AC OVERLAY(2 INCHES)

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2024

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
SUMMERFIELD DR	LINDEN RD	HIGGINS RD	SUMMDR	020	1,491	36	53,676	R	AC	53	40	100	$153,109	17,817	THIN AC OVERLAY(2 INCHES)
										Treatment Total			$289,828		
ALPINE ST	BRIDGEWAY LAKES	STONEYFORD RD	ALPIST	010	249	32	7,968	R	AC	84	79	86	$5,815	21,904	SLURRY SEAL
ATHERTON CT	END S	STONEGATE DR	ATHECT	010	330	33	14,065	R	AC	82	77	85	$10,264	22,546	SLURRY SEAL
BENTON ST	CHEROKEE RD	TRINITY WY	BENTST	020	655	32	20,960	R	AC	84	79	87	$15,295	22,779	SLURRY SEAL
LAKE WASHIGNTON BL	VILLAGE PW	REDWOOD AV	LAKEBL	010	1,950	30	58,500	A	AC	77	79	87	$46,569	31,134	SLURRY SEAL
MARSHALL RD	SOUTHPORT PW	MARTINIQU ST	MARSRD	020	1,110	32	35,520	A	AC	73	76	84	$28,276	32,021	SLURRY SEAL
MICHIGAN BL	ROCKROSE RD	JEFFERSON BL	MICHBL	040	165	54	8,910	C	AC	81	79	87	$7,093	16,562	SLURRY SEAL
MUSCOVY RD	WIGEON ST	CONDOR ST	MUSCRD	010	1,000	32	32,000	R	AC	84	79	87	$23,351	22,779	SLURRY SEAL
OTIS AV	SOUTHPORT PW	KAWEAH ST	OTISAV	010	1,715	32	54,880	C	AC	83	84	91	$43,687	23,529	SLURRY SEAL
POPLAR AV	EVERGREEN AV	BUCKEYE DR	POPLAV	020	345	32	11,040	C	AC	40	82	89	$8,789	15,767	SLURRY SEAL
SOUTHPORT PW	OTIS AV	JEFFERSON BL	SOUTPW	140	1,435	36	51,660	A	AC	78	80	88	$41,124	30,619	SLURRY SEAL
SQUAW RD	SILVERWOO RD	OTIS V	SQUARD	010	1,440	32	46,080	R	AC	84	79	87	$33,625	23,583	SLURRY SEAL
STONEGATE DR	LINDEN RD	MALLARD RD	STONDRS	010	179	32	5,728	C	AC	80	80	88	$4,560	20,095	SLURRY SEAL
STONEGATE DR	MALLARD RD	MUSCOVY RD	STONDRS	020	770	32	24,640	C	AC	82	83	90	$19,615	21,670	SLURRY SEAL
STURGEON LN	VILLAGE PKWY	END	STRLN	010	2,145	20	42,900	R	AC	83	78	86	$31,305	22,695	SLURRY SEAL
SYCAMORE AV	EVERGREEN AV	EVERGREEN AV	SYCAAV	010	135	30	4,050	C	AC	77	75	83	$3,224	15,944	SLURRY SEAL
VILLAGE PW	REDDING RD	ELK VALLE ST	VILGPW	020	555	69	38,295	A	AC	79	81	88	$30,485	31,491	SLURRY SEAL
VILLAGE PW	ELK VALLE ST	LAKE WASHINGTON BLVD	VILGPW	030	2,546	28	71,288	A	AC	74	76	84	$56,748	28,509	SLURRY SEAL
										Treatment Total			$409,825		
ARROWHEAD ST	HENSHAW RD	S END	ARROST	010	1,245	32	39,840	R	AC	85	88	88	$122	766,702	SEAL CRACKS
BRIDGEWY LAKES DR	HENSHAW RD	BERRYESSA PL	BRIDDR	010	1,490	32	47,680	R	AC	85	88	88	$146	766,702	SEAL CRACKS

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2024

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
CLAUDIA CT	BETTY WY	E CDS	CLUDCT	020	388	33	14,832	R	AC	43	87	88	$19	1,154,449	SEAL CRACKS
COLD SPRINGS RD	BERRY CRE RD	ROARING C ST	COLDRD	010	855	33	28,215	R	AC	85	88	88	$86	766,702	SEAL CRACKS
COYOTE RD	SPALDING CT	OTIS AV	COYARD	020	1,225	32	39,200	R	AC	85	88	88	$120	766,702	SEAL CRACKS
CUMMINS WY	ARTHUR DR	INGLEWOOD DR	CUMMWY	010	1,565	36	56,340	C	AC	46	84	86	$166	787,321	SEAL CRACKS
DELTA LN	W END	RISKE LN	DELTLN	010	545	41	22,345	A	AC	85	81	82	$118	686,201	SEAL CRACKS
DORAN AV	MARIGOLD ST	GARNET ST	DORAAV	010	643	32	20,576	R	AC	44	87	88	$26	1,154,449	SEAL CRACKS
FIFTH ST	TOWER BRIDGE GATEWAY	WEST CAPI AV	FIF5ST	240	74	75	5,550	A	AC	91	84	85	$23	661,449	SEAL CRACKS
FIFTEENTH ST	JEFFERSON BL	SOUTH RIV RD	FIFTST	010	895	48	39,380	A	AC	79	76	78	$271	641,935	SEAL CRACKS
FERNDALE CR	END S	LAKEWOOD DR	FRNDCR	010	130	31	6,669	R	AC	47	87	88	$9	1,154,449	SEAL CRACKS
GRAND ST	RIVERFRONT	GATE	GRANST	010	86	38	3,268	R	AC	82	82	83	$16	529,463	SEAL CRACKS
HARBOR BL	REED AV	COP 382' S/O RIVERPOINT CT	HARBBL	050	1,301	60	78,060	A	AC	80	77	79	$515	651,161	SEAL CRACKS
HEMLOCK ST	EVERGREEN AV	BUCKEYE DR	HEMLST	010	430	32	13,760	R	AC	45	87	88	$17	1,154,449	SEAL CRACKS
JEFFERSON BL	SACRAMENTO AV	TRIANGLE CT	JEFRBL	010	985	81	79,785	A	AC	66	86	87	$133	1,556,283	SEAL CRACKS
LONGCROFT ST	MEADOW RD	END N	LNGCST	020	211	32	6,752	R	AC	45	87	88	$9	1,154,449	SEAL CRACKS
MAPLE ST	HOLLY ST	MICHIGAN BL	MAPLST	020	1,040	32	33,280	C	AC	84	80	82	$179	513,448	SEAL CRACKS
MARIGOLD ST	PALOMAR AV	DORAN AV	MARIST	010	805	32	25,760	R	AC	45	87	88	$32	1,154,449	SEAL CRACKS
MILLERTON PL	ARROWHEAD ST	END E	MILLPL	010	374	32	11,968	R	AC	85	88	88	$37	766,702	SEAL CRACKS
N BEACH AV	COLUMBUS RD	STONEGATE DR	NBEAAV	010	400	33	13,200	C	AC	84	80	82	$71	513,415	SEAL CRACKS
OAKLAND BAY DR	MALCOLM I ST	SOUTHPORT PW	OAKLDR	010	1,455	32	46,560	C	AC	85	81	83	$235	529,008	SEAL CRACKS
OAKLAND BAY DR	TACOMA NA ST	GOLDEN GA DR	OAKLDR	040	715	33	23,595	C	AC	86	82	84	$112	545,062	SEAL CRACKS

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2024

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
ORCHARD WY	OAKMONT WY	HICKORY WY	ORCHWY	010	310	32	9,920	R	AC	46	87	88	$13	1,154,449	SEAL CRACKS
PILLSBURY RD	MARTIS ST	PROSSER ST	PILLRD	010	495	32	15,840	R	AC	85	88	88	$49	766,702	SEAL CRACKS
REED AV	I80 WB ONRAMP	I80	REEDAV	030	531	78	41,418	A	AC	66	86	87	$69	1,556,283	SEAL CRACKS
ROSE CT	END	ELEVENTH ST	ROSECT	010	141	29	6,793	R	AC	44	87	88	$9	1,154,449	SEAL CRACKS
STONE BL	INDUSTRIA BL	CEBRIAN ST	STONBL	010	390	28	10,920	C	AC	47	84	86	$32	787,321	SEAL CRACKS
STONEGATE DR	MESA GRAN ST	N BEACH AV	STONDR	030	655	32	20,960	A	AC	81	78	80	$133	660,602	SEAL CRACKS
STONEGATE DR	N BEACH CT	LAKE WASHINGTON BL	STONDR	040	1,215	32	38,880	A	AC	83	79	81	$224	671,088	SEAL CRACKS
THIRD ST	TOWER BRIDGE GATEWAY	BALLPARK DR	THRDST	060	540	46	24,840	C	AC	85	82	83	$123	535,598	SEAL CRACKS
VILLAGE PW	STONEGATE DR	REDDING RD	VILGPW	010	750	56	46,320	A	AC	90	84	85	$199	670,931	SEAL CRACKS
WESTWOOD CR	END	LAKEWOOD DR	WESTCR	010	180	31	8,219	R	AC	44	87	88	$11	1,154,449	SEAL CRACKS
										Treatment Total			$3,324		
				Year 2024 Area Total			2,138,546			Year 2024 Total			$1,980,474		

Year: 2025

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
LIGHTHOUSE DR	FOUNTAIN DR	MARINA WY	LIHODR	040	370	73	27,010	C	AC	64	40	100	$116,267	13,752	2" MILL AND FILL
STONE BL	CEBRIAN ST	PARK BL	STONBL	020	390	59	23,010	C	AC	65	42	100	$99,048	13,681	2" MILL AND FILL
TERMINAL ST	S END	INDUSTRIAL BL	TERMST	005	466	58	27,028	C	AC	64	40	100	$116,344	13,752	2" MILL AND FILL
										Treatment Total			$331,659		
BRYTE AV	MAY ST	RIVERBANK RD	BRYTAV	020	1,370	32	43,840	C	ST	42	63	100	$65,900	13,177	CAPE SEAL
CABLE CT	GATEWAY DR	END	CABLCT	010	99	44	6,563	R	ST	63	48	100	$8,969	17,667	CAPE SEAL
CANVASBACK WY	HIGGINS RD	AIRONS CT	CNVKWY	010	660	36	23,760	R	ST	63	48	100	$32,469	17,668	CAPE SEAL

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2025

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
DAVIS RD	SEYMORE AV	JEFFERSON BL	DAVIRD	010	1,500	19	28,500	A	ST	28	67	100	$42,841	13,846	CAPE SEAL
DAVIS RD	1800' E/O ANTIOCH	SOUTH RIV RD	DAVIRD	060	1,895	19	36,005	A	ST	23	65	100	$54,122	16,129	CAPE SEAL
GATEWAY DR	N BEACH AV	SAUSALITO RD	GATEDR	020	1,475	36	53,100	R	ST	62	47	100	$72,563	18,033	CAPE SEAL
KINSINGTON ST	ASTER AV	DEVON DR	KSGTST	010	830	36	29,880	R	ST	62	47	100	$40,832	18,032	CAPE SEAL
LINDEN RD	CONSTITUTION AV	SUMMERFIELD DR	LINDRD	S100	1,050	29	34,652	A	ST	78	63	100	$52,089	18,282	CAPE SEAL
SANDPIPER LN	PHESANT H DR	STARLING LN	SANDLN	010	510	33	16,830	R	ST	68	47	100	$22,999	19,269	CAPE SEAL
SOLANO ST	SACRAMENT AV	LISBON AV	SOLAST	010	1,540	36	55,440	R	ST	62	47	100	$75,761	18,032	CAPE SEAL
SOUTHPORT PW	3400' W/O Lake Wa	RAMCO ST	SOUTPW	030	1,850	50	92,500	A	ST	15	63	100	$139,045	18,302	CAPE SEAL
SOUTHPORT PW	RAMCO ST	CARLIN DR	SOUTPW	050	1,905	79	150,495	A	ST	45	63	100	$226,221	18,302	CAPE SEAL
SOUTHPORT PW	CARLIN DR	PROMENADE WY	SOUTPW	060	2,215	55	121,825	A	ST	42	63	100	$183,125	18,302	CAPE SEAL
STARLING LN	PHESANT H DR	END N	STARLN	020	550	33	18,150	R	ST	68	47	100	$24,803	19,269	CAPE SEAL
										Treatment Total			$1,041,739		
ARMFIELD AV	FISHER AV	JEFFERSON BL	ARMFAV	010	700	24	16,800	R	AC	55	40	100	$49,359	17,305	THIN AC OVERLAY(2 INCHES)
KODIAK ISL PL	ALEUTIAN ST	E CDS	KODKPL	010	244	33	11,230	R	AC	56	41	100	$32,995	17,292	THIN AC OVERLAY(2 INCHES)
LAUREL LN	WESTACRE RD	ROCKROSE RD	LAURLN	020	1,050	32	33,600	R	AC	55	40	100	$98,718	17,305	THIN AC OVERLAY(2 INCHES)
SEYMOUR CT	STABLE DR	END N	SEYMRCT	010	195	32	6,240	R	AC	60	46	100	$18,334	16,718	THIN AC OVERLAY(2 INCHES)
										Treatment Total			$199,406		
BRIDLE CT	STABLE DR	END N	BRIDCT	010	219	32	10,271	R	AC	83	76	84	$7,720	21,156	SLURRY SEAL
DOUGLAS ST	CUMMINS WY	LIGHTHOUS DR	DOUGST	030	710	36	25,560	C	AC	79	75	83	$20,958	15,479	SLURRY SEAL
GREGORY AV	1500' N/ BEVAN	JEFFERSON BL	GREGAV	040	1,830	32	58,560	A	AC	62	82	89	$48,015	22,220	SLURRY SEAL
HAITI RD	GUADELUPE ST	DOMINICA ST	HAITIRD	010	890	33	29,370	R	AC	79	83	90	$22,075	23,036	SLURRY SEAL

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2025

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
HORNBY ISL ST	OAKLAND BAY DR	MARSHALL RD	HORBST	010	1,480	35	51,800	R	AC	82	85	92	$38,933	22,712	SLURRY SEAL
JEFFERSON BL	SOUTH RIV RD	GATEWAY DR	JEFRBL	110	1,570	62	106,460	A	AC	73	73	81	$87,289	25,300	SLURRY SEAL
LEMON TREE RD	REDWOOD AV	PEACHTREE ST	LEMORD	030	185	36	6,660	R	AC	80	84	90	$5,006	23,448	SLURRY SEAL
OTIS AV	BRIDGEWY DR	MARSHALL RD	OTISAV	030	1,730	32	55,360	C	AC	73	72	81	$45,391	17,905	SLURRY SEAL
POPPY ST	RIVERMONT ST	LILY PL	POPPST	010	1,255	32	40,160	R	AC	82	85	92	$30,184	22,712	SLURRY SEAL
RICE AV	RICE AV	MAPLE ST	RICEAV	020	984	40	39,360	C	AC	83	79	86	$32,272	16,072	SLURRY SEAL
SACRAMENTO AV	EIGHTH ST	SIXTH ST	SACRAV	080	619	38	23,522	A	AC	73	72	81	$19,287	21,157	SLURRY SEAL
SOUTHPORT PW	SILVERWOO RD	OTIS AV	SOUTPW	130	1,570	36	56,520	A	AC	75	75	83	$46,342	27,034	SLURRY SEAL
TOBAGO ST	TRINIDAD RD	ST CROIX RD	TOBAST	010	665	33	21,945	R	AC	79	83	90	$16,494	23,036	SLURRY SEAL
										Treatment Total			$419,966		
ASH AV	WILLOW AV	PROCTOR AV	ASHAVE	010	280	32	8,960	R	AC	47	87	88	$12	1,120,824	SEAL CRACKS
CRESTED ST	MUSCOVY RD	MALLARD RD	CRESTST	010	757	32	24,224	R	AC	86	88	89	$75	747,719	SEAL CRACKS
DONNER RD	SHASTA WY	END E	DONNRD	010	610	32	19,520	R	AC	86	88	89	$61	747,719	SEAL CRACKS
DEERWOOD CR	DEERWOOD ST	END E	DRWD	010	190	31	8,529	R	AC	49	87	88	$11	1,120,824	SEAL CRACKS
GRAND ST	GATE	FIFTH	GRANST	020	86	38	3,268	R	AC	82	81	82	$18	509,802	SEAL CRACKS
HAVASU ST	SILVERWOO RD	LEWISTON RD	HAVAST	010	270	32	8,640	R	AC	84	86	87	$33	720,681	SEAL CRACKS
LAKE WASHIGNTON BL	STONEGATE DR	STOPLIGHT AT SOUTHPORT TOWN CE	LAKEBL	050	690	73	50,370	A	AC	68	86	87	$86	1,510,954	SEAL CRACKS
LIGHTHOUSE DR	DOUGLAS ST	FOUNTAIN DR	LIHODR	030	1,330	38	56,540	C	AC	78	71	74	$480	375,172	SEAL CRACKS
MADRONE AV	WESTACRE RD	ROCKROSE RD	MADRAV	020	1,030	32	32,960	R	AC	47	87	88	$42	1,120,824	SEAL CRACKS
MARSHALL RD	W END	SOUTHPORT PW	MARSRD	010	1,540	32	49,280	A	AC	67	86	87	$85	1,510,954	SEAL CRACKS
MARSHALL RD	MARTINIQU ST	GUADELUPE ST	MARSRD	030	1,010	32	32,320	A	AC	68	86	87	$56	1,510,954	SEAL CRACKS
MERKLEY AV	EL RANCHO CT	159' N/O EL RANCHO CT	MERKAV	040	159	60	9,540	C	AC	87	81	83	$50	514,864	SEAL CRACKS
MERKLEY AV	327' N/O EL RANCHO CT	WEST CAPI AV	MERKAV	060	253	30	7,590	C	AC	88	82	84	$37	528,347	SEAL CRACKS

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2025

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
OAKMONT WY	W END	BRIARWOOD DR	OAKMWY	010	510	36	18,360	R	AC	47	87	88	$24	1,120,824	SEAL CRACKS
OTIS AV	KAWEAH ST	BRIDGEWAY DR	OTISAV	020	1,520	32	48,620	C	AC	87	81	83	$251	514,827	SEAL CRACKS
ROCK CREEK RD	HUME ST	BRIDEWAY LAKES	ROCKCRD	010	791	32	25,312	R	AC	85	87	88	$85	752,205	SEAL CRACKS
SILVERWOOD RD	SOUTHPORT PW	TAHOW ST	SILWRD	020	1,405	32	44,940	R	AC	85	87	88	$151	752,205	SEAL CRACKS
STONE BL	CAROLINA AV	JEFFERSON BL	STONBL	050	286	40	11,440	C	AC	88	82	84	$56	528,367	SEAL CRACKS
STONEGATE DR	VILLAGE PW	LA JOLLA ST	STONDR	010	725	32	23,200	A	AC	94	84	85	$101	646,079	SEAL CRACKS
TAHOE ST	SQUAW RD	COYOTE RD	TAHOST	020	1,550	32	49,600	R	AC	85	87	88	$167	752,205	SEAL CRACKS
TERMINAL ST	INDUSTRIA BL	BOATMAN AV	TERMST	010	580	61	35,380	C	AC	50	84	86	$107	764,389	SEAL CRACKS
TRADEWINDS CR	END	PORT ST	TRADCR	010	313	41	14,251	R	AC	46	87	88	$18	1,120,824	SEAL CRACKS
VILLAGE PW	LAKE WASHINGTON BLVD	LINDEN RD	VILGPW	040	1,075	32	34,400	A	AC	92	83	84	$158	664,503	SEAL CRACKS
WEST CAPITOL AV	JEFFERSON BL	MERKLEY AV	WECAAV	140	1,320	71	97,320	A	AC	86	80	82	$556	657,169	SEAL CRACKS
WEST CAPITOL AV	MERKLEY AV	GARDENT ST	WECAAV	150	1,006	56	59,936	A	AC	90	82	84	$292	661,627	SEAL CRACKS
WHITNEY PL	LASSEN ST	END W	WHITPL	010	142	32	4,224	R	AC	86	88	89	$14	747,719	SEAL CRACKS
										Treatment Total			$3,026		
				Year 2025 Area Total			2,160,730			Year 2025 Total			$1,995,796		

Year: 2026

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
ARLINGTON RD	CROWN AV	END N	ARLNRD	020	600	18	10,800	R	ST	65	48	100	$15,202	17,145	CAPE SEAL
ASTER AV	MANCHESTE ST	END E	ASTRAV	010	965	32	30,880	R	ST	65	48	100	$43,465	17,145	CAPE SEAL
CROWN AV	ARLINGTON RD	KINSTINGTON ST	CRWNAV	010	850	32	27,200	R	ST	65	48	100	$38,285	17,145	CAPE SEAL
DIABLO ST	KINSINGTO ST	ESSEX AV	DBLOST	010	593	36	21,348	R	ST	64	47	100	$30,048	17,505	CAPE SEAL
ESSEX AV	DIABLO ST	KINSINGTO ST	ESSXAV	010	420	36	15,120	R	ST	64	47	100	$21,282	17,505	CAPE SEAL

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2026

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
HIGGINS RD	211' E/ SUMMERFIELD DR	311' E/ CONSTITUTION	HIGGRD	020	710	24	17,040	C	ST	41	61	100	$26,383	14,235	CAPE SEAL
HIGGINS RD	CONSTITUTION AV	PARADISE WY	HIGGRD	030	728	32	23,296	C	ST	46	61	100	$36,069	14,235	CAPE SEAL
HIGGINS RD	PARADISE WY	JEFFERSON BL	HIGGRD	040	1,030	34	35,020	C	ST	47	61	100	$54,221	14,235	CAPE SEAL
HOBSON AV	TODHUNTER AV	FOURNESS DR	HOBSAV	040	1,075	31	33,325	R	ST	65	48	100	$46,906	17,145	CAPE SEAL
LAKE WASHIGNTON BL	STOPLIGHT AT SOUTHPORT TOWN CE	SOUTHPORT PW	LAKEBL	060	916	110	100,760	A	ST	64	63	100	$156,005	17,769	CAPE SEAL
LINDEN RD	BRENDA WY	HIGGINS RD	LINDRD	N170	1,121	34	43,437	A	ST	66	65	100	$67,253	15,659	CAPE SEAL
LINDEN RD	CONSTITUTION AV	BRIDGE ST	LINDRD	N200	1,864	30	60,122	A	ST	64	63	100	$93,086	17,769	CAPE SEAL
LINDEN RD	JEFFERSON BD	COFFEEBERRY	LINDRD	S080	1,252	32	44,266	A	ST	69	67	100	$68,536	13,443	CAPE SEAL
MANCHESTER ST	DEVON AV	ASTER AV	MANCST	020	788	32	26,004	R	ST	64	47	100	$36,602	17,505	CAPE SEAL
MASSIE DR	END	OATES DR	MASSDR	010	623	53	34,942	R	ST	65	48	100	$49,182	17,145	CAPE SEAL
PARADISE WY	HIGGINS RD	LAGOON LN	PARAWY	010	560	36	20,160	R	ST	65	48	100	$28,376	17,145	CAPE SEAL
REED AV	RIVERSIDE	STILLWATER RD	REEDAV	010	2,000	62	137,044	A	ST	62	63	100	$212,182	17,769	CAPE SEAL
REED AV	HARBOR BL	SUNSET AV	REEDAV	070	950	77	73,150	A	ST	61	63	100	$113,257	17,769	CAPE SEAL
RICE AV	HARBOR BL	RICE AV	RICEAV	010	1,474	26	38,324	C	ST	46	61	100	$59,337	14,235	CAPE SEAL
RIVERBANK RD	TODHUNTER AV	END	RIVERD	040	1,680	36	60,480	C	ST	43	61	100	$93,640	14,235	CAPE SEAL
TABER ST	HOLLAND DR	KEGLE DR	TABEST	010	1,210	36	43,560	R	ST	64	47	100	$61,312	17,504	CAPE SEAL
TODHUNTER AV	MAY ST	RIVERBANK RD	TODHAV	020	1,905	36	68,580	C	ST	0	61	100	$106,181	14,235	CAPE SEAL
										Treatment Total			$1,456,810		
BONITA CT	END	FERNWOOD ST	BONACT	010	141	32	4,512	R	AC	61	45	100	$13,655	16,351	THIN AC OVERLAY(2 INCHES)
FERNWOOD ST	N END	MEADOW RD	FRNWST	005	650	32	20,800	R	AC	57	40	100	$62,945	16,803	THIN AC OVERLAY(2 INCHES)
MONTE VISTA CT	MONTE VISTA	END W	MONCT	010	224	32	10,431	R	AC	59	41	100	$31,567	16,822	THIN AC OVERLAY(2 INCHES)

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

										Treatment Total			$108,167		
BARBERRY PL	W CDS	RIVERMONT ST	BERYPL	010	269	32	11,874	R	AC	81	83	90	$9,193	22,164	SLURRY SEAL
CEDARBROOK RD	NEWBERRY ST	MARLIN ST	CEDERD	020	650	32	20,800	R	AC	81	83	90	$16,103	21,380	SLURRY SEAL
FIFTH ST	PAVEMENT CHANGE	TOWER CT	FIF5ST	210	275	41	11,275	A	AC	71	82	89	$9,522	21,572	SLURRY SEAL
FIFTH ST	TOWER BRIDGE GATEWAY	WEST CAPI AV	FIF5ST	240	74	75	5,550	A	AC	91	81	88	$4,688	18,676	SLURRY SEAL
FIFTEENTH ST	JEFFERSON BL	SOUTH RIV RD	FIFTST	010	895	48	39,380	A	AC	79	74	82	$33,257	20,599	SLURRY SEAL
GATEWAY DR	JEFFERSON BL	N BEACH AV	GATEDR	010	1,190	32	38,080	C	AC	41	81	88	$32,160	16,097	SLURRY SEAL
GOLDEN GATE DR	MARSHALL RD	OAKLAND BAY DR	GOGADR	080	708	33	23,364	C	AC	43	81	88	$19,732	16,097	SLURRY SEAL
GOLD HILL ST	SANDY POI RD	QUAIL RD	GOLDST	010	530	32	16,960	R	AC	83	85	92	$13,130	21,355	SLURRY SEAL
JAMAICA ST	OAKLAND BAY DR	MARSHALL RD	JAMAST	010	640	33	21,120	R	AC	78	81	88	$16,350	22,067	SLURRY SEAL
JEFFERSON BL	LINDEN RD	HIGGINS RD	JEFRBL	140	1,600	62	104,720	A	AC	65	82	89	$88,438	21,572	SLURRY SEAL
LAKE WASHIGNTON BL	SILVERADO ST	STONEGATE DR	LAKEBL	040	610	51	31,110	A	AC	64	82	89	$26,273	21,572	SLURRY SEAL
LILY PL	W CDS	POPPY ST	LILYPL	010	514	32	19,714	R	AC	80	82	89	$15,262	21,676	SLURRY SEAL
MALIBU CT	REDONDO RD	END N	MALICT	010	344	33	14,527	R	AC	79	81	89	$11,246	21,923	SLURRY SEAL
MILLBROOK CT	END S	QUAIL RD	MILLCT	010	60	64	4,287	R	AC	88	76	84	$3,319	15,085	SLURRY SEAL
N BEACH AV	COLUMBUS RD	STONEGATE DR	NBEAAV	010	400	33	13,200	C	AC	84	78	86	$11,148	15,549	SLURRY SEAL
OAKLAND BAY DR	TACOMA NA ST	GOLDEN GA DR	OAKLDR	040	715	33	23,595	C	AC	86	80	87	$19,927	15,709	SLURRY SEAL
SANTA BARBARA ST	CATALINA RD	SAN NICOLAS	SANBST	010	220	33	7,260	R	AC	77	80	87	$5,621	22,166	SLURRY SEAL
THIRD ST	C ST	SECOND ST	THRDST	020	350	46	16,100	C	AC	42	81	88	$13,597	16,097	SLURRY SEAL
THIRD ST	TOWER BRIDGE GATEWAY	BALLPARK DR	THRDST	060	540	46	24,840	C	AC	85	79	87	$20,978	15,644	SLURRY SEAL
TWITCHEL ISL RD	GOLDEN GA DR	END E	TWITRD	010	1,049	33	37,792	R	AC	78	81	88	$29,257	22,062	SLURRY SEAL
WEST CAPITOL AV	GARDENT ST	300' N/O GARDENT ST	WECAAV	160	300	54	16,200	A	AC	80	74	82	$13,682	20,598	SLURRY SEAL
										Treatment Total			$412,883		
ANNA ST	KEGLE DR	HARDY DR	ANNAST	050	1,000	35	35,000	C	AC	56	84	86	$109	742,126	SEAL CRACKS
BARONA ST	CORANADO RD	LA JOLLA ST	BAROST	010	1,440	33	47,520	R	AC	84	85	86	$196	697,687	SEAL CRACKS

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

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Scenario: Current Funding Levels

Year: 2026

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
BLACK BUTTE RD	STONEYFORD RD	HUME ST	BLACRD	010	1,041	32	33,312	R	AC	87	88	89	$104	730,339	SEAL CRACKS
COLLINS ST	TOPAZ RD	N CDS	COLLST	010	1,394	32	47,874	R	AC	84	85	86	$197	697,687	SEAL CRACKS
COLUMBUS RD	N BEACH AV	230' E/ NBEACH	COLURD	010	230	33	7,590	R	AC	83	84	85	$35	661,543	SEAL CRACKS
E ST	SEVENTH ST	WIDTH CHANGE	ESTREE	030	613	30	18,390	R	AC	84	85	86	$76	697,687	SEAL CRACKS
GARNET ST	WEST CAPI AV	DORAN AV	GARNST	010	1,083	32	34,656	R	AC	48	87	88	$45	1,088,179	SEAL CRACKS
GREGORY AV	1300' N/ SO. RIVER	BEVAN RD	GREGAV	020	1,525	34	51,850	A	AC	71	86	87	$92	1,466,946	SEAL CRACKS
HARBOR BL	I80	RIVERBANK RD	HARBBL	020	560	26	14,560	A	AC	73	76	78	$105	608,221	SEAL CRACKS
HAVERHILL ST	GRANDE VI AV	MEADOW RD	HVHR	010	810	34	27,540	R	AC	48	87	88	$36	1,088,179	SEAL CRACKS
LAKE WASHIGNTON BL	REDWOOD AV	HIGHLAND ST	LAKEBL	020	1,424	38	59,533	A	AC	70	86	87	$105	1,466,946	SEAL CRACKS
MAPLE ST	RICE AV	HOLLY ST	MAPLST	010	455	32	14,560	C	AC	95	86	87	$55	541,169	SEAL CRACKS
MARKET ST	RIVERFRONT	GATE	MARKST	010	79	22	1,738	R	AC	88	83	84	$9	431,229	SEAL CRACKS
PRINCETON RD	HIGHLAND DR	460' E/ HIGHLAND	PRINRD	010	460	32	14,720	R	AC	83	84	85	$66	690,277	SEAL CRACKS
REDWOOD CT	BREENBRIER RD	END	REDWCT	010	85	32	2,720	R	AC	86	81	83	$15	435,361	SEAL CRACKS
SEVENTEENTH ST	VERMONT AV	JEFFERSON BL	SEVEST	040	270	23	6,210	R	AC	53	87	88	$9	1,088,179	SEAL CRACKS
SOUTHPORT PW	MARSHALL RD	BRITTON PL	SOUTPW	110	1,855	36	66,780	A	AC	81	88	89	$213	1,075,970	SEAL CRACKS
SOUTHPORT PW	BRITTON PL	SILVERWOOD RD	SOUTPW	120	1,860	46	85,500	A	AC	81	88	89	$272	1,075,970	SEAL CRACKS
STONEGATE DR	LA JOLLA ST	MESA GRAN ST	STONDR	020	850	32	27,200	A	AC	79	86	87	$104	1,015,336	SEAL CRACKS
STONEGATE DR	LAKE WASH BL	REDONDO RD	STONDR	050	365	36	13,140	A	AC	81	81	82	$74	646,061	SEAL CRACKS
STONEGATE DR	REDONDO RD	LINDEN RD	STONDR	060	1,155	32	36,960	A	AC	79	86	87	$135	1,067,530	SEAL CRACKS
SEVENTH ST	E ST	F ST	SVNTST	010	440	35	15,400	R	AC	48	87	88	$20	1,088,179	SEAL CRACKS
										Treatment Total			$2,072		

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year 2026 Area Total 2,165,102 Year 2026 Total $1,979,932

Year: 2027

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
ANNA ST	TODHUNTER AV	HOLLAND DR	ANNAST	030	1,110	35	38,850	C	AC	71	42	100	$177,417	12,860	2" MILL AND FILL
SOUTH RIVER RD	1800 SOUTH RIV RD	15TH ST	SOUTRD	180	2,077	28	58,156	A	AC	74	67	100	$240,288	14,947	2" MILL AND FILL
										Treatment Total			$417,705		
CLARENDON ST	GRANDE VI AV	NORFOLK AV	CLRDST	020	1,300	34	44,200	R	ST	66	47	100	$64,079	16,990	CAPE SEAL
DEVON AV	MANCHESTE ST	KINSINGTO ST	DEVOAV	010	740	32	23,680	R	ST	67	48	100	$34,331	16,647	CAPE SEAL
FILLMORE PL	END	GATEWAY DR	FILLPL	010	364	37	16,291	R	ST	67	48	100	$23,618	16,645	CAPE SEAL
INDEPENDENCE AV	DRIFTWOOD CT	SAFFLOWER PL	INDPAV	020	480	42	20,160	R	ST	66	47	100	$29,227	16,990	CAPE SEAL
KEGLE DR	SACRAMENT AV	FREMONT BL	KEGLRD	010	590	45	26,550	C	ST	31	63	100	$42,340	12,421	CAPE SEAL
LINDEN RD	HIGGINS RD	SUMMERFIEL D DR	LINDRD	N180	1,811	30	63,675	A	ST	54	65	100	$101,544	15,203	CAPE SEAL
LINDEN RD	SUMMERFIELD DR	BETTY WY	LINDRD	S110	1,811	34	63,675	A	ST	55	65	100	$101,544	15,203	CAPE SEAL
LINDEN RD	CANVASBACK WY	JEFFERSON BD	LINDRD	S140	1,207	35	46,447	A	ST	70	65	100	$74,071	15,203	CAPE SEAL
N BEACH AV	STONEGATE DR	GATEWAY DR	NBEAAV	020	735	33	24,255	C	ST	32	63	100	$38,681	12,421	CAPE SEAL
PITZER CR	ALLAN AV	ALLEN AV	PITZCR	010	1,680	36	60,480	R	ST	66	47	100	$87,681	16,990	CAPE SEAL
RAMCO ST	CARLIN DR	S END	RAMCST	020	1,585	52	82,420	C	ST	50	61	100	$131,438	13,820	CAPE SEAL
RENEE CT	END	BETTY WY	RENECT	010	348	33	13,510	R	ST	67	48	100	$19,587	16,645	CAPE SEAL
RUBICON WY	MERCED WY	LINDEN RD	RUBIWY	020	280	36	10,080	R	ST	67	48	100	$14,614	16,647	CAPE SEAL
SOUTHPORT PW	LAKE WASH BL	1700' W/O Lake Wa	SOUTPW	010	1,700	60	102,000	A	ST	27	63	100	$162,662	17,251	CAPE SEAL
STONE CT	END S	STONE BL	STONBL	060	181	32	5,792	C	AC	76	66	76	$9,237	6,873	CAPE SEAL
TRINITY WY	MOJAVE DR	APACHE ST	TRINWY	020	1,132	32	36,224	R	ST	68	46	100	$52,516	17,986	CAPE SEAL
										Treatment Total			$987,170		
CHEROKEE RD	APACHE ST	STONEGATE DR	CHERRD	010	415	32	13,280	R	AC	62	40	100	$41,394	16,378	THIN AC OVERLAY(2 INCHES)
COLUSA RD	STONEGATE DR	END E	COLSARD	020	618	32	19,776	R	AC	62	40	100	$61,641	16,378	THIN AC OVERLAY(2 INCHES)

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2027

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
LOPEZ ISLAND RD	MALCOLM I ST	HORNBY IS ST	LOPZRD	010	485	33	16,005	R	AC	60	40	100	$49,887	16,320	THIN AC OVERLAY(2 INCHES)
										Treatment Total			$152,922		
DAVIS RD	JEFFERSON BL	GREGORY AV	DAVIRD	020	115	25	2,875	A	AC	18	0	100	$32,094	6,701	RECONSTRUCT SURFACE / FDR
										Treatment Total			$32,094		
ARROWHEAD ST	HENSHAW RD	S END	ARROST	010	1,245	32	39,840	R	AC	85	86	92	$31,767	20,301	SLURRY SEAL
CORTINA RD	REDDING RD	BARONA ST	COTRD	010	545	33	17,985	R	AC	85	86	92	$14,341	20,523	SLURRY SEAL
CUMMINS WY	ARTHUR DR	INGLEWOOD DR	CUMMWY	010	1,565	36	56,340	C	AC	46	81	88	$49,008	15,628	SLURRY SEAL
HENSHAW RD	SOUTHPORT PW	EAGLE ST	HENSRD	020	200	32	6,400	R	AC	84	84	91	$5,104	20,369	SLURRY SEAL
JEFFERSON BL	SACRAMENTO AV	TRIANGLE CT	JEFRBL	010	985	81	79,785	A	AC	66	82	89	$69,401	20,944	SLURRY SEAL
MERKLEY AV	EL RANCHO CT	159' N/O EL RANCHO CT	MERKAV	040	159	60	9,540	C	AC	87	79	87	$8,299	15,158	SLURRY SEAL
MERKLEY AV	327' N/O EL RANCHO CT	WEST CAPI AV	MERKAV	060	253	30	7,590	C	AC	88	80	87	$6,603	15,259	SLURRY SEAL
MILLERTON PL	ARROWHEAD ST	END E	MILLPL	010	374	32	11,968	R	AC	85	86	92	$9,543	20,301	SLURRY SEAL
MONTE VISTA ST	CHINOOK RD	MONTE VISTA	MONTST	010	670	32	21,440	R	AC	83	84	91	$17,096	21,252	SLURRY SEAL
REED AV	I80 WB ONRAMP	I80	REEDAV	030	531	78	41,418	A	AC	66	82	89	$36,028	20,944	SLURRY SEAL
SAN CARLOS CT	REDONDO RD	END N	SANCCT	010	344	33	14,527	R	AC	83	84	91	$11,584	21,252	SLURRY SEAL
SPOKANE RD	STONEGATE DR	HIGHLAND DR	SPOKRD	010	1,135	33	37,455	R	AC	83	84	91	$29,866	21,252	SLURRY SEAL
STONE BL	INDUSTRIA BL	CEBRIAN ST	STONBL	010	390	28	10,920	C	AC	47	81	88	$9,499	15,628	SLURRY SEAL
STONE BL	CAROLINA AV	JEFFERSON BL	STONBL	050	286	40	11,440	C	AC	88	80	87	$9,952	15,258	SLURRY SEAL
STONEGATE DR	MESA GRAN ST	N BEACH AV	STONDR	030	655	32	20,960	A	AC	81	73	82	$18,233	19,994	SLURRY SEAL
VILLAGE PW	STURGEON LN	GREGORY AV	VILGPW	080	1,950	34	66,300	A	AC	93	73	82	$57,671	19,998	SLURRY SEAL
										Treatment Total			$383,995		
ANNA ST	HOLLAND DR	KEGLE DR	ANNAST	040	1,065	35	37,275	C	AC	59	84	86	$120	720,510	SEAL CRACKS
BIG BEAR ST	HENSHAW RD	N CDS	BIGBST	010	459	32	17,954	R	AC	83	83	84	$91	636,677	SEAL CRACKS

** - Treatment from Project Selection

Scenarios Criteria:

Scenarios - Sections Selected for Treatment

Interest: 3.00% Inflation: 3.00% Printed: 08/06/2018

Scenario: Current Funding Levels

Year: 2027

Street Name	Begin Location	End Location	Street ID	Section ID	Length	Width	Area	FC	Surf Type	Current PCI	Treatment PCI Before	Treatment PCI After	Cost	Rating	Treatment
BOATMAN AV	INDUSTRIA BL	TERMINAL ST	BTMNAV	010	1,100	40	44,000	R	AC	50	87	88	$59	1,056,485	SEAL CRACKS
CLAUDIA CT	W CDS	BETTY WY	CLUDCT	010	373	33	14,337	R	AC	50	87	88	$20	1,056,485	SEAL CRACKS
ELK VALLEY ST	VILLAGE PW	BERRY CRE RD	ELKVST	010	240	33	7,920	R	AC	83	83	84	$40	636,677	SEAL CRACKS
LAKE WASHIGNTON BL	VILLAGE PW	REDWOOD AV	LAKEBL	010	1,950	30	58,500	A	AC	77	83	84	$285	949,292	SEAL CRACKS
LEWISTON RD	EAGLE ST	TAHOE ST	LEWIRD	010	1,310	32	41,920	R	AC	84	84	85	$190	671,664	SEAL CRACKS
MARSHALL RD	SOUTHPORT PW	MARTINIQU ST	MARSRD	020	1,110	32	35,520	A	AC	73	80	82	$209	947,333	SEAL CRACKS
MERKLEY AV	JEFFERSON BL	EL RANCHO CT	MERKAV	030	980	40	39,200	C	AC	58	84	86	$126	720,510	SEAL CRACKS
PYRAMID PL	END W	COLLINS ST	PYRAPL	010	314	32	13,311	R	AC	84	84	85	$61	671,664	SEAL CRACKS
REDDING RD	LA JOLLA ST	VILLAGE PW	REDDRD	010	1,150	33	37,950	R	AC	84	84	85	$172	671,664	SEAL CRACKS
SILVERWOOD RD	END W	SOUTHPORT PW	SILWRD	010	1,524	32	52,031	R	AC	84	84	85	$236	671,664	SEAL CRACKS
SOUTHPORT PW	OAKLAND BAY DR	ABACO RD	SOUTPW	090	950	55	52,250	A	AC	71	86	87	$95	1,424,220	SEAL CRACKS
SOUTHPORT PW	OTIS AV	JEFFERSON BL	SOUTPW	140	1,435	36	51,660	A	AC	78	84	85	$241	941,613	SEAL CRACKS
VILLAGE PW	REDDING RD	ELK VALLE ST	VILGPW	020	555	69	38,295	A	AC	79	85	86	$166	983,313	SEAL CRACKS
VILLAGE PW	ELK VALLE ST	LAKE WASHINGTON BLVD	VILGPW	030	2,546	28	71,288	A	AC	74	80	81	$438	821,428	SEAL CRACKS
WATERFRONT PL	THIRD ST	WATERFRONT PL	WAFRPL	010	395	42	16,590	C	AC	60	84	86	$54	720,510	SEAL CRACKS

		Treatment Total	$2,603
Year 2027 Area Total	1,872,290	Year 2027 Total	$1,976,489
Total Section Area:	18,942,087	Grand Total	$19,667,532

APPENDIX E

PCI Maps

PCI Map

Current Pavement Conditions

(2018)

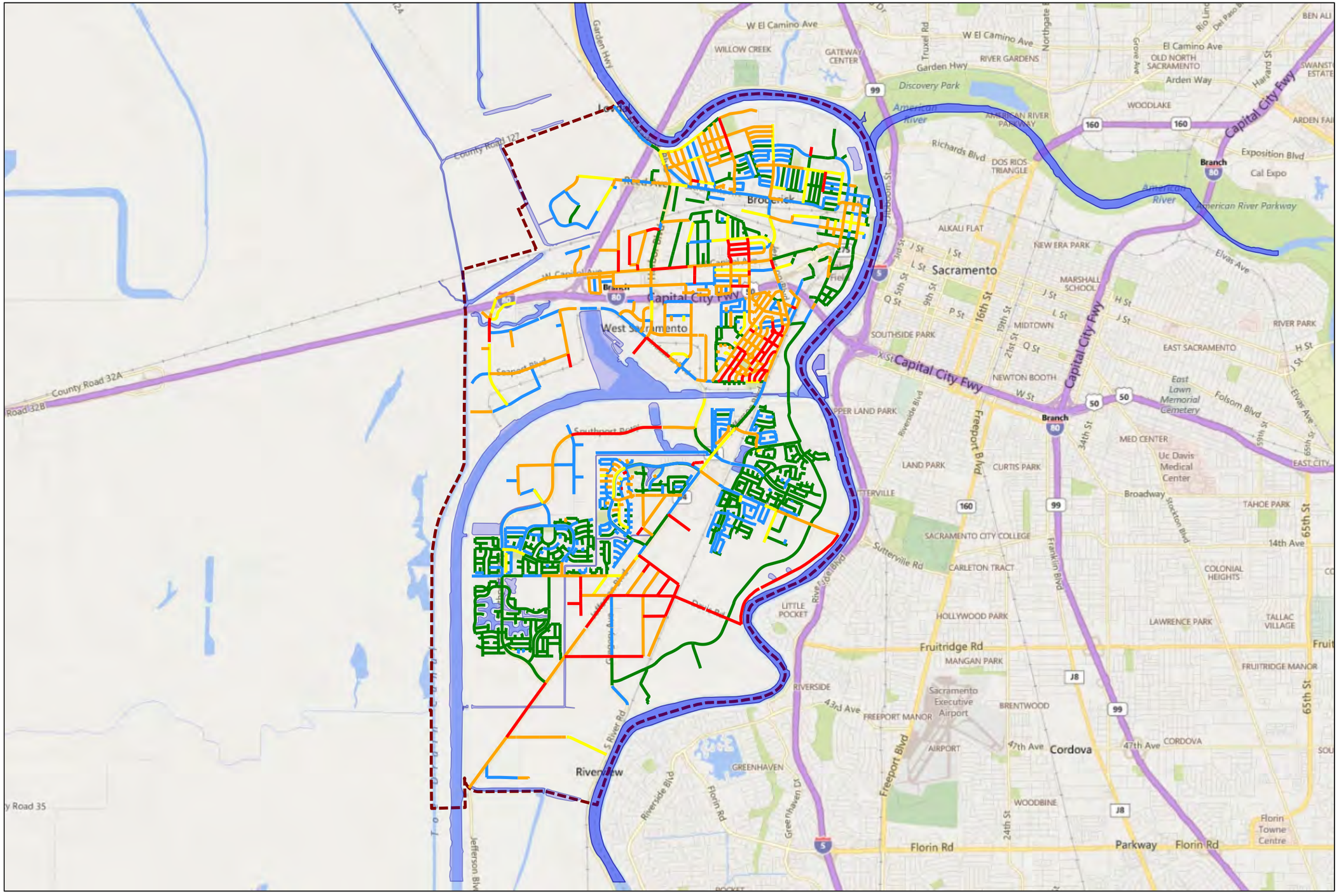
City of West Sacramento

Current PCI Condition

Printed: 8/6/2018

Feature Legend

- Category I - Very Good
- Category II - Good (Non-Load)
- Category III - Good (Load)
- Category IV - Poor
- Category V - Very Poor



0 0.5 1 2 Miles

PCI Map

Scenario 1: Current Funding Levels

(2027)

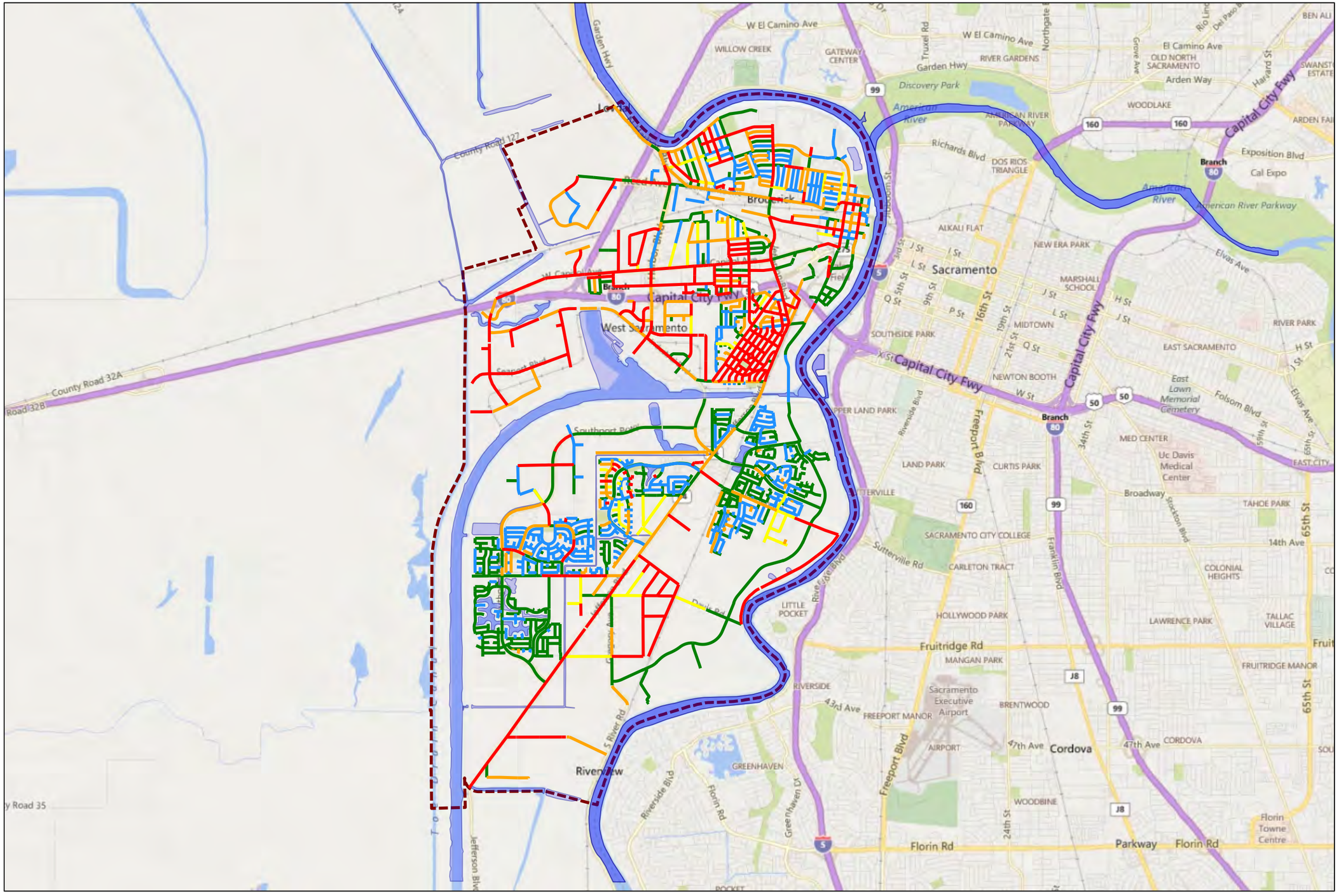
City of West Sacramento

Scenario PCI Condition

Current Funding Levels - 2027 Project Period - Total Rehab: $1,589,891 - Printed: 8/6/2018

Feature Legend

- Category I - Very Good
- Category II - Good (Non-Load)
- Category III - Good (Load)
- Category IV - Poor
- Category V - Very Poor



0 0.5 1 2 Miles

PCI Map

Scenario 2: Maintain Current PCI

(2027)

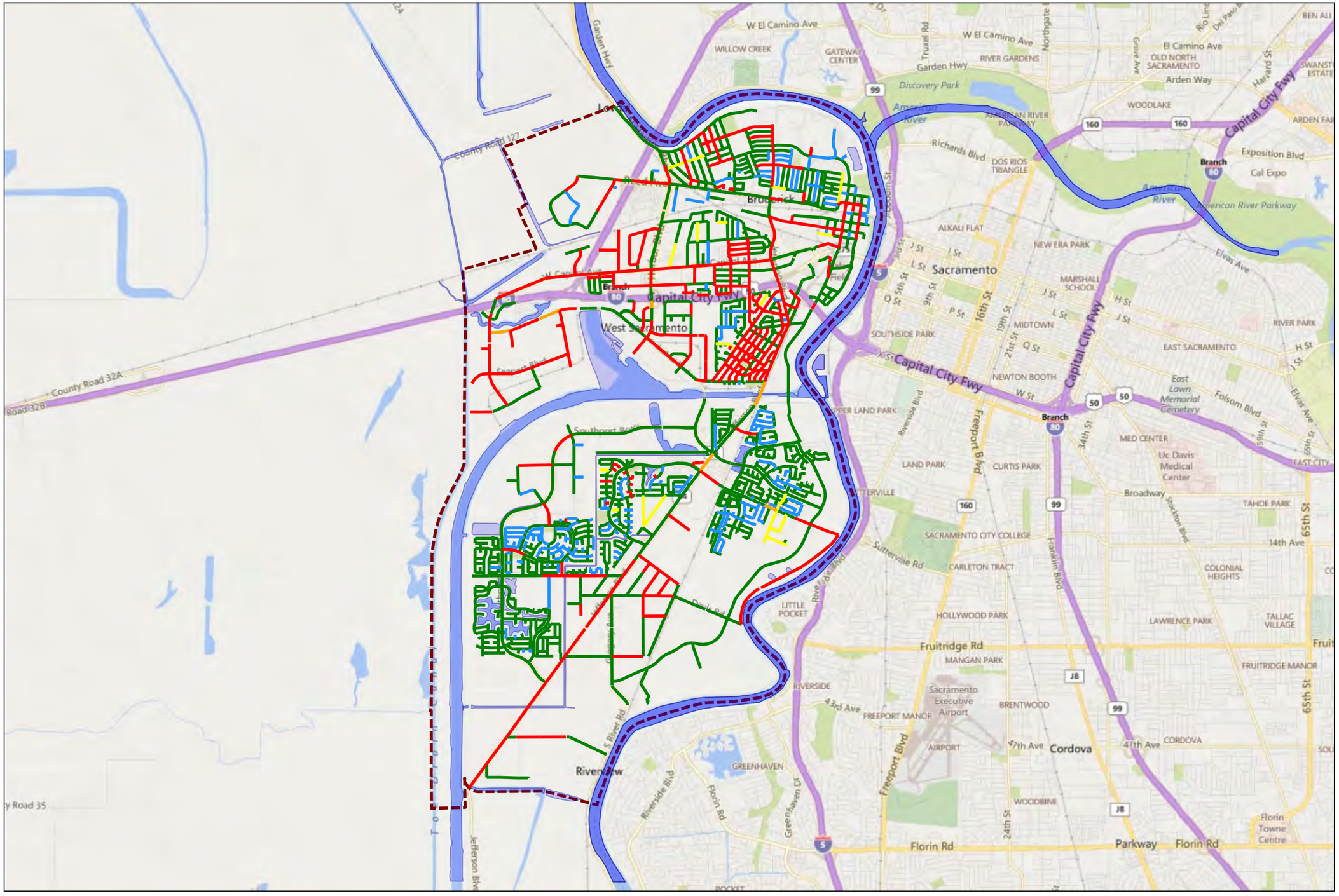
City of West Sacramento

Scenario PCI Condition

Maintain Current PCI - 2027 Project Period - Total Rehab: $5,589,550 - Printed: 8/6/2018

Feature Legend

- Category I - Very Good
- Category II - Good (Non-Load)
- Category III - Good (Load)
- Category IV - Poor
- Category V - Very Poor



0 0.5 1 2 Miles

PCI Map

Scenario 3: Increase PCI to 70

(Arterial/Collector Priority)

(2027)

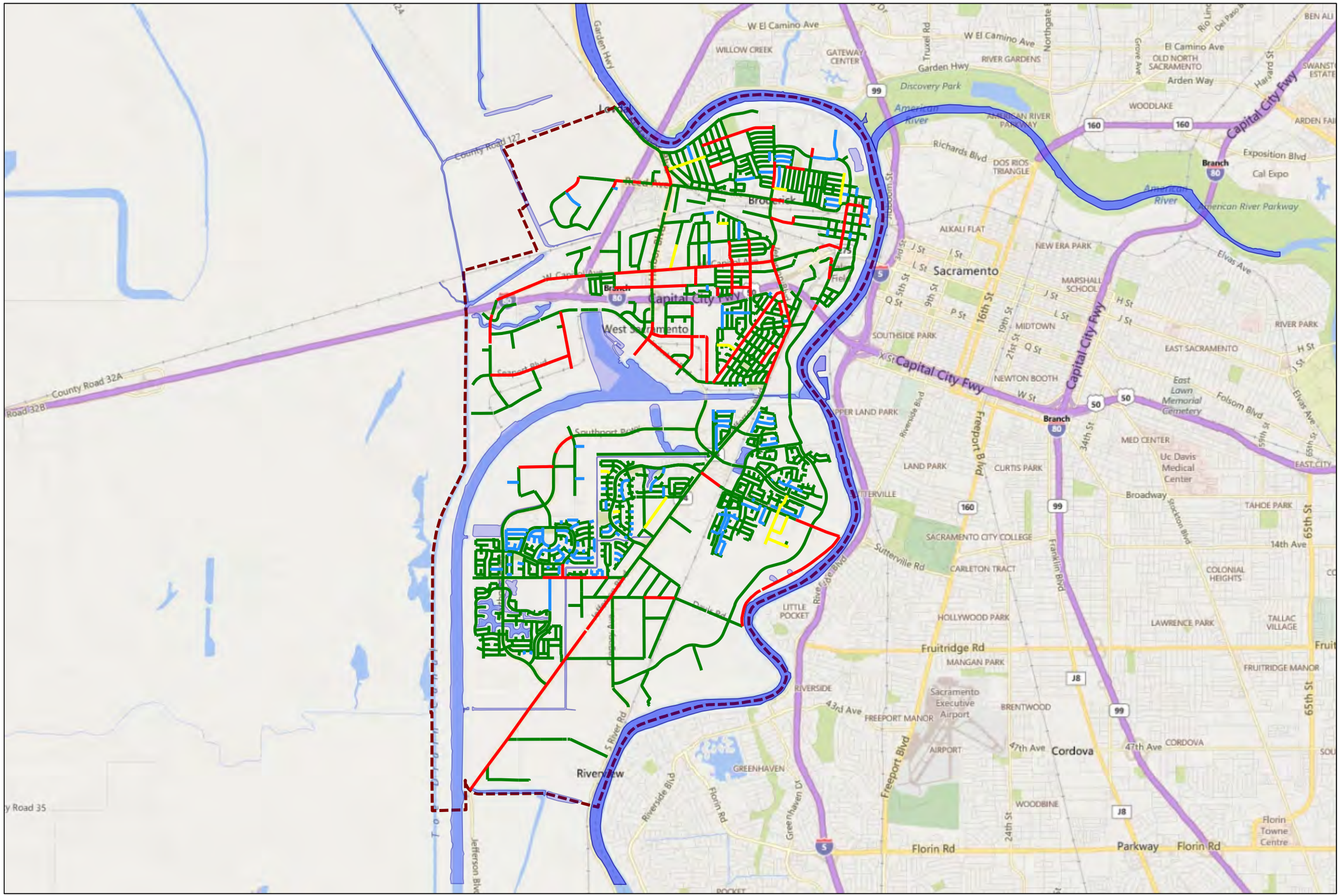
City of West Sacramento

Scenario PCI Condition

Increase PCI to 70 (Arterial/Collector Priority) - 2027 Project Period - Total Rehab: $6,719,592 - Printed: 8/6/2018

Feature Legend

- Category I - Very Good
- Category II - Good (Non-Load)
- Category III - Good (Load)
- Category V - Very Poor



0 0.5 1 2 Miles

PCI Map

Scenario 4: Increase PCI to 70

(Residential Priority)

(2027)

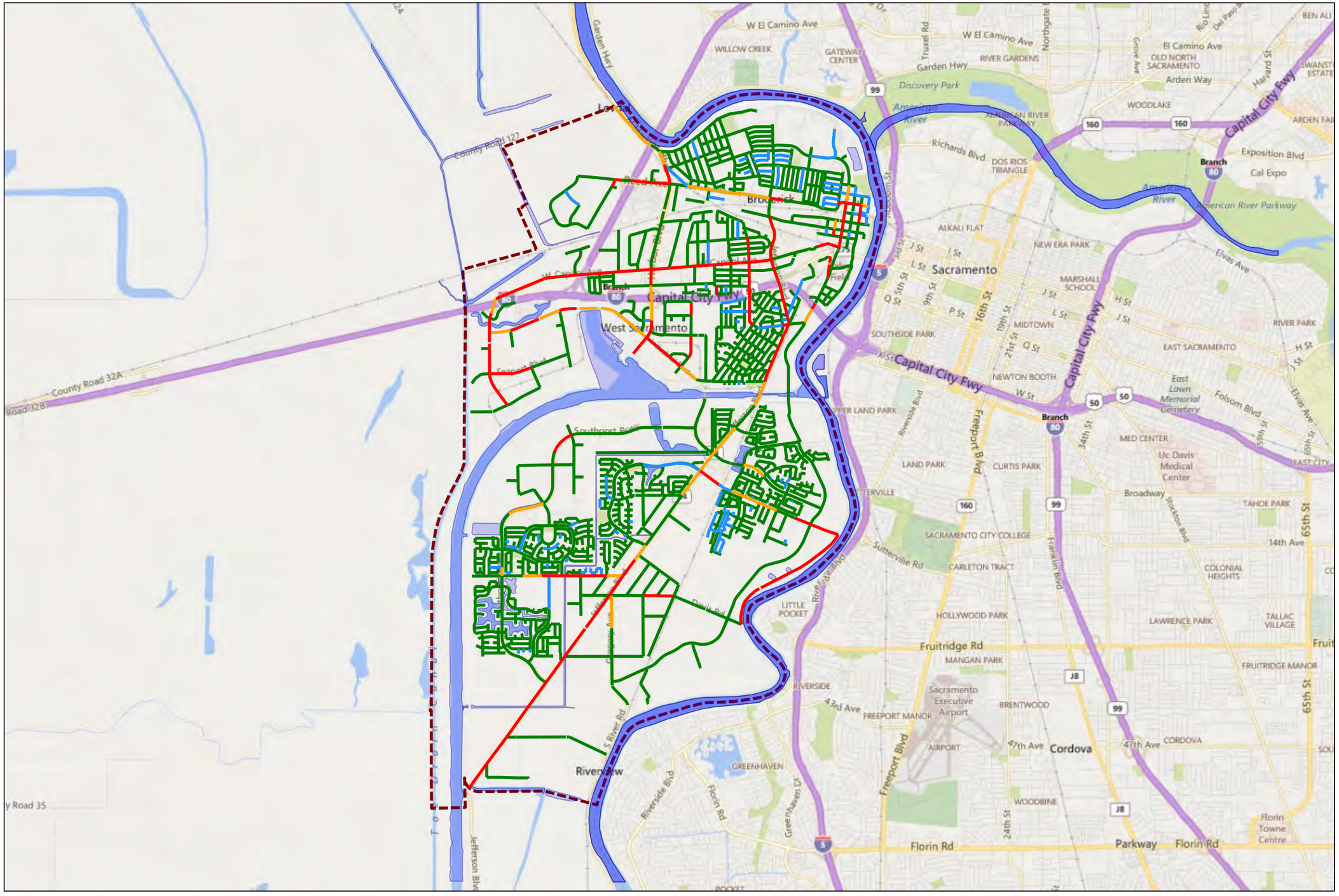
City of West Sacramento

Scenario PCI Condition

Increase PCI to 70 (Residential Priority) - 2027 Project Period - Total Rehab: $6,710,880 - Printed: 8/6/2018

Feature Legend

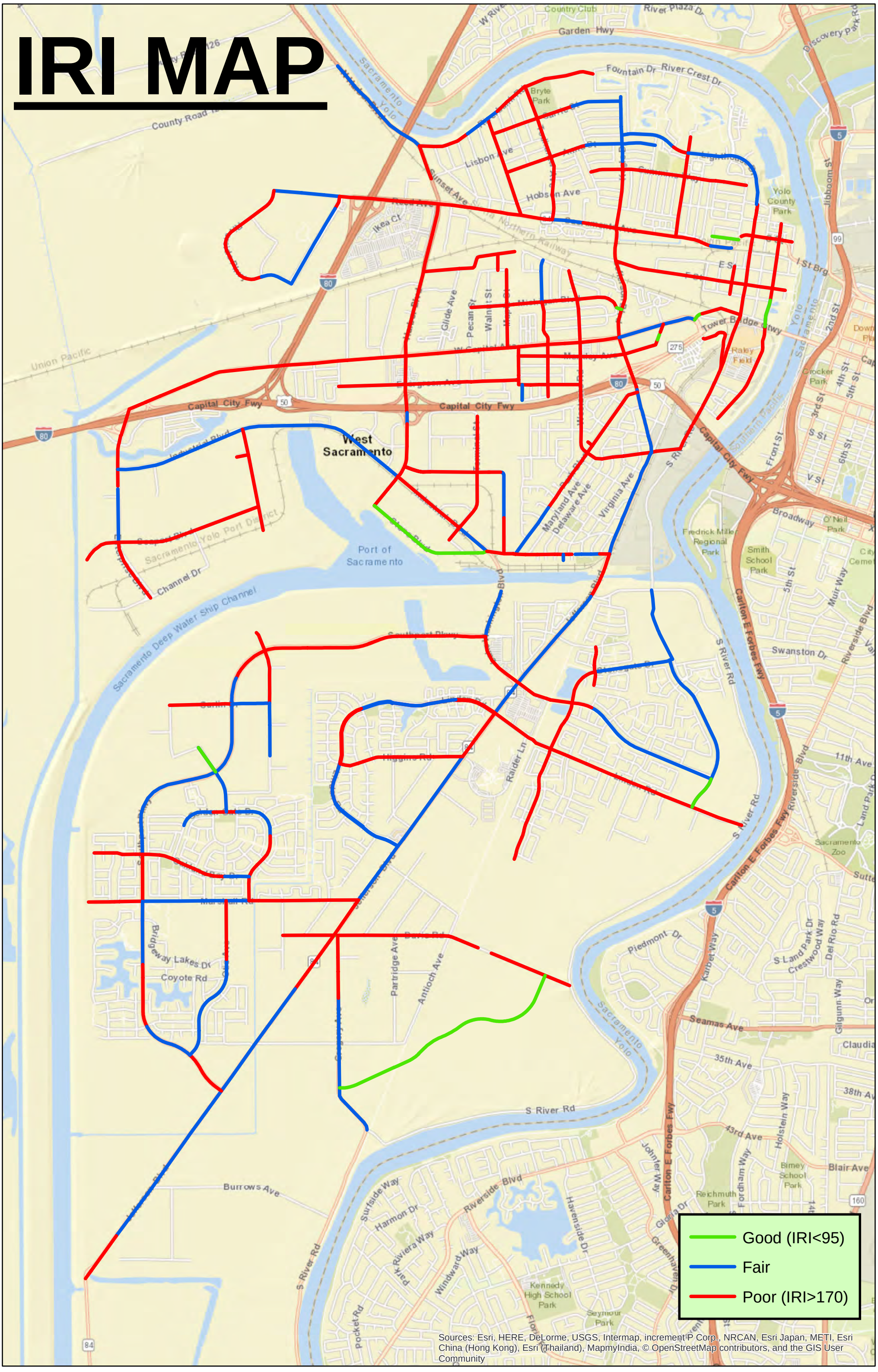
- Category I - Very Good
- Category II - Good (Non-Load)
- Category IV - Poor
- Category V - Very Poor



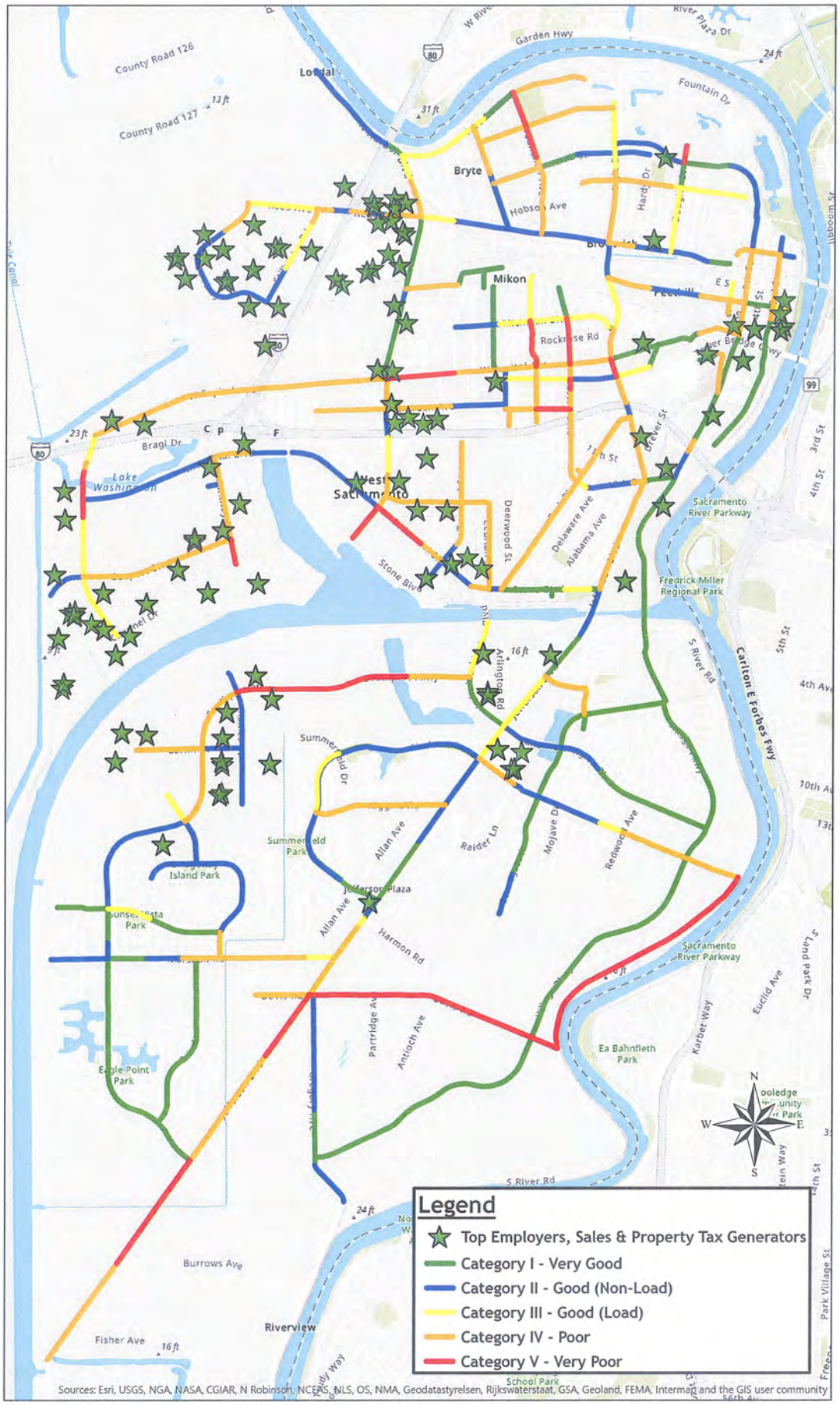
0 0.5 1 2 Miles

IRI Map

IRI MAP


Good (IRI<95)
Fair
Poor (IRI>170)
West Sacramento
Port of Sacramento
Sacramento Deep Water Ship Channel
Capital City Fwy
Sources: Esri, HERE, DeLorme, USGS, Intermap, increment P Corp., NRCAN, Esri Japan, METI, Esri China (Hong Kong), Esri (Thailand), MapmyIndia, © OpenStreetMap contributors, and the GIS User Community

ATTACHMENT 2



CITY OF WEST SACRAMENTO **AGENDA REPORT**

MEETING DATE: August 22, 2018 **ITEM #** 21

SUBJECT:

CONSIDERATION OF RESOLUTION 18-52 DELEGATING AUTHORITY TO THE CITY MANAGER TO APPROVE DEVELOPMENT IMPACT FEE DEFERRAL AGREEMENTS AND REVIEW OF ADMINISTRATIVE POLICY SPECIFYING CERTAIN AUTHORITIES DELEGATED TO THE CITY MANAGER

INITIATED OR REQUESTED BY:

[ ] **Council** [X] **Staff**

[ ] **Other**

REPORT COORDINATED OR PREPARED BY:

Charline Hamilton, Community Development Director
Roberta Raper, Administrative Services Director

Amanda Berlin, Assistant City Manager

ATTACHMENT [X] **Yes** [ ] **No** [ ] **Information** [ ] **Direction** [X] **Action**

OBJECTIVE

The purpose of this report is to seek Council review and guidance on the delegated authorities that have been granted to the City Manager and consideration of adoption of Resoultion 18-52 which would authorize delegation of approval of development impact fee deferral agreements to the City Manager.

RECOMMENDED ACTION

Staff respectfully recommends that the City Council:

1. Consider and adopt Resolution 18-52 delegating authority to the City Manager to approve development impact fee deferral agreements;
2. Review, approve or modify the attached Administrative Policy providing more specificity to and consolidating certain authorities that have been granted to the City Manager within the context of those outlined in Section 2.08 of the Municipal Code; and
3. Authorize the City Clerk to amend this policy as additional authorities are granted or existing ones are modified by the City Council.

BACKGROUND

The general powers and duties of the City Manager are outlined in Section 2.08 in the Municipal Code. The City Council has periodically added more specificity to the broad powers and duties or delegated other administrative authority. This has taken place in the form of minute orders, resolutions and/or ordinances.

One area in which the Council has previously discussed deferral of authority to the City Manager was with development fee deferral agreements. In 2008, the Building Industry Association supported enabling legislation for local fee deferrals on residential development known as Assembly Bill (AB) 2604. AB 2604 was signed by the Governor in August 2008 and went into effect on January 1, 2009. During that time, Finance and Public Works/Community Development Departments conducted a City Council workshop on potential development impact fee deferral programs. The programs in the region at the time ranged from simple to complex. The simplest deferral programs were tied to final inspection or temporary certificate of occupancy with minimal administrative work for implementation and tracking. The next level tied deferrals to final certificate of occupancy, requiring additional administrative processing. The most complex deferrals were tied to close of escrow, requiring submittal of a title report, recording of first deed of trust on the property to insure payment, and preparation of reconveyance and demand statements. After hearing the presentation and taking public comment, the City Council deliberated and provided feedback to the City Manager to allow such deferrals on a case-by-case basis, but not beyond final inspection, and at the City Manager's discretion for projects that were in economic or housing targeted areas. The City also joined the Statewide Community Infrastructure Program (SCIP) to provide an additional development fee financing mechanism which could extend beyond final inspection.

Over the last ten years, City staff from Finance and Community Development have been jointly working on development impact fee deferral requests for both residential and commercial projects, staying within the direction provided by the City Council at the workshop in 2008. The development impact fee deferral agreement has transformed over the years, updating after each audit and City Attorney review (See Attachment 3). To date, approximately a dozen development impact fee deferral agreements have been executed. Currently, there are no active agreements. Consideration of development impact fee deferral agreements were placed on hold by the Administrative Services Department and City Manager's office until such time as City Council reconsideration could be sought.

In 2012, in order to have one centralized listing of the delegated authorities that can be amended as needed, the Council adopted Administrative Policy 1-A-15 . If Council approves Resolution 18-52, delegating authority to the City Manager to approve development impact fee deferral agreements, such authority would be added to Administrative Policy 1-A-15. Given that City has a new City Manager, this is also an opportunity for the City Council to review the delegated authorities outlined in the policy and make any changes.

ANALYSIS

Staff is requesting Council consider and approve a delegation of authority to the City Manager for approval of development impact fee deferral agreements. Much of the analysis conducted in 2008 is still valid today. Cities in the region still have a variety of programs much as they did back in 2008. The following is the analysis of the three typical levels of deferral programs, including fiscal and administrative impacts, potential risks, and benefits:

	Timing of Fee Payment		
	Final Inspection	Certificate of Occupancy	Close of Escrow or Beyond
Administrative Impacts	Minimal; easily implemented in permit system	Moderate; additional programming required for permit system	Significant; additional programming required for permit system; new tracking system
Fiscal Impacts	No additional processing costs; delayed funding to impact fee funds (estimated 6-24 months)	May incur additional processing costs to track occupancies for residential since certificates are not issued in many cases; delayed funding to impact fee funds (unknown length of time if building remains vacant)	Additional processing costs; deed of trust recording; potential lien filing or collection costs; further delay in funding to impact fee funds (unknown length of time if building remains vacant or does not sell)
Financing Consideration	Allows financing of select Developer Impact Fees through Statewide Communities Infrastructure Program (SCIP)	SCIP issues bonds twice a year, and deferral allows developer time to apply for funding	SCIP cannot be used for public safety impact fees
Potential Risks	Minimal if fee payment also tied to temporary certificate of occupancy	Potential collection problems; who pays- owner, builder, occupant?	Potential collection problems; what if no sale or escrow?
Benefits	Reduces developer upfront financial burden; may provide for incentive for development in slow economy; provides economic competitiveness	Extends time for reduction of developer upfront financial burden; may provide incentive for development in slow economy; provides economic competitiveness	Further extends time for reduction of developer upfront financial burden; may provide incentive for development in slow economy; provides economic competitiveness

City staff feel that the selective program implemented in 2008 served its purpose well, with little risk and administrative work. The agreement has been successfully used for the Washington Firehouse renovation, expansion of Tony's Fine Foods, B Street West Project, and TOMRA, to name a few examples. Based on the latest feedback from auditors, the City Council needs to formally delegate the authority to the City Manager to continue its current process, or develop and adopt a formal development impact fee deferral program.

In addition, the attached policy outlines the ongoing authority that has been delegated to the City Manager. Not included in this policy are case-specific delegations to the City Manager that end when the project or program ends. As noted on Attachment 2, staff is proposing a limited number of non-substantive edits to the policy. In particular, under Human Resources, the authority granted for the approval of severance packages incorrectly

stated the packages could only be up to 60 days of salary and benefits, when in fact the authority delegated was for up to six months. Under Finance, the Date Granted section for approval of professional services and equipment purchases was amended to include the Municipal Code section authorizing this authority. Under Public Works, the authority delegated was amended to state that it applies only to acceptance and filing of notices of completion for CIP public improvements. In accordance with Section 16.44.120 of the Muncipal Code, improvements completed by subdividers have continued to be presented to the City Council for acceptance. The resolution number in the Date Granted section for delegation of authority for Caltrans agreements was also corrected.

Upon review of the existing authorities, Council may also request any other changes deemed necessary or appropriate. If Council approves Resolution 18-52, such authority would also be added to the list attached to Administrative Policy I-A-15.

Environmental Considerations
N/A

Commission Recommendation
N/A

Strategic Plan Integration
N/A

Alternatives
As an alternative to the recommended action, Council may decline to approve the requested delegation of authority regarding development impact fee deferral agreements and/or request staff return at a subsequent meeting with additional information regarding specific authorities prior to Council approving the administrative policy.

Coordination and Review
This report was coordinated with the Administrative Services Department, Community Development Department and City Manager's Office.

Budget/Cost Impact
The City Finance Division will review deferral requests with respect to each impact fee category to be included in the deferral agreement. The Administrative Services Director may recommend that certain fees are not deferred if such a deferral would have a significant financial impact on the respective fund.

ATTACHMENTS

1. Resolution 18-52
2. Administrative Policy: City Manager Authorities
3. Current Template for Residential Agreement for Payment of City Impact Fees

RESOLUTION 18-52

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF WEST SACRAMENTO DELEGATING AUTHORITY TO THE CITY MANAGER TO APPROVE DEVELOPMENT IMPACT FEE DEFERRAL AGREEMENTS

WHEREAS, Assembly Bill (AB) 2604 was signed by the Governor in August 2008 with an effective date of January 1, 2009 and enabled local jurisdictions to approve local fee deferrals on residential development; and

WHEREAS, the City Council held a workshop in August 2008 to discuss potential development impact fee deferral programs; and

WHEREAS, after deliberation at the 2008 workshop, the City Council provided feedback to the City Manager to allow development impact fee deferrals on a case by case basis, not beyond final inspection, and at the City Manager's discretion for projects that were in economic or housing targeted areas; and

WHEREAS, since that direction, the City Manager or designee has approved several development impact fee deferral agreements for both residential and commercial projects, staying within the direction provided by City Council at the 2008 workshop; and

WHEREAS, it is necessary to have documented and official approval of delegation of authority to the City Manager to approve development impact fee deferral agreements for audit purposes.

NOW, THEREFORE, BE IT RESOLVED by the City Council as follows:

1. The City Council hereby delegates authority to the City Manager to approve development impact fee deferral agreements for projects in economic or target housing areas on a case by case basis, not beyond final inspection.

PASSED AND ADOPTED this 22nd day of August 2018, by the following vote:

AYES:
NOES:
ABSENT:

_____
Christopher L. Cabaldon, Mayor

ATTEST:

_____
Kryss Rankin, City Clerk

CITY OF WEST SACRAMENTO

ADMINISTRATIVE POLICY

NUMBER: I-A-15

DATE: June 20, 2012
Revised: April 24, 2015
August 22, 2018

SUBJECT: CITY MANAGER AUTHORITIES

AUTHORITY: City Council

PURPOSE

The purpose of this policy to have a centralized listing of specific delegations of authority granted to the City Manager by the City Council. This policy is to be amended by the City Clerk when and if the City Council grants additional authority or modifies existing authority.

POLICY

Section 2.08 of the Municipal Code outlines the broad powers and duties of the City Manager. The City Council may from time to time provide more specificity to an authorized power or duty or it may delegate certain other administrative authority.

PROCEDURE

The attached table summarizes the more specific powers and duties that have been delegated by the City Council. In the event that the City Council adds to these authorities or makes any modifications the City Clerk is to amend this policy accordingly.

ATTACHMENT

Table of City Manager Delegations of Authority

CITY MANAGER DELEGATIONS OF AUTHORITY

AUTHORITY	DESCRIPTION	DATE GRANTED
Human Resources		
Approval of industrial disability retirements for public safety employees	The City Manager has been delegated this first line of approval for industrial disability retirements for safety employees. The ultimate decision is made by CalPERS but is not done until the employer first acts.	Resolution 88-25
Place management employees at a higher vacation accrual schedule when hired	New employees generally begin earning vacation at the lowest tier. The City Manager is authorized to place new management employees at the 2nd level accrual tier.	12-08/1999 Minute Order 99-184
Fill vacant unrepresented positions at a lower classification level	Positions are authorized by the City Council per the Authorized Position List. This authority essentially allows the City Manager to assess the need to replace a vacant unrepresented position at the same level or fill it a lower level.	02/12/2003 Minute Order 03-23
Approve the transfer of positions from one cost center to another within the same fund	The Authorized Position List adopted by the City Council authorizes positions by number, title and cost center. This authority allows the City Manager to move a position from one cost center to another within the same fund.	02/12/2003 Minute Order 03-23
Approve changes to unrepresented classification descriptions	The Council approves all classification descriptions and any substantive changes to them. This authority authorizes the City Manager to make substantive changes to unrepresented classification descriptions.	1/21/2004 Minute Order 04-08
Approve Severance Packages up to ~~60 days~~Six (6) months of salary and benefits	Section 2.08 gives the City Manager general authority to approve severance packages in an amount approved by the City Council. The Council has authorized up to 6 months of salary and benefits.	July 2008 Admin Policy IV-A-2
Make changes to Authorized Position List based on retirements and other personnel actions	The Council approves the Authorized Position List. Retirements and negotiated separations necessitate changes to the Authorized Position List. The City Manager is allowed to make these changes in these circumstances.	06/16/2010 Minute Order 10-51
Administration		
Accept interest in or easements to real property	Accepting interest in or easements in real property is generally part of a larger development or real estate transaction approved by the City Council. This delegation of authority is one of efficiency.	03/05/2003 Resolution 03-19
Amend Records Management Retention Schedule	The City has a Records Management Retention Schedule for all City records. Many of the retention times are set by statute. This authority allows the City Manager to amend the retention times for records that do not have statutorily set retention times. The City Clerk will report to ~~Council every~~Council every two years on any changes.	11/06/2005 Resolution 05-112
Finance		

Authorize replacement of vehicles that were damaged beyond reasonable repair	The City Council authorizes the purchase and replacement of vehicles. The City Manager is authorized to replace a vehicle that is damage due to an accident and is beyond reasonable repair. Insurance is to be pursued when appropriate.	01/11/2006 Minute Order 06-01
Approve Professional Services and Equipment purchases up to $50,000	The City Manager is authorized to approve contracts for professional services and equipment up to $50,000. Solicitation guidelines are to be followed. This does not include authority for public works contracts as provided for in the Public Contracts Code. The City Manager is to inform the Council twice a year of all contracts approved.	08/01/2007 ~~Ordinance 07~~Ordinance 07-18; Municipal Code §2.08.050 (enabling authority to set amounts via Admin. Policy~~.~~ ~~(Admin. Policy~~ 1V-81)
Alternatives to payment of Childcare Impact Fee	The City Manager may approve alternatives to payment of the fee. The Book of Fees will define the parameters.	Municipal Code §17.47
Appropriation authority for the Child Care Impact Fee fund	The City Manager may approve transfers from the Child Care Impact Fee Fund up to $25,000 per fiscal year, should funding be deemed necessary for the function and improvement of child care and preschool facilities within the City of West Sacramento.	12/17/2014 Minute Order 14-103
Public Works		
Acceptance of CIP public improvements and file notice of completion	These public improvements are typically~~!~~ part of a development project. This is a ministerial action that has been delegated to the City Manager.	02/08/2006 Minute Order 06-10
Execute all master agreements program supplemental agreements, fund exchange agreements, fund transfer agreements and any other agreements with Calt~~-T~~rans	These agreements would generally follow approval by the City Council of a larger project involving Caltrans. This delegates the signing authority of the City Manager.	01/10/2007 Resolution 0~~1~~7-04

Last Updated: 4/27/15

AGREEMENT FOR PAYMENT OF CITY IMPACT FEES

This Agreement ("the Agreement") is made and entered into this _____ day of _____ _____, 2018, between the City of West Sacramento, a municipal corporation, ("City") and _____ ("Developer").

RECITALS

A. The Developer intends to submit applications for building permits to the City for construction of various residential units within the _____ residential subdivision ("Project").

B. As a condition of approval of said building permits, Developer is required to pay City Impact Fees.

C. The Developer has requested that payment of certain Impact Fees, including the Traffic Impact Fee, Raley's Landing Traffic Impact Fee, Police Impact Fee, Corporation Yard Impact Fee, City Hall Impact Fee, and Childcare Impact Fee be deferred until just prior to the time of final inspection for each individual residential unit within the Project.

D. The Developer is proposing ___ *[provide project description and any phasing detail]* _____.

NOW, THEREFORE, the parties agree as follows:

1. Fee.

1.1 Calculation and Payment of Impact Fee

1.1.1 Amount of Fee. The Parties agree that the current total value of the City Impact Fees to be deferred shall be _____ dollars and _____ cents ($__,___.__) per unit, as summarized in Attachment "A". These amounts reflect the current value for those impact fees listed in Attachment "A" for the unit sizes noted above. Should any of the parameters which affect the fees be different when the City receives such building permit applications for those units, including but not limited to the sizes of the units, the date on which the City receives the permit application, and/or the application of fee credits; the value of the impact fees to be deferred will be increased or decreased to reflect the current impact fees for those units. In all cases, the value of the impact fees to be deferred will exactly match the value of the impact fees for the particular residential unit. The total number of units to which this deferral will apply shall not exceed _____ units on _____ lots.

1.1.2 Payment of Fee. The Impact Fees to be deferred for each individual unit shall be due and payable prior to final inspection of each residential unit permitted.

1.1.3 Rights and Obligations Personal to Developer. The right or obligation under this Section to receive a payment from the City or to make a payment to the City shall be personal to the Developer, regardless of whether or not the Developer is the owner of all or any portion of the Project at the time payment is due.

1.2 Penalty for Non-Payment. The City reserves the right to deny final inspection of any permitted building pending full payment of the deferred impact fees for that particular unit.

2. Due Diligence. City and Developer agree to proceed in good faith and in a reasonable and diligent manner to implement the provisions this Agreement.

3. Amendments. Except as otherwise provided herein, this Agreement may be amended only by mutual written consent of the Parties.

4. Force Majure. Performance by either party hereunder shall not be deemed to be in default where delays or defaults are due to war, insurrection, strikes, walkouts, riots, floods, earthquakes, fires, casualties, acts of God, governmental entities, or enactment of conflicting state or federal laws or regulations.

5. Notices. All notices required by this Agreement, or State law, shall be in writing and delivered in person or sent by U.S. mail, first class postage prepaid, to the following addresses:

City	Developer
Community Development Director	_____
City of West Sacramento	Attn: _____
1110 West Capitol Ave.	_____
West Sacramento, CA 95691	_____

Either party may change its address by giving notice in writing to the other party.

6. Indemnification. Developer shall defend, indemnify and hold harmless City, its officers, agents, and employees, from liability or claims for damage of any nature, including but not limited to personal injury to any person, bodily injury to any person, including death, or property damage, which may arise from or be connected with the direct or indirect operations, inaction, or actions of Developer or its contractors, subcontractors, agents or employees arising out of or connected with this Agreement, except those matters arising from the negligence of City. This provision is intended to be broadly construed and extends to, among other things, any challenge to the validity of this Agreement.

7. Attorney Fees. If either party brings legal action against the other for breach of this Agreement or to compel performance of this Agreement, the prevailing party shall be entitled to reasonable attorneys' fees and costs. Any action arising out of this Agreement shall be brought in Yolo County, California, regardless of where else venue may lie.

8. Integration. This is an integrated Agreement, and contains all of the terms, considerations, understanding and promises of the parties. It shall be read as a whole.

9. Severability. The parties agree that the provisions of this Agreement are severable. If a court of competent jurisdiction deems any provision invalid, the balance of this Agreement shall be considered to remain intact and be enforceable on its terms consistent with the findings and purposes set forth herein.

CITY OF WEST SACRAMENTO,
a municipal corporation

BY: _____
Charline Hamilton
Director of Community Development

BY: _____
Roberta Raper
Director of Administrative Services

ATTEST: _____
Kryss Rankin
City Clerk

APPROVED AS TO FORM:

BY: _____
Jeff Mitchell
City Attorney

[Project Applicant/Developer]

BY: _____
Name: _____
Title: _____